

**Meeting of the London Legacy Development Corporation Planning Decisions Committee****Meeting Date:** Tuesday 25 September 2018**Time:** 6.00 pm**Venue:** Rooms 1, 2 & 3, LLDC, Level 10, 1 Stratford Place, Montfichet Road, London E20 1EJ

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Members of the London Legacy Development Corporation Planning Decisions Committee are hereby notified and requested to attend the meeting of the Planning Decisions Committee of the Corporation at 6.00 pm on Tuesday 25 September 2018 to transact the business set out below.

This meeting will be open to the public, except for where exempt information is being discussed as noted on the agenda. A guide for the press and public on attending and reporting meetings of local government bodies, including the use of film, photography, social media and other means is available at

<https://www.gov.uk/government/publications/open-and-accountable-local-government-plain-english-guide>

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**Committee Members:**

Philip Lewis (Chair)  
Pam Alexander  
Sukhvinder Kaur-Stubbs  
Piers Gough CBE RA  
James Fennell MRTPI MRICS  
Emma Davies MRICS  
Louise Wyman MRICS MLA  
Councillor Rachel Tripp  
Councillor James Beckles  
Councillor Dan Tomlinson  
Councillor Nick Sharman

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|----------|---|---------------|
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- 4 Minutes of the Previous Meeting held on 24 July 2018** (Pages 7 - 18)
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- 6 Land adjacent to Eastcross Bridge, Queen Elizabeth Olympic Park - 18/00340/FUL** (Pages 245 - 298)
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- 10 The International Quarter London, Building S9 - 18/00255/REM** (Pages 501 - 548)
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- 13 Any Urgent Business**



## **London Legacy Development Corporation**

### **Updates, Order of Business and Requests to Speak at 64<sup>th</sup> meeting of the Planning Decisions Committee**

**Date:** Tuesday 25 September 2018

**Time:** Commencing at 06:00 pm

**Venue:** Rooms 1, 2, 3 & Marketing Suite, LLDC, Level 10, 1 Stratford Place, Montfichet Road E20 1EJ

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**This report will be considered in public**

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**Subject:** Declarations of Interests Received for the 64<sup>th</sup> meeting of the Planning Decisions Committee

**Date:** 25 September 2018

**Venue:** Rooms 1, 2, 3 & Marketing Suite, LLDC, Level 10, 1 Stratford Place, Montfichet Road, London E20 1EJ

FOR NOTING

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**This report will be considered in public**

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**1. DECLARATION OF INTERESTS**

1.1 For the purposes of transparency, where a Member of the Committee is an elected Member of a Host Borough to which a planning application and/or other matter to be dealt with at this meeting relates, that fact will be set out in this report, noting that being an elected Member of a Host Borough in itself does not constitute a disclosable pecuniary interest. However, if the Member concerned does have a disclosable pecuniary interest for the reasons set out in section 3 of this report he/she will need to declare it prior to and/or at the meeting and take the necessary consequential actions. Any Member in attendance as a substitute will similarly need to declare any interests in the business on the agenda, including disclosable pecuniary interests, at the meeting.

**2. RECOMMENDATIONS**

2.1 In light of the items of business listed on the agenda for this meeting of the Committee, the relevant Members are asked to declare any disclosable interests and state whether or not any of the interests declared are or could:

2.1.1 fall within the definition of pecuniary interests as set out in The Relevant Authorities (Disclosable Pecuniary Interests) Regulations 2012 (Disclosable Pecuniary Interests Regulations) (as the same may be amended); or

2.1.2 If they are not disclosable pecuniary interests are classed as interests which are material and which conflict or may conflict with the interests of the Corporation.

2.2 That the interests set out below be noted.

### **3. EXPLANATION OF INTERESTS**

- 3.1 A Member of this Planning Decisions Committee who is present at a meeting of this Committee and who has and/or may reasonably be considered to have a pecuniary interest in any item of business before the meeting, shall at the meeting and as soon as practicable after its commencement disclose the nature and extent of his or her interest. Provided that, with regard to any disclosable pecuniary interest that falls within the definition of a sensitive interest (as set out in regulation 32 of the Disclosable Pecuniary Interests Regulations), the Member shall be permitted to disclose not the interest but the fact that he or she has a disclosable pecuniary interest in the matter concerned.
- 3.2 A Member who has and/or may reasonably be considered to have a disclosable pecuniary interest shall not unless he or she is granted a dispensation pursuant to regulation 33 of the Disclosable Pecuniary Interests Regulations:
- (a) participate, or participate further, in the consideration or discussion of the matter and shall leave the meeting during its consideration; and
  - (b) vote, or further vote, on any question or matter with respect to it.
- 3.3 A Member of this Planning Decisions Committee who is present at a meeting of this Committee and who has an interest that is not a disclosable pecuniary interest but is an interest (whether held directly or indirectly) which is material and which conflicts or may conflict with the interests of the Corporation shall at the meeting and as soon as practicable after its commencement disclose the nature and extent of that interest.
- 3.4 A Member who has disclosed an interest that is not a disclosable pecuniary interest but which is material and which conflicts or may conflict with the interests of the Corporation may, notwithstanding his or her interest, participate in the consideration or discussion and vote on the matter and be included for the purposes of a quorum at any meeting at which the matter is considered provided that:
- a) the Member or a Connected Person does not have a Registrable Interest in the matter, other than, in accordance with SO 6.4(a), where the Registrable Interest constitutes the holding of office as an elected member of one of the Growth Boroughs who is appointed to sit on the Committee, in which case the holding of that office of itself shall not constitute a matter which is material and which is considered to conflict with the interests of the Corporation; and
  - b) his/her interest does not give rise to a real danger of bias or is one which a member of the public aware of all the facts will regard as so significant that it is likely to prejudice the Member's judgement of how to act in the public interest.
- 3.5 For the purposes of determining whether or not a Member has an interest which is material and which conflicts or may conflict with the interests of the Corporation the meanings given to the terms "Registrable Interest" and "Connected Person" are set out in the Corporation's Standing Orders (approved September 2012, updated March 2017).

3.6 A Member shall not be counted in the quorum present at a meeting in relation to a resolution upon which s/he is not entitled to vote (SO6.2).

#### **4 FOR INFORMATION**

4.1 Elected Members of Host Boroughs to which planning applications relate (where applicable):

- Land adjacent to Eastcross Bridge, QEOP – High Ropes - 18/00340/FUL  
Councillor Rachel Tripp, London Borough of Newham  
Councillor James Beckles, London Borough of Newham  
Councillor Nick Sharman, London Borough of Hackney
  
- Marshgate Lane – 17/00669/VAR  
Councillor Rachel Tripp, London Borough of Newham  
Councillor James Beckles, London Borough of Newham
  
- 1-2 Hepscott Road – 15/00446/FUL – and 33-35 Monier Road 15/00212/FUL (Aitch Group)  
Councillor Dan Tomlinson, London Borough of Tower Hamlets
  
- Sugar House Island (formerly known as Strand East) Plots R7 and R8 – 17/00468/NMA and 18/00366/NMA and 17/00369/REM and 15/00384/REM  
Councillor Rachel Tripp, London Borough of Newham  
Councillor James Beckles, London Borough of Newham
  
- The International Quarter London, Building S9 – 18/00255/REM  
Councillor Rachel Tripp, London Borough of Newham  
Councillor James Beckles, London Borough of Newham
  
- The International Quarter London, Building S4 (Substructure) – 18-00355-REM and (Superstructure) – 18-00354-REM  
Councillor Rachel Tripp, London Borough of Newham  
Councillor James Beckles, London Borough of Newham

4.2 Independent Members

- James Fennel is Chief Executive Officer of Nathaniel Lichfield & Partners and Emma Davies is Managing Director of CBRE Planning UK.
- Neither member has been involved in decision making on any of the matters listed in the Decisions Made under Delegated Authority item on this agenda.

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**Minutes of the Meeting of the London Legacy Development Corporation Planning Decisions Committee**

**Date:** Tuesday 24 July 2018  
**Time:** 6.00 pm  
**Venue:** Rooms 1, 2 & 3, LLDC, Level 10, 1 Stratford Place, Montfichet Road, London E20 1EJ

**PRESENT:**

Philip Lewis (Chair)  
Pam Alexander OBE  
Sukhvinder Kaur-Stubbs  
Councillor Terry Wheeler  
Councillor James Beckles  
Emma Davies MRICS  
James Fennell MRTPI MRICS  
Piers Gough CBE RA  
Councillor Nick Sharman  
Councillor Rachel Tripp

**IN ATTENDANCE:**

Anthony Hollingsworth, Director of Planning Policy and Decisions, LLDC  
Catherine Smyth, Head of Development Management, LLDC  
Zena Hassan, Assistant Committee Secretary, LLDC  
Anne Ogundiya, Senior Planning Decisions Manager, LLDC  
Josh Hackner, Planning Development Manager  
Russell Butchers, Senior Planning Development Manager  
Susanne Andreasen, Legal Advisor, Pinsent Masons  
Jamie Mordue, Committee Secretary, GLA

## **1 Updates, Order of Business and Requests to Speak (Item 1)**

1.1 The Chair stated that there was an update report for Item 8 – IQL Pavilion 18/00252/REM.

1.2 The Chair stated that Item 6 – Marshgate 17/00669/VAR had been withdrawn from the agenda. The item would be heard at a future Committee meeting.

1.3 The Chair stated that the following requests to speak had been received:

- Item 9 – 11 Burford Road 18/00293/REM

In support of Officers' recommendation:  
Oliver Coleman (Rolf Judd Architects)

Objecting to Officers' recommendation:  
Janet Sullivan (local resident)

## **2 Apologies for Absence (Item 2)**

2.2 Apologies for absence were received from Councillor Rachel Blake, Councillor Jenny Gray, Councillor Dan Tomlinson and Louise Wyman MRICS MLA.

## **3 Declarations of Interest (Item 3)**

3.1 The Committee received the report of the Director of Planning Policy and Decisions which set out, for the purposes of transparency, where a Member of the Committee was an elected Member of a Host Borough to which a planning application and/or other related matters were to be dealt with at the meeting.

**3.2 Resolved (unanimously):**

**3.2.1 That the following declarations of interest be noted:**

**Councillor James Beckles, London Borough of Newham, Elected Member (item 7 – Meridian Steps advert – 18/00225/ADV, item 8 – IQL Pavilion – 18/00252/REM and item 9 – 11 Burford Road – 18/00293/FUL)**

**Councillor Rachel Tripp, London Borough of Newham, Elected Member (item 7 – Meridian Steps advert – 18/00225/ADV, item 8 – IQL Pavilion – 18/00252/REM and item 9 – 11 Burford Road – 18/00293/FUL)**

## **4 Minutes of the Previous Meeting held on 22 May 2018 (Item 4)**

4.1 The Committee received the minutes of the Planning Decisions Committee meeting held on 22 May 2018.

**4.2 Resolved:**

**4.2.1 That the minutes of the Committee meeting held on 22 May 2018 be signed as a correct record.**

## **5 Clarnico Quay 18/00171/FUL (Item 5)**

5.1 The Committee received the report of the Principal Planning Development Manager. A presentation was also provided.

5.2 The Committee was told that the applicant sought planning permission for the construction of five buildings between two and three storeys. It was envisaged that the buildings would be in place for seven years. The plans would provide: 776sqm of class B1 floor space; 131sqm of class B1/A1 floorspace; 417sqm of class A1/A3/A4 floor space; 37sqm of class D1 floorspace. An area for pop-up shops, food hall and market stalls would also provide 490sqm of class A1/A3/A4 floorspace. The proposals also included a mobile garden and associated landscape, cycle parking, five accessible parking bays and parking. The permission would cease on 25 September 2025 and the site would be reinstated within three months.

5.3 The Committee noted that the application was submitted by Make Shift East Wick & Sweetwater Projects and LLDC. The proposals did not adversely affect delivery of the Legacy Communities Scheme (LCS) development at the site.

5.4 The buildings on site would contain:

- a) A bar, community hall and restaurant;
- b) A pop up food-hall;
- c) Flexible event space and a bar;
- d) Workshops and a yard for spill out maker spaces; and
- e) Further maker/retail space, a café and offices.

The workshop would be finished in charred timber board. All other buildings would be finished in corrugated metal.

- 5.5 Officers told the Committee that a Thames Water service road had meant there was a limited build area.
- 5.6 The Committee heard that 337 neighbour notification letters were sent to surrounding residents, businesses and local interest groups and the application was advertised in the Hackney Today newspaper in April 2018. Four site notices were placed around the site on 13 April. No objections had been received and the scheme had received support from a local business and a local resident.
- 5.7 Members stated that the public consultation in the local community was exemplary and asked how the applicant intended to ensure that tenants give back to the community. The applicants told the Committee that they typically had a social contract with tenants that required tenants to give one hour per week to a local initiative and gave an example of teaching school children how to cook pizza. LLDC would include within the condition relating to the requirement for the submission of a commercial unit and affordable rent strategy a requirement to set out the allocation of hours for community benefits. Officers explained that the reason this was recommended to be via condition and not to have a s.106 legal agreement, was that the permission would be temporary, and LLDC as joint applicant and landowner would have, via the lease, a management agreement in place between the applicant (Make Shift) and LLDC, which also includes the requirement to provide community benefits.
- 5.8 The Committee was told that the mobile garden would be moved from its current site at Chobham and it would be managed as it has been previously. The remainder of the site would be used to provide staircasing for local businesses.
- 5.9 To ensure local resident, schools and businesses were not adversely affected, dust created in the construction of the site would be managed through the application of the LCS code of conduct, which contained dust reduction measures. This would be secured in condition four.
- 5.10 The Committee noted that the proposal included platform lifts, which would ensure that the site was fully accessible.
- 5.11 Officers told the Committee that Makeshift was a hands-on operator and a site management plan, including stewardship of the site, was to be drawn up. The application had the added benefit of LLDC being a joint applicant and this would ensure strong site management.

5.12 One member commented that with the publication of a new National Planning Policy Framework earlier that day, that for each application, if agreed as per the recommendation, that powers should be delegated to the Director of PPDT to consider any implications from this new policy framework for each proposal prior to the issue of decision notices. The Director of PPDT acknowledged that it was unlikely that there would be any material change in policy which would affect the recommendations, but agreed that this should be undertaken for each application on the agenda.

5.13 **Resolved (unanimously):**

**5.12.1 To APPROVE the application 18/00171/FUL for the reasons given in the report and grant full planning permission subject to the conditions set out in this report.**

**5.12.2 To DELEGATE AUTHORITY to the Director of Planning Policy and Decisions to approve the application subject to the conditions and informatives set out in section 13 of the report including refinements, amendments, additions and/or deletions as the Director of Planning Policy and Decisions considers reasonably necessary.**

## **6 Land at Marshgate Lane 17/00669/VAR (Item 6)**

6.1 The Committee noted that the item was withdrawn from this agenda and would come before the Committee on a future date.

## **7 Meridian Square 18/000225/ADV (Item 7)**

7.1 The Committee received the report of the Principal Planning Development Manager. A presentation was also provided.

7.2 The Committee was told that the applicant sought planning permission for an internally illuminated digital media screen, which would be affixed to the south-western elevation of the proposed 18 storey Westfield office block. The media screen would replace the existing, standalone media screen and would be of largely the same dimensions. The existing screen would remain in place until the new office development is completed.

7.3 Members were shown the site location and CGI drawings of what the media screen would look like, which contained comparisons to the existing media screen. The screen would emit light at 300cd/sqm luminance levels and would include moving images. The media screen would be used for general commercial advertising and it would emit no sound.

- 7.4 The Committee noted that four site notices were placed in various locations within the vicinity of the site in May 2018. No responses had been received.
- 7.5 Members were told that the content of the advertisements could not be controlled by LLDC as it was not a planning matter, but that relevant legislation would apply to control what could be advertised.
- 7.6 **Resolved (unanimously)**
- 7.6.1 **To GRANT advertisement consent for the application ref 18/00225/ADV for the reasons given in the report subject to the conditions set out at section 13 of this report.**
- 7.6.2 **To DELEGATE AUTHORITY to the Director of Planning Policy and Decisions to finalise the recommended conditions as set out in this report including such refinements, amendments, additions and/or deletions as the Director of Planning Policy and Decisions considers reasonably necessary.**

## **8 The Pavilion, Endeavour Square 18/00252/REM (Item 8)**

- 8.1 The Committee received the updated report of the Planning Development Executive. A presentation was also provided. The Committee noted the updates to conditions 1,3,4,5 and 6, and to the recommendation.
- 8.2 The Committee was told that the applicant had submitted a reserved matters application, which sought approval for a 1,343sqm, part two, part three storey building for complementary retail with basement plant room and accessible roof. The ground floor would also incorporate a visitor centre. The proposals were in conformity with the parameters described in the Outline Consent, s.106 agreement and Zonal Masterplan for Zone 2.
- 8.3 Members noted that four site notices were placed around the application site and that a public notice was placed in the Newham Recorder in May 2018. No objections had been received. The scheme was presented to the Quality Review Panel in July 2017 and February 2018.
- 8.4 Members welcomed that the building would encourage entry and the use of the rooftop but stated that it was essential that the finish was of a high quality, particularly where glass panels met wood. The wood would be specially treated so that it had durability but the applicants acknowledged that the building would naturally wear over time and that life

cycle works would be necessary. It was agreed with members that the details submitted pursuant to condition 4 (Detailed Drawings) would be checked either by the LLDC design team or by the Quality Review Panel as appropriate.

8.5 The Committee asked how the applicant would ensure that the Pavilion was continually in use. Members were told that the building was central to place creation in the International Quarter (IQL) and being in use throughout the day was essential. It was envisaged that there would be spill out furniture and the roof top facilities could change throughout the year. Partnerships with organisations on the East Bank were also being explored.

8.6 The visitor centre would take on the function of the existing welcome centre and operate during the same hours. The restaurants would likely open between 8am and midnight, and the shops would likely operate within usual retail hours. Public toilets would be located on the roof top, which was accessed by stairs or by a lift, and the ground and first floor would have toilets within the outlets. An accessible toilet was also on the ground floor.

8.7 Members stated that it was essential to work with partners and Park management to effectively manage the building on football game days.

**8.8 Resolved (unanimously):**

**8.8.1 To APPROVE the application for the approval of reserved matters pursuant to conditions B1, B8 and B9 attached to the Outline Consent comprising details of layout, scale, appearance, access and landscaping of the IQL Pavilion, comprising a part two, part three storey building for complementary retail (Use Classes A1-A5) with associated works.**

**8.8.2 To DELEGATE AUTHORITY to the Director of Planning Policy and Decisions to finalise the recommended conditions as set out in the committee and update report including such refinements, amendments, additions and/or deletions as the Director of Planning Policy and Decisions considers reasonably necessary.**

## **9 11 Burford Road 18/00293/FUL (Item 9)**

9.1 The Committee received the report of the Senior Planning Development Executive. A presentation was also provided.

9.2 The Committee noted that the application sought permission for alterations and extensions to an existing commercial building, including the demolition of the existing

fourth floor, the erection of a two-storey extension, and the installation of plant equipment and an acoustic screen. Officers told the Committee that an application for the erection of a three-storey extension at roof level was refused by Committee in March 2018.

- 9.3 On the ground floor it was proposed that 12 car parking spaces be removed and a cycle parking area would be incorporated. The floorspace would change from class A1 and A2 to B1. The alterations on the ground floor had previously been approved under planning application 17/00368/FUL.
- 9.4 Members were told that consultation letters were sent to statutory and non-statutory consultees, and that 196 neighbouring properties in the immediate vicinity were notified of the application by letter. In addition, a site notice was placed at the site in June 2018, to further advertise receipt of the planning application to the local community. A notice was also placed in the Newham Recorder in June 2018. 13 letters of objection were received from neighbouring properties within the residential development at 10 Burford Road, on the grounds of:
- a) Loss of daylight and sunlight;
  - b) Loss of privacy;
  - c) Loss of outlook and overbearing impact;
  - d) Concerns over security;
  - e) Fumes and odour from proposed ventilation ducts;
  - f) Noise nuisance from proposed plant equipment;
  - g) Nuisance during construction phase;
  - h) The development would harm the appearance of the Stratford Workshops;
  - i) Concerns over design quality and that the development would have a detrimental impact upon the streetscene and character of the neighbourhood;
  - j) Nuisance from proposed café, including litter, noise and odour; and
  - k) Increased traffic and car parking stress.
- 9.5 Officers detailed the reasons why the previous application had been refused and gave an update on the issues. The negative impact identified previously upon the setting and significance of the Stratford Workshops was considered to have been addressed in officers' view with a smaller proposed bulk and height such that, on balance, the public benefit of the scheme in providing additional employment floorspace would now outweigh any less than substantial harm in this matter. A revised design had addressed the previously excessive bulk, scale, massing, unacceptable appearance and impact upon the host building and streetscene. The revised scheme was now a similar height to 10 Burford Road and there would be no unacceptable impact on amenity and outlook to neighbouring residential occupiers; and the revised scheme would not result in an unacceptable reduction of daylight and sunlight to Stratford Workshops.



- 9.6 The proposed scheme would be subject to a £4,953 carbon off-set contribution; inclusion of blue badge parking space provision; provision of eight short-stay cycle parking spaces within the public realm; and a highways agreement to reinstate the footway and remove the crossover and dropped kerb. This would be secured by s.106 legal agreement.
- 9.7 The Committee heard from the applicant, Mr Oliver Coleman. Mr Coleman told the Committee that the business model for the proposal would be to offer small spaces to start-up companies and individuals, who would rent as much space as they needed. The applicant had had success with the model in the City of London. The proposal would increase the quality, flexibility and sustainability of office floorspace in the area. The plant equipment would generate noise at 32db at one metre away, and the Newham Council limit was 35db. No sunlight issues had been raised by studies.
- 9.8 The Committee heard from Janet Sullivan, who objected to the application. Mrs Sullivan provided a document for Members to refer to. Mrs Sullivan told the Committee that she spoke on behalf of residents at 10 Burford Road (where she lives), and it was felt that the reasons for refusal of the previous planning application still applied to the revised proposal. The proposed fifth floor would result in a loss of outlook and would only result in 22 people being employed. The proposals showed that 23 plant equipment units would be installed, compared to the current 10. The noise from these units would cease at night but would continue into the evening. Mrs Sullivan stated that the height of the proposal would impact negatively upon the amount of sunlight and daylight her home received, and that she had bought it on assurances that nothing could be built in front of her terrace. A proposed door on the fifth floor would be directly opposite Mrs Sullivan's terrace, and it was felt this could create security issues. Mrs Sullivan told the Committee that she felt the previous reasons for refusal, relating to heritage and design had not been addressed.
- 9.9 Officers told the Committee that a condition required that the maintenance door would only be accessible by building management and maintenance staff. The acoustic reports had identified no issues with the plant equipment. The units were proposed to be 40-50m away from the objector's property and may be visible but would not block out the view above and around them. Members queried what the impact on daylight and sunlight on the residential properties would be. The impact was found to be negligible. Similarly, there would be no adverse impact on the levels of sunlight and daylight at Stratford Workshops on the opposite side of Burford Road.
- 9.10 Officers told the Committee that the demand for short term rental space was not a planning matter but that there was a lot of large scale office space coming forward in Stratford, but not many options for short term, more flexible space. The Committee noted that the internal remodelling had already been given consent. Members heard that the proposal would provide public benefits in terms of increased office floorspace, which was suited to small and medium enterprises, and could generate the equivalent of 90 full time jobs.

9.11 Members discussed the height of the proposed building and its relationship with the residential properties at 10 Burford Road and in particular Mrs Sullivan's dwelling. Members sought clarification from officers on the distance to the proposed new plant and equipment and on the proximity of the proposed development and its likely impact upon the amenity, including outlook, of Mrs Sullivan's property.

9.12 Following careful consideration of the details of the proposal, its likely impact and the planning merits of the development, Members **Resolved (nine votes in favour; one abstention):**

9.12.1 To **APPROVE** the application for the reasons given in the report and grant planning permission subject to:

- a) The conditions set out in the report; and
- b) The satisfactory completion of a legal agreement under section 106 of the Town and Country Planning Act 1990 and other enabling powers to secure the planning obligations set out in the recommended heads of terms which are set out in this report.

9.12.2 To **DELEGATE AUTHORITY** to the Director of Planning Policy and Decisions to:

- a) Finalise the recommended conditions as set out in the report, including such refinements, amendments, additions and/or deletions as the Director of Planning Policy and Decisions considers necessary;
- b) Finalise the recommended legal agreement under section 106 of the Town and Country Planning Act 1990 and other enabling powers as set out in this report, including refining, adding to, amending and/or deleting the obligations detailed in the heads of terms set out in this report as the Director of Planning Policy and Decisions considers reasonably necessary; and
- c) Complete the section 106 agreement and issue the decision notice.

## 10 Annual review of Planning Decisions Committee's Terms of Reference (Item 10)

10.1 The Committee received the report of the Director of Planning Policy & Decisions. The Committee noted that the only amendment to the Committee's Terms of Reference was the inclusion of the authority to oversee the Corporation's work on Diversity and Inclusion, within the remit of the Committee's activities. Officers would continue to consider these matters in their planning application reports.

10.2 Members requested information on how the Corporation intended the oversight of Diversity and Inclusion to come into effect and how it would be measured. Officers confirmed that they would bring a further report to the Committee which would set this out in detail.

**10.4 Resolved:**

**10.4.1 To note the proposed changes to the terms of reference which will be submitted to the Board for approval.**

## **11 Decisions Made Under Delegated Authority (Item 11)**

11.1 The Committee received the report of the Director of Planning Policy and Decisions and its appendices, which listed all the decisions taken by officers between 1 and 31 May 2018 and 1-30 June 2018.

11.2 Officers highlighted that, in May 2018, 50 decisions had been taken, 88 per cent of applications were dealt with within target time and only one refusal had been issued. In June 2018, 48 decisions had been taken, 96 per cent of applications were dealt with within target time and only one refusal had been issued.

**11.3 Resolved:**

**11.3.1 That the report and attached appendix be noted.**

## **12 Any Urgent Business (Item 12)**

12.1 The Chair thanked Councillor Terry Wheeler on behalf of the Committee for this work on the Committee and wished him well for the future.

## **13 Close of Meeting (Item 13)**

13.1 The meeting ended at 8.20pm

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Chair

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Date

**Contact Officer:** Zena Hassan, Assistant Committee Secretary; Tel: 020 3288 8834; Email: [planningcommittee@londonlegacy.co.uk](mailto:planningcommittee@londonlegacy.co.uk)





**Subject:** Review of the Local Plan – Publication stage draft Local Plan  
**Meeting date:** 25 September 2018  
**Report to:** Planning Decisions Committee  
**Report of:** Anthony Hollingsworth, Director of Planning Policy and Decisions

FOR COMMENTING

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**This report will be considered in public**

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## **1. SUMMARY**

- 1.1. A review of the Legacy Corporation Local Plan was initiated in the autumn of 2017 to ensure that the Plan remains up to date and relevant, following the agreement of the Legacy Corporation Board in September 2017.
- 1.2. Public consultation, including community engagement and consultation with statutory and other stakeholders, businesses, landowners and developers has been undertaken on the potential scope for changes to the Local Plan and later on the potential directions for changes. The four boroughs have been frequently engaged and continue to input into this process including through the regular meetings of the 'Planning Policy Forum'. The refresh of the Infrastructure Delivery Plan, a key element of evidence for both the review of the Local Plan and the Infrastructure Delivery Plan (IDP) has also involved specific engagement with the four boroughs and other relevant infrastructure providers. A full consultation report is included at Appendix 6.
- 1.3. As a result of the review process, a number of potential changes to the adopted Local Plan are proposed which draw on the outcome of the consultation undertaken and also the requirements of the new National Planning Policy Framework (NPPF) and the draft new London Plan. These are summarised in the body of this report and set out in detail in Appendix 1.
- 1.4. A revised CIL Charging Schedule is also proposed, updating existing charging rates to reflect the amount actually now charged as a result of indexation. It is also proposed to include a charge for new office floorspace within the Stratford Retail Area, a new charge for 'Shared Living/Co-living' housing products and an "All other uses" category of charge is proposed that would include other uses not specifically charged but with an exclusion of education and healthcare use and affordable workspace. The Preliminary Draft Charging Schedule is included at Appendix 7 and the accompanying draft Infrastructure Project List and Draft Infrastructure (Regulation 123) List is included at Appendices 8 and 9. The evidence from the draft revised Infrastructure Delivery Plan has identified an infrastructure funding gap of approximately £290 million. The proposed charges in the Preliminary Draft Charging Schedule would make a contribution towards meeting this funding gap. It is estimated that the proposed revised charges could raise up to £16 million in the ten-year period subsequent to its adoption.

- 1.5. The proposed revisions to the Local Plan have, and continue to be, prepared alongside a process of Integrated Impact Assessment (IIA) and a Habitat Regulations Assessment. A report from the latest stage of this process is included at Appendices 4 and 5. The IIA incorporates a Sustainability Appraisal/Strategic Environmental Assessment, an Equalities Impact Assessment, a Health Impact Assessment and a Crime and Safety Impact Assessment.
- 1.6. This report provides the Planning Decisions Committee an opportunity to comment formally on changes proposed to the Local Plan and the CIL Charging Schedule. The comments and views expressed by the Committee will be reported to the Board when it considers a report seeking approval of the changes for the purpose of formal consultation and subsequent submission of the revised Local Plan and CIL Charging Schedule for Examination. The Examination is likely to take place during 2019 and adoption is therefore anticipated towards the end of next year.

## **2. RECOMMENDATIONS**

### **2.1 The Planning Decisions Committee is asked to:**

- 2.1.1. **Provide comment on the draft revised Local Plan with those comments being made available to the Board when it's approval of the revised draft Local Plan is sought prior to Publication stage consultation and submission for Examination.**
- 2.1.2. **Provide comment on the draft CIL Preliminary Draft Charging Schedule.**
- 2.1.3. **Note the contents of the supporting documents attached to this report and, in particular, the contents of the Integrated Impact Assessment**

## **3. BACKGROUND**

- 3.1. Local Planning Authorities are required to have an up to date Local Plan in place. The Legacy Corporation adopted its current Local Plan in July 2015. With changes to national planning policy through the introduction of a new National Planning Policy Framework (NPPF) and the publication of the Mayor of London's new London Plan, there is a need to ensure that the Local Plan and its policies remain in general conformity with these. This has also presented the opportunity identify those parts of the adopted Plan that it might be appropriate to update based on the most recent evidence gathered from monitoring of Local Plan Key Performance Indicators, updating specific areas of evidence through research and through consultation with the community and other stakeholders. This presents the opportunity to ensure that the Local Plan reflects the Mayor of London's priorities and the Legacy Corporation's plans and programmes as they currently stand.
- 3.2. Alongside the review of the Local Plan, the opportunity has also been taken to review the Community Infrastructure Levy (CIL) charging schedule to ensure the charging rates and the types of development against which the levy is charged remain appropriate. A viability review has been undertaken to inform this review and revisions have been proposed as a result of this. The proposed Preliminary Draft Charging Schedule has been included with this report for comment before approval for public consultation on this draft is sought from the Board.

## **Community and stakeholder engagement**

- 3.3. As agreed by the Board in September 2017, a period of consultation and engagement has been undertaken to identify the scope of any changes that might be required to the Local Plan to ensure it remains up to date. A consultation report has been prepared to provide the detail of what has been undertaken and the outcomes of that process. The report also sets out how this has been undertaken in accordance with the Legacy Corporations Statement of Community involvement. Appendix 6 to this report is the draft consultation report that will include a full table of representations made and responses to these, identifying how these have influenced the proposed changes to the Local Plan where relevant. Specifically, views were sought on what aspects of the Local Plan it was felt required review and consequent updating.
- 3.4. A period of formal consultation was undertaken between 6<sup>th</sup> November 2017 and 29<sup>th</sup> January 2018. During this period the focus for the consultation was:
- A series of topic based community workshops;
  - Meetings with individual groups and stakeholders;
  - A series of topic based workshops with the Planning Policy Forum, including the four boroughs;
  - The use of an online consultation portal to a provide convenient and accessible web-based option viewing information and responding;
  - Distribution of a consultation booklet and response form to local locations and contacts; and
  - Publication of consultation material and response forms on the Legacy Corporation website.

The consultation report sets out in detail how the consultation process was undertaken and opportunities for different parts of the community were provided to take part in this part of the review process

- 3.5. From the end of January to the end of June 2018 consultation channels were kept open, including the online consultation portal, to enable further comments to be made. During April and May 2018, a second series of topic based community workshops were held which focused on updated evidence and the potential directions of change to Local Plan policy. The outcomes for this are also reflected in the consultation report.
- 3.6. The Planning Decisions Committee has been updated on progress of the Local Plan review over time. In particular, specific briefings for committee members were held in June and August 2018 where the proposed changes to policy in the Local Plan and changes to the CIL charging schedule were discussed. The following key matters were raised during those briefings:
- Housing need and how the Local Plan revisions were approaching this, including the proposed continuation of a 60/40 affordable (inc social) rent/intermediate balance and confidence levels in meeting this;
  - The impacts of the higher housing target on achieving the employment ambitions in the Local Plan;
  - The approach to protecting industrial uses;
  - The robustness of the retail floorspace projections;
  - The basis for the significant increase in the numbers for the new population projections and also how this might impact on infrastructure requirements;

- How the need for a progressive approach to transport infrastructure is being taken into account and emphasising walking and cycling;
- The approach to revised design policies and the involvement of the Quality Review Panel in the process; and
- For the proposed revisions to the CIL Charging Schedule, wanting to understand the approach to and justification of the emerging changes that included introduction of a separate rate for Shared Living accommodation and the approach to a general low level charge for all other uses outside of the Stratford Retail Area.

### **Evidence base review**

- 3.7. To ensure that the Local Plan evidence base remains up to date, a series of studies and reports have been commissioned and completed. Completed reports have been made available on the Legacy Corporation website. These are listed below:
- Residents survey and population projections for the Legacy Corporation area;
  - Housing Requirements Study;
  - Combined Economy Study (including a Business Survey, Employment Land Review, Creative and Cultural Opportunities Assessment, and Retail and Town Centre Needs Assessment);
  - Flood Risk Study;
  - Open Space and Playspace Assessment;
  - Transport Study; and
  - Schools Study.

In addition to the above, a draft Characterisation Study has been produced and will be available for comment alongside the draft revised Local Plan. With the parallel review of the Legacy Corporation Infrastructure Delivery Plan (IDP), a draft IDP Report and draft new Infrastructure List will also be published alongside the drafted revised Local Plan. This provides background and evidence both for the Local Plan and the CIL review CIL Preliminary Draft Charging Schedule.

### **Technical Assessments of the draft revised Local Plan**

- 3.8. The following technical assessments of the draft revised Local Plan are required and have been undertaken:
- An Integrated Impact Assessment (IIA) (incorporating a Sustainability Assessment (SA)/Strategic Environmental Assessment (SEA), Equalities Impact Assessment (EqIA), Health Impact Assessment (HIA) and Crime and Safety Impact Assessment (CSIA);
  - A Habitat Regulations Assessment (HRA); and
  - A viability assessment of the proposed Local Plan changes (including and affordable housing viability assessment).

These assessments provide both an assessment of the potential impacts and effects of the proposed changes to the Local Plan and evidence that has helped to inform the final scope of those changes at this stage of the review process.



#### 4. Changes proposed to the Local Plan

- 4.1. The proposed changes to the Local Plan are set out in detail at Appendix 1 to this report. Appendix 2 shows the changes integrated into the Local Plan to show what the changes look like within the context of the whole Plan. This also shows the amended relevant drawings and illustrations, including site allocation diagrams. The latter document is intended to provide context to the changes, while the Schedule of draft Changes will become the formal document on which comment can be made.
- 4.2. It should be noted that the overall strategy for the LLDC area, mainly set out in Section 3 of the Local Plan, while being brought up to date in its detail has not changed and that specific changes to policies within the Plan as a whole are considered to bring these individually into general conformity with the new London Plan and meet the requirements of national planning policy. Changes have also been proposed in response to consultation during the Regulation 18 consultation stage and in response to revised or new evidence.
- 4.3. The key potential changes that are recommended are as follows:
  - 4.3.1. **Housing policies**, including reflecting the new London Plan regarding the housing target for the Legacy Corporation area of 2,161 per annum and changes to the approach to affordable housing. It is anticipated that this target can be achieved without adversely impacting upon other Local Plan aspirations, such as for employment, as the relevant densities have already often been realised in schemes within the area. The changes integrate the Mayor of London's affordable housing threshold approach so that schemes of more than 10 units are required to deliver affordable Housing based on a threshold of 35%, or 50% if on publicly owned land. These changes particularly affect Policies SP.2, H.1 Housing Mix and Policy H.2 Affordable Housing. Evidence within the Housing Requirements Study (HRS) confirmed the need to maintain the affordable housing tenure split of 60 low cost (affordable or social rented)/ 40 intermediate products. The HRS and the London Strategic Housing Market Assessment evidence also confirms the need to maintain the policy for a 'balanced mix' (33/33/33) of one, two and three bedroom properties.
  - 4.3.2. New policies, Policy H7 (Shared Living Accommodation) and H8 (Innovative Housing Models), are introduced to set approaches to newer forms of housing that are not defined within Use Class C3. These policies set out approaches to suitable locations, to design standards and suitability in achieving mixed and inclusive neighbourhoods and to achieving affordable housing.
  - 4.3.3. To ensure that the approach to be taken to delivery of housing and affordable housing by the Legacy Corporation on its own land is clear, the Local Plan includes an insert that makes reference to its housing portfolio approach through which it aims to deliver a total of 50% affordable housing across its sites that are not already subject to detailed planning consent or have a development partner appointed under the framework of the Legacy Communities Scheme. These being the sites at Stratford Waterfront, Pudding Mill, Bridgewater Road and Rick Roberts Way.
  - 4.3.4. **Business and employment policies**, in particular Policy B.1, have been updated to align with the approach in the new London Plan, including recognition that the LLDC area is within the new London Plan 'retain capacity' category for industrial floorspace capacity. To achieve this and comply with the new NPPF, changes to Policy B.1 remove the previous requirement for an impacts assessment for Use Class B.1a office accommodation but applying a sequential approach to ensure that large scale office use is directed towards the Metropolitan Centre to reinforce its

function. The other changes to this section also specifically support the introduction of a Cultural Enterprise Zone in Hackney Wick and Fish Island. The retail floorspace requirements within Policy B.2 have increased in line with new evidence, which reflects the growing population of the area and the new and enhanced centres as Bromley-by-Bow, Pudding Mill and Hackney Wick. A Night Time Economy SPD is being prepared to provide further guidance on the development of these uses, particularly within and around the existing and new centres and how these can successfully interface with the existing and emerging new residential communities in these locations. The Planning Decisions Committee will be asked to provide comment on this SPD once an initial draft has been prepared.

- 4.3.5. **Design policies**, within the Built and Natural Environment Section have a number of draft minor changes proposed as a result of specific consultation responses. However, while not representing a significant change in policy approach, the design policies BN.4 and BN.10 have been significantly amended reflecting for example the outcome of a review undertaken with the Quality Review Panel. Policy BN.4 has been redrafted to contain policy addressing the design of both residential and non-residential development, including mixed-use schemes. Policy BN.10, Tall Buildings, has been redrafted and moved next to Policy BN.4, so that is more specific, about the locations for tall buildings and clearer about the specific policy tests for tall buildings. Rather than seeking that schemes 'exhibit outstanding architecture' it now requires 'exceptionally good design', to be demonstrated through independent design review. This definition has been selected as it aligns with that within the CABE guidance (Guidance on tall buildings, English Heritage/CABE, 2007).
- 4.3.6. **Transport Policies**. Whilst in general the policies remain as previously drafted, they have been updated to reflect the changing context of projects such as Crossrail and have been expanded to include reference to the Healthy Streets approach as set out in the Draft New London Plan. Whilst Local Plan transport policies have always supported the transport hierarchy as set out in the London Plan, these continue to put active travel at the top and private vehicle last in terms of importance. The approach in supporting active travel and public transport usage has been further enhanced to support the Mayor's target of 80% of journeys taking place using these modes by 2041. There is also continued focus on the importance of development and enhancement of local connectivity, which is further supported by the Transport Study undertaken as part of the evidence base for this Local Plan Review.
- 4.3.7. **Population projections and infrastructure provision**. The revised plan takes account of the revised population projections that have been prepared for the LLDC as a part of the Local Plan background evidence. On the assumption of the delivery of all projected development capacity within the LLDC area, this suggests that the area population could rise from a current 27,000 up to 109,000 by 2036. This has used a bespoke methodology that takes the known development mix into account along with the outcome of a detailed residents survey undertaken in the summer of 2017. The study report is available publicly on the LLDC Local Plan Review website.
- 4.3.8. This data has also been used as part of the review of the LLDC Infrastructure Delivery Plan. Appendix 9 to this report, (draft Infrastructure Projects List), is the primary outcome from this review, setting out an updated list of identified projects for which LLDC and its partners will seek to find funding and resources, including CIL funding and delivery as part

of relevant development schemes. These are also reflected in the Local Plan, particularly within specific relevant site allocations.

4.3.9. As part of the review of the Infrastructure Delivery Plan, the current Local Plan position on planning for schools within the LLDC area has been reviewed. This has included a school's study undertaken by consultants to examine this position in the context of the schools that have now been delivered or are planned within the LLDC area and also the wider area around it. This has involved specific engagement with each borough schools place planning team. While no specific need for change to the Local Plan Schools policy has been identified through this process, this has confirmed the need to continue to allocate sites or locations already identified in the Local Plan and to continue an approach which would pursue school expansion in the longer term where and when that need begins to arise as a result of population change and growth, working within the context of wider borough school and schools place planning at the time. Updated existing and planned school's tables are included at Appendix 1 (Section 5).

#### 4.3.10. **Amended site allocations**

- SA1.5 East Wick and Here East: reflecting completion of the East Wick Primary School and the implementation of the Here East planning permission, minor changes to the site allocation wording have been introduced.
- SA1.6 Sweetwater: a minor change to exclude the area of the now completed primary school buildings and playfield of the new Bobby Moore Academy.
- SA3.1 Stratford Town Centre West: amendments to the areas shown as development sites to recognise where development has now been completed in some places and where the 'Call for Sites' consultation has identified additional areas for future development.
- SA3.2 Stratford Waterfront East: a reshaping of the site allocation and an adjustment to the area it covers, to make more explicit reference to the proposed cultural and education institutions that form part of East Bank.
- SA3.3 Stratford Waterfront West: a reshaping of the site allocation to recognise the outline permission and project for the new UCL East university campus.

#### 4.3.11. **Deleted site allocations**

- SA1.4 Bream Street: deleted as entire scheme has detailed planning permission and is currently under construction.
- SA1.5 Wick Lane: deleted as entire scheme also has detailed planning permission and is currently under construction.

#### 4.3.12. **New site allocations**

- SA4.4 Three Mills: recognising the opportunity to accommodate an element of new mixed-use development that would enable the long-term investment in the heritage assets that form the major part of this location and ensure their long-term active cultural and associated use.
- SA4.5 Bow Goods Yards: recognising the long-term potential for the reconfiguration of the railhead sites to provide consolidated, high quality strategic industrial uses which may, in the context of a secured long-term masterplan, lead to potential for other more mixed-use

development, in accord with the approach to planning for Strategic Industrial Land within the draft new London Plan.

- SA2.4 Chobham Farm North: has been introduced as a result of the 'Call for Sites' consultation identifying the land as available for future development, with agreement expressed by the three landowners. The site is seen as primarily appropriate for residential development, complementing the completed phase 1 of the Chobham Farm development scheme immediately to the south and providing an opportunity to further improve the street and other public realm along Leyton Road.
- 4.3.13. For all site allocations that include housing as a use, these now identify a minimum housing number and also the applicable affordable housing threshold, of either 35% or 50%, depending on whether the site is defined as privately or publicly owned. In the case of SA3.4 Greater Carpenters District, the inclusion of a minimum housing number will be relevant to the current preparations of a Neighbourhood Plan, with the new NPPF requiring that where it is relevant to do so that Local Plans set a housing target for that neighbourhood plan.
- 4.3.14. **Policies Map.** Several changes are proposed to the Policies Map in addition to the inclusion of new and removal of the deleted site allocations. These are primarily minor corrections to areas identified as Metropolitan Open Land and to areas of Local Open Space. Five new Local Open Space areas have also been added where the 2018 Open Space and Playspace Assessment had identified specific exist spaces that had not been included in the adopted Local Plan but were considered to merit inclusion based on this updated assessment work. Each change is identified in a schedule at the end of Appendix 1 and on the revised draft Policies Map (Appendix 3).
- 4.3.15. **Lee Valley Hock and Tennis Centre.** Representations were received during the formal consultation period from the Lee Valley Regional Park Authority (LVRPA) and have been taken into account as set out in the consultation Report at Appendix 6. However, subsequent discussions with the LVRPA have resulted in a request for wording within the Local Plan that highlights the importance of the implementation of future plans for the Hockey and Tennis Centre to secure a viable future for the venue. The LVRPA have not as yet determined what those future plans might be but consider that the long-standing designation of the site as Metropolitan Open Land might become an obstacle. As a result of these discussions an insert into the Sub Area 2 section of the Local Plan has been included, set out at Appendix 1, that highlights the importance of the Hockey and Tennis Centre and the underlying support of the LLDC for future plans as a partner organisation. However, the LVRPA have requested that this include a statement that would allow future proposals to be excepted from the policy requirements within the Local Plan, including those that apply to metropolitan open land. Officers consider that such a statement would not meet the requirements of sound plan making as this would represent a general exemption from policy requirements for an individual prospective applicant for planning permission. It remains the view of officers that, once the LVRPA have defined the future form of any proposals for the Hockey and Tennis Centre, these proposals should be discussed and tested in the usual way through the planning application process. This would enable the specific circumstances of that proposal to be considered and for example, if any proposed development in the MOL was considered to be inappropriate, for the 'very special circumstances' test, as required by the

National Planning Policy Framework and London Plan, to be considered at that stage and in the context of the specific proposal.

- 4.4. While the underlying structure of the Local Plan remains unchanged, some policies have been moved. In particular, Section 7 (Securing the Infrastructure to Support Growth) has been amended to focus exclusively on transport related policies, while other infrastructure policies are now moved to Section 8 (Creating a sustainable place to live and work). With the relocation of the tall buildings policy within Section 6 (Creating a high quality built and natural environment) this now follows on from Policy BN.4 resulting in a general renumbering of policies from this point onward in that section.

#### **Changes proposed to the CIL Charging Schedule**

- 4.5. The current CIL Charging Schedule came into effect in April 2015. A review has been undertaken to determine whether there is scope for increasing the CIL rates charged and the range of uses that could be capable of paying CIL.
- 4.6. A report has been prepared by PPDT's consultants BNP Paribas that examines the economic viability case for any potential change. This has concluded that it would be possible and viable to make the following changes:
- Increase all existing rates by the amount equivalent to the current indexation that is applied. For example, the current rate for residential would rise from £60 per sqm to £73.90 per sqm.
  - Include a charge for new Shared-Living/Co-Living development at £123.17 per sqm.
  - Include a charge for new B1a office floorspace development within the currently defined 'Stratford retail area' at £123.17 per sqm.
  - Include a charge for all other uses (not already defined within the charging schedule) across the LLDC area at £20 per sqm, with the exception of education, healthcare and affordable workspace which would remain zero rated.
- 4.7. The proposed changes are set out in detail in the proposed Preliminary Draft Charging Schedule at Appendix 7 to this report.
- 4.8. To justify the charges proposed two sets of evidence are required. The first is evidence of an infrastructure need in support of the planned development for the area and also evidence of a funding gap for delivery of that infrastructure which the CIL charges would then help to fill. The second is evidence of viability testing to show that the development types that would then be required to pay the identified level of CIL can viably pay this charge. Regulation 14 of the CIL Regulations (2010) (as amended) requires that:

“(1) In setting rates (including differential rates) in a charging schedule, a charging authority must strike an appropriate balance between—  
(a) the desirability of funding from CIL (in whole or in part) the actual and expected estimated total cost of infrastructure required to support the development of its area, taking into account other actual and expected sources of funding; and  
(b) the potential effects (taken as a whole) of the imposition of CIL on the economic viability of development across its area.”

- 4.9. Regulation 14(5) states that “a charging authority’s draft infrastructure list is appropriate evidence to inform the preparation of their charging schedule”. A review of the 2014 Infrastructure Delivery Plan has been undertaken in consultation with the four boroughs and other relevant infrastructure providers and a revised draft Infrastructure Projects List has been prepared to identify an up to date list of infrastructure needed to support the delivery of the development proposed within the draft revised Local Plan.
- 4.10. Alongside this a revised draft Infrastructure List has been prepared in accordance with Regulation 123 of the Regulations and this draws on this wider list. This identifies the specific projects that have been identified as those which will be wholly or partly funded by CIL and by requirements of the Regulations would not be able to be funded through any contribution secured through a s.106 Planning Obligation. Projects that are not on this list but are on the wider Infrastructure Projects List would potentially be able to secure CIL funding, provided that the relevant requirements of the CIL Regulations were complied with in each case.
- 4.11. The drafts of the Infrastructure (Regulation 123) List and the Draft Infrastructure Delivery Plan Projects List are included at Appendix 8 and Appendix 9 respectively. These will be refined and finalised at the point that the revised CIL Charging Schedule is adopted, ensuring that they remain up to date. As has been the case with the current lists, these will then be reviewed in consultation with the boroughs and other infrastructure providers and updated where necessary.
- 4.12. The Preliminary Draft Charging Schedule, if agreed by the Board, will be the subject of public consultation alongside the Local Plan. It is intended that the required second stage of consultation will also be undertaken soon afterward to enable the submission of this and the Preliminary Draft Charging Schedule and the Local Plan to the Secretary of State for the purpose of an Examination to take place at the same time and therefore be examined concurrently. In the event that the first round of public consultation results in the need for a significant change to the revised charging schedule (e.g. a more than minor change to a proposed charging rate) the further approval of the Board will be sought before being submitted for Examination.
- 4.13. The draft revised Infrastructure Delivery Plan shows that, for those infrastructure requirements where a predicted cost is known, that this amounts to a potential infrastructure cost of £290 million. In total, existing financial commitments towards those projects amount to approximately £35 million, leaving a substantial infrastructure funding gap. The Preliminary Draft Charging Schedule is considered, based on the development projections within the draft revised Local Plan evidence, to be capable of raising up to £16 million during the next ten-year period. It is, as a result, considered necessary and justifiable to expand the scope of the existing CIL charging schedule to the uses identified in order to help meet the identified infrastructure funding gap and as a result enable the delivery of the type and level of development planned for within the draft revised Local Plan.

#### **Programme and process – what happens next?**

- 4.14. The comments of the Planning Decisions Committee will be taken into account and also reported to the Legacy Corporation Board when it is asked to consider approval of the revised Local Plan and CIL Preliminary Draft Charging Schedule for the purpose of their relevant stages of public consultation.
- 4.15. For the draft revised Local Plan, if approved by the Board, a six-week period of formal public consultation will take place during November and December 2018. The Local Planning Regulations require that this consultation provide the opportunity for formal comments to be made to the proposed draft Plan, or specifically in this instance, to the changes proposed to the adopted Plan. Any

comments made in writing at this stage will be recorded and made available as part of the Examination of the Local Plan.

- 4.16. It is intended that the proposed changes to the Local Plan will be submitted to the Secretary of State in February 2019, who will appoint an independent examiner to undertake the examination of the Plan. Once an examiner has been appointed, they will begin the Examination and, where considered necessary, set dates for public hearings as part of that process. Anyone who has made a representation during the Publication stage consultation will be able to request to appear and speak in support of their case at any hearings that are held.
- 4.17. It is anticipated that any public hearings would be held in mid-2019. After this, any proposed modifications to the Local Plan are likely to require further public consultation before the examiner concludes the Examination and provides their report into the changes to the Plan. If the examiner concludes that the Plan is sound and legally compliant, the Legacy Corporation will then be able to proceed to adopting the revised Local Plan. It is anticipated that, in this event, it would be possible to adopt the revised Plan by the end of 2019.
- 4.18. For the revised CIL Charging Schedule, this has to be subject to two stages of formal public consultation. The first stage is consultation on the Preliminary Draft Charging Schedule (PDCS) which will set out the revised CIL rates per square metre for those types of development it is proposed to make a charge against. It is proposed that this consultation take place alongside that for the revised draft Local Plan. The results of this consultation will be taken into account when finalising the CIL Draft Charging Schedule (DCS) which will then be subject to a further four-week period of public consultation. Following this the CIL DCS and the comments made at that stage of consultation will be submitted for Examination. It is intended that this takes place at the same time as the submission of the draft revised Local Plan and that the examinations take place concurrently. Should the revisions to the CIL Charging Schedule be found to be sound at Examination it is proposed that this be adopted by the LLDC Board and a date set for it coming into effect set. As with the Local Plan, it is anticipated that this process would be concluded by the end of 2019.

## **5. LEGAL IMPLICATIONS**

- 5.1. When the Legacy Corporation became the local planning authority for its area in October 2012, it also became the first development corporation to be given plan making powers. As a consequence it undertook the preparation of its own Local Plan in accordance with the powers and responsibilities conferred on it as a local planning authority by the London Legacy Development Corporation, in accordance with the requirements of the Planning and Compulsory Purchase Act 2004 (as amended) (the "Act"), the Town and Country Planning (Local Planning)(England) Regulations 2012 (the "Regulations") and the requirements set out in the National Planning Policy Framework. Amendments to the Regulations were made in December 2017 and the subsequent work in reviewing the Local Plan has taken these amendments into account. Where the Regulations are referred to below, these are the regulations as amended.
- 5.2. In July 2018 a revised NPPF was published and it is required that any Local Plans that are to be submitted for examination six months from this date are required to have been prepared in accordance with the provisions within the new NPPF. The revised LLDC Local Plan will be caught by this requirement and has been prepared taking the provisions of the new NPPF into account.
- 5.3. National planning policy requires that Local Plans are kept up to date and that local planning authorities review the relevance of their plans at regular intervals to assess whether update is required. This review of the Local Plan is being

undertaken within the same legislative framework as applied to its original production.

- 5.4. The review of the Local Plan is being undertaken in compliance with the details set out in the Local Development Scheme and Statement of Community Involvement (both February 2017), as required by the Act. The consultation on the scope of changes and continuous engagement process detailed in this Report, and in more detail in the consultation report at Appendix 6, has been undertaken in accordance with Regulation 18 of the Local Planning Regulations.
- 5.5. The draft revised Local Plan, set out in Appendix 1 and 2, has been prepared in accordance with Regulation 18, while the proposed public consultation on the draft revised Local Plan will be undertaken in accordance with Regulation 19. Following that consultation, the draft revised Local Plan will be submitted to the Secretary of State for the purpose of examination by an independent examiner in accordance with Regulation 22 of the Local Planning Regulations.
- 5.6. The review of the Community Infrastructure Levy Charging Schedule (2015) is being undertaken in accordance with the provisions of the Localism Act 2011 and the Community Infrastructure Levy Regulations (2010) (as amended). The Preliminary Draft Charging Schedule at Appendix 7, if approved by the Legacy Corporation Board, will be subject to a period of public consultation in accordance with Regulation 15. A second consultation will then be undertaken in accordance with Regulation 16 incorporating any changes made as a result of the first consultation. Submission of the Draft Charging Schedule following this stage will be undertaken in accordance with Regulation 19.

## **6. PRIORITY THEMES**

- 6.1. The Legacy Corporations priority themes are: Promoting convergence and community participation; Championing equalities and inclusion; Ensuring high quality design; Ensuring environmental sustainability.
- 6.2. These themes are specifically addressed within the strategy and policies contained in the adopted Legacy Corporation Local Plan and will continue to be taken forward as the Local Plan is reviewed and revised. They are also reflected within the approach to the Community Infrastructure Levy and the management of the established associated collecting and spending arrangements, with the revision of the CIL Charging Schedule continuing to fit within this approach. The Statement of Community Involvement specifically addresses the approach to community participation in respect of the Legacy Corporations planning authority functions and will be taken into account in undertaking engagement with communities and other stakeholders on the review of the Local Plan and CIL Charging Schedule. The review and recommended changes to the Local Plan have also been developed alongside an Integrated Impact Assessment which incorporates an Equalities Impact Assessment, Health Impact Assessment, Crime and Safety Impact Assessment, alongside a Sustainability Appraisal.

## **7. APPENDICES**

- 7.1. The following appendices form part of this report:
  - Appendix 1 – Draft revised Local Plan schedule of changes
  - Appendix 2 - Draft revised Local Plan, illustrative document (weblink only)
  - Appendix 3 – Draft revised Policies Map (weblink only)
  - Appendix 4 – Integrated Impact Assessment Non-technical summary
  - Appendix 5 - Integrated Impact Assessment & Habitats Regulations Assessment Report (weblink only)
  - Appendix 6 – Local Plan Early Engagement Consultation Report (weblink only)



- Appendix 7– CIL Preliminary Draft Charging Schedule
- Appendix 8 – Draft Infrastructure (Regulation 123) List
- Appendix 9 – Draft Infrastructure Delivery Plan Projects List

Appendices available via weblink can be accessed at:

<https://www.london.gov.uk/moderngov/ldc/ieListDocuments.aspx?CId=273&MId=6103&Ver=4>

**List of Background Papers:**

Legacy Corporation Local Plan, July 2015  
Legacy Corporation CIL Charging Schedule 2015  
Infrastructure (Regulation 123) List 2015  
Infrastructure Projects List 2017  
Local Development Scheme, February 2017  
Statement of Community Involvement, February 2017

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## Appendix 1: Local Plan Schedule of Proposed Changes

The following tables set out the proposed changes to the Local Plan by each of the 14 sections within the Plan and each of the appendices, where these require updating. Where text is to be deleted this shown as crossed out and where new text is to be added, this is shown as underlined. Where policies, supporting paragraphs or other written elements of the Plan remain entirely unchanged they are not reference within this schedule. Appendix 2 is an illustrative version of the proposed new Local Plan as it would appear with the proposed changes incorporated. The ‘Publication’ stage consultation will specifically seek representations on the proposed changes rather than any elements of the Local Plan that remain unchanged.

Note - where paragraph numbering has changed within the amended document the new paragraph number is shown in brackets eg (Para 4.5).

### Section 1, Introduction

Policy, para	Type of change	Proposed change
Para. 1.1	Minor	<del>In 2012, the Mayor of London established</del> <u>The London Legacy Development Corporation was established in 2012 as the first of two mayoral development corporations in London.</u> The purpose of the Legacy Corporation is “to promote and deliver physical, social, economic and environmental regeneration of the Olympic Park and its surrounding area, in particular by maximising the legacy of the 2012 Olympic and Paralympic Games, by securing high-quality sustainable development and investment, ensuring the long-term success of the facilities and assets within its direct control and supporting and promoting the aim of convergence”.
Para 1.2	Minor	As the Local Planning Authority for its area, the Legacy Corporation has prepared a Local Plan. The Local Plan sets out the Legacy Corporation’s strategy for the sustainable development of its area as a whole, including the general amount, type and location of new development it considers could take place and the policies to which applications for planning permission should conform in order to meet these objectives. Its planning powers, including preparing and implementing the Local Plan, represent one part of the Legacy Corporation’s role as a development corporation. Alongside the development of its own land and working with its partners, including the local communities, the four Boroughs, landowners and developers, it will use its <del>wide-ranging</del> powers to implement projects and bring about change that will meet the established purpose of creating a lasting legacy from the 2012 Games and supporting and promoting the aims of convergence.
Para 1.3	Minor	<u><b>Review of the Local Plan – how to respond to the consultation</b></u> <del><b>CONSULTATION AND EXAMINATION PROCESSES</b></del>

		<p>The current Local Plan was adopted by the London Legacy Development Corporation Board in July 2015. In order to ensure that it remains up to date and relevant, the Legacy Corporation has reviewed the Plan and developed a number of changes that take account of any changed circumstances for development within the local area, the views expressed during early (Regulation 18) consultation on the review of the Plan and any changes that have taken place to both national and London planning policy.</p>
Para 1.4	Minor	<p>This draft of the revised Local Plan sets out the specific changes that are now proposed to the Plan, while it is also accompanied by a consultation report that sets out the views that have been expressed through the consultation to date and how those views have been taken into account. It is also accompanied by an Integrated Impact Assessment Report which includes a Sustainability Appraisal, an Equalities Assessment, Health Impact Assessment and a Crime and Safety Impact Assessment. A separate Local Plan viability assessment report has also been prepared. These and other background reports and papers that support the approach taken within the proposed changes to the Plan are published on the Legacy Corporation website and can be viewed or downloaded using the following link <b>[insert link]</b>.</p>
Para 1.5	Minor	<p>The consultation on the proposed revised Local Plan is being undertaken within the requirements of Regulation 19 of the Local Planning Regulations. More information about how you can respond to the consultation can be found in the following places:</p> <ul style="list-style-type: none"> <li>• The consultation response booklet and form (paper copies can be requested or an electronic copy can be downloaded from the website <b>[insert link]</b>). The form can be returned by email or by post.</li> <li>• The online consultation portal, where you can view the local plan changes and leave comments online <b>[insert link]</b>.</li> </ul> <p>The consultation starts on [DATE X] and all comments must be received no later than 5 pm on [DATE Y]. You should read the accompanying instructions to ensure that your comments are made in the correct way. This is important as any comments you make will be reported in public and will form part of the formal Examination of the changes to the Local Plan that will be held by an independent planning inspector. The appointed inspector will examine the changes to determine whether these are 'sound', 'legally compliant' the required process of preparation and conform to national and London planning policy.</p>

		<p>You can contact the LLDC Planning Policy Team in the following ways:</p> <ul style="list-style-type: none"> <li>• Email: <a href="mailto:planningpolicy@londonlegacy.co.uk">planningpolicy@londonlegacy.co.uk</a></li> <li>• Telephone: 0203 288 1800</li> </ul> <p>You can also inspect the revised draft Local Plan and any accompanying documents at the office of the London Legacy Development Corporation provided that you first contact us and make an appointment.</p>
Para 1.6-1.9	Deleted	<p>Local Plan has been produced following extensive periods of consultation and engagement which began shortly after the Legacy Corporation took on its planning powers in October 2012.</p> <p>Between 4th December 2013 and 7th February 2014, consultation was undertaken on the 'Draft Local Plan Consultation Document'. The comments received were reviewed and influenced the Publication version of the Local Plan. Consultation on the Publication Local Plan took place between 18th August and 6th October 2014 seeking comments on its 'soundness' as defined by the National Planning Policy Framework (2012).</p> <p>Following this consultation, the Local Plan and consultation responses were submitted to the Secretary of State on 21st November 2014, who appointed an independent Planning Inspector. The Examination hearing sessions took place in March 2015 and following receipt of the Inspector's Report in July 2015, the Local Plan was adopted at the Legacy Corporation's Board on 21st July 2015.</p> <p>For any queries in relation to the Local Plan please contact the Planning Policy and Decisions team in the following ways:</p> <p>Telephone: 020 3288 1800 Email: <a href="mailto:planningpolicy@londonlegacy.co.uk">planningpolicy@londonlegacy.co.uk</a></p>

## Section 2, Our Area

Policy, para	Type of change	Proposed change
Para 2.1	No change	This section sets the scene in regard to the role of the Legacy Corporation and the baseline position of the Local Plan. It sets out the historical context and current profile of the Legacy Corporation area, and the challenges and opportunities faced in creating the economic growth and development proposed.
Para 2.2	No change	The timeline at paragraph 2.4 shows how this part of east London has evolved from the significant development which occurred within the Victorian era through industrial decline to the current rejuvenation. The current profile at paragraph 2.5 provides context to the establishment of the Legacy Corporation, <del>particularly the lower educational attainment, skills and jobs opportunities of many residents,</del> and justifies the role of the Legacy Corporation in improving prospects and achieving convergence. Paragraph 2.6 highlights the main challenges and opportunities faced in this task of creating employment and educational and commercial growth, building a significant number of new homes and providing infrastructure, all which will take place over the Plan period.
Para 2.3	No change	The Legacy Corporation area is located within east London, approximately four miles from the Central Activities Zone. It occupies a key strategic location at the meeting point of the London– Stansted–Cambridge–Peterborough growth corridor and the Thames Gateway Growth Corridor. Within London, the area is directly connected to the major business and growth hubs of Central London, Canary Wharf and the Royal Docks.
Economic	Minor	<p><b>ECONOMIC</b></p> <ul style="list-style-type: none"> <li>• <del>Above average unemployment levels – 11 per cent for the four Growth Borough</del></li> <li>• <u>High employment rate – 63 per cent, and most are full time employees</u></li> <li>• <u>Below London average unemployment level</u></li> <li>• <u>The highest proportion of employees work in professional occupations</u></li> <li>• A greater potential workforce, with lower levels of retired people than the London average</li> <li>• <del>More low-level, and fewer managerial employees than the London average</del></li> <li>• <u>Considerable growth of businesses operating within the area; high employment growth, more than six times the growth in London</u></li> <li>• <del>Mixed employment picture across the Boroughs – Tower Hamlets shows the highest jobs growth</del></li> </ul>

Policy, para	Type of change	Proposed change
		<ul style="list-style-type: none"> <li>• <u>A greater proportion of micro businesses and SMEs, more than London and the Growth Boroughs</u></li> <li>• <u>Substantial increase in creative businesses since 2012, more than three times the rate of increase in London</u></li> <li>• <u>High proportion of jobs in ICT and Digital industries indicates that the area is becoming an innovation and hi-tech hub</u></li> <li>• <u>Lower than London average house prices – but still a significant gap between earnings and house prices</u></li> <li>• <u>Increase in service sector industries and a decline in manufacturing and employment land.</u></li> </ul>
Social	Minor	<p>SOCIAL</p> <ul style="list-style-type: none"> <li>• <u>The Mayoral Development Corporation (MDC) Area is a fast-growing area, with a current population of 26,274, up 16,000 from 2012 low existing population of 10,273</u></li> <li>• <u>The population of the Legacy Corporation area is relatively young with over 60 per cent being under 34, and only four per cent over 65</u></li> <li>• <u>Above London average proportion of people with no qualifications. More than a half of the population hold a degree level qualification, outperforming London and England</u></li> <li>• <u>Greater private rented housing stock, nearly double the London average social rented/ Registered Provider housing stock than London average</u></li> <li>• <u>Lower than London average health levels, and life expectancy below London and UK averages</u></li> <li>• <u>Overall very high level of residents expressing satisfaction with the area</u></li> <li>• East London has some of the most deprived local authority areas within England: Hackney, Newham and Tower Hamlets have some of the highest concentrations of deprivation.</li> </ul>
Environmental	No change	<p>ENVIRONMENTAL</p> <ul style="list-style-type: none"> <li>• The overall Legacy Corporation area is 480ha</li> <li>• This includes about 100ha of Local Open Space in more than 40 locations</li> <li>• The area of Queen Elizabeth Olympic Park is 226ha</li> <li>• The Legacy Corporation area contains 6.5km of waterways and a range of Biodiversity Action Plan habitats</li> <li>• The area also contains vacant land and some areas of potentially contaminated land.</li> </ul>
Challenges	Minor	<p>CHALLENGES</p> <ul style="list-style-type: none"> <li>• Maintaining and strengthening the area's economic base, while diversifying into new sectors</li> </ul>

Policy, para	Type of change	Proposed change
		<ul style="list-style-type: none"> <li>• Attracting international investment and businesses to the new office and other employment locations, <u>and support local enterprises</u></li> <li>• Creating an expanded but integrated centre at Stratford, without severance from the existing Stratford town centre, <u>and maintaining and</u> creating other <del>new</del> thriving new centres</li> <li>• Maintaining the character and strengths of existing communities and creating new neighbourhoods with distinct identities</li> <li>• Providing for housing needs in number, size, <del>form</del> <u>and tenure</u></li> <li>• Delivery of planned infrastructure to support growth, including improving connectivity and supporting pedestrians and cyclists</li> <li>• Protecting and enhancing the natural and built environment, including mitigating the effects of climate change</li> <li>• Improving health outcomes and life opportunities for those who live and work within the area</li> </ul>
Opportunities	Minor	<p>OPPORTUNITIES</p> <ul style="list-style-type: none"> <li>• The supply of large areas of land, enabling the development of homes and communities</li> <li>• The sporting legacy of the 2012 Olympic and Paralympic Games and maximising the use of the Games venues – creating a thriving sport, tourist and visitor destination</li> <li>• Attracting high-profile institutions, including arts, culture and education to invest in the area’s future</li> <li>• Raising the profile of the area through building on its appeal as a sport, tourist and visitor destination and creating well designed new developments</li> <li>• <u>Continued</u> improvements to transport capacity and connectivity, including the enhancement of the waterways</li> <li>• Further capacity becoming available within the public transport network – for example, Crossrail</li> <li>• Working with new and existing communities to create stronger neighbourhoods</li> <li>• Greening and improving the environment, including biodiversity</li> <li>• Continued educational expansion for all-ages</li> <li>• Creating high quality buildings and places, which have inclusive design and maintain and build upon existing local character</li> <li>• Being an exemplar of sustainability</li> <li>• To <u>continue to build on the existing</u> <del>recently installed</del> low-carbon, drainage and other infrastructure – for example, heating and cooling networks</li> <li>• Remediation of land and utilising vacant and underused land for positive purposes</li> </ul>



## Section 3, Our Vision – what we want to achieve

Policy, Para	Type of change	Proposed change
Our vision – Mission & purpose	Minor	<p><u>PURPOSE</u></p> <p><b>3.2</b> The Legacy Corporation’s <u>purpose strategy</u> is to focus on <del>three</del> <u>the following</u> areas:</p> <p><u><b>LIVE:</b> Establish successful and integrated neighbourhoods, where people want to live, work and play.</u></p> <p><u><b>WORK:</b> Retain, attract and grow a diverse range of high quality businesses and employers, and maximise employment opportunities for local people.</u></p> <p><u><b>VISIT:</b> Create a diverse, unique, successful and financially sustainable visitor destination.</u></p> <p><u><b>INSPIRE:</b> Establish a 21st century district promoting cross-sector innovation, education, culture, sport, aspiration and participation in east London.</u></p> <p><u><b>DELIVER:</b> Deliver excellent value for money, and champion new models and standards which advance the wider cause of regeneration, in line with LLDC’s core values.</u></p> <p><del><b>PARK:</b> a successful and accessible Park with world-class sporting venues offering leisure space for local people, arenas for thrilling sport, enticing visitor entertainment and a busy programme of sporting, cultural and community events to attract visitors.</del></p> <p><del><b>PLACE:</b> a new heart for east London, securing investment from across London and beyond, attracting and nurturing talent to create, design and make world-beating 21st-century goods and services, and becoming a place where local residents and new arrivals choose to live, work and enjoy themselves, and where businesses choose to locate and invest.</del></p> <p><del><b>PEOPLE:</b> opportunities and transformational change for local people, opening up access to education and jobs, connecting communities and promoting convergence – bridging this gap between east London and the rest of the capital.</del></p>
Objectives	Minor	<p><b>Objective 1:</b> Increase the prosperity of east London through growth in business and quality jobs with an emphasis on cultural and creative sectors, promotion as a visitor and tourist destination and high-quality <u>lifelong learning higher education and training opportunities.</u></p>

Policy, Para	Type of change	Proposed change
		<p><b>Objective 5:</b> Deliver a <u>smart</u>, sustainable and healthy place to live and work.</p>
<p>Legacy Corporation area in 2031</p>	<p>Minor</p>	<p><b>The Legacy Corporation area in 2031</b></p> <p>By 2031, the Legacy Corporation area will have become an established location for working, living, leisure and culture. Based upon locally distinctive urban districts, linked by green spaces and waterways, with Queen Elizabeth Olympic Park, <del>and</del> its world-class sports venues <u>and the Culture and Education District</u> as a centrepiece, the benefits of sustained investment and renewal radiate well beyond the area, blurring boundaries to create a new heart for east London.</p> <p>Stratford has become a Metropolitan Centre with an international role, a home or focus for international businesses served by international trains and quick links to airports, with universities and cultural institutions alongside the commercial, retail and sporting centres. Universities have established a reputation for undergraduate and postgraduate education with associated research and development activity, and businesses are an important part of the local economy, particularly around Stratford Waterfront and Pudding Mill.</p> <p>Here East is a technology- and media-focused hub which, together with the rest of Hackney Wick <del>and Fish Island as a</del> <u>Cultural Enterprise Zone</u>, provides space for creative and productive businesses, complementing the established clusters of artists and makers.</p> <p>In addition to being a location where thousands of people live, work and relax, Queen Elizabeth Olympic Park is recognised as one of London’s unmissable attractions for visitors, and as a global centre for cultural and sporting excellence.</p> <p>A District Centre at Bromley-by-Bow, the Neighbourhood Centre at Hackney Wick and the Local Centres at Pudding Mill and East Village provide a focus for local shops, services and community activities, surrounded by thousands of well designed new homes, including family and affordable homes, to create a network of distinctive and mixed urban districts. Accessible and well maintained local footpaths, cycleways and roads tie these urban districts together, and into their wider surroundings, making it easy to access the public transport hubs at Stratford, Hackney Wick and Bromley-by-Bow. The networks of parks, local routes, community sports facilities, schools and other community facilities make this a healthy place to live and work.</p> <p>The design of buildings respects the character of the area and these have become examples of high-quality design. District heating and cooling networks, the bio-diverse landscapes and waterways, and trees and general urban greening complete the</p>

Policy, Para	Type of change	Proposed change
		picture of a sustainable and comfortable place to live and to work. The Legacy Corporation area has become somewhere that people aspire to work and live, a unique and exemplary place that has set the standard for London as a whole as it continues to change and grow.
Para 3.4	Minor	<b>3.4</b> The vision for the Legacy Corporation area, set out on the previous page, draws on its corporate vision and sketches a picture of the area at the end of the Plan period in 2034 <del>16</del> . As a Development Corporation, its planning powers are one set of tools for achieving the regeneration and legacy benefits that the organisation has been created to realise. The five objectives translate the corporate vision into the aspects that its planning powers can be used to achieve. These set the policy themes that are relevant to the circumstances of the area and the benefits that achieving these can bring for the surrounding areas of east London
The strategic context Para 3.5	Minor	Proposed deletion of existing text at paragraph 3.5 and replacement as follows:  “The Mayor has set out his strategic <u>planning</u> objectives for the Legacy Corporation and for this Local Plan within his London Plan. <u>This clearly identifies the Legacy Corporation as one of the London planning authorities whose Local Plan will need to be in general conformity with his London Plan. The area boundary is recognised and a specific housing target is set. Meeting housing and employment land needs within the context of the wider opportunity area is highlighted. In order to achieve this, many of the policies in this Plan are strongly linked to the policies and standards established within the Draft New London Plan and the associated Supplementary Planning Documents.</u> ”
The strategic context Para 3.6	Deletion	Paragraph 3.6 deleted.  <del><b>3.6</b> The economic and cultural importance of Queen Elizabeth Olympic Park and also that of the Lee Valley Regional Park and their visitor, sporting and wider cultural attractions is also underlined in London Plan Policy 4.5 London’s Visitor Infrastructure and its accompanying Map 4.2. This specifically identifies both as Strategic Cultural Areas and seeks to promote, enhance and protect their special characteristics.</del>
Policy SD.1	Minor	Policy SD.1 amended as follows:  <b>Policy SD.1: Sustainable development</b> When considering development proposals, the Legacy Corporation will take a positive approach that reflects the presumption in favour of sustainable development contained in the National Planning Policy Framework (NPPF). It will always

Policy, Para	Type of change	Proposed change
		<p>work proactively with applicants jointly to find solutions which mean that proposals can be approved wherever possible, and to secure development that improves the economic, social and environmental conditions in the area.</p> <p>Planning applications that accord with the policies in this Local Plan, the London Plan (and, where relevant, with policies in neighbourhood plans) will be approved without delay, unless material considerations indicate otherwise.</p> <p>Where there are no policies relevant to the application or <del>relevant policies that are most important for determining the application</del> are out of date at the time of making the decision, then permission will be granted unless material considerations indicate otherwise – taking into account whether:</p> <ul style="list-style-type: none"> <li>• <u>The application of policies in the NPPF that protect areas or assets it defines as being of particular importance provide a clear reason for refusing the development proposed; or</u></li> <li>• Any adverse impacts of granting permission would significantly and demonstrably outweigh the benefits, when assessed against the policies in the NPPF taken as a whole. ; or</li> <li>• <del>Specific policies in the NPPF indicate that development should be restricted.</del></li> </ul>
Page 17 Para 3.7	Minor	<p><del>Deletion of paragraph</del></p> <p>The United Nations World Commission on Environment and Development (WCED) in its 1987 report ‘Our Common Future’ defines sustainable development as “development that meets the needs of the present without compromising the ability of future generations to meet their own needs”. Achieving sustainable development in the Legacy Corporation area means securing development that complies with the relevant policies set out in this Local Plan as a whole. As set out in paragraph 7 of the National Planning Policy Framework, there are three dimensions to sustainable development that give rise to the need for the planning system to perform a number of roles – economic, social and environmental – and “these roles should not be taken in isolation, because they are mutually dependent”. Paragraph 9 of the NPPF provides that pursuing sustainable development involves seeking positive improvements in the quality of the built, natural and historic environment, as well as in people’s quality of life, including (but not limited to):</p> <ul style="list-style-type: none"> <li>• <del>making it easier for jobs to be created in cities and towns</del></li> </ul>

Policy, Para	Type of change	Proposed change
		<ul style="list-style-type: none"> <li>● <del>moving from a net loss of bio-diversity to achieving net gains for nature</del></li> <li>● <del>replacing poor design with better design</del></li> <li>● <del>improving the conditions in which people live, work, travel and take leisure, and</del></li> <li>● <del>widening the choice of high-quality homes.</del></li> </ul>
Page 17 Para. 3.8	Minor	<p>Paragraph 3.8 amended as follows and becomes paragraph 3.7:</p> <p><b>3.7</b> <u>The United Nations World Commission on Environment and Development (WCED) in its 1987 report ‘Our Common Future’ defines sustainable development as “development that meets the needs of the present without compromising the ability of future generations to meet their own needs”.</u> The NPPF provides that <u>planning policies and decisions should play an active part in guiding development towards sustainable solutions but in doing so should take local circumstances into account to reflect the character, needs and opportunities of each area</u> <del>plans and decisions need to take local circumstances into account, so that they respond to the different opportunities for achieving sustainable development in different areas. The planning system should play an active role in guiding development to sustainable solutions.</del> In relation to making planning decisions for new development in the Legacy Corporation’s area, understanding the strategy for sustainable development and the elements that need to be implemented to achieve it will mean, for each development proposal in question, taking and applying the policies in this Local Plan as a whole.</p>
Page 17 Para. 3.9	Minor	<p>Paragraph 3.9 amended as follows and becomes paragraph 3.8:</p> <p><b>3.8</b> To help us understand how well we are managing to achieve the objectives that are set out in this Local Plan, the Legacy Corporation will monitor a set of indicators. Where the objectives are not being met, this may then trigger a review of part or all of the Local Plan. <u>Responsibility for creation, monitoring and review of planning policy will return to the four boroughs once planning powers have returned to them.</u> Section 14 of the Local Plan, ‘Delivery and Implementation, includes a table that sets out the performance indicators against which the objectives in the Local Plan will be monitored.</p>
New insert page	Minor (non-policy amendment)	<p><b>Insertion to be included to highlight LLDC priority projects - images of East Bank and LLDC Housing and short text outlining these as main corporate priorities. Text as below:</b></p> <p><b><u>The Legacy Corporations Priority Projects</u></b></p>

Policy, Para	Type of change	Proposed change
		<p><u>The Legacy Corporation and its partners will be focusing on delivery of the following projects that are central to its purposes and objectives.</u></p> <p><b><u>East Bank</u></b>  <u>Providing new homes for Sadler’s Wells, BBC Music and the V&amp;A (in partnership with the Smithsonian Institution), as well as University College London and University of the Arts London’s, London College of Fashion.</u></p> <p><b><u>Housing Delivery</u></b>  <u>Delivering the planning permissions for approximately 2,400 new homes at Chobham Manor, East Wick and Sweetwater.</u></p> <p><u>Delivering development across its portfolio of other sites (see map opposite)) and in doing so achieving 50% affordable housing across this portfolio, which combines the following site allocations:</u></p> <ul style="list-style-type: none"> <li>• <u>SA3.2 – Stratford Waterfront East</u></li> <li>• <u>SA3.5 – Bridgewater Road</u></li> <li>• <u>SA3.6 Rick Roberts Way</u></li> <li>• <u>SA4.3 – Pudding Mill.</u></li> </ul>

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## Section 4: Developing business growth, jobs, higher education and training

Paragraph, etc	Type of change	Proposed change
Para 4.1	Minor	...The transformation will be brought about through opportunities for employment, education and skills attainment and by drawing additional investment into the local economy through retail, leisure, <u>cultural</u> and visitor attraction expansion.
Para 4.2	Minor	The results of investment are already being seen. <u>Significant employers are being attracted to locate to the area in a way that rapidly raises the local employment density and new manufacturing and service sectors are emerging from the strong employment foundations that currently exist, while interest from prominent education and cultural institutions keen to invest in the area boosts.</u> <u>Proposals for significant cultural and education investment are progressing which will boost the area profile nationally and internationally.</u> The success of the opening of Queen Elizabeth Olympic Park and <u>its role as a cultural quarter as well as the continued and expanding role of the retail and leisure core</u> has renewed an interest in and heightened the appeal of this part of east London. The Legacy Corporation's area has transformed into a hotspot for development and activity, and is rapidly becoming a highly desirable place to live, work and visit. These successes will be built upon to draw further investment into the area, which will benefit local businesses and communities as well as enhance local employment and educational opportunities through provision of new and varied forms of employment, higher education, research and development and enhancing local access to jobs and training opportunities. This will continue the renewal of one of the most dynamic and interesting parts of London.
Objective 1	Minor	Objective 1: Increase the prosperity of east London through growth in business and quality jobs with an emphasis on cultural and creative sectors, promotion as a visitor and tourist destination and high-quality <del>lifelong learning</del> <u>higher education and training opportunities</u> . This will mean: <ul style="list-style-type: none"> <li>• An internationally focused office and business quarter established around the Metropolitan Centre at Stratford and a technology- and media-focused business hub at Here East</li> <li>• A centre of cultural and sporting excellence based around the retained Games venues and at <u>Stratford Waterfront (East Bank)</u></li> <li>• Established centres for town centre and business uses at Stratford, Hackney Wick, Bromley-by-Bow, East Village and Pudding Mill</li> <li>• Diversity of employment provision within business clusters, focused around Fish Island and Sugar House Lane, and expansion in research and development activity focused at Pudding Mill</li> </ul>

Paragraph, etc	Type of change	Proposed change
		<ul style="list-style-type: none"> <li>• New established university campuses, including at Here East <u>and East Bank</u></li> <li>• Excellent access for local people and businesses to a range of skills and training opportunities that meet their needs.</li> </ul>
Para 4.3	Minor	... Central to the transformation of the area is acceleration of this trend, leading to Stratford and Queen Elizabeth Olympic Park becoming key locations of business, <u>culture, education, leisure and tourist and visitor expansion.</u>
Para 4.4	Minor	...Opportunities for enhancing the employment offer and mix throughout the area will be utilised, including promoting the creative, productive and cultural industries and social enterprise <u>including through the potential Creative Enterprise Zone at Hackney Wick and Fish Island</u> . The area will become a seat for learning, drawing students from a great distance to east London but also facilitating the training and employment prospects of local people, which in turn generate local wealth. <u>Measures within this section will in combination support role of a Strategic Area for Regeneration, as identified within the London Plan.</u> This policy will strengthen and build upon this base, enabling economic expansion and diversification. Queen Elizabeth Olympic Park will provide an interactive and smart experience, with more integrated approaches to building and technology enabling the digital economy to grow <span style="color: red;">[New Smart reference]</span> . This position is complemented by existing and new town centres generating local wealth and investment through agglomeration.
Para 4.5	Minor	Figure 4 demonstrates the overall economic strategy within the Local Plan. The employment clusters (see Policy B.1) will be the focus for B Use Classes, where office and workshop accommodation are appropriate in accordance with the description within Table 2. Generally, town centre uses of retail, leisure, office and visitor attractions and accommodation will be directed towards the Centres through Policy B.2, where other uses may be appropriate according to the location. <u>The Stratford Waterfront sites (East Bank) will become a new cultural focus to the area and relationships with other cultural offers of the area including at Three Mills and Hackney Wick will be built upon within the strategy.</u>
Figure 4	Minor	<p>To include the Stratford Waterfront as an edge of centre site and to show location of future expansion of the Metropolitan Centre boundary</p> <p>To show 'cultural connections' across the area</p>
Table 1	Minor	Table 1 changes made below.
B.1	Minor/major	...1. <u>Applying the sequential assessment of sites to direct large-scale office uses towards the Metropolitan Centre to support the potential Central Activities Zone reserve and locating smaller scale-<del>Locating</del> office uses within the other centres and requiring impacts assessment where B1a office accommodation</u>



Paragraph, etc	Type of change	Proposed change
		<p><del>over 2,500 sqm is proposed outside Stratford Metropolitan Centre boundary</del></p> <p>2. Ensuring new provision is flexible, meeting the needs of a wide range of end users, <u>including through different-sized units</u>, contains adequate access and servicing and has no conflict with immediate uses</p> <p>3. Safeguarding land <del>and buildings</del> within Strategic Industrial Locations (SIL) for the balance of B Use Classes identified within Table 2 <del>in density and floorspace</del>. <u>The industrial floorspace capacity and job densities of the SIL will be protected and intensified, where appropriate. The intensification and consolidation of SIL for other uses will only be acceptable where identified within Table 2 and the relevant Site Allocations.</u></p> <p><del>4. Only allowing proposals providing equivalent use, in density and floorspace, which maintain the existing balance of uses identified within Table 2 and meeting needs of small and medium sized businesses within the Locally Significant Industrial Sites (LSIS) and Other Industrial Locations (OIL)</del></p> <p><u>4. Protecting the industrial floorspace capacity and job densities of the Locally Significant Industrial Sites (LSIS) and Other Industrial Locations (OILs) for uses identified within Table 2. Proposals for intensification, consolidation or co-location with other uses will only be acceptable where identified within Table 2 and the relevant Site Allocations.</u></p> <p><del>5- Proposals on non-designated industrial sites employment land outside the clusters and including where new uses are proposed within the OILs shall maintain or re-provide existing employment capacity by applying the following:</del></p> <p>a) <u>Proposals involving a change from B2 or B8 use class floorspace (including working yardspace) shall re-provide industrial floorspace capacity within the same use class category or intensify capacity through increased job densities within other B class uses, according to location by applying the town centres first principle. Maintain or re-provide equivalent industrial floorspace within B2/B8 Use Classes; or</u></p> <p>b) <u>Proposals involving a change from B1 use class floorspace shall intensify capacity through increased job density. Maintain or re-provide equivalent employment floorspace within B1 Use Classes or significantly increase job densities within B Use Classes</u></p>

Paragraph, etc	Type of change	Proposed change
		<p><u>6. Proposals considered under 4-5 above will only be acceptable where:</u></p> <ul style="list-style-type: none"> <li>a) <u>the role and function of the designated and non-designated industrial sites are not compromised</u></li> <li>b) <u>any new uses including residential development are phased behind the intensification or consolidation of the industrial functions</u></li> <li>c) <u>the development is well-designed to allow the long-term co-location of uses including the mitigation of any negative impacts of noise, nuisances and air quality</u></li> </ul> <p><u>7. Proposals, including conversions, shall also be considered against:</u></p> <ul style="list-style-type: none"> <li>a) <del>e)</del> Proximity of incompatible uses to the existing and proposed use;</li> <li>b) <del>d)</del> The effective potential reuse of <u>heritage assets buildings of value</u> for employment;</li> <li>c) <del>e)</del> Re-location strategies showing how existing businesses can be suitably accommodated;</li> <li>d) <del>f)</del> <u>Evidence of demand for this form of employment space, through viability appraisal showing demonstrating suitability of maintaining or re-providing industrial or employment within the building location;</u></li> <li><del>(g)</del> marketing strategies <u>with appropriate lease terms</u> for two immediately preceding years <del>showing attempts to market the property for employment uses;</del> and <del>h)</del> other overriding factors potentially inhibiting the continuation of employment use.</li> </ul>
Para 4.7	Minor	<p>The diversity of the economic offer and its ability to transform and grow is a key feature of and a major factor in the potential of the area. The range of employment sectors operating across the area is remarkable, providing the key conditions for cultural and creative uses, makers and other manufacturers to flourish while heavier industries, office, retail and leisure <u>and sporting industries and uses</u> provide for broader employment needs.</p>
Para 4.8	Minor	<p>Strengthening the foundations of creative and cultural industries <u>including through a potential Creative Enterprise Zone</u> together with new economic uses at Hackney Wick and Fish Island will provide a crucial environment for the stimulation of growth, while heavier industries and transportation uses largely towards the south of the area and within the employment clusters provide for more established employment requirements. The economic profile in and around Stratford will be diverse, where office development will form much of the B Use Class development, <u>alongside retail and leisure and the Queen</u></p>

Paragraph, etc	Type of change	Proposed change
		<u>Elizabeth Olympic Park’s attractions and sporting venues will provide economic value..</u>
Para 4.9	Minor	<p>The draft New London Plan requires that the Legacy Corporation area ‘retains capacity’ of industrial land. Local evidence within the <u>Combined Economy Study (2018)</u> Study of the economic profile of the area has identified a diverse range of employment opportunities available, with an incredible mix of office, industrial businesses, makers, manufacturers, artists and other creative businesses present within the area, but a slight shift in focus towards more established businesses since the 2014 study. Many of these businesses operate within the industrial use classes. B2 Use Class, and much of the area’s available workshop space is currently occupied by this use class.<sup>5</sup> Nevertheless, evidence<sup>6</sup> has found a small surplus of industrial land (B2/B8), recommending the release of some sites from purely industrial designation for other uses; further release may be appropriate, provided that a suitable quantity of employment space is re-provided within mixed use developments. In response, sites have been released from industrial designation using the Land for Industry and Transport Supplementary Planning Guidance (SPG) criteria, but principally due to their regeneration potential or lack of industrial capacity.<sup>7</sup> As a result, industrial land supply over the Plan period is broadly equivalent to demand. The Combined Economy Study (2018) confirms that if employment space can continue to be included as part of mixed-use development and the general principle of no net loss is applied across the Employment Clusters then the demand for industrial land is likely to be met over the plan period.</p>
Para 4.10	Minor	<p>This also means that capacity could remain for further release of land from large-format industrial uses that are incompatible with mixed-use development and re-provision in a different employment format on those sites through specific development proposals. In these cases, regeneration programmes should aim to facilitate the circumstances which enable valuable existing businesses to remain within the area. This may include intensification, consolidation and co-location where the benefits of shared materials and resources can also be achieved. Further loss of B2/B8 use class industrial floorspace within the Legacy Corporation will not be supported except in the very particular circumstances set out below. This principle will be applied through protection of B Use Classes in accordance with Table 2 within designated clusters, and according to a criteria-based approach outside the clusters.</p>
Para 4.11	Minor	<p>The Legacy Corporation supports the provision of employment floorspace which can accommodate the types of businesses currently drawn to the area, in particular the creative, productive and cultural industries, <u>night time economy uses</u> as well as new innovative technology sectors. Many of these</p>

Paragraph, etc	Type of change	Proposed change
		activities can be accommodated in workshops as well as larger flexible spaces, so proposals incorporating these formats this format of floorspace will be supported.
Para 4.13	Minor	<p><b>Clusters</b></p> <p>The Town Centre boundaries are shown on the Policies Map, which also shows the Metropolitan Centre boundary as being the location for the potential Central Activities Zone (CAZ) reserve. Larger-scale office uses are defined as floorspace over 2,500sqm should be directed towards the Metropolitan Centre. The sequential assessment directs office uses below this threshold to within the other centre boundaries, and only small-scale, complementary office uses will be permitted outside these boundaries. Proposals of this scale should also consider the provision of space suitable for SME including affordable workspace or low-cost business space, see Policy B.4. Table 3 sets out further detail of the role of each Centre in relation to main town centre uses.</p> <p><b>4.14</b> The boundaries of each of the employment cluster designations are shown on the Policies Map. Table 2 makes clear what balance of uses and form of development will be suitable within each location as well as setting out the potential for intensification, consolidation and co-location. For the purposes of clarity, due to the limited amount of storage and distribution uses within the LLDC area it is not considered that substitution will be appropriate. The existing industrial floorspace capacity balance of floorspace and density will be maintained. Distinctions between the LSIS and OILs follow the London Plan Land for Industry and Transport SPG terminology whereby the Other Industrial Locations are most susceptible to change. The Draft New London Plan identifies three categories of industrial land: Strategic Industrial Locations (SIL), Locally Significant Industrial Sites (LSIS), and Non-Designated Industrial Sites. The industrial and associated specialisms of Here East (Hackney Wick) focus on technological and creative industries, therefore is also identified within Table 2 as a new local category of SIL (Strategic Technology Cluster). Within the category of Non-Designated Industrial Sites this Plan designates some sites as Other Industrial Locations (OILs) which are also included as employment clusters in Table 2. OILs are locally designated industrial sites considered most susceptible to change, likely to include the introduction of new uses, including residential integrated across the site through delivery of mixed use development. Where acceptable change identified within the OILs is proposed, Bullet points 5 (c) and (d) will be applied. The Legacy Corporation will support and promote measures to improve employment clusters through Section 106 Agreements.</p>

Paragraph, etc	Type of change	Proposed change
		Where identified within Table 2, residential will be appropriate when the employment-generating potential and industrial <u>floorspace capacity</u> are <u>maintained not compromised</u> and amenity and servicing issues have been addressed.
Para 4.14	Minor	<p><b>4.15</b></p> <p><b>Outside Clusters</b></p> <p>The intention of the policy is to maintain employment outside the clusters as it plays a pivotal role in the economic performance of the area. <del>Use Classes and, in some cases, locational circumstances will determine whether 5 (a) or (b) applies. Bullet points (c) and (d) are</del> <u>Part 5 of the Policy requires that the employment capacity of these sites is maintained or re-provided. Part 6 sets out the additional criteria which proposals considered under parts 4 and 5 will need to satisfy. Part 7 contains other considerations which shall be taken into account, including how the existing and the proposed use integrates or conflicts with the surrounding area or the development proposed within this Local Plan, and where the reuse of buildings of value shall be considered positively. <del>Bullet points (e) to (h) apply where a loss of employment, including through conversion, is proposed.</del> Industrial floorspace capacity is defined as the existing floorspace on site or the potential amount measured on a 65 per cent plot ratio, whichever the greater</u> <small><a href="#">[insert footnote reference to London Plan definition]</a></small>. Where a job density approach is applied, densities should either be above average for the B Use Classes where existing job density is low, or significantly increase densities from existing levels, whichever is the greater. Where density is applicable, the additional jobs created should meet local requirements.</p>
Para 4.15	Minor	<p><b>4.16</b> Under Bullet 5 (a), where the premises are within, or most recent permanent use is within, B2/B8 industrial uses, equivalent floorspace <u>capacity</u> shall be maintained or re-provided. The only exceptions shall be where the current use is clearly and demonstrably in direct conflict with its immediate surroundings, or where the current use is clearly incompatible with mixed-use development proposed within this Local Plan for the specific site, including at Hackney Wick <del>and</del> <u>Fish Island</u>. In these circumstances the equivalent employment floorspace to be re-provided should be in the form of workshops which are compatible with mixed-use development, including within B2 Use Classes, in the first instance; or proposals should significantly increase job density within B Use Classes, appropriate to location, with proven ability to let. This will ensure redevelopment proposals enable existing businesses which contribute to the economic variety of the area to remain.</p>

Paragraph, etc	Type of change	Proposed change
		In the case of Hackney Wick Neighbourhood Centre, for example, reconfiguration of floorspace for employment uses (B1 and B2 Use Classes) compatible with the mixed-use development proposed will be acceptable. Sub Area 1 policies also provide additional guidance on where a floorspace <u>capacity</u> or job density approach will be applied.
Para 4.16	Minor	<b>4.17</b> Bullet point 5 (b) will apply for proposals relating to current B1 Use Class employment. As B1 Use Classes are generally compatible with mixed-use development, any re-development proposals of B1 <u>use class</u> floorspace should maintain equivalent B Use Class employment floorspace or significantly increase job density within the B Use Classes. <del>A job density approach will also be applied for proposals at Leyton Road North and the site at Eastway, Osbourne Road, which have been released from designation.</del>
Para 4.17	Minor	<p><b>4.18</b> Only where a convincing case for a loss of employment floorspace or density, including through conversion, is made through <del>Bullet points (c) to (h)</del> <u>Part 7</u> of this policy shall an exception be made. This should include:</p> <ul style="list-style-type: none"> <li>• Re-location strategies demonstrating no negative financial implications for existing businesses and potential for relocation to suitable premises (<u>by type, size, use and specification</u>) nearby at similar rates. <u>For Sub Area 1 additional guidance is provided in the Hackney Wick and Fish Island SPD.</u></li> <li>• Marketing strategy evidence demonstrating a lack of demand for all appropriate forms of employment uses and site configurations through marketing at <u>appropriate terms and a reasonable rate</u> for the area, within appropriate forums, for at least two years prior to the submission of the proposal</li> <li>• Viability appraisals assessing the suitability of location, quality, condition, character and function, and ability of a business to thrive under such circumstances; suitability of the premises for conversion to any employment use; the potential costs and configurations for improvements; and the ability to attract market rates for the area</li> <li>• The existence of other overriding factors which could potentially inhibit the ability to provide equivalent employment on the site in the future, such as building configuration or conversely the presence of premature lease-termination issues.</li> </ul>
Para 4.18	Minor	<b>4.19</b> New employment floorspace should be designed flexibly to maximise potential uses and take-up, <u>through provision of variable sizes, flexible and adaptable space, which are capable of meeting the needs of SME occupiers</u> including the way the units are accessed and managed. <u>When co-locating with residential proposals should pay particular regard to noise insulation issues</u>

Paragraph, etc	Type of change	Proposed change
		<p><u>to maximise the range of potential occupants.</u> Mixed-use developments should be designed to maximise the forms and types of employment uses which can be incorporated into the development, including how B1 and some forms of B2 Use Classes can be compatible with mixed-use development through good design, including vertical and horizontal integration. Where existing businesses are capable of taking up the space proposed through mixed-use redevelopment, temporary re-location strategies shall be sought as described in paragraph 4.178 to enable these businesses to remain within the area for the long term. <u>When designing flexible space within mixed use schemes consideration of the relationship between home-based work and dedicated workspace or potential for integrated employment and leisure offers may also be a factor</u> <small>[footnote to Work Live Study 2014 and CCOA, 2018].</small> On a case-by-case basis proposals requiring planning permission involving a change of use to B1 will be protected from future change to residential through conditions.</p>
Table 2	Major	Table 2 changes made below
Case Study 1	Minor	<p><b><u>Case Study 1- Hackney Wick and Fish Island Creative Enterprise Zone proposal</u></b></p> <p><u>In 2018 the London Legacy Corporation, London Borough of Hackney and London Borough of Tower Hamlets submitted a joint proposal for Creative Enterprise Zone status. At the time of writing the proposal has reached the final 10 it is yet not known whether it will be successful. However, an extensive amount of work has taken place to promote such a CEZ within the Hackney Wick and Fish Island area. Although not a planning initiative there are clear links between the two including a shared evidence base in the Combined Economy Study (2018).</u></p> <p><u>As a Creative Enterprise Zone, HWFI would benefit from an overarching economic strategy which is also supported by policies within the Local Plan, such as the emphasis on support for businesses within flourishing sectors on the economy (SP.2); protection of the current supply of a range of traditional manufacturing and heavier industries whilst encouraging the forms of appropriately located and designed workspace appropriate to new and existing creative, productive and cultural industries, enabling them to thrive within the area (B.1); and support for the provision of new affordable workspace and low cost business space secured at sub-market rates in particular where existing space is present (B.4). Policies supporting the new Neighbourhood Centre at Hackney Wick also acknowledge the unique circumstances of the area and the close relationship with employment space and a model of dispersal of these uses across the centre (B.2).</u></p>

Paragraph, etc	Type of change	Proposed change
		<p><u>In addition, the area benefits from various socio-economic programmes and investment, including business development programmes, employment and skills programmes and community capacity building programmes. These initiatives and emerging projects fall within the categories of new creative clusters and networks; creative production space; business development; enterprise and skills; and community links and socially inclusive spaces. Together these will create a single form of governance, provide new spaces, supply chain support, showcasing a cultural strategy, partnerships with schools and other training organisations and engagement with outside community and community representatives.</u></p>
B.2	Minor	<p>Main town centre uses shall be focused according to the scale, format and position in the retail hierarchy identified in Table 3. <u>In addition to the comparison floorspace requirements, Centres should contribute towards the identified need for convenience floorspace phased by 2036.</u> The identified function for each Centre will be protected by:</p> <ol style="list-style-type: none"> <li>1. Maintaining appropriate A1 retail presence and resisting potential harm from the concentration of other uses, in particular A2 <u>some sui generis</u> and A5 uses.....</li> <li>2. Maintaining active retail frontages</li> <li>3. The sequential assessment of sites for main town centre uses <u>and subject to paragraph (1) of this policy, providing support for existing and proposed cultural and night time economy uses</u></li> <li>4. Requiring a retail and leisure impacts assessment where a retail or leisure use is proposed of more than 2,500 sqm outside the Metropolitan Centre boundary and 200 sqm outside other Centres</li> <li>5. Allowing edge-of-centre development supporting cultural, sporting and visitor growth associated at the Metropolitan Centre, subject to (3) above</li> <li>6. <del>Allowing</del> <u>Promoting complementary residential development in all Centres to optimise housing delivery.</u></li> </ol>
Para 4.20	Minor	<p><b>4.21.....</b>The Metropolitan Centre will provide for a range of London-wide retail and leisure requirements, <u>including a focus on the night time economy.</u> <del>whereas the</del> The District, Neighbourhood and Local Centres will provide a range of small-scale uses to <u>overall meet the varied</u> local day-to-day</p>



Paragraph, etc	Type of change	Proposed change
		requirements, <u>with each Centre having a level of specialism and function set out within Table 3. Table 3 also sets out the role and function for each Centre as they develop, including the type and size of units to be directed to each Centre</u> <del>The type and size of units will be directed to these Centres as shown within Table 3.</del>
Para 4.21	Minor	<del>4.22</del> <del>Once redevelopment occurs and uses are established, a future review of this Local Plan or its successors may highlight primary and secondary retail frontages around the core and fringes of the retail offer.</del> As Westfield Stratford City has a format that does not enable primary or secondary frontage definition, <del>no such designations for primary or secondary retail frontages are included for Stratford Metropolitan Centre within this Local Plan.</del> However, Westfield Stratford City, and parts of other centres capable of designation as primary or secondary frontages in the future, will be considered key shopping areas. The Centre hierarchy is identified within Table 3 and boundaries of the Centres are shown on the Policies Map, where established. <u>The Policies Map also shows a location for future expansion of the Metropolitan Centre boundary to facilitate the potential for International Centre designation.</u>
New Para	Major	<del>4.23</del> <u>As the East Village Centre has developed significantly in recent years all non-residential floorspace within the Town Centre boundary will be designated as Primary Frontage (also shown on Figure X below). The Hackney Wick Neighbourhood Centre boundary is shown on the Policies Map, however once the redevelopment is more progressed a future successor of this Local Plan will draw appropriate Primary and Secondary frontages. As the Centres at Bromley-by-Bow and Pudding Mill emerge more closely defined town centre boundaries and frontages will also be able to be drawn.</u>
Para 4.22	Minor	<del>4.24</del> <u>The ability of the Centres to perform their primary retail function will be strengthened by provision of a wide range of retail provision, including provision of convenience floorspace. Retail floorspace should be flexible and adaptable and be designed to enable the occupation by various industries, including that within the night time economy, and consider noise issues through the Agent of Change principle <a href="#">[see Culture and Creative Opportunities Assessment, 2018 for typologies of space]</a>. It should also include the provision of smaller and larger units within Centres. The function of the Centres should not be compromised by over-concentration in number and position of non-A1 uses. Particular threats can be posed from A2 Use Classes and betting shops, which can also negatively impact upon the appeal of a centre, and uses contributing to the night time economy (A3–A5; D2).</u>

Paragraph, etc	Type of change	Proposed change
Para 4.23	Minor	<p><b>4.25</b> <u>As well as setting out the scale, format and position in the hierarchy of each Centre, Table 3 also sets out the role and function, identifying where there should be a focus on culture and the night time economy. The Cultural and Creative Opportunities Assessment has highlighted particular opportunities at Stratford and Hackney Wick. In accordance with the Town Centres First Principle, cultural and night-time economy uses should be directed towards the Centres. Further support for such industries will be provided by the provision of flexible, well-designed and adaptable space suitable for a variety of occupiers including those serving the night time economy; through requiring appropriate and sustainable management measures; through appropriate interim uses (see Policy B.3) and the application of the Agent of Change Principle to protect the interests of existing operators as well as new communities (see Policy BN.12). In addition, across the whole of the area public houses of cultural or heritage value will be protected and entertainment venues providing a clear community-based, specialised function will be maintained or re-provided through Policy CI.1. The Night-time Economy SPD will provide further guidance.</u></p>
New para	Major	<p><b>4.26</b> <u>Food and drink uses (A3, A4 and A5) support the day-time retail and leisure function of the Centres and contribute towards the night time economy by bringing <del>can bring</del> vitality for longer hours of the day; however, takeaway facilities (A5) should be managed to minimise health implications, noise and disturbance, which will include siting such uses more than 400m walking distance from existing schools and schools proposed within this Local Plan; and through other appropriate management measures. The extended hours of A1 convenience stores can also serve the night time economy, providing an alternative to A5 provision, contributing towards the health objectives of this Local Plan.</u></p>
New para	Major	<p><b>4.27</b> <u>Where a change of use or a new non-A1 use is proposed within the Centres, the concentration of the proposed use will be considered against the role and function of the Centre identified in Table 3, as well as the quantum, cumulative impacts and positioning of the existing A1 retail provision and the potential for the new use to enhance, rather than undermine, the function. Residential development shall be appropriate within all Centres. Residential densities should reflect the transport accessibility and central locations. The availability of community facilities and open space should be considered in relation to housing mix. Residential should be located away from any defined primary frontages and further guidance on integration of residential into the town centres are provided within the three area-based SPDs (Bromley-by-Bow;</u></p>

Paragraph, etc	Type of change	Proposed change
		<del>Hackney Wick and Fish Island; Pudding Mill). except on the ground floor, unless the quantum proposed, individually or cumulatively, will prejudice the function of the Centre.</del>
Para 4.25	Minor	<b>4.29</b> Where not allocated, edge-of-centre proposals will be subject to sequential and impacts assessments. Appropriate proposals for the edge of the Metropolitan Centre may be large-scale cultural, leisure or visitor attractions and other D Use Class town centre uses which contribute towards the aims of the Legacy Corporation as a whole, including those which combine to generate a strong cultural, tourist and visitor experience. <u>It is envisaged that the introduction of cultural and education uses will contribute towards Stratford becoming a centre of International significance, and this location is shown on the Policies Map as a future location for extension of the town centre boundary. Policy 3.1 also sets out the nature of appropriate edge-of Metropolitan Centre development. Policy B.6 will be used to assess edge-of-centre development for higher education, research and development.</u>
Table 3	Major	Table 3 changes made below
Page 32 image	Minor	Amend to show the location of the future potential extension of the town centre boundary and to show the northern part of the Stratford High Street Policy Area
Policy B.3	Minor	Proposals for temporary interim uses will be supported where: 1. Land has been set aside for development in the longer term and the proposed interim uses will <u>contribute towards housing requirements, or reinforce the long-term, leisure, cultural, <u>night time economy</u> or event-based uses;</u>
Para 4.27	Minor	<b>4.31</b> ...For these reasons, interim uses shall be supported <u>in particular where they create vitality and viability to streets, are developed in partnership with the community, create or improve public realm and create active frontages, as well as 'green' proposals such as community allotments and gardens.</u>
Para 4.28	Minor	<b>4.32</b> The Legacy Corporation shall encourage new commercial units to be designed to be flexible to interim uses, including formats suitable for start-up businesses and the creative and cultural sector. <u>Affordable housing and other innovative forms of housing will be encouraged as interim uses.</u> Where appropriate, it will utilise Section 106 Agreements to prevent units being left as 'shell and core' and mitigate potential impacts of the development.
Figure 7	Minor	Amend to reflect show the future extension of the Metropolitan Centre boundary
Case Study 1	Minor	<del><b>Case Study 1: Interim uses as skate park at Frontside Gardens, Hackney Wick</b></del>  <del>This case study demonstrates how derelict land has been used for temporary community uses. Land owned by the Legacy Corporation left by a demolished warehouse has been used for</del>

Paragraph, etc	Type of change	Proposed change
		<p>a temporary skate park, which has now established itself as a popular destination for local skateboarders. It provides an example of how other unused spaces awaiting redevelopment can help meet the wider aims of community cohesion and convergence.</p> <p><b><u>Case Study 2: Clarnico Quay</u></b></p> <p><u>This case study relates to an approved scheme for a variety of interim uses on a future development plot at Sweetwater for seven years. This will include workshops, studios and maker space; shops, cafes and bars; event, community space and meeting rooms; and pop up shops and market stalls. The proposals also include a mobile garden, and associated landscape and cycle parking.</u></p>
B.4	Minor	<p><b>Policy B.4 Providing low-cost <u>business space</u>, <u>affordable</u> and <u>managed workspace</u></b></p> <p>Existing managed <u>affordable workspace</u> or <del>and</del> <u>low-cost business space workspace</u> shall be retained, <u>or re-provided where viable and where it complements wider plans for the area in accordance with Policy B.1.</u></p> <p>New managed <u>affordable workspace</u> and/or <u>low-cost business space workspace</u> will be encouraged where it:</p> <ol style="list-style-type: none"> <li>1. Is flexible and able to meet the needs of various end users within B Use Classes;</li> <li>2. Includes an appropriate management scheme secured through Section 106 Agreements; and</li> <li>3. <u>Re-provides existing low-cost business space or affordable workspace in accordance with Policy B.1 and it does not result in a net loss of employment.....</u></li> </ol> <p><u>Affordable or low-cost provision will be supported and secured through Section 106 where:</u></p> <ol style="list-style-type: none"> <li>4. Rents are up to 75 per cent of historic market rent for the previous year for the equivalent floorspace in the same area for an equivalent B Class Use;</li> <li>5. It is secured at the current market rate for cultural or creative purposes;</li> <li>6. It is subsidised to reduce the cost to the user for charitable purposes; or</li> <li>7. It establishes robust management links with a registered workspace provider <u>within the relevant borough.</u></li> </ol>
Para 4.30	Minor	<p><b>4.34</b> Managed workspace would normally comprise a number of business units or workspaces for independent individuals or</p>

Paragraph, etc	Type of change	Proposed change
		<p>small businesses, which together are communally managed and provided with a range of shared support services and facilities. <del>Low-cost</del> <u>Affordable</u> workspace can be managed workspace, studio or unsupported independent business space that is made available to tenants below the prevailing market rent for that type of space, the current rate for the cultural or creative use or subsidised at a lower user-cost. <u>Low cost business space refers to workspace which normally is of a lower specification which is reflected in the costs.</u> The Legacy Corporation will support the maintenance of such workspaces where possible, and in accordance with relevant Sub Area policies.</p>
Para 4.31	Minor	<p><b>4.35</b> In locations where a clear demand exists <u>such as within a potential Creative Enterprise Zone and within larger B1 use class developments of more than 2,500sqm and where a degree of flexibility of uses is secured</u>, proposals for new managed, <u>affordable or low-cost business space workspace</u> will be considered favourably, particularly as part of mixed-use development. Provision should be clustered into small groups to ensure agglomeration benefits and potential for lettings are maximised. Scheme viability should be based upon delivery within the initial phases of larger schemes. Links with registered workspace providers <u>within the relevant borough</u> will also be supported. In order to ensure that new space is appropriately managed for the long term, proposals should be accompanied by a Management Scheme. Proposals <u>re-providing replacing</u> existing employment floorspace with managed, <u>affordable or low-cost business space workspace</u> should re-provide <u>suitable equivalent floorspace or job density</u>, subject to Policy B.1 and be secured for the future through Section 106.</p>
Para 4.32	Minor	<p><b>4.36</b> The quantum, mix of unit sizes and scheme of rent levels for <u>affordable and low-cost business space workspace</u> will be assessed in the light of overall scheme viability. <u>As guidance, the Legacy Corporation will be able to provide monitoring information on appropriate rates achieved from other similar schemes as well as guidance on the, relevant local area and distance from the site, and the inclusion of other rates and charges.</u> Use of sliding scales will be supported where it can ensure a transition to market level as the business matures and overall scheme viability changes.</p>
Case Study 2	Minor	<p><del><b>Case Study 2: Low-cost workspace and local labour agreements secured at Neptune Wharf, Fish Island</b></del></p> <p>This case study demonstrates how low-cost workspace can be sought through development proposals. In this case approximately 500 sqm of B1 low-cost workspace was secured. The affordability of the units was determined at 75% of Historic Market Rent for the previous year throughout Fish Island and the units have been secured for at least ten years with no</p>

Paragraph, etc	Type of change	Proposed change
		<p>upward rent review for the first five years. Other detailed requirements of the permission include:</p> <ul style="list-style-type: none"> <li>• Submission of annual reports for the ten-year period evidencing compliance with affordability regime, financial terms of the leases and how relevant rental levels were determined</li> <li>• A Workspace Strategy showing how the space is to be designed flexibly to meet the needs of users</li> <li>• Advertising job vacancies locally in Local Labour and Business Schemes and Job Centres</li> <li>• Recruiting from within the Tower Hamlets area for 20% of construction jobs and between 25% and 85% of end-user employment</li> <li>• Paying employees the living wage, promoting for end users and apprenticeships</li> <li>• Demonstrating actions seeking local business occupation of units.</li> </ul> <p><b><u>Case Study 3: Duncan House Affordable Workspace</u></b></p> <p><u>This case study demonstrates how affordable workspace can be sought through development proposals. In this case approximately 634 sqm of B1 use class artists workspace was secured as affordable workspace. The affordability of the units was determined at 60% of market rate at £6psf (exclusive of service charges). The units have been secured for at least five years.</u></p>
B.5	No change	No change to policy itself.
Para 4.35	Minor	<p><b>4.39</b> A key element will be working with the Growth Boroughs and other partners through employment training initiatives and apprenticeships to promote jobs, skills and employment training. Where appropriate, the planning system will be used to secure targets and commitments in relation to associated job and training opportunities, both for construction-related employment and training that increases access to long-term employment. <u>The Legacy Corporation will seek to work with the boroughs and other partners to seek to ensure that apprenticeships and training can be completed; that they maximise potential take-up; and that they seek to increase representation in the construction industry of currently under-represented groups.</u> Rather than setting specific targets in policy, targets or commitments will be maximised on a case-by-case basis, taking into account the size and nature of the scheme proposed and, where relevant, scheme viability.</p>
B.6	No change	
Evidence base references	Update	<p><u>Artists' Workspace Study (2014, updated)</u>  <u>Work Live Study (2014)</u></p>

Paragraph, etc	Type of change	Proposed change
		<u>Combined Economy Study Part A1: Economy Study and Employment Land Review (London Legacy Development Corporation, 2018)</u> <u>Combined Economy Study Part A2: Business Survey (London Legacy Development Corporation, 2018)</u> <u>Combined Economy Study Part B: Creative and Cultural Opportunities Assessment (London Legacy Development Corporation, 2018)</u> <u>Combined Economy Study Part C: Retail and Town Centre Needs Study (London Legacy Development Corporation, 2018)</u> <u>Employment Space Study (2015)</u>

**Table 1- Direct jobs from proposals**

DEVELOPMENT	GROSS DIRECT JOBS (TOTAL), 2031	SECTORS
Here East	7,500 including 5,300 on site	IT, <u>technology, creative and cultural industries, info and communications</u> , finance, real estate, professional, admin and support, education, health, arts/entertainment, wholesale and retail, transport, accommodation, other services
Queen Elizabeth Olympic Park – Legacy Communities Scheme	Approximately 3,000	Business, office, wholesale and retail, transport, accommodation and food, broadcasting and communications, admin and support, arts/ entertainment, other services
The International Quarter	26,200	Office, business, professional services, admin and support, wholesale and retail, arts/entertainment
Strand East (Sugar House Lane)	2,450	Business, office, retail, financial and professional services, food, leisure
Westfield Stratford City	10,000	Retail, food, hotel, leisure and entertainment, office and professional services, admin and support
Chobham Farm	403	Retail and business
<u>Stratford Waterfront (UCL East and East Bank)</u>	<u>Approximately 5,000</u>	<u>Academic institution and commercial research space, student accommodation and retail, cultural and education institutions</u>

Table 2, Employment clusters

REFERENCE	EMPLOYMENT CLUSTERS	CLUSTER FUNCTION
B.1a1	<p><b>Here East (Hackney Wick)</b> Strategic Industrial Location- (<del>Industrial Business Park</del>) <u>Strategic Technology Cluster</u></p>	<p>A range of complementary employment uses within B1 and B8 Use Classes, <u>D1 and further education uses of regional significance</u>, including <u>creative and technology-based industries, light industrial industry, offices, research and development, media, broadcasting and production uses, culture/arts and smaller workshops</u>. Also including supporting uses of <u>further and higher education and conference facilities within D2 Use Classes</u>, and small-scale <u>subsidiary retail and leisure</u>.</p> <p><u>Only Development will be supported which complements support the media, education, technological and creative functions including light industrial; storage and distribution; flexible B1c/B2/B8 use class floorspace and/or small-scale subsidiary retail, leisure or other ‘walk to’ services will be supported. Opportunities for intensification and redevelopment of under-utilised areas for activities falling within the supported identified use classes will be supported.</u></p>
B.1a2	<p><b>Fish Island South including Bow Midland West Rail Site</b> Strategic Industrial Location (<del>Preferred Industrial Location</del>)</p>	<p>A range of significant B2 and B8 Use Classes of industrial, warehousing, transport, waste management and distribution. A safeguarded rail head and associated bulk freight distribution use. Uses should make effective use of the railhead, including potential for aggregate distribution and for concrete batching, the manufacture of coated materials, other concrete products and handling, processing and distribution of or aggregate material. Only small-scale supporting ancillary uses will be supported. <u>Potential for intensification of the floorspace capacity of existing industrial uses through modernisation of facilities, development of multi-storey schemes and more efficient use of land through increased plot ratios. Only where new industrial uses providing consolidated and intensive, high quality and sustainable facilities minimising the environmental, visual and amenity impacts of the site are provided will other new uses be supported (see Site Allocation 4.5).</u></p>
B.1a3	<p><b>Bow Goods Yard East</b> Strategic Industrial Location (<del>Preferred Industrial Location</del>)</p>	<p>A safeguarded rail head and associated bulk freight distribution use. B2, B8 and waste management uses are appropriate. Only development supporting the rail-related and small-scale ancillary uses will be supported. <u>Potential for intensification of the floorspace capacity of existing industrial uses through modernisation of facilities, development of multi-storey schemes and more efficient</u></p>



		<u>use of land through increased plot ratios. Only where new industrial uses providing consolidated and intensive, high quality and sustainable facilities minimising the environmental, visual and amenity impacts of the site are provided will the introduction of other new uses be supported (see Site Allocation 4.5).</u>
B.1b1	<b>Site at junction of Lee Conservancy Road and Eastway</b> Locally Significant Industrial Site	B Use Class industrial use and suitable for a future industrial, storage and distribution or transport-related use. <u>Potential for intensification of the floorspace capacity of existing industrial uses through modernisation of facilities and more efficient use of land through increased plot ratios.</u>
B.1b2	<b>Trafalgar Mews, Eastway</b> Locally Significant Industrial Site	Mixed industrial and business use and transport associated use. <u>Potential for intensification of the floorspace capacity of existing industrial uses through modernisation of facilities and more efficient use of land through increased plot ratios.</u>
B.1b3	<b>Site at Chapman Road</b> Locally Significant Industrial Site	A mix of small-scale industrial, storage and distribution uses with supporting retail. <u>Potential for intensification of the floorspace capacity of existing industrial uses through modernisation of facilities and more efficient use of land through increased plot ratios.</u>
B.1b4	<b>Bartrip Street North</b> Locally Significant Industrial Site	A mix of small-scale industrial, storage and distribution uses. <u>Potential for intensification of the floorspace capacity of existing industrial uses through modernisation of facilities and more efficient use of land through increased plot ratios. Proposals involving intensification and co-location or release will be appropriate where it helps facilitate the delivery of the Bartrip Street South (SA1.9) allocation.</u>
B.1b5	<b>Wick Lane and Crown Close, Fish Island</b> Other Industrial Location	An employment-led mix of uses, including warehouse, storage, distribution, with some potential for residential development and live work in appropriate locations, subject to Bullet point 6 and 7 5. <u>Potential for redevelopment to re-provide existing industrial floorspace and intensify the floorspace capacity through more efficient use of land and increased plot ratios, facilitating the co-location with residential across the whole of the designation. An appropriate and gradual transition between nearby uses of residential and industrial.</u>
B.1b6	<b>Cooks Road</b> Other Industrial Location	Land within B1c/B2/B8 Use Classes. Land between Cooks Road and River Lea, redevelopment opportunity with a significant proportion of employment use providing floorspace within a range of use B1–B8 Uses Classes alongside other uses, with an element of residential, providing a transition to the lower employment mix of uses within the remainder of Pudding Mill. <u>Potential for redevelopment to make more efficient use of land including re-provision of intensive industrial floorspace at northern part of the designation, and to intensify the floorspace capacity through increased plot ratios facilitating the co-</u>

		<u>location with residential within the remainder of the designation.</u>
B.1b7	<b>Sugar House Lane/ Stratford High Street</b> Locally Significant Industrial Site	Land within B2/B8 Use Classes. Area at the northern end of the Strand East site, partly fronting Stratford High Street, with an existing planning permission for a cluster of development for a mix of predominantly office, workshop, retail, hotel and associated business and employment-generating uses. <u>The introduction of new industrial uses will provide intensive, modern and flexible accommodation.</u>
B.1b8	<b>Rick Roberts Way North</b> Locally Significant Industrial Site	A cluster of existing high-quality industrial design and manufacturing uses of B2 and B8 Use Classes in modern buildings. <u>Potential for intensification of the floorspace capacity of existing industrial uses through development of multi-storey schemes and more efficient use of land through increased plot ratios.</u>
B.1b9	<b>Temple Mills Lane</b> Locally Significant Industrial Site	Transport uses appropriate to or subsidiary to current use as bus depot.

**Table 3: Retail centre hierarchy**

NAME AND TYPE	DESCRIPTION AND FUNCTION	RETAIL QUANTUM
<b>Stratford Metropolitan (including existing town centre)</b> See Allocation SA3.1	<ul style="list-style-type: none"> <li>• <u>Serving London- and regional wide catchment</u></li> <li>• <u>Large and small scale retail (A1-A5) and D1 commercial leisure Significant retail floorspace within varied sizes, providing for comparison, convenience and service functions (A1-A2)</u></li> <li>• <u>Significant Grade A B1 use class office space to support the potential Central Activities Zone (CAZ) reserve</u></li> <li>• <u>Food and drink and leisure uses providing a night-time economy A focus on the day to night time economy, providing a wide range of food and drink (A3-A5); leisure; cultural and visitor attractions (D1, D2)</u></li> <li>• <u>Subsidiary Residential development to be optimised and well-integrated into the Centre, focused around the transport hubs and other attractors</u></li> <li>• <u>Maximised reuse of buildings of heritage value and protection of public houses of cultural or heritage value</u></li> <li>• <u>Entertainment venues providing clear community-based function or speciality will be maintained or re-provided</u></li> </ul>	Provision of approximately <del>64,000</del> <u>55,000</u> sqm (net sales area) of additional comparison floorspace across the whole of the Metropolitan Centre to <u>2036</u> <del>2030, focused to the centre-east.</del> <u>Contribution towards the area-wide convenience floorspace requirement by 2036.</u>

	<ul style="list-style-type: none"> <li>• <u>Development to facilitate the potential future expansion of the Metropolitan Centre boundary to include Stratford Waterfront</u></li> </ul>	
<p><b>Bromley-by-Bow Potential District</b> See Policy 4.1 and Allocation SA4.1</p>	<ul style="list-style-type: none"> <li>• <u>Enhancement/reconfiguration of the existing superstore Re-provision of large-scale convenience floorspace</u></li> <li>• <u>Small-scale A1–A5 retail floorspace providing for comparison, convenience and service functions (A1-A2)</u></li> <li>• <u>Small-scale, food and drink (A3-A5) leisure and community uses which also contribute towards the emergence of the night-time economy within the area</u></li> <li>• <u>B Use Class employment and business space in a range of sizes</u></li> <li>• <u>Residential development is to be delivered at densities appropriate to the location across the whole of the Bromley-by-Bow site allocation</u></li> <li>• <u>Serving a local catchment, accessed via public transport, walking and cycling</u></li> </ul>	<p>Total retail, <u>leisure and service floorspace, including existing</u> of between 10,000 and 50,000 sqm. Approximately <u>8,000 6,200-sqm</u> (net sales area) comparison retail floorspace <u>to 2036, and re-provision of existing convenience floorspace. Remodelling/enhancement of superstore</u></p>
<p><b>Hackney Wick Neighbourhood</b> See Allocation SA1.1</p>	<ul style="list-style-type: none"> <li>• <u>A mix of small-scale retail (A1-A2), leisure and community uses, flexible and adaptable for a range of different uses and compatible with a range of different uses</u></li> <li>• <u>A significant contribution to the day to night time economy of the area through a varied mix of food and drink (A3-A5), cultural, community and creative offers, providing vitality over longer periods of the day</u></li> <li>• <u>Employment uses in a range of sizes, flexible and adaptable and compatible with mixed-use development including offices and workshops and, in some cases, industrial uses</u></li> <li>• <u>Non-residential uses dispersed throughout the centre alongside residential development which should be optimised</u></li> <li>• <u>Active ground-floor uses and frontages, considering flooding issues</u></li> <li>• <u>Maximised reuse of buildings of heritage value and protection of public houses of cultural or heritage value</u></li> <li>• <u>Entertainment venues providing clear community-based function or speciality will be maintained or re-provided</u></li> <li>• <u>Serving a localised catchment</u></li> </ul>	<p>Total retail, <u>leisure and service floorspace, including existing</u> up to 10,000 sqm <u>including the provision of convenience floorspace</u></p>

<p><b>Pudding Mill</b></p> <p><b>Local</b></p> <p>See Allocation SA4.3</p>	<ul style="list-style-type: none"> <li>• Small-scale retail (A1-A5) and office/workshop/research and development uses</li> <li>• Serving a localised catchment</li> <li>• Residential development to be delivered at densities appropriate to the accessibility levels across the site</li> </ul>	<p>Total retail, leisure and service floorspace should not exceed 10,000 sqm. About 2,500 sqm retail permitted including the provision of convenience floorspace</p>
<p><b>East Village</b></p> <p><b>Local</b></p> <p>See Policy 2.4</p>	<ul style="list-style-type: none"> <li>• Small-scale retail (A1-A5) and office/workshop uses</li> <li>• Serving a localised catchment</li> <li>• Residential development on upper storeys across the site</li> </ul>	<p>Total retail, leisure and service floorspace, including existing, should not exceed 10,000 sqm, including the provision of convenience floorspace</p>

*Footnote- Focused to the eastern part of Stratford Metropolitan Centre (as extended) within the London Borough of Newham's administrative area for planning purposes. There is limited capacity for new comparison goods floorspace between 2018 and 2021 after allowing for all known commitments, and capacity emerges by 2026. At Stratford Metropolitan Centre this amounts to c16,000 sqm net at 2026; 40,000 sqm net by 2031 and 64,000 sqm net by 2036. The requirement at Bromley-by-Bow amounts to c2,000 sqm net at 2026; 5,000 sqm net by 2031 and 8,000 sqm net by 2036. The remainder of the phased requirement of 8,000 sqm net to 2036 is expected to be provided primarily at Hackney Wick and Pudding Mill. The floorspace figure over whole plan period is indicative, and is not considered to be a cap, due to the short-term validity of the information, a future review of retail requirements will be required from 2023 onwards with a confirmed requirement of 14,000 sqm to 2021 and with the requirement from 2021 to 2030 subject to review before 2021. Proposals for significant new retail floorspace capacity to be provided in advance of the identified requirements will be required to submit detailed Retail Impacts Assessments.*

## Section 5, Providing housing and neighbourhoods

Policy, para.	Type of change	Proposed change
Para 5.1	Minor	..... <u>Considerable progress has already been made to achieving these goals.</u> It is expected that <u>by the end of 2019 about 11,000</u> <del>24,000</del> homes will have been built within the Legacy Corporation area. <del>by 2031</del> <u>With future planned development this figure is expected to reach 33,000 by 2036.</u>
Objective 2	Minor	Delivering more than <del>24,000</del> <u>22,000</u> new homes <u>between 2020 and 2036</u> within a range of sizes, <u>types</u> and tenures  The delivery of at least <del>four</del> <u>two</u> new primary schools <del>and one new secondary school.</del>
SP.2 (1)	Major	The Legacy Corporation will work with its partners to maximise opportunities for delivering high-quality, sustainable and affordable homes <u>within a variety of types and tenures</u> and provision of supporting infrastructure through:  1. Delivering in excess of the <u>Draft New London Plan target of 1,471</u> <del>2,161</del> housing units per annum <u>through</u> , <del>of which a minimum of 455 will be affordable</del> <u>optimising housing delivery on suitable and available sites</u>  2. <u>Maximising affordable housing delivery through a 35% target on a habitable room basis, with a 50% target on public sector land (see Draft New London Plan Policy H6)</u>
SP.2 (2)-(4)	Major	3. Providing for <u>a full range of identified size, accommodation and tenure requirements, particularly including family housing in all tenures, Providing specialist housing and specific housing products which contribute towards the overall housing mix and meet identified requirements</u>
Para 5.2	Minor	The Legacy Corporation has an annual housing delivery target, set out within the London Plan. This has been developed by the Greater London Authority (GLA), on the basis of its Strategic Housing Land Availability Assessment, <del>2013</del> <u>2017</u> , and robust assessment of housing needs within its Strategic Housing Market Assessment, <del>2013</del> <u>2017</u> . The Legacy Corporation fully supports this housing delivery target and will seek to achieve and exceed this through the application of this policy.
Para 5.3	Minor	This target will be achieved through a range of sources, including large identified sites, non-self-contained accommodation (including <del>hostels and student accommodation,</del> <u>shared living</u> ), <del>an annualised through delivery on small sites and through conversions potential of 33 units</del> <u>18</u> and reuse of long-term vacant properties. <u>Where appropriate, the Legacy Corporation will also support innovative means of site-assembly to support housing delivery, including for example, longer-term over-station development.</u> <u>Where</u>

Policy, para.	Type of change	Proposed change
		<p>appropriate, <u>custom/self-build opportunities shall also contribute towards the housing supply where all Local Plan requirements are met and the site is optimised for housing delivery.</u> Figure 9, the housing trajectory, shows the ability to deliver housing against the housing target over the Plan period. <del>It shows that within the last five years delivery is less certain; however, London Plan targets will be reviewed by 2019/2020.</del> <u>The trajectory includes a five per cent buffer of deliverable sites which is expected to will be met for the first five years, but it may not be possible on a rolling five-year basis past 2028/2029.</u> <del>The London Plan recognises the difficulty of this approach.<sup>19</sup> Nonetheless, it is expected that the cumulative housing target is expected to be exceeded, with more than 24,000</del> <u>22,000 homes will be delivered over the Plan period of 2020 to 2036 through optimised housing delivery on suitable, available and achievable sites over the period.</u> <del>the creation of additional capacity and greater delivery on small sites than anticipated within the London SHLAA.</del></p>
Para 5.3	Major	<p><b>5.4</b> <del>The Legacy Corporation will monitor and keep under review progress in seeking to achieve and where possible exceed the housing target, in particular against potential sources contained within each part of the draft New London Plan target (i.e. small, large sites and non-self-contained) Policy 3.3. Where relevant it will introduce</del> <u>introducing measures to enhance delivery on all applicable sites, including optimising delivery on sites within the Legacy Corporation's ownership through the portfolio-based approach, update updating evidence and design codes, investigate investigating capacity requirements or amend amending targets where required. The quantum and timescale of development are subject to change. The Legacy Corporation will also work with the boroughs through the Duty to Cooperate to develop and devise a joined-up strategic approach to housing delivery and in particular to meet delivery targets towards the latter part of the plan period where, as shown within the housing trajectory within Figure 9 below, delivery becomes less certain. Where relevant these discussions should also cover the transition arrangements or the transfer of those powers to a future relevant body.</u> <del>The trajectory and the list of key sites available in Appendix 2 will be kept under review within the Authority Monitoring Report (AMR), with delivery rates reflected within the rolling five year target.</del></p>
Para 5.4	Minor	<p><b>5.5</b> <del>The London Plan Policy 3.11 allows flexibility in setting the affordable housing target in terms of how this is calculated and demonstrated; the Legacy Corporation has set the target in absolute terms, which has been calculated by removing non-self-contained accommodation<sup>20</sup> from the housing target and apportioning the remainder by using the 35 per cent benchmark (see Policy H.2), i.e. 1,471 – 171 × 35% = 455.<sup>21</sup> This should be achieved across the area, and will be subject to review to reflect changing market conditions. The draft New London Plan (2017) sets out an affordable housing threshold of 35 per cent affordable homes across London, including</del></p>

Policy, para.	Type of change	Proposed change
		<p><u>50 per cent on public sector land, and industrial land where there is a net loss in industrial floorspace capacity. It also sets out the requirement for an equivalent of 35 per cent affordable delivery on non-self-contained residential accommodation therefore monitoring will be based upon the proportion of affordable housing achieved over the monitoring year, rather than against an absolute figure. For the avoidance of doubt, in accordance with draft New London Plan policy H13 and the Affordable Housing and Viability SPG this applies to Build to Rent tenures as well as traditional, build for sale. Policies H.1 and H.2 provide further detail in relation to housing mix and affordable housing requirements. Delivery will be reported annually in the AMR.</u></p>
Para 5.5	Minor	<p><b>5.6</b> Protecting existing residential stock is also a key component of mixed and balanced communities. Loss of residential units, including affordable housing, floorspace or land <u>essential to housing delivery</u> will be resisted. Self-contained units or floorspace will be protected or re-provided unless unacceptable amenity issues are present. Residential land will only be released where an equivalent number of residential units or floorspace is re-provided across all applicable sites.</p>
Figure 9	Minor	<p>New trajectory.</p>
H.1 Housing mix	Major	<p><u>H.1: Providing for and diversifying the housing mix a mix of housing types</u></p> <p><u>The Legacy Corporation will seek to diversify the range of housing provision by securing an appropriate mix of housing and accommodation types to meet identified requirements. It will promote and diversify delivery on a range of different site types including through small sites and conversions by utilising tools such as the Characterisation Study, the Brownfield Register and PTAL mapping to identify potential locations for yielding additional housing capacity. This information will be kept up to date within the Authority Monitoring Report.</u></p> <p><u>All residential Residential proposals, including schemes utilising Built to Rent models should:</u></p> <ol style="list-style-type: none"> <li>1. Meet identified local and strategic requirements, containing a mix of one-, two- and three-bedroom units <del>and larger</del>, with units of two bedrooms and more constituting more than half the total;</li> <li>2. Integrate a mix of unit <u>and tenure</u> types including flatted developments, maisonettes and family <del>houses</del> <u>housing</u> into the design;</li> <li>3. <del>Have no unacceptable adverse impacts on</del> <u>Promote the creation of mixed and inclusive communities the mix and balance of the proposed area;</u> and</li> <li>4. Meet London Plan and applicable Housing SPG design considerations, subject to Policy BN.4.</li> </ol> <p><u>Schemes on small sites and conversions will be supported where:</u></p>

Policy, para.	Type of change	Proposed change
		<p>5. <u>the site is well-designed to optimise housing delivery;</u>  6. <u>the proposal is in accordance with all relevant policies of the Local Plan, including loss of open space, social infrastructure and employment floorspace capacity; and</u>  7. <u>there is no unacceptable loss of amenity.</u></p> <p><u>Where Built to Rent and other similar rental products are proposed, accommodation will be required to utilise unified ownership and management structures; include mechanisms to secure the accommodation as such for the long term, with appropriate clawback procedures, including options for long-term secured tenancies.</u></p>
Para 5.7	Minor	<p><del>5.8 Providing for a range of different forms of residential accommodation by dwelling sizes and types size, form, tenure and typology is essential to create sustainable new mixed and inclusive communities and meet identified housing requirements. neighbourhoods and to avoid problems which may arise from over-concentration of certain sizes and types of accommodation. For this reason, a range of sizes to meet identified requirements and different configurations of accommodation will be sought. The principal aim is to achieve mixed and balanced communities. Build to Rent accommodation is playing an increasing role providing additional assurances to that normally found within the private rented sector through flexible tenancies, greater certainty over rent rises and a managed approach to the whole development. In combination, this approach should help enable residents to remain in the area for the longer term, contributing to community cohesion.</del></p>
New para	Major	<p><del>5.9 The draft New London Plan emphasises the potential contribution of small sites towards overall housing delivery, however due to the constrained nature of the area the contribution from small sites and conversions is likely to be comparatively small. Nonetheless measures are being introduced to boost delivery from these sources. Should small sites delivery fall below the anticipated 80 units per annum from year 6 onwards (2025) the Legacy Corporation or responsible body should consider the use of more prescriptive design codes setting out what forms of small site delivery may be appropriate.</del></p>
Para 5.8	Major	<p><del>5.10 Evidence The Housing Requirements Study (2018) suggests that there is a particularly high local requirement for the following types of housing size: two bedroom market homes and, within the affordable sector, for two and three bedroom homes. The Greater London Authority SHMA (2017) identifies a greater strategic need for low cost rented properties, in particular for one-bedroom properties.</del></p> <ul style="list-style-type: none"> <li><del>• One bedroom properties within market and affordable/social rented</del></li> <li><del>• Two bedroom properties in market housing</del></li> <li><del>• Larger units (of three bedrooms or more) particularly within affordable/social rented and low-cost home ownership sectors.</del></li> </ul>



Policy, para.	Type of change	Proposed change
Para 5.9	Major	<b>5.11</b> All proposals should reflect these identified size, form and tenure requirements, providing an appropriate balanced mix of one-, two- and three-bedroom units, <del>including within affordable tenures.</del> <u>Provision of low cost rented units should provide an equal mix of one, two and three-bedroom properties.</u> All proposals should contain more two-bedroom-plus units than one-bedroom units, and should not avoid the provision of any single size or tenure. When considering the detailed mix of dwelling sizes, the Legacy Corporation will consider individual site circumstances, including location, viability and the <del>maintenance</del> <u>promotion</u> of mixed and <del>inclusive balanced</del> communities. Proposals which cluster units of a particular size and tenure and do not reflect these requirements will not be permitted. In all cases, proposals should show how the provision of family housing has been maximised.
New para	Major	<b>5.12</b> <u>The Legacy Corporation will apply the Mayor's definition of Build to Rent (BTR) as set out within Policy H13 of the draft New London Plan which includes schemes of more than 50 units, covenanted for at least 15 years and containing specific management measures. This generally includes proposals for a number of units which are purpose-built, or redeveloped for rent, normally by an institution or management company within the private sector to individuals, groups or families who do not share specialist-use requirements (i.e. excluding student and older persons' accommodation). The Legacy Corporation will utilise S106 agreements and conditions to secure Build to Rent provision. Alike traditional build for sale proposals, schemes containing BTR will be expected to provide a mix of dwelling types and sizes.</u>
Para 5.10	Minor	<b>5.13</b> Where appropriate and viable, units intended for family housing should be within a mix of flatted development <del>and traditional</del> houses or maisonettes. Consideration should also be given to <u>different living requirements and lifestyles such as how developments can be designed to suit the lifestyles of large family groups, including layouts with kitchens separate from other living space and, for example to support home working practices.</u>
Para 5.11	Major	<b>5.14</b> Proposals shall be supported which address existing stock imbalances by introducing market and intermediate housing within existing predominantly social rented areas or those which introduce affordable rented accommodation where currently under-represented, provided that they meet an identified local housing need. A mixed and <u>inclusive neighbourhood</u> <del>balanced community</del> will generally be considered to reflect roughly the local demographic norms in relation to tenure, age structure and income and enable people to remain within the community for the long term. Where the Legacy Corporation considers that a proposal could impact negatively on mixed and <u>inclusive neighbourhoods</u> <del>balanced communities</del> , or harm the residential amenity, character or function of the area, additional justification of the need for the development should be provided in the form of local studies,

Policy, para.	Type of change	Proposed change
		waiting lists, business cases for the development and potential economic repercussions, should the proposal not take place.
Para 5.12	Major	<p><b>5.15</b> The Legacy Corporation is directed by the London Plan on a range of housing policies which will be applied including: play space, residential amenity, daylight and sunlight, accessibility and safety design principles. Policies BN.4 and BN.5 sets out how the <del>Baseline Standards within the Mayor's Housing SPG will be applied alongside optional requirements and the Nationally Described Space Standards – Technical Requirements will be applied.</del> <u>The London Plan's density matrix is a useful tool, referencing Public Transport Accessibility Levels (PTAL) for determining densities; however, All proposals, including self/custom-build will be expected to optimise housing delivery on deliverable sites, when considering the appropriate density, the Legacy Corporation will consider individual site circumstances including: location, constraints, transport accessibility, connectivity and capacity; management, occupancy and tenure of the development; and contribution of the site to the Legacy Corporation's convergence aims. Schemes referable to the Mayor will also be required to undergo design review through the Quality Review Panel and submit a Management Plan where the density thresholds as set out in Draft New London Plan Policy D6 are triggered.</u></p>
New para	Minor	<p><b>5.16</b> <u>The housing trajectory sets out the anticipated delivery rates, but does not include an allowance from small sites for the first five years as delivery rates from these sources are currently considerably below these levels. However it is anticipated that delivery from these small sites can be increased by applying the principles of this new policy approach. The Characterisation Study, 2018 identifies the character of each part of the sub area and highlights broad locations which can potentially yield residential capacity from small sites, defined as less than 0.25ha or through residential conversions. Other locations within PTAL levels 3 to 6 are also considered broad locations of search. Residential proposals will be supported where they are of a suitable scale and design, have no unacceptable impacts on amenity and meet all other Local Plan policies.</u></p>
New para	Major	<p><b>5.17</b> <u>The Legacy Corporation will also seek to identify further opportunities to boost small site delivery through promoting brownfield sites and utilising the Brownfield Register. The previous 0.25 threshold has been amended to invite sites capable of delivery of more than 5 homes. The Call for Sites process of the Brownfield Register will remain open and the list will be updated regularly, at least on an annual basis.</u></p>
New para	Major	<p><b>5.18</b> <u>Other potential sources of residential capacity will come from conversions of other uses to residential. Due to other Local Plan policies protecting employment floorspace capacity for example, and the amount of development taking place within the area it is not anticipated that this form of development will yield any significant new capacity. The Legacy Corporation will monitor within the Authority Monitoring Report and keep under review the amount</u></p>

Policy, para.	Type of change	Proposed change
		<p><u>of capacity coming forward from small sites and conversions, introducing measures to enhance delivery where necessary such as further detailed housing capacity work and design codes.</u></p>
Case Study 3	Minor	<p><b><u>Case Study 3: Chobham Farm – Housing Mix</u></b></p> <p><del>The Chobham Farm site is located to the east of the Legacy Corporation area. It is adjacent to the railway line and part of it is situated on former Strategic Industrial Land currently within industrial use. The site has outline planning permission for up to 1,036 units.</del></p> <p><del>The first phase of the development is for mixed-use development: six buildings between three and ten storeys providing 173 residential units (Use Class C3) and 1,161 sqm of commercial floorspace (Use Class A1-A3, B1, D1 and D2) plus car and cycle parking. The residential density of development in this phase is 680 habitable rooms per hectare.</del></p> <p><del>The proposed mix of the first phase is well balanced, with family units of larger three and four bedroom units occupying approximately a third of the units. This mix meets demand identified within the Strategic Housing Market Assessment Review (2013) for three bed units, with 30 per cent of the affordable units being family dwellings and the whole of the scheme is structured in favour of two and three bed units, rather than one bed units.</del></p> <p><del>In terms of the affordability mix, this is also in favour of units with more than two bedrooms, and only 14 per cent of the affordable units are one bed properties, with more than half of these being affordable rent.</del></p> <p><del>The later phases of the proposal, permitted in outline, are also biased in favour of family housing units, with the scheme as a whole delivering approximately 43 per cent of units with more than three bedrooms. The viability of the latter phases of the development will be re-assessed to secure the maximum viable proportion of affordable units which has been set out within the terms of the Section 106 Agreement.</del></p> <p><b><u>Case Study 3: Housing Mix 52-54 White Post Lane</u></b></p> <p><u>This scheme is located in Hackney Wick but just outside the boundary of the Hackney Wick Masterplan site. This is a mixed-use redevelopment containing around 2,400 sqm of employment floorspace plus 55 residential units. The proposed mix of the scheme is well balanced with 34.5% 1 beds, 34.5% 2 beds and 31% 3 beds. This conforms to the Local Plan policy aspiration of a ‘balanced mix’.</u></p>
H.2 Affordable housing (1)-(4)	Major	<p>Affordable housing will be <del>maximised</del> <u>sought</u> on sites capable of providing ten units or more <del>and over 1,000sqm combined floorspace</del> or has an area of 0.5 hectares or more, <u>based on the affordable housing targets set out within SP.2</u>, broken down as 60 per cent <u>low cost rented Affordable Rent and Social Rent</u>, and 40 per</p>

Policy, para.	Type of change	Proposed change
		<p>cent intermediate <small>[Footnote- NPPF 10% low cost home ownership should be included within this category].</small> <del>7.</del> <u>The Legacy Corporation will apply the Mayor’s Fast Track and Viability Tested Routes and thresholds to maximise affordable housing delivery, including utilising viability re-appraisal where relevant. The following shall be considerations for maximising provision:</u></p> <ol style="list-style-type: none"> <li><del>1.</del> <u>Identified needs and tenure requirements</u></li> <li><del>2.</del> <u>Affordable housing targets and delivery rates</u></li> <li><del>3.</del> <u>The need to facilitate development</u></li> <li><del>4.</del> <u>Viability including phased viability re-appraisal.</u></li> </ol> <p><u>For self-contained residential schemes, affordable housing should be delivered on site in the first instance. Off-site provision will only be considered where it:</u></p> <ol style="list-style-type: none"> <li><del>1.</del> <u>5.</u> <u>Provides equivalent number and type of affordable units across all sites related to the proposal;</u></li> <li><del>2.</del> <u>6.</u> <u>Does not prejudice the delivery of affordable housing;</u></li> <li><del>3.</del> <u>7.</u> <u>Is delivered at no financial advantage to developer;</u></li> <li><del>4.</del> <u>8.</u> <u>Is linked to the completion of the market housing elements of the scheme;</u></li> <li><del>5.</del> <u>9.</u> <u>Is located where able to provide for local housing needs; and</u></li> <li><del>6.</del> <u>10.</u> <u>Would be beneficial to achieve and maintain mixed and balanced communities.</u></li> </ol> <p><u>Financial contributions will only be acceptable when on-site provision and all potential off-site options have been fully explored and discounted, and linked to a particular site or proposal. Other specific policies of the Local Plan set out where contributions towards off-site provision of affordable housing are appropriate.</u></p>
Para 5.13	Minor	<p><b>5.19</b> <u>Providing for housing needs including through different affordable tenures is crucial to achieving mixed and inclusive neighbourhoods balanced communities. For this reason, a proportion of total housing delivery will be within affordable tenures. This has been set as a minimum target of 35 per cent of affordable homes across the whole of the Legacy Corporation area and 50% on public owned land and as set out within Policy H6 of the draft New London Plan. Where residential is proposed within designated and non designated industrial sites and there will be a net loss of industrial floorspace capacity, 50% affordable housing is expected or the VTR will be utilised. This has been determined according to evidence and subject to rigorous viability testing <sup>[2018 Affordable Housing Viability testing]</sup> to determine viability across the whole of the area. This should be used as a minimum and will be used to commence discussions on individual schemes. In relation to the affordable housing tenure split requirements, the draft New London Plan sets out that 30 per cent shall be provided as low cost rented homes and 30 per cent intermediate products, with the remainder to be determined by the local planning authority through the Development Plan process. As local and strategic</u></p>

Policy, para.	Type of change	Proposed change
		<p><u>evidence confirms a higher need for low cost rented accommodation, within the Legacy Corporation area the remaining 40 per cent shall normally be provided as 30/10 in favour of low cost rented. When determining the detailed affordable housing mix the intermediate offer should also meet the 10% low cost home ownership requirement of the NPPF (where relevant</u> <sup>[Footnote- exclusions for solely for Build to Rent schemes, specialist accommodation for a group of people with specific needs, by people who wish to build or commission their own homes; or is exclusively for affordable housing])</sup>.</p>
Para 5.14	Major	<p><del>Affordable rent is a relatively new product, where eligibility is based upon local incomes and local house prices rather than local authority lists alone. Affordable rent is intended to address similar types of need to that within traditional social housing. Half of the affordable rented product will include rental rates capped to ensure the requirements of those most in need are met (similar to those within local authority nomination lists). For the remainder within the ‘discounted’ level, this should have typical rents of no more than 80 per cent of market value, including service charge. This level is broadly achievable across the area.</del><sup>27</sup></p>
New para	Major	<p><b>5.20</b> <u>The Mayor has set out appropriate income caps for his preferred intermediate tenures of London Living Rent and London Shared Ownership which will be applied. In relation to affordable housing allocations the Legacy Corporation will follow the approaches of the Growth Boroughs. In practice, the Legacy Corporation is not setting specific local rental caps, but will expect developers and registered providers to agree the proposed rental levels, maintained as low as possible, based on meeting local Borough needs, benefit caps and maximising output. The Mayor’s annual London Affordable Rent benchmarks shall be used as the starting point for setting appropriate rental rates, and other similar products should also demonstrate similar levels of affordability. Developers will be expected to demonstrate that they have engaged with a registered provider and secured a commitment for provision from the outset. Subject to the availability of appropriate funding, delivery of social rented accommodation within the area will be supported. Affordable housing should maximise larger, family type housing. Affordable accommodation should be indistinguishable externally from other tenures.</u></p>
Para 5.15	Major	<p><b>5.21</b> <u>Policy H.2 will apply to all residential schemes over 10 units or more or on sites of over 0.5 hectares, including future changes of use of residential floorspace. Proposals which provide affordable housing at the relevant threshold as set out within SP.2 without public subsidy, meet the 60/40 tenure mix requirement as above and all other Local Plan policy requirements can qualify for the Fast Track Route (FTR). All other proposals shall be assessed under the Viability Tested Route (VTR). This will mean that the scheme’s viability will be tested at the application stage, and further viability</u></p>

Policy, para.	Type of change	Proposed change
		<p><del>testing will be secured via As-scheme viability can vary across its lifetime, to ensure affordable housing is maximised, the Legacy Corporation will utilise Section 106 Agreements using the formula and the process set out within the Mayor's Affordable Housing and Viability SPG to secure early, mid and late stage reviews (as appropriate). Viability re-appraisal will also be secured within FTR schemes for circumstances where an agreed level of progress has not been met within two years to ensure that proposals within phased development re-appraise viability prior to each subsequent phase of the development. Outcomes of the re-appraisal may result in additional affordable housing provision within later phases. For Build To Rent schemes to qualify for the FTR the tenure mix should consist entirely of Discounted Market Rent with 60 per cent being offered at a discount equivalent to London Affordable Rent, 30 per cent as London Living Rent and the remainder offered at equivalent rates to other intermediate housing offers. All other tenure mixes will be subject to the VTR. In accordance with the draft New London Plan Estate regeneration schemes should go through the VTR. Policy H.2 will apply to all residential schemes over 10 units or more or over 0.5 hectares, including future changes of use of residential floorspace.</del></p> <p>5.31 The Legacy Corporation acknowledges the viability implications of delivery of purpose-built private rented accommodation, as opposed to market sale, so specialist PRS viability appraisal, based on yield, shall be accepted when demonstrating how affordable housing provision has been maximised. In these circumstances, the Legacy Corporation will expect PRS provision to be covenanted<sup>40</sup> for long-term private rented sector use. The long-term management of PRS will be secured by condition.</p>
New para	Minor	<p><u>5.22 Where affordable housing is provided as dedicated blocks within a larger scheme the affordable housing units should be appropriately located across the site, avoiding parts of the site which may be more constrained or less accessible. Affordable accommodation should be indistinguishable externally from other tenures.</u></p>
H.3 Older persons	Minor	<p>Net loss of older persons' accommodation will only be acceptable where it is unsuitable or below relevant standards and incapable of meeting standards at reasonable expense. The Legacy Corporation will support provision of new specialist older persons' accommodation <u>within C2 or C3 use classes</u> which will be acceptable where:</p> <ol style="list-style-type: none"> <li>1. Suitably located in relation to transport modes, <u>social infrastructure</u> and Centres;</li> <li>2. Meeting identified strategic needs for the <del>tenure and</del> <u>specialist type of accommodation</u>;</li> </ol>

Policy, para.	Type of change	Proposed change
		<p>3. Meeting a local need for the level of care provided within the accommodation;</p> <p>4. It meets <u>accessible and inclusive design and other relevant standards</u> for the type of accommodation; and</p> <p>5. Delivery of non-specialist housing is not compromised.</p> <p><u>Self-contained C3 accommodation should also provide affordable housing in accordance with Policy H.2.</u></p>
Para 5.16	Minor	<p><b>5.23</b> The average age in the Legacy Corporation area is set to increase over the Plan period, but with a younger population than within other parts of London. Nonetheless, evidence suggests that there is a <u>strategic London-wide need for specialist accommodation within all use classes and tenures</u> <sup>[Reference London Plan evidence]</sup> <u>as well as a small local need but most of the strategic needs for older persons' accommodation mainly for come from C3 accommodation with varying types of specialism and support those within the C3 Use Class and owner occupation tenures.</u><sup>29</sup> <sup>[Housing Requirements Study, 2018]</sup> . Policies will ensure that needs for specialist older persons' accommodation can be met. <u>Generally C3 accommodation will be considered self-contained sheltered or extra care accommodation whereas C2 accommodation will be residential sheltered care homes with a significant level of care such as healthcare, domestic help or emergency support. For the purposes of clarification, non-self-contained older person's accommodation will be monitored on the basis of 3 bedspaces accounting for a single home.</u></p>
Para 5.17	Minor	<p><b>5.24</b> All types of older persons accommodation should meet National Minimum Standards for Care Homes for Older People or be designed for the needs of future occupants, <u>including specifically for dementia care</u>, staff and visitors while maintaining flexibility of tenure, in accordance with the Design Principles for Extra Care Housing, 2008. Existing accommodation shall only be lost where these design standards are incapable of being met through re-configuration of the accommodation. This will be assessed by the cost of work to meet standards and specialist viability appraisal.<sup>31</sup> Identified increased strategic and local needs for older persons' accommodation mean it is unlikely that a case for a lack of need for specialist older persons' accommodation can be proven. <u>Proposals involving changes of use between different forms of older person's accommodation should demonstrate changing local requirements for the level of care and utilise viability appraisals to maximise affordable housing.</u></p>
Para 5.18	Minor	<p><b>5.25</b> The Legacy Corporation does not have an indicative benchmark target within the London Plan; however, it will seek to provide accommodation to meet strategic and local requirements. To enable occupants of specialist accommodation to carry out day-to-day tasks, proposals should be located within easy access of public transport, <u>social infrastructure</u> and Centres, as defined within Table 3.</p>

Policy, para.	Type of change	Proposed change
Para 5.19	Minor	<p><b>5.26</b> Provision should align closely with requirements, so the Legacy Corporation will seek to ensure that specialist, and sometimes high-cost, accommodation does not compromise the overall delivery of housing requirements. Proposals should therefore demonstrate how the development meets the strategic need within east London for the tenure and type of <u>specialist accommodation, including accommodation suitable for people with dementia</u>. Proposals for <u>accommodation with an element of care should demonstrate and</u> local requirements for the <u>specific type and level of care</u>. <del>This will be key for accommodation where the level of care is minimal, e.g. retirement complexes.</del> Methods of demonstrating local need should include <u>local demographic information, waiting lists, survey information and demand information from similar developments within a nearby equivalent area</u>. <u>Proposals for new C2 accommodation should also submit business models and management plans to demonstrate the ability to deliver and manage the development for the long-term</u>. Specialist viability appraisal will be expected to take into account the distinct economics of specialist accommodation provision.</p>
New para	Minor	<p><b>5.27</b> <u>New C3 older persons accommodation should deliver 35% affordable housing in accordance with Policy H.2, or be assessed through the Viability Tested Route</u>. Given that almost half of the <u>total requirement is for Leasehold Schemes for the Elderly (LSE) the tenure split for affordable older person's accommodation should be balanced in favour of shared ownership products</u>.</p>
Case Study 4	Minor	<p><del><b>Case Study 4: Stratford Halo, 150 High Street, Stratford</b></del>  <del>Older persons' accommodation within a mixed and balanced community</del></p> <p><del>The development as a whole won Inside Housing's 'Development of the Year (large schemes)' award 2013, and has delivered 65 extra-care units sitting alongside market and affordable rented, HomeBuy, outright sales, commercial, business and workshop space. The scheme is a good example of how introducing older persons' accommodation to a scheme can contribute to mixed and balanced communities by providing a range of tenures and introducing a population diverse in terms of income levels.</del></p> <p><del>The developers worked in partnership with Newham Adult Social Care team to deliver the variety of different care services in one extra-care scheme. Residents have the use of dedicated outside space, as well as that for the development as a whole. This provides a safe environment for vulnerable residents, as well as encouraging integration. It also provides a number of learning opportunities for residents, including IT classes, arts and crafts, music therapy and coaching skills.</del></p> <p><b>Case Study 4: Older persons' housing, William Guy Gardens</b></p>



Policy, para.	Type of change	Proposed change
		<p><u>This scheme completed in 2016 involved the redevelopment of a vacant older persons housing development to provide a total of 41 units for the over 50's including associated communal and private amenity space, hard and soft landscaping and disabled car parking. It provided 36 affordable dwellings and 5 market dwellings for the over 50's age group, involving a net increase of 28 older persons' units.</u></p>
H.4 Student accommodation	Minor/major	<p>Net loss of student accommodation will be acceptable where the proposal meets identified requirements within the housing supply. Proposals for new <u>purpose-built student accommodation (PBSA) should:</u></p> <ol style="list-style-type: none"> <li>1. <u>Secure</u> Secures the accommodation through planning agreement or condition for long-term student use and <u>be secured by nomination agreement for occupation by students of one or more identified Higher Education provider.</u> a) <u>Establishes relationships with higher education institutions (HEIs) including where it is part of a wider scheme for a new HEI campus, and is available at an affordable rate; or b) Maximises affordable student provision according to viability where there is not an undertaking with a specified HEI</u></li> <li>2. <u>Meet identified strategic needs for student accommodation and be directed to appropriate locations within or on the edge of the Metropolitan Centre. Proposals outside these locations will be acceptable where they are suitably located for easy access by walking, cycling or public transport to the higher education provider/s to which the proposal is linked.</u></li> <li>3. <u>Facilitate a positive balance of tenure and income in the locality and has 4. Has have no unacceptable adverse amenity impacts; and 5. Is suitably located in relation to public transport and HEI/s; and</u></li> <li>4. <u>Provide the maximum level of on-site affordable student provision, or assessed through the Viability Tested Route (VTR) (utilising the thresholds set out within paragraph 5.19) and be secured through a legal agreement.</u></li> </ol> <p>In locations where the proposal has the potential to impact negatively upon the aims of (3) <del>through over concentration,</del> justification of the requirement for the development should be provided by additional local market need information.</p>
Para 5.20	Minor	<p><b>5.28</b> <u>The draft New London Plan specifies that there is a strategic need across London for new student bedspaces and provision of new purpose-built student accommodation (PBSA) can contribute towards the overall supply of housing but should be more dispersed. For this reason, the Legacy Corporation will ensure that student accommodation permitted within the area meets genuine</u></p>

Policy, para.	Type of change	Proposed change
		<p>student needs and is appropriate in location and that uses are integrated well into the wider environment through application of this policy. <u>For the purposes of clarification, PBSA will be monitored on the basis of 3 bedspaces accounting for a single home.</u></p>
Para 5.21	Minor	<p><b>5.29</b> To ensure that the accommodation is provided at a rate affordable to current students, proposals should be linked to <u>one or more established higher education providers institutions (HEIs)</u> <del>or</del> <u>and secured for long-term student use through Section 106 Agreement or conditions. If these links are not established, rental levels should be kept low, subject to specialist viability testing, through maximising the number of units available to students at rates equivalent to affordable provision tied to London HEIs.</u> Proposals shall only be determined under this policy where they will be robustly secured for students through <u>Higher Education Provider HEI links, conditions or a Section 106 Agreement, or where all the bedspaces qualify as affordable student accommodation.</u> In all other cases, it shall be subject to the requirements of Policy H.2: Affordable housing, and any other relevant policies within this Local Plan.</p>
Para 5.22	Minor	<p><b>5.30</b> <u>The Metropolitan Centre and edge of centre sites within the northern Stratford Policy Area Zone (see Policy 3.1) are considered most appropriate for PBSA due to the enhanced public transport accessibility, the location of existing and future higher education providers and the ability of the proposals to add to the diversity and vitality of the centre. However by</u> <del>By</del> <u>its nature, the student population is transient, so proposals in all locations should ensure that this does not impact negatively on the long-term sustainability of the community. In areas of existing high provision, most notably within the vicinity of Stratford, proposals should consider carefully their impact on the management of the night time economy, including crime and safety and the impact on mixed and balanced communities inclusive neighbourhoods and where necessary justified through, justifying the proposal through</u> additional local market need information. This may include consideration of <del>planning permissions and delivery against the apportionment of student accommodation within the annual housing target of 171 per annum (see SP.2), HEI</del> <u>higher education provider waiting lists, survey information and demand information from similar developments within the area.</u> <del>5.23</del> <u>All proposals should demonstrate how potential for noise, disturbance and amenity impacts will be minimised through a management scheme, as well as be located within a short walking distance of public transport or HEIs</u></p>
New para	Minor	<p><b>5.31</b> <u>The draft New London Plan expects non-self-contained schemes such as student accommodation to contribute to the supply of affordable housing. Any new proposals should therefore provide at least 35% on-site affordable student bedspaces available at a rate affordable to students on maximum state-funded financial</u></p>

Policy, para.	Type of change	Proposed change
		<p><u>support (defined by the Mayor’s Annual Monitoring Report and appropriately indexed in later years) or be rigorously assessed through the VTR. Proposals located on public land, or on industrial sites where there is a net loss of industrial capacity should provide at least 50% affordable student rooms to follow the FTR. Affordable student provision should be equivalent to all other bedspace provision by nature of the design, size and services provided, with nominations managed by the relevant higher education provider. Any future proposed change of use will trigger a full Viability Review to maximise affordable housing.</u></p>
New para	Major	<p><u>5.32 The Legacy Corporation will expect the submission of information demonstrating how the population density of the proposal compares to that of similar housing schemes meeting the Local Plan housing mix requirements as set out in Policy H.1. Where the population density is above that of equivalent schemes and there are considered to be sufficient additional impacts on transport or infrastructure demand compared to other conventional housing schemes, for example for healthcare facilities, or a substantial increase in footfall, further S106 contributions may also be sought for mitigation of impacts. The use of student accommodation for other ancillary uses within the vacation period will only be acceptable where the accommodation is offered to conference delegates, university interns, and short course students at an equivalent daily rate to that charged the student occupants of the accommodation. Where acceptable occupancy conditions will be secured through conditions or S106 agreements.</u></p>
H.5 G&T	Minor	<p>The Legacy Corporation will seek to provide for the needs of gypsies and travellers generated within its area through working strategically with neighbouring boroughs and co-operating <u>and engaging</u> with gypsy and traveller communities to allocate <u>and deliver</u> suitable sites.</p>
Para 5.25	Major	<p><u>5.34 The Housing Requirements Study (2018) identifies Evidence suggests that there is a net requirement for between ten and 19 nine gypsy and traveller pitches within the Legacy Corporation area over the Plan period to 2036 plus a need for an additional 15 pitches arising from households falling within the draft New London Plan definition, but do not meet the planning definition defined within the Planning Policy for Traveller Sites (2015). The first five year, needs-based pitch target amounts to between six and 13 pitches. The existing site at Chapman Road shall be is safeguarded for gypsy and traveller accommodation uses to continue to contribute a supply of five pitches over the Plan period. Although site assessments<sup>35</sup> have concluded that there are no suitable, available and achievable sites to meet the identified needs over the Plan period, an audit of open space has identified that the Local Open Space designation at Bartrip Street South has minimal value and</u></p>

Policy, para.	Type of change	Proposed change
		<p><del>functionality and so has been removed from designation.<sup>36</sup> The allocated site at Bartrip Street South is expected to be able to provide around nine new pitches therefore it has the potential to meet the needs of households who meet the planning definition over the plan period to 2036. This means that this site has potential to contribute towards the pitch requirements and so has been allocated for gypsy and traveller use (see Allocation SA1.9). This site is only expected to be able to meet the lower end of the first five-year pitch target of up to approximately nine pitches. It is not yet known how the identified needs for 15 pitches for households who fall outside the planning definition over the whole of the Plan period will be met. In order to do so the Legacy Corporation will continue to cooperate with neighbouring authorities to explore potential to meet need associated with its area at a strategic level. The Legacy Corporation will therefore identify and update on an annual basis the availability of sites to meet the first five years' supply of sites against the pitch target; identify specific sites or broad locations of sites to meet supply for years 6 to 10, and thereafter; and monitor performance against these targets and review Local Plan Policy H.5 if these aims are not being met by 2018/19.</del></p>
Para 5.26	Minor	<p><b>5.35</b> Where any additional sites can be identified for potential gypsy and traveller use within the area, suitability will be assessed on the same grounds as housing in general, including deliverability and developability tests. <u>Where small gypsy and traveller sites are proposed, proximity to existing sites to ensure the cohesion of the gypsy and traveller community will be considered positively.</u> The policy criteria shall be used to assess proposals for potential sites within the plan-making and development management processes where potential arises. The Legacy Corporation will monitor any unmet need through the monitoring and review process which will include updating evidence, investigating capacity requirements or amending targets where required and will cooperate with each of the Growth Boroughs to address wider strategic issues of accommodating need for gypsy and traveller accommodation once they have reached an appropriate point of review for their local plans. <u>On an annual basis the Legacy Corporation will also work with the boroughs and other partners to audit existing pitches for overcrowding, capacity and refurbishment requirements</u> <del>Where small sites are proposed, proximity to existing sites to ensure the cohesion of the gypsy and traveller community will be considered positively. and will utilise the relevant borough's nominations process where new sites are provided.</del></p>
<b>H.6 HMOs</b>	Minor	<p>Net loss of HMOs will only be permitted where an HMO is no longer required, is unsuitable or below standards, or is being replaced with housing meeting localised needs.</p> <p><u>Schemes of 30 units and more will deliver a cash in lieu contribution towards the delivery of 35% C3 affordable housing, or be subject to the Viability Tested Route.</u> Proposals for purpose-built HMOs and</p>

Policy, para.	Type of change	Proposed change
		<p>conversions requiring planning permission will be acceptable where:</p> <p><del>1. Maximising affordable housing according to viability;</del>  <del>1. 2. Meeting an identified local need;</del>  .....</p>
Para 5.27	Minor	<p><del>5.36 The importance of HMOs to the housing supply has been heightened by benefit reforms. Single households under 35 will now often require space in shared accommodation, as opposed to single-person dwellings. It is considered that HMOs provide an important role in the housing supply, particularly to meet the needs of younger households. To maintain this important supply of low-cost housing, the Legacy Corporation will seek to protect HMOs which meet relevant standards and provide a high standard of new HMO accommodation.</del></p>
New para	Minor	<p><del>5.37 HMOs are defined within this policy as having at least 3 tenants living together to form one household, sharing washing and cooking facilities but let on a room by room basis, rather than as a single property. Large HMOs are over three storeys high with 5 tenants or more. HMOs do not normally provide any additional shared amenity or facilities, whether on-site or through off-site linked, or shared facilities. In accordance with the London Plan, HMOs, being non-self-contained should normally be monitored on a 3:1 basis. The affordable housing requirement will be triggered at 30 units, on this basis of this being equivalent to 10 residential units, and thus meeting the threshold as set out within Policy H.2. Any future proposed change of use will trigger a full Viability Review to maximise affordable housing.</del></p>
Para 5.28	Minor	<p><del>5.38 New provision should meet relevant housing space standards as well as normal internal and residential amenity standards. Premises should be licensed, ensure mixed and balanced communities (see Policy H.1) and should not compromise the overall delivery of housing requirements. The Legacy Corporation shall monitor and enforce against any 'beds in sheds' and the impact of HMO provision through permitted development rights. An Article 4 Direction will be considered if evidence suggests that there has been a substantial loss of family housing to HMOs.</del></p>
H.7 PRS	Major	<p><i>Policy requirements now included within Policy H.1.</i></p> <p><b>Policy H.7: Large scale investment in private rented sector</b></p> <p><del>New purpose-built private rented sector accommodation, including large-scale, long-term investment, will be supported where the proposal:</del></p> <p><del>1. Maximises affordable housing according to viability;</del></p>

Policy, para.	Type of change	Proposed change
		<p>2. Meets local and strategic needs;</p> <p>3. Provides well-managed accommodation meeting high design standards; and</p> <p>4. Includes a mechanism to secure the accommodation as private rented for the long term.</p> <p><b>Cross-reference to policies: SP.2; BN.1; BN.4; BN.5; H.2 London Plan policies: 3.8; Housing SPG</b></p> <p><b>Reasoned justification</b></p> <p><b>5.29</b> Large-scale investment in the private rented sector as a concept can play an important role in meeting the housing needs of those who choose not to or are unable to afford to buy market homes. It also provides a supply of accommodation available for those who are in housing need and supported by benefits. Advantages of this provision are flexible tenancies, greater certainty over rent rises and a managed approach to the whole development, ensuring consistent quality. These assurances shall also enable residents to remain in the area for the longer term, contributing to community cohesion. Despite the planning system offering limited powers to influence provision, proposals for such accommodation which meet a number of criteria will be supported.</p> <p><b>Policy application</b></p> <p><b>5.30</b> In the context of this policy, the private rented sector (PRS) is considered to refer to proposals for a number of units which are purpose-built, or redeveloped for rent, normally by an institution or management company within the private sector to individuals, groups or families who do not share specialist use requirements (i.e. excluding student and older persons' accommodation).</p> <p><b>5.31</b> The Legacy Corporation acknowledges the viability implications of delivery of purpose-built private rented accommodation, as opposed to market sale, so specialist PRS viability appraisal, based on yield, shall be accepted when demonstrating how affordable housing provision has been maximised. In these circumstances, the Legacy Corporation will expect PRS provision to be covenanted<sup>40</sup> for long term private rented sector use. The long term management of PRS will be secured by condition.</p>
New shared living policy (H.7)	Major	<p><b>H.7 Shared living accommodation</b></p> <p><u>Proposals for large scale shared living accommodation should be directed towards the Metropolitan Centre or identified appropriate edge of Metropolitan Centre locations. Large scale and other smaller shared living proposals outside these locations should demonstrate the role of this form of accommodation in meeting identified housing requirements. Proposals will be acceptable where they:</u></p>

Policy, para.	Type of change	Proposed change
		<ol style="list-style-type: none"> <li>1. <u>Relate positively to the aim of mixed and inclusive neighbourhoods</u></li> <li>2. <u>Include suitable and secured management processes; and provide sufficient on-site facilities, services and amenity space in accordance with Draft New London Plan Policy H18, which should be appropriate to the scale, density and population of the proposed development;</u></li> <li>3. <u>Meet high residential design standards; and</u></li> <li>4. <u>Contribute financially towards the provision of off-site C3 affordable housing (for schemes of 30 bedspaces and above), equivalent to 35% of residential units within the proposal</u></li> </ol>
New para	Major	<p><b><u>Reasoned justification</u></b></p> <p><b>5.39</b> <u>The extent of the need for housing across London in particular has facilitated the growth in new and alternative forms of accommodation which can contribute towards the overall supply. It is believed that shared living can play a key role in meeting housing delivery and provide a new product in demand across London, however it is important that proposals provide high standards of accommodation and are appropriately located, as well as contribute appropriately to the supply of affordable housing.</u></p>
New para	Major	<p><b><u>Policy application</u></b></p> <p><b>5.40</b> <u>Shared living accommodation is defined within this policy as a non-self-contained residential development (demonstrably not C3) which do not meet minimum housing standards; delivered under single management; with tenancies of at least three months; containing on-site, or linked off-site shared communal facilities encouraging shared interaction, above that required for washing and cooking; and which fall outside within the scope of policy H.6 (HMOs). Large-scale shared living is defined by the draft New London Plan as schemes containing 50 or more non-self-contained units as described above. All proposals for shared living should provide appropriately sized on-site communal services, facilities and amenity space, meet relevant design and management requirements as set out within Policy H18 of the new London Plan. Detailed justification of the ratio of bedroom space to services, facilities and amenity space should also be provided.</u></p>
New para	Major	<p><b>5.41</b> <u>The Metropolitan Centre is an appropriate location for large scale shared living proposals where the residential density can contribute towards the housing mix of town centre living and aid the vitality and viability of the centre. Edge of centre sites at the northern zone of Stratford High Street within the Stratford High Street Policy Area are also considered appropriate locations for large-scale shared living as the integrated non-residential elements of schemes will promote increased vitality along a street lacking in</u></p>

Policy, para.	Type of change	Proposed change
		<p><u>current identity (see Policy 3.1). Where large-scale proposals are considered to have a potential negative impact upon mixed and inclusive communities additional local need information may be sought and/or S106 funds secured for mitigation. Proposals for large scale or other shared living outside these locations will be expected to demonstrate that there is a genuine unmet demand for this accommodation through marketing and demand information from similar schemes across London and how the scheme contributes towards meeting localised housing requirements.</u></p>
New para	Major	<p><b>5.42</b> <u>For schemes of 30 bedspaces and above, proposals should deliver a cash in lieu contribution to C3 affordable housing, equivalent to at least 35 per cent of bedspaces (see Policy H.2 thresholds) at a 50% discount from the market rent. In accordance with the draft New London Plan this can be as a one-off payment or an on-going in perpetuity payment. All proposals will be assessed under the VTR and any future proposed change of use will trigger a full Viability Review to maximise affordable housing.</u></p>
New para	Major	<p><b>5.43</b> <u>Shared living should be secured in perpetuity under single management and applications should include a Management Plan which will be secured through S106 or condition. This should ensure that the development is managed and maintained to ensure the quality of the development. Proposals for a conversion away from shared living or a change in configuration or quantum of the residential elements shall re-appraise affordable housing viability and clawback any additional requirement. Any proposed amendments to the use, terms or quantum of shared facilities should provide evidence that the facilities are no longer required, that the facilities can be lost or re-configured without negative impacts on the shared living offer or demand for such accommodation.</u></p>
New para	Major	<p><b>5.44</b> <u>For all proposals for shared living accommodation the Legacy Corporation will also expect the submission of information demonstrating how the population density of the proposal compares to that of similar schemes meeting the Local Plan housing mix requirements as set out in Policy H.1. Where the population density is above that of equivalent schemes and there are considered to be sufficient additional impacts on transport or infrastructure demand compared to other conventional housing schemes, for example for healthcare facilities, or a substantial increase in footfall, further S106 contributions may also be sought for mitigation of impacts.</u></p>
New para	Major	<p><b>5.45</b> <u>In accordance with the new London Plan, shared living accommodation of any size will be monitored on a 3:1 basis. Where any change in residential configuration is proposed, the impact on housing numbers will also be assessed to ensure changes are</u></p>



Policy, para.	Type of change	Proposed change
		appropriately monitored, with a presumption against loss of <u>residential accommodation.</u>
New Innovative Housing models policy (H.8)	Major	<p><b><u>H.8 Innovative housing models</u></b>  <u>Proposals for new innovative forms of residential accommodation or housing provision which fall outside the scope of policies H.3 to H.8 will be acceptable where they:</u></p> <ol style="list-style-type: none"> <li>1. <u>Relate positively to the aim of mixed and inclusive neighbourhoods;</u></li> <li>2. <u>Demonstrably contribute towards the wider, strategic housing supply;</u></li> <li>3. <u>Meet an identified local need for the unique form of accommodation proposed, including where non-residential elements are included, or where the intended occupants have a shared set of characteristics with identified requirements which are unable to be met within the market;</u></li> <li>4. <u>Provide 35% affordable housing on a habitable room basis or via the Viability Tested Route in accordance with Policy H.2. Off-site contributions towards C3 provision will only be acceptable where the scheme is Sui Generis and demonstrably not C3 accommodation;</u></li> <li>5. <u>Contain suitable management or investment model processes;</u></li> <li>6. <u>Are secured through the planning system for an appropriate time-period; and</u></li> <li>7. <u>Meet relevant residential design standards.</u></li> </ol>
New para	Major	<p><b><u>Reasoned justification</u></b></p> <p><b>5.46</b> <u>It is likely that new and innovative housing delivery models will continue to develop in order to meet accommodation demands. These products may be new or unique by the way they are developed, managed or in the final product itself. Through time it is also envisaged that new forms of living will continue to emerge from shifting perceptions of potential living solutions and to take account of technological changes in the process of designing and producing residential accommodation. This may also enable more wider community needs to be met, such as the potential for shared amenity space or facilitating home-based working.</u></p>
New para	Major	<p><b>5.47</b> <u>The form of innovation of a product could be by the level of self-containment; size of accommodation; flexibility or nature of use of space; lifetime of the scheme; occupancy; fit out; management or investment model; or a combination of the above. This policy sets out some key policy principles to help guide future proposals which are not covered by other specialist housing policies.</u></p>

Policy, para.	Type of change	Proposed change
New para	Major	<p><b><u>Policy application</u></b></p> <p><b><u>5.48</u></b> All proposals should demonstrate how they contribute to mixed and inclusive communities and contribute to the supply of housing. Where any proposals are considered to have a potential negative impact upon mixed and inclusive communities S106 funds may be secured for mitigation. Proposals should clearly meet a defined need for the form and type of accommodation and should be located where conventional housing would also be acceptable. Information should be provided on how the proposal contributes towards the supply of housing across London, but also that it meets a clearly identified, specific need for the innovative nature of the product. Proposals should also demonstrate how they contribute towards wider policy aspirations such as economic growth, community infrastructure provision or enhance the cultural and creative sectors.</p>
New para	Major	<p><b><u>5.49</u></b> To ensure a high quality of delivery the proposed product's form of innovation should be appropriately secured through the planning system. Dependent on the product this may be through use of management plans, occupancy conditions or for a specific time-period. Any innovative proposals for residential as an interim use should be secured for the relevant period and enable the re-use of materials on another site. Any innovative products provided at sub-market rates should be secured as such by legal agreement to prevent the scheme being used for investment purposes or short-term financial gain.</p>
New para	Major	<p><b><u>5.50</u></b> Self-contained accommodation should provide affordable housing in accordance with Policy H.2 of the Local Plan on a habitable room basis. Under certain circumstances affordable housing may be calculated on a residential unit or floorspace basis. This may include, for example, where the innovative nature of the product relates to the level of internal specification or similar; or where the specialism relates to the shared characteristics of occupants.</p>
New para	Major	<p><b><u>5.51</u></b> Where a scheme is innovative with regard to its tenure and/or financial modelling, units will only be considered as part of the supply of affordable housing where rental or purchase rates are comparable to affordable housing tenures set out within the Mayor's Housing Strategy. Financial contributions towards the provision of C3 affordable housing will only be appropriate for non-self-contained, sui generis schemes that are demonstrably not C3 or C2 accommodation, provided at discounts equivalent to the requirements of Policy H.2. Any future proposed change of use will trigger a full Viability Review to maximise affordable housing. Where the population density is above that of equivalent schemes and there are considered to be sufficient impacts on transport or infrastructure demand, S106 contributions may also be sought for mitigation of impacts.</p>

Policy, para.	Type of change	Proposed change
New para	Major	<u>5.52 All proposals for new forms of residential accommodation should demonstrate that they make appropriate and effective use of the site and thus do not compromise overall housing delivery. They should meet all relevant internal and external space standards, or for non-self-contained scheme demonstrate how equivalent flexible space is provided to meet this requirement. Where the proposal also includes an element of supporting non-residential space or shared facilities there should be no unacceptable impacts on amenity on a site or neighbourhood level.</u>
New para	Major	<u>5.53 In accordance with the London Plan, generally any scheme which is non-self-contained and removes pressure of conventional supply will be monitored on a 3:1 basis; anything which frees up larger units will be monitored on a 1:1 basis.</u>
Page 59 Policy Cl:1 Community Infrastructure	No change	No change to policy proposed
Para 5.35	Minor	<u>5.57 There are also existing community facilities within the area falling generally within D1 uses, but may also include D2 venues of a specific community or cultural value.</u> With the significant population growth predicted and the need to ensure that new and existing communities have access to places where they can meet and where community activities can take place, these existing and new facilities are essential for long-term community development in the Legacy Corporation area. As such facilities will also generally be available for use by residents in the wider surrounding communities, these facilities will also play a part in a wider integration of communities in this part of east London, particularly as improved connections provide greater access and permeability across the Lower Lea Valley. <u>Therefore existing community facilities and venues will be protected through this policy.</u> The existing and proposed <u>D1</u> community facilities are shown at Figure 12 opposite.
Cl.2	No change	No change to policy proposed
Page 64 Para 5.38	Minor	<u>5.60</u> While uncertainty exists as to the precise number of new school places that will be required within and around the Legacy Corporation area within the lifetime of this Local Plan, evidence indicates that there <del>will</del> <u>may</u> be a general deficit in the capacity of existing and currently planned schools, particularly for primary-age pupils.
Page 64 Para 5.39	Minor	<del>5.61 Uncertainty also exists around the number of children requiring school places that will be generated from the population living in the new development planned for the area. Initial assessment through the LLDC Schools Study (2018) and Infrastructure Delivery</del>

Policy, para.	Type of change	Proposed change
		<p>Plan indicates a requirement of 2,138 [XXXX] primary school places and 650 [XXXX] secondary school places in addition to the existing and planned supply during the Plan period to 2036<sup>1</sup>. However, the population forecast used factors in a full build out of all potential new development identified in this Plan by the end of the Plan period. Monitoring will be required in order to determine the need more accurately within the second half of the Plan period when this possible deficit has the potential to occur.</p> <p>Assessment through the Legacy Corporation Schools Study (2018) and Infrastructure Delivery Plan indicates an additional requirement for primary school places and secondary school places in addition to the existing and planned supply during the Plan period to 2036, in particular, for later in the Plan period. Monitoring will be required in order to determine the need more accurately within the second half of the Plan period when this possible deficit has the potential to occur. This will be reviewed as part of the Local Plan process within future Legacy Corporation Local Plan Reviews or by successor organisations. The Legacy Corporation will continue to work closely with local education authorities.</p>
Page 64 Para 5.40	Minor	<p><b>5.62</b> However, a more detailed study indicates that this figure could be lower. The existing schools in the area and the planned provision are set out in the tables 4 and 5.</p>
Pages 64/65 Table 4 and Table 5	Minor	<p>Existing and proposed schools tables to be updated to reflect current context, see table changes below.</p>
Page 65 Para 5.41	Minor	<p><b>5.63</b> It is considered that the existing and planned schools as set out above are likely to provide sufficient school provision within the Legacy Corporation area <u>within at least the first half of the Plan Period</u>. However, depending on admissions criteria, the schools will also serve residents outside the Legacy Corporation area, and will also be part of the wider school networks in the surrounding area. <u>Schools planning will need to take into account the wider picture for school place planning within the four boroughs.</u> The LLDC Schools Study (2018) provides information on this wider context. The Legacy Corporation in its role as Local Planning Authority will work with the boroughs to ensure that schools proposals meet long-term identified needs. <u>New schools that are close to the area include School 21, a new all through free school at Rokeby Street, Stratford, and the relocated and expanded Bow School, a secondary school and sixth form located at Bow Locks, Bromley-by-Bow.</u> <u>It will therefore be necessary to monitor, and review school provision and population change over time to ensure provision keeps pace with need.</u> Schools will be encouraged to use the assets of Queen Elizabeth Olympic Park and economic growth within the Legacy Corporation area to inspire a generation of east Londoners to</p>

Policy, para.	Type of change	Proposed change
		believe that, with the right support, ambition and determination, they can compete with the best of London and beyond.
Page 65 Para 5.42	Major	<del>5.42 Although the tables above set out the existing and planned school provision, free school and other new school proposals are likely to come forward over time. Conversely, school allocations, where tied to specific development schemes, may not come forward, or could come forward sooner or later than currently anticipated, depending on the development cycle. It will therefore be necessary to monitor and review school provision and population change over time to ensure provision keeps pace with need.</del>
Page 66 Para 5.43	Minor	<del>5.64 The Legacy Corporation will work to secure implementation of planned new provision. Also, where population statistics indicate that school place requirements are greater than planned, it will work with the schools providers and local education authorities to seek to expand expansion of the provision within existing and planned schools where capacity exists. If evidence indicates that this further capacity is unlikely to be sufficient, delivery of additional new schools will be required later in the Plan period. At present, the site allocation at Greater Carpenters District allows for new or replacement schools to come forward as part of new development in that area. When planning applications are considered in those areas locations where existing and planned schools have been identified, the impact on school places provision should be assessed and should inform proposals.</del>
Page 66 Para 5.44	Minor	<del>5.65 Sites for schools will need to be of sufficient size and in a location and form that meet the requirements of Policy CI.2. It is expected that new schools will be designed to meet or exceed the best practice standards current at the time. These are currently the Baseline Designs for Schools: gGuidance published by the Education and Skills Funding Agency in March 2014. The introduction of university technical colleges, for 14 to 18 year olds, specialising in technical skills, is also likely to have an influence on form of provision.</del>
Supporting evidence base		<u>Housing Requirements Study (2018)</u> <u>Work Live Study (2014)</u> <u>Schools Study (2018)</u> <u>Infrastructure Delivery Plan (2018)</u>

**Existing and Planned Schools (updated tables 4 and 5):**

**Table 4**

Existing Schools	Area	Borough	Notes
Chobham Academy	East Village	Newham	All-through school, capacity for 1,800 students
London Academy of Excellence	Stratford High Street	Newham	Sixth-form college, free school
East London Science School	Three Mills	Newham	A free school in temporary accommodation, providing secondary education for up to 240 pupils ( <u>relocating to nearby permanent, larger Stephenson Street site in early 2020's</u> )
Gainsborough Primary School	Hackney Wick	Hackney	Recently expanded to three-form entry
Carpenters Primary School	Stratford	Newham	Recently expanded to three-form entry, <u>in the future there may be the potential to redevelop and further expand this school as part of on-going regeneration in the area.</u>
<u>Bobby Moore Academy Primary School</u>	<u>Sweetwater</u>	<u>Tower Hamlets</u>	<u>Primary School to open in September 2018 for 400 students</u>
<u>Bobby Moore Academy Secondary School</u>	<u>Stadium Island</u>	<u>Newham</u>	<u>Secondary school for 1160 students</u>
<u>Mossbourne Academy Riverside Primary School</u>	<u>Hackney Wick, adjacent to Here East</u>	<u>Hackney</u>	<u>Three-form entry primary</u>

**Table 5**

Planned Schools	Secured Through	Site Allocation	Notes
<u>Secondary school at Rick Roberts Way, Stratford</u> <u>Potential for Primary School at Rick Roberts Way</u>	<u>Legacy Communities Scheme Planning Permission</u> No existing planning permission	SA3.6	<u>Identified in 2015 Local Plan as potential school site. May be required in second half of Plan Period. Requirement</u>

			to be identified at the time based on <u>monitoring of need.</u>
Hackney Wick, adjacent to Here East	Legacy Communities Scheme Planning Permission	SA1.7	Three form entry primary scheduled to open in September 2015 or 2016
Fish Island East/Sweetwater	Legacy Communities Scheme Planning Permission	SA1.8	Three form entry primary school scheduled to open in September 2016
Bromley-by-Bow	No existing planning permission	SA4.1	Two-form entry
<u>Sugar House Island</u>	<u>Existing planning permission REF: 12/00336/LTGOUT/LBNM</u>	<u>SA4.2</u>	<u>Two-form entry</u>
Neptune Wharf	<u>Existing outline planning permission REF: 12/00210/OUT</u>	<u>SA1.64</u>	<u>Three-form entry</u>

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## Section 6: Creating a high quality natural and built environment

	Type of change	Proposed change
Page 69, para 6.1	Minor	The unique interplay of green spaces, waterways and the built environment shape and unify the diverse places that make up the Legacy Corporation area. Many local places have undergone great change in recent years, with <u>areas of mainly industrial land and buildings being replaced by Westfield Stratford City and the Queen Elizabeth Olympic Park, Stratford City and other large scale mixed use development</u> . This has delivered huge benefits to the area and east London more widely, creating new high-quality environments while reviving and adding to the strategic network of green infrastructure.
Page 69, para 6.1	Minor	Paramount to the Legacy Corporation’s vision is planning for sustainable places and communities. To achieve this, the focus of change is shifting towards the places surrounding Queen Elizabeth Olympic Park. As the urban structure of these places evolves, the Local Plan will ensure that change to their form utilises the assets of the <u>Park area</u> to deliver sustainable development of the highest quality. Development will integrate local features of environmental and heritage value and create distinctive environments in which people can live, work and play. Convergence is at the heart of this agenda.
Objective 3	No change	No change to objective proposed
Policy SP.3 Integrating the natural and built environment	No change	No change to policy proposed
Policy BN.1: Responding to place	Minor	Proposals <u>for development</u> will be considered acceptable where they respond to place in accordance with the principles outlined below: <ul style="list-style-type: none"> <li>1. Landscape and water: <del>relate well to</del> <u>respect and enhance</u> the local area’s defining natural and man-made landscape features, in particular the linear form of waterways and parklands</li> <li>4. Connectivity: ensure that new and existing places link to route networks and facilitate movement along direct, permeable, safe and legible pedestrian and cycle routes, <u>as well as connect habitats to provide wildlife corridors</u>. Routes should cater for the requirements of all users. Opportunities to connect areas to strategic road, rail, bus and cycle networks must be utilised</li> <li>7. Amenity and wellbeing: minimise impact within proposed and upon existing development, by preventing overshadowing, <u>mitigating noise and air</u></li> </ul>



	Type of change	Proposed change
		<u>pollution</u> and an unacceptable provision/loss of sunlight, daylight or privacy.
Page 73, paragraph 6.10	Minor	Change reference to LLDC Design Quality Policy to the 2018 version.
Page 73 Case Study 7	Replacement	Replace case study
Policy BN.2: Creating distinctive waterway environments	No change	No change to policy proposed.
Paragraph 6.13	Minor	<p><b>Policy application Becomes Para 6.13</b></p> <p>When developing proposals for specific locations, reference should be made to the guidance published by the Environment Agency, the Canal and River Trust and the Lee Valley Regional Park Authority, as well as to relevant Biodiversity Action Plans (BAPs) and the opportunities set out in the Olympic Legacy Waterways Framework. <u>The most recent version of the Town and Country Planning Associations Policy Advice Note: Inland Waterways (2009) should also be referred to.</u> These outline suitable locations for various functions and locally specific ambitions for these environments. In support of the aims of the Thames River Basin Management Plan (TRBMP) and Water Framework Directive, all developments along the waterways will need to integrate Sustainable Drainage Systems (SuDS), including the use of oil and petrol interceptors, effective setbacks from watercourses, the naturalisation of the banks and other measures that will improve the management of surface water run-off. <u>Where works are proposed within 8 metres of a main river, a separate formal consent will be required from the Environment Agency.</u> In order to achieve locally distinctive environments while optimising their functionality, proposals will need to integrate local heritage features and environmental assets, helping create unique and sustainable environments for local communities, visitors and wildlife alike.</p>
Policy BN.3: Maximising biodiversity	Minor	<p>The Legacy Corporation will work with its partners to ensure that biodiversity is protected and enhanced and new habitats are created <del>ensure the protection and enhancement of biodiversity</del>, within open space, parks and built-up neighbourhoods. Development proposals will be required to:</p> <p>1. Maximise opportunities to protect and enhance biodiversity</p>

	Type of change	Proposed change
		<p>2. Provide a net gain in the extent of <u>good quality habitat suitable for species to thrive</u></p> <p>3. Integrate habitat <u>into the existing network</u> and other measures that will support biodiversity <u>across the built environment and open spaces</u></p> <p>4. Ensure measures are taken to conserve, <del>and</del> promote and <u>designate</u> Sites of Importance for Nature Conservation where relevant ...</p> <p>8. Ensure major <u>planning</u> applications are accompanied by a Biodiversity Statement <u>facilitating a net gain in biodiversity through any proposals.</u></p>
Page 77, Para 6.15, Policy application	Minor	<p>Policy will be important to protect and maximise biodiversity, extending the amount of habitat suitable for species to thrive. In built-up areas, where there is less opportunity to introduce large areas of open space, it will be necessary to maximise green infrastructure by channelling it through built structures, streets and open spaces, joining up wildlife corridors and utilising urban greening initiatives such as green roofs, green walls, nest boxes and rain gardens. <u>The urban greening should be planned to promote connectivity through the urban landscape, facilitating species movement. Applications for major development schemes will be expected to provide the appropriate, high-quality and well maintained urban greening, as a fundamental element of site and building design meeting the Urban Greening Factor target score as set in the Draft New London Plan Policy G5 Urban Greening.</u> Proposals for development will <u>also</u> be expected to complement existing ecological networks taking into account the priority species, habitats and targets within the relevant Biodiversity Action Plans (BAPs).</p>
Policy BN.4: Designing residential schemes	Major	<p><b>Policy BN.4: Designing development <del>residential schemes</del></b></p> <p><u>All residential development (including residential development within mixed-use development) should achieve the highest possible standards and quality in both design, construction and use. To achieve this:</u></p> <p><u>1. All residential development will be required as a minimum to meet the Nationally Described Space Standards – Technical Requirements; and</u></p>

	Type of change	Proposed change
		<p><del>2. All mixed-use and residential development should take account of the best practice guidance in the Legacy Corporation Design Quality Policy; Proposals will be considered acceptable where residential elements meet the ‘Baseline’ Quality and Design Standards outlined within Annex 1 of the Mayor of London’s Housing Supplementary Planning Guidance (November 2012) (“Annex 1 Baseline Standards”), including any future revisions or superseding guidance save that the following elements of Annex 1 Baseline Standards shall not apply:</del></p> <p><del>1. To any elements of the Annex 1 Baseline Standards that are addressed by the Nationally Described Space Standards – Technical Requirements unless they are equivalent.</del></p> <p><del>2. Any elements of the Annex 1 Baseline Standards that are addressed by other policies in this Plan.</del></p> <p><u>Proposals for incorporating residential development must also demonstrate that a high standard of liveability will be achieved by:</u></p> <p>3. <u>Contributing</u> towards the creation of distinctive, integrated, legible, connected and sustainable places;</p> <p>4. <u>Exhibiting</u> the principles of good design, by incorporating high-quality landscape and architectural design, including high-quality materials (<u>that age well over-time</u>), finishes and details;</p> <p>5. <u>Minimising adverse impacts</u> upon existing surrounding development and not resulting in an unacceptable loss of privacy or an <del>unreasonable</del> <u>unacceptable</u> degree of overlooking towards habitable rooms and private amenity spaces within or around existing development;</p> <p>6. <u>Demonstrating</u> that the scheme will receive <u>acceptable</u> <del>adequate</del> levels of daylight and sunlight, and that existing surrounding development will not experience an unacceptable loss of sunlight and daylight in accordance with Site Layout Planning for Daylight and Sunlight (Building Research Establishment, 2011), including any future revisions or superseding guidance; <u>and</u></p> <p>7. <u>Ensuring</u> surrounding open spaces receive adequate levels of daylight and sunlight.</p> <p><u>In meeting the above, all mixed use and residential developments should:</u></p>

	Type of change	Proposed change
		<p><u>8. Respect the scale and grain of their context</u></p> <p><u>9. Relate well to street widths and make a positive contribution to the streetscape</u></p> <p><u>10. generate an active street frontage</u></p> <p><u>11. Incorporate sufficient, well designed and appropriately located communal and private amenity space</u></p> <p><u>12. Contribute to defining any existing or identified new public routes and spaces;</u></p> <p><u>13. Promote legibility of the site; and</u></p> <p><u>14. Where relevant, preserve or enhance heritage assets and the views to/from these, and contribute positively to the setting of heritage assets, including conservation areas.</u></p> <p><u>Development not incorporating residential use should also take account of the Legacy Corporation Design Quality Policy and meet the relevant principles in this policy (3-14 above) to demonstrate that it achieves an acceptably high quality and contributes positively to its context. Alterations and extensions to non-residential buildings should respect the scale, proportions and materials used in the existing building. All Major development schemes should demonstrate an acceptably high quality, through independent design review undertaken by a panel appointed by the Local Planning Authority.</u></p>
Paragraph 6.16	Minor	<p><b>Reasoned justification</b></p> <p><b>6.16</b> It is imperative that <del>residential</del> development within the Legacy Corporation area provides a liveable environment for its occupants and users, and exhibiting the principles of good design that are set out within government-endorsed publications such as the 'Urban Design Compendium' (HCA, 2000) and 'By Design' (DETR, 2000). <u>The Legacy Corporation area continues to develop with a significant number of entirely new areas, and other locations that are changing within the context of their existing and historic character. Against this background, it is important that the design of new development contributes to making these places successful, achieves high standards of amenity and supports and enhances a sense of community and neighbourliness.</u></p>

	Type of change	Proposed change
Paragraph 6.17	Minor	<b>Becomes Para 6.17</b> In order to ensure this, relevant London Plan Supplementary Planning Guidance will be applied in assessing proposals for residential development. The London Plan forms part of the adopted Development Plan for the Legacy Corporation area and the Mayor is clear that development within it must achieve exemplary design and sustainability standards. <u>The Legacy Corporation also publishes a Design Quality Policy. This sets out best practice guidance which is primarily aimed at achieving the best possible design outcome in its own development schemes, underlining its commitment to achieving the highest quality of design. The guidance within the Design Quality Policy also provides a helpful benchmark for other new development in the Legacy Corporation area and is a useful reference point for applicants in demonstrating that a proposed scheme’s quality meets the expected standards.</u>
Paragraph 6.19	Minor	<b>Becomes Para 6.19</b> It is expected that applications for residential development show how the <u>Nationally Described Space Standards – Technical Requirements required by the policy have been met. Reference to the relevant elements of the Legacy Corporations Design Quality Policy may also help to demonstrate that new residential development reaches the highest achievable design quality and liveability. The current version of the Design Quality Policy can be downloaded from the Legacy Corporation website. The evidence required as part of a planning application to demonstrate how a proposal meets the requirements in this policy will be proportionate to the size of the development proposal and its potential impacts, with applications for minor development relying on an assessment of the application detail.</u> <del>The Baseline Standards are those endorsed by the Mayor as addressing issues of particular strategic concern and set the baseline for quality and design that new homes should meet. The extent to which proposed developments depart from this baseline will be taken into account when making planning decisions. Proposals for development that depart significantly, either in terms of failure to meet a number of baseline standards, or where failure to meet particular baseline standards has a negative material effect on the proposed scheme or surrounding development, are unlikely to be acceptable.</del>
New para	Minor	<b>Becomes Para 6.20</b> Design review will be an important element of demonstrating that Major schemes successfully exhibits acceptable design. <u>Design review is also encouraged for other schemes that are likely to have a significant impact on their surroundings to help provide evidence of the appropriateness of the proposed design. Detailed discussion of scheme design at the pre-application</u>

	Type of change	Proposed change
		<u>stage is strongly encouraged along with use of formal review by the Legacy Corporation Quality Review Panel. Information on the Legacy Corporations Quality Review Panel, who would usually undertake the formal design review function, can be found at paragraph 14.4 of the Plan.</u>
New para	Minor	<b>Becomes Para 6.21</b> <u>The successful integration of business and commercial floorspace into mixed use development will be an important factor in ensuring mixed use schemes are acceptable. Guidance is provided in particular in the Hackney Wick and Fish Island SPD (March 2018) and the LLDC Employment Space Study (2015).</u>
Policy BN.5: Requiring inclusive design	Minor	<b>Becomes Policy BN.6 - delete the final paragraph of the policy:</b>  “ <u>The relevant elements of the Mayor of London’s Housing Supplementary Planning Guidance (November 2012)(“Annex 1 Baseline Standards”), will only be applied where they are equivalent to the Optional Requirements in Part M of the Building Regulations (as applied by this policy).”</u> ”
Paragraph 6.21	Minor	<b>Add the following to the end of the paragraph (now Para 6.31):</b>  “ <u>To ensure that the design of buildings and their surroundings ensure they are accessible to all it will be important to take into account the needs of those with sensory or cognitive impairments.”</u> ”
Policy BN.6	No change	<b>Becomes Policy BN.7 – no changes to policy proposed</b>
Para 6.23	Minor	<b>Policy application 6.33 -</b> <u>The draft New London Plan Policy 7.17 G3 defines MOL and provides MOL with the same status and protection as Green Belt, supporting its current extent and extension in appropriate circumstances. MOL provides the same level and type of protection to coherent areas of open land within London as is provided by Paragraphs 87 to 91 of the National Planning Policy Framework. Unless ‘very special circumstances’, as defined by the NPPF, can be demonstrated, development within areas designated as MOL will not be permitted. Proposals for loss of MOL that include provision of new open space elsewhere will not be accepted as meeting the ‘very special circumstances’ test.</u>
Policy BN.7	Minor, associated to the policy.	<b>Becomes Policy BN.8</b> <u>New Figure 11 Map of Local Open Spaces and Annex 4 listing each open space and its identified function(s).</u>
Paragraph 6.25	Minor	<b>Policy application 6.35</b> <u>The Policies Map identifies the Local Open Space in this area that will be protected over the Plan period, in addition to open amenity space and other elements of the green infrastructure network outlined</u>

	Type of change	Proposed change
		<p>within this Local Plan. <u>In addition to the Policies Map, Figure 14 also shows the location of these Local Open Spaces and Annex 4 lists each of these and describes their primary function.</u> The protected spaces are based on <u>previous assessments of open space which have been reviewed and amended through production of new Open Space and Play Space Assessment (2018).</u> <del>equivalent designations in earlier adopted plans and open spaces resulting from development proposals linked to the legacy of the London 2012 Olympic Games.</del> The map at Figure 15 (page 83) identifies additional Local Open Spaces that have planning permission or are included within a Site Allocation set out in this Local Plan, but have not yet been developed. Where there is an identified deficiency in a type of public open space, new developments will address this through the provision of open space in accordance with the London Plan Benchmark Public Space Hierarchy and other aspirations for maximising green infrastructure set out within this Local Plan.</p>
Policy BN.8: Maximising opportunities for play	Minor	<p><b>Becomes Policy BN.9</b> Major development proposals will be required to improve or provide new play and/or <u>youth space</u><sup>1</sup>, maximising opportunities for play and informal recreation. New play spaces should create high-quality, dynamic and stimulating play space. Proportionate to the size of development, proposals are expected to:</p> <ol style="list-style-type: none"> <li>1. Take account of the existing or future need for play space in the local area, identifying relative deficiencies and incorporating the most appropriate type of new play space into the proposal</li> <li>2. Be of an appropriate size and design for the age of children and <u>young people</u> whom the space is designed to serve, <u>including older children and teenagers, who have their own distinct needs</u></li> <li>3. Be inclusive and accessible to children and <u>young people</u> of all abilities ...</li> </ol> <p><b>Cross-reference to policies: BN.2; BN.6; BN.7; BN.8</b>  <b>London Plan polices: S4; Mayor’s Shaping Neighbourhood Plan and Recreation SPG</b></p>
Para 6.28 –	Minor	<p><b>Policy application, becomes Para 6.38</b>  The Mayor’s Shaping Neighbourhoods Play and Informal Recreation SPG (2012) sets out a requirement for 10 sqm of play space per child aged 0 to 17, with a requirement for proximity to play space, characteristics <u>and location that is dependent on age.</u> <u>In developing proposals for a new youth</u></p>

<sup>1</sup> The Mayor’s Shaping Neighbourhoods Play and Informal Recreation SPG 2012 describes youth space as: "a place where young people aged 12 and above can meet and take part in informal sport-based activities and other informal recreation".

	Type of change	Proposed change
		<u>space, young people should be meaningfully engaged from an early stage to ensure its relevance to the users and general success. Youth facilities should also be provided where possible.</u> Significant regeneration in the area presents further opportunities to improve the quantity and quality of play and <u>youth</u> space. Maximising these opportunities will help to meet local needs, creating dynamic and stimulating child and <u>youth</u> friendly places that contribute to the green infrastructure network and wildlife corridors across the area.
Page 87, Case Study 9	Replacement	Replace case study
Policy BN.9: Protecting key views	Minor	<b>Becomes Policy BN.10</b> Proposals for development that impact a key view will only be considered acceptable when the development makes a positive contribution to the characteristics and composition of that view. Key views are identified in Figure 18. <u>Development proposals should take account of view 9 of the London View Management Framework (2012) where relevant.</u>
Policy BN.10: Proposals for tall buildings	Major	<b>Becomes Policy BN.5: Proposals for tall buildings</b> Tall buildings should be located within the Centre boundaries outlined within this Local Plan. In order of hierarchy, these are: <ul style="list-style-type: none"> <li>• Stratford Metropolitan Centre (parts within the Legacy Corporation Area)</li> <li>• Bromley-by-Bow District Centre</li> <li>• Hackney Wick Neighbourhood Centre</li> <li>• Pudding Mill Local Centre</li> <li>• East Village Local Centre.</li> </ul> Tall buildings are defined by the Legacy Corporation as those that are higher than a Sub Area’s prevailing or generally expected height as set out in this Plan <u>at tables 9, 10, 11 and 12.</u> Proposals for tall buildings <u>both inside and outside the Centre boundaries will be assessed against the criteria in this policy and Policies BN.1 and BN.4.1–13 of Policy BN.10.</u> <u>Outside of the Centre boundaries, unless a location for tall buildings is identified within a site allocation within this Plan, more than minor exceedance of the prevailing or generally expected height identified in tables [9, 10, 11,12] will need to demonstrate that, in addition to meeting the criteria in this policy and policies BN.1 and BN.4, the proposal would achieve significant additional public benefit.</u>  <u>Subject to the above P</u> proposals for tall buildings will be considered acceptable where they <u>exhibit exceptionally</u>



	Type of change	Proposed change
		<p><u>good design, demonstrating this through independent design review undertaken by a panel appointed by the Local Planning Authority. To achieve this, they must, in addition to meeting requirements of Policies BN.1 and Policy BN.4, demonstrate:</u></p> <ol style="list-style-type: none"> <li>1. <u>An appropriate proportion, form, massing, height and scale in context with the character of its surroundings;</u></li> <li>2. <u>Use of material appropriate to the height of the building;</u></li> <li>3. <u>Acceptable access and servicing arrangements;</u></li> <li>4. <u>A positive contribution to the public realm at ground level;</u></li> <li>5. <u>A positive contribution to the surrounding townscape; and</u></li> <li>6. <u>Creation of new or an enhancement to existing views, vistas and sightlines where there is an opportunity to do so.</u></li> </ol> <p><u>Proposals for groups of tall buildings will need to demonstrate an appropriate relationship with each other and to the wider surrounding area, including the value of the group of buildings to longer distance views in addition to the immediate context.</u></p> <p><u>Outline planning applications for tall buildings will only be considered as an acceptable approach where the application is accompanied by a sufficiently detailed design code, coordinated with parameter plans, with these secured as part of any planning permission.</u></p> <p>Proposals for tall buildings that are likely to have a significant adverse impact on one or more of the following will be considered unacceptable:</p> <ol style="list-style-type: none"> <li>7. <u>Micro-climatic conditions (specifically down-draughts and lateral winds over public or other amenity spaces)</u></li> <li>8. <u>Impacts to Amenity of the surrounding area (including open spaces and other buildings and waterways) that relate to: overlooking, daylight, overshadowing, light spill/reflection and wider amenity impacts</u></li> </ol> <p>Existing views of landmarks, parkland, heritage assets, waterways, and views along street corridors (in accordance with the Policy BN.9 on Protecting Key Views).</p>
Para 6.34 (Para 6.36)	Major	<p><b>6.24</b> Tall buildings are defined in the London Plan as “those that are substantially taller than their surroundings, cause a significant change to the skyline or are larger than the threshold sizes set for the referral of planning applications to the Mayor” (i.e. 30 metres within the Legacy Corporation area). In accordance with this, Policy BN.5 defines tall buildings as those that are higher than a Sub Area’s</p>

	Type of change	Proposed change
		<p>prevailing or generally expected height as set out in <u>tables (9, 10, 11 and 12) of the Sub Area sections in this Plan</u>. Policy BN.10 5 will ensure that only acceptable tall building proposals (i.e. those that meet the policy criteria) are approved. The prevailing height of each Sub Area is defined in <u>tables [9, 10, 11 and 12) in the Sub Area sections and based upon general heights rather than the tallest existing element. Proposals for tall buildings outside the Centre boundaries and the relevant site allocations that are higher than their surrounding context, in applying the policy, are much less likely to be considered acceptable. Some minor exceedances to the prevailing or generally expected heights may be justified where it can be demonstrated that this will make a proposal workable and aid achievement of an acceptable design quality. Apart from such minor exceedances, proposals for tall buildings outside of the centre boundaries (or other locations identified as acceptable in a site allocation within this Plan) will need to demonstrate that the design and use of the building and the treatment of the public realm, along with the effects on the surrounding context, will be positive and result in a significant public benefit that would not be achieved by a lower height of building. will also be assessed against Criteria 1–13 of Policy BN.10.</u></p>
Para 6.35 (Para 6.37)	Minor	<p><b>6.25</b> Proposals for development should not incorporate low floor-to-ceiling heights in order to avoid triggering Policy BN.10, or to meet the requirements of it. Figure 18. illustrates how two buildings with the same number of storeys can differ in height. This is <u>usually</u> due to commercial buildings incorporating storeys with greater floor-to-ceiling heights.</p>
New paragraph	New paragraph	<p><b>6.27</b> <u>Outline planning applications for tall buildings are not encouraged as the acceptability of tall building proposals are much more likely to depend on their detailed design in order to demonstrate overall acceptability. For those occasions when an outline application is considered to be necessary and justified, a sufficiently detailed design code will need to be produced to provide evidence of the principles that will be applied in the detailed design and to demonstrate acceptability of the proposal. The design code and its implementation will be secured as part of any planning permission and is likely to include a commitment to securing and retaining an appropriately skilled design team and sufficient measures to ensure the monitoring of the design implementation during construction, to ensure that the original design intent is achieved.</u></p>

	Type of change	Proposed change
Para 6.36	Minor	<b>6.28</b> Policy BN.10 <u>5</u> should result in a mixture of intermittent buildings heights which together form a unique arrangement that contributes <u>positively to that part of the area's built form</u> . The Legacy Corporation area will have one of the highest concentrations of tall buildings within Greater London, and therefore it is important that they incorporate the highest standards of sustainability and design.
New paragraph	Major	<b>New para 6.29</b> <u>Design review will be an essential element of demonstrating that a scheme successfully exhibits exceptional design. It will also be important to ensure that the exceptional design quality achieved at planning application stage is not lost in the delivery of tall buildings. A best practice approach to selection and appointment of the design team and the development of the design approach is encouraged to aid a positive outcome through design review. A planning obligation will be sought to either retain the original design team or ensure that an adequately skilled design team is appointed for the delivery phase of the scheme to ensure that the original design intent is achieved.</u>
Policy BN.11 Reducing noise and improving air quality	Minor change/split of policy to separate air quality and noise policies	<p>Split into two separate policies:</p> <p><b>Becomes Policy BN.11 – Air Quality</b></p> <p><u>Development Proposals should contribute to improving air quality through the approach taken to energy use and energy efficiency and minimising the need for travel.</u></p> <p><u>Development should:</u></p> <ol style="list-style-type: none"> <li>1. Be constructed and designed, <u>including appropriate use of green infrastructure</u>, in a manner that minimises emissions of pollutants to the air</li> <li>2. Demonstrate compliance with policies in this Local plan and the London Plan which contribute to minimising the effects of emissions to the air</li> <li>3. Have appropriate regard to the relevant London Borough and the <u>London Environment Strategy 2018 and Mayor's Air Quality Strategy</u>.</li> </ol> <p>Applications for major development proposals <u>must be at least air quality neutral and</u> include an air quality assessment where they are located in areas identified in the most recent strategies and guidance as being significantly affected by poor air quality (by being adjacent</p>

	Type of change	Proposed change
		to or within an Air Quality Management Area <u>or London Plan Air Quality Focus Area</u> ).  <b>Cross-reference to policies: SP.5; S.4; <del>S.7</del>; S.9; T4</b> <b>London Plan polices: S11; T2; T4</b>
Paragraphs 6.37 and 6.38	Minor change	<b>Reasoned justification – becomes Para 6.42 and 6.43</b>  Air quality <del>and noise</del> have a significant role to play in the health and wellbeing of communities and the prospects of the natural environment, reducing both life expectancy and biodiversity in heavily polluted areas. Major roads within the area including the A12 and Stratford High Street contribute to poor air quality <del>and generate noise</del> .  The surrounding London Boroughs have identified locations in this area where there is a significant incidence of poor air quality, designating Air Quality Management Areas and preparing Air Quality Management Plans. It will be important that this Local Plan contributes to the Mayoral target to bring London’s air quality in line with EU limit values for local pollutants and in particular PM10, PM2.4 and NO <sub>2</sub> , as expressed in the EU Air Quality Directive and implemented in the UK through the 2010 air quality regulations.
Page 92, Para 6.39	Minor change	<b>Policy application, becomes Para 6.44</b>  The relevant policies in this Local Plan will work together to contribute to the strategy of achieving at least no worsening in air quality <del>and noise quality</del> and, where it is achievable, result in an improvement, as outlined in the <del>Mayor’s Air Quality Strategy, Ambient Noise Strategy</del> <u>London Environment Strategy (May 2018)</u> and relevant Borough Action Plans. However, as the measures necessary to address poor air and noise quality are not confined to one planning authority area and are often governed by processes that fall outside of planning, such as building regulations, development proposals are also subject to separate regulatory regimes and legislation.
New Policy BN.12 – Noise	Major change /split of policy to separate air quality and noise policies	<b>Becomes Policy BN.12 – Noise</b>  <u>Development proposals should contribute towards minimising the effects of noise on amenity of the occupiers and users of existing and planned new development within the Legacy Corporation Area. New development should be constructed to:</u> 1. <u>Minimise exposure to the adverse impacts of noise</u>

	Type of change	Proposed change
		<ol style="list-style-type: none"> <li>2. <u>Demonstrate compliance with policies in this Local plan and the London Plan that contribute to minimising the effects of noise</u></li> <li>3. <u>Have appropriate regard to the London Environment Strategy (May 2018)</u></li> <li>4. <u>Demonstrate compliance with the Agent of Change Principle by mitigating and managing noise impacts from new development and designing new development to minimise the effects on occupiers and users from existing noise sources.</u></li> </ol>
New Para	Major change	<p><b><u>Reasoned justification</u></b></p> <p><b><u>6.45</u></b> <u>Noise pollution is one of the main agents of loss of quality of life and environmental quality in an urban area. Prolonged exposure to elevated sound levels can have a detrimental effect on the health and wellbeing of the community and the prospects of the biodiversity and natural environment. Reducing and minimising exposure to noise has a potential to improve wellbeing, promote the enjoyment of the natural environment and enhance biodiversity at a local level, contributing to a healthier and more pleasant place to live and work.</u></p>
New Paras 6.46 and 6.47	Major change	<p><b><u>Policy Application</u></b></p> <p><b><u>6.46</u></b> <u>Policy BN.12, in conjunction with other relevant policies within this Local Plan, including planning for minimising travel demand and increasing opportunity for walking and cycling, will work together to achieve improvements to the acoustic environment and reduce exposure to high levels of noise.</u></p> <p><b><u>6.47</u></b> <u>Noise management should be an integral part of any development proposal and considered at an early stage of the design process. Proposals for new development should demonstrate compliance with the Agent of Change Principle whereby the onus is on the new development to ensure its building or activity is designed to protect existing occupiers and users from noise and minimise impacts from existing noise sources.</u></p> <p><b><u>6.48</u></b> <u>In determining whether noise is likely to be an issue to be addressed by development proposals and to determine the appropriate approach to assessing and addressing noise effects, reference should be made to the most up to date guidance within the national Planning Practice Guidance.</u></p>

	Type of change	Proposed change
Policy BN.12: Protecting archaeological interest	No change/updated map	<b>Becomes Policy BN.13</b> - Update Figure 19 to reflect the updates to borough Archaeological Protection Area map.
Policy BN.13 Improving the quality of land	Major change	<p><b>Becomes Policy BN.14</b></p> <p>To prevent harm to health and the environment from the effect of contamination and the release of pollutants and to bring <u>land affected by contamination</u> into beneficial use, development proposals will be required to <u>ensure</u> that:</p> <ol style="list-style-type: none"> <li><u>1. The site is suitable for its new use, taking into account of ground conditions, including pollution from previous uses and any proposals for mitigation such as land remediation</u></li> <li><u>2. Adequate site investigation information, prepared by a competent person, is provided</u></li> <li><u>3. After remediation, as a minimum, the site is not capable of being determined as contaminated land under Part IIA of the Environmental Protection Act 1990</u></li> <li><u>4. Drainage methods are suitable for the site conditions and protect groundwater</u></li> <li><u>5. Suitable measures are taken where construction works are carried out.</u></li> </ol> <p><del>1. Demonstrate that land is of an appropriate standard for the proposed end use</del></p> <p><del>2. Ensure drainage methods are suitable for the site conditions and protect groundwater</del></p> <p><del>3. Ensure suitable measures are taken where construction works are carried out</del></p> <p><del>4. Take account of the impacts from any existing consented hazardous substances installation.</del></p> <p><u>Applications for all sites potentially affected by contamination should be carried out in accordance with established procedures as required in the NPPF.</u></p> <p><del>Where land is likely to be contaminated, applicants will be required to carry out a site investigation in line with paragraph 121 of the NPPF to identify existing or potential contaminants and pollution pathways. Where a site investigation has identified the presence of or potential for contamination, applicants will be required to carry out a Contaminated Land Assessment to identify measures required to remediate any contamination to a suitable level.</del></p>
Paragraph 6.43	Minor	<b>Policy application 6.52</b>

	Type of change	Proposed change
		<p>In some cases, significant work has been undertaken to clean up <del>contaminated</del> land <u>affected by contamination</u> <del>to remove</del> or reduce that risk, or to make it suitable for any new use that is being proposed. The area of Queen Elizabeth Olympic Park, for example, has been subject to significant remediation work which made it suitable for the staging of the 2012 Games <u>and Legacy land-uses</u>. <del>However, m</del> <u>Many of the sites that may become available for new development within the Legacy Corporation area are likely to require some form of investigation, assessment testing and appropriate remediation and verification work to make them suitable for development, including appropriate proposals for drainage and construction. The Proposed Planning Approval Process diagram outlines the typical requirements for new schemes. Other specific planning processes may apply to existing permissions within the Legacy Communities Scheme Revised Global Remediation Strategy (2012) sets out the requirements for new schemes. Where there is the potential for significant ground contamination to be present, an intrusive site investigation and contamination assessment report may also be required to be submitted with the planning application, in addition to the preliminary risk assessment report. While this policy identifies the approach to information that should be provide with any relevant planning application, it is also the developer's or landowner's responsibility for securing a safe development. In keeping with National Standards, it will be important to carry out Site Investigations and produce Contaminated Land Assessments when appropriate to make land suitable for use.</u></p>
Policy BN.14: Designing residential extensions	Minor	<p><b>Becomes Policy BN.15</b> 5. Do not reduce natural daylight and sunlight reaching neighbouring properties to an unacceptable level.</p>
Policy BN.15: Designing advertisements	Minor change	<p><b>Becomes Policy BN.16</b> <b>Policy application 6.56</b></p> <p>...Policy BN.156 will ensure that advertisements are well designed and protect the interests of amenity and public safety.</p>
Policy BN.16: Conserving or enhancing heritage assets	Major	<p><b>Becomes Policy BN.17</b></p> <p>Update Policy BN.16 and delete Policy 1.4 as follows:</p> <p><b>Policy BN.17: Conserving or enhancing heritage assets</b> Proposals will be considered acceptable where they conserve or enhance heritage assets and their settings, and</p>

	Type of change	Proposed change
		<p>promote the significance of those assets by incorporating viable uses consistent with their conservation and heritage-led regeneration.</p> <p><u>In particular, proposals for development within the boundary or immediate setting of heritage assets (see Figure 21), will be considered acceptable where they:</u></p> <ol style="list-style-type: none"> <li><u>1. Preserve or enhance the special architectural or historic interest that has been identified within the appraisals of those heritage assets, in particular historic buildings, structures, yards, waterways and the pre-war residential and industrial street patterns or other characteristics that give that area its unique character</u></li> <li><u>2. Enhance and reveal the significance of heritage assets, including the waterways, such as the Lee Navigation and Hertford Union Canal</u></li> <li><u>3. Restore and reuse heritage assets located within application boundaries as part of new development and are accessible to all.</u></li> <li><u>4. Exhibit an understanding of and reference the architectural and historic interest of the area within their design</u></li> <li><u>5. Retain street trees and/or provide these, where appropriate.</u></li> </ol>
Paragraph 6.49	Minor	<p><b>Policy application 6.58</b></p> <p>Designation of an area as a Conservation Area does not stop development being promoted. However, it does introduce extra protections against inappropriate forms of development. If a proposal is submitted that affects the land within the boundary or setting of a Conservation Area or heritage asset, the Legacy Corporation will pay special attention to the desirability of preserving or enhancing the special architectural or historic interest of that area or asset when making a planning decision. Policy BN.167 also applies to heritage assets situated outside Conservation Areas, such as Statutory Listed Buildings, Locally Listed Buildings or Buildings of Townscape Merit. <del>Heritage assets are identified within the Sub Area sections of this Local Plan alongside further policies concerning their preservation and enhancement.</del> Proposals should be in general conformity with Conservation Area appraisals and Management Plans, and other relevant Guidelines.</p>



## Section 7 – Securing ~~the~~ transport infrastructure to support growth

	Type of change	Proposed change
Objective 4	Minor	<p>Objective 4: Secure the infrastructure required to support growth and convergence</p> <p>This will mean:</p> <ul style="list-style-type: none"> <li>• Working with partners to secure the infrastructure identified within the Local Plan and Infrastructure Delivery Plan</li> <li>• Reviewing regularly infrastructure need to ensure the identified requirements remain up to date</li> <li>• Use Section 106 Agreements, <u>in line with the Planning Obligations SPD</u>, and the Community Infrastructure Levy to contribute towards infrastructure delivery</li> <li>• Managing the effects of new development</li> <li>• Improving local connectivity, including delivery of new bridges and routes to maximise walking and cycling.</li> </ul>
SP.4: Planning for and securing transport and utility infrastructure to support growth and convergence	Minor	<p>Strategic Policy SP.4: Planning for and securing transport <del>and utility</del> infrastructure to support growth and convergence</p> <p>The Legacy Corporation will work with its partners to promote and deliver the infrastructure necessary to support the growth and development identified within this Local Plan and its Infrastructure Delivery Plan.</p> <p>In particular, the Legacy Corporation will support the provision of the following <del>types on infrastructure areas of</del> <u>transport infrastructure</u>:</p> <ol style="list-style-type: none"> <li><del>1. The expansion of electronic communication networks, including telecommunications and high-speed broadband</del></li> <li>1. Public transport infrastructure and services that will help to deliver the growth objectives set out within this Local Plan, including those that will improve international, national, regional and local connectivity – <u>this will include prioritising work to improve capacity and access at Stratford Station</u></li> </ol>

	Type of change	Proposed change
		<p>The Legacy Corporation will safeguard land for the provision of the following infrastructure:</p> <ol style="list-style-type: none"> <li>2. DLR double racking between Bow and Stratford</li> <li><del>3. Crossrail 1</del></li> <li>3. Crossrail 2 (existing safeguarded corridor and any updated route)</li> </ol> <p>The Legacy Corporation will require the retention of:</p> <ol style="list-style-type: none"> <li><del>4. Existing waste management facilities (subject to the provisions of Policy IN.2)</del></li> <li>4. Existing public transport infrastructure.</li> </ol> <p>The Legacy Corporation will use its Community Infrastructure Levy funding to help deliver the infrastructure on the CIL Infrastructure list. Where appropriate and lawful, infrastructure or contributions towards its delivery will also be secured through the use of Planning Obligations.</p>
Paragraph 7.1	Minor	<p>Infrastructure is a broad term and can range from energy infrastructure, roads and bridges, transport and communications networks to health facilities, libraries, community centres and schools. New community infrastructure and schools are dealt with in policies in the ‘Providing housing and neighbourhoods’ section <u>and utilities and communications networks in the ‘Creating a sustainable place to live and work’ section</u> of this Local Plan. <u>The IDP process and Infrastructure Liaison Group referred to below, the forums and consultation processes with the boroughs and the Project Proposals Group (PPG) will also be used to ensure they come forward alongside development.</u> The same applied to new and improved open space and projects such as the Lea River Park and Leaway, which are covered by policies BN.7<u>8</u> and T.10.</p>
Paragraph 7.4	Minor	<p>The Legacy Corporation is committed to working alongside the Boroughs to bring forward the infrastructure set out in the IDP, <del>and an Infrastructure Liaison Group has been established by the Legacy Corporation and the Boroughs which meets on a quarterly basis to do this.</del> In its role as landowner, the Legacy Corporation is also bringing forward infrastructure, such as the new schools that will be provided as part of the Legacy Communities Scheme Section 106 Agreement. The existing cooperative arrangements working arrangements between the Legacy Corporation and infrastructure providers, including the Growth Boroughs, will be used to ensure the long term security of</p>

	Type of change	Proposed change
		infrastructure within the Legacy Corporation area. The development management process will be used to secure new infrastructure through planning obligations and the Community Infrastructure Levy (CIL). A Planning Obligations SPD will provide advice on how it will use Section 106 Agreements alongside CIL and the Local Plan to secure its planning policy objectives. The 'Delivery and implementation' section of this Local Plan provides more information on how CIL and Section 106 Agreements will be used to help implement the Plan.
IN.1: Telecommunications infrastructure and impact of development on broadcast and telecommunications services	Policy moved	<u>Moved to Chapter 8 – creating a sustainable place to live and work</u>
IN.2: Planning for waste	Policy moved	<u>Moved to Chapter 8 – creating a sustainable place to live and work</u>
Transport and Connectivity Paragraph 7.12	Major	<u>Delete paragraph 7.12 and removal of the strengths, opportunities, weaknesses and threats box.</u>
Figure 21	Minor	<u>Update to reflect updated context of Crossrail and name change of line.</u>
T.1: Strategic transport improvements	Minor	<p>Policy T.1: Strategic transport improvements</p> <p>The Legacy Corporation will promote improved connections to support international and national economic growth within its own area and more widely within the Growth Boroughs, east London and Thames Gateway. In particular, it will seek to secure:</p> <ol style="list-style-type: none"> <li>1. Stopping international trains at Stratford International Station</li> <li>2. Benefits to the area from any new proposals to link High Speed 1 to High Speed 2</li> <li>3. Improved connections to airports</li> <li>4. Benefits to the area from the proposed Crossrail 2, depending on the final route alignment selected</li> <li>5. <u>Improvements to Stratford Station access and station upgrade</u></li> <li>6. <u>Works to upgrade the strategic road network within the Legacy Corporation area to support regional traffic issues and improve routes to encourage multi-modal usage.</u></li> </ol>
Paragraph 7.7 (7.15)	Minor	The Legacy Corporation area already has direct DLR links to London City Airport and coach links to Stansted Airport and, <del>from 2018 via Crossrail</del> <u>via the Elizabeth Line</u> , direct to Heathrow. During the lifetime of this Local Plan, the Airports Commission will examine the need for additional

	Type of change	Proposed change
		UK airport capacity and recommend to government how this can be met in the short, medium and long term. The Legacy Corporation will support improved rail access to airports.
Paragraph 7.8 (7.16)	Minor	Transport for London (TfL) and Network Rail are working closely together to develop Crossrail 2. <del>A series of themes emerged from the consultation in 2013, which are being reviewed, prior to recommending a preferred regional alignment.</del> The main areas of analysis include the alignment through Hackney and Haringey and Network Rail branches. <u>The proposed route map as confirmed in 2015 would provide a link across London from the north east to the south west.</u> The concept of an eastern branch <del>is continuing to be</del> <u>has previously been explored focused on an alignment through Hackney and Haringey and Network Rail branches.</u> An eastern branch could provide significant benefits to the Legacy Corporation area and <u>continues to be a priority for the growth boroughs that it would include.</u>
T.2: Transport improvements	Minor	Policy T.2: Transport improvements The Legacy Corporation will use its powers and influence to support and bring forward transport improvements as set out in the Infrastructure Delivery Plan which are necessary to support the level of growth anticipated in the Local Plan. Where development proposals come forward that are near or adjacent to identified transport schemes, development proposals will be required to demonstrate <u>(1) that adequate provision for the implementation of those schemes has been made in the design of the development, or that development proposals do not compromise implementation of transport schemes; (2) how they relate to the Healthy Streets indicators; and (3) support the increase of cycling, walking and public transport usage to meet the Mayor's target of 80% of journeys being made up by these modes by 2041.</u> Existing bus stands and bus stops will be protected and new provision sought where necessary to support new development.
Paragraph 7.9 (7.17)	Minor	The Legacy Corporation has already secured substantial funding towards some of the transport projects in the IDP and <u>has delivered or is working on delivery of these projects</u> (Hackney Wick Station, Stratford Station Access). In other cases, such as Bromley-by-Bow Station, the Legacy Corporation is working with TfL to bring forward the improvements.
T.3: Supporting transport schemes	No change	No change to policy proposed

	Type of change	Proposed change
<p>T.4: Managing development and its transport impacts to promote sustainable transport choices, facilitate local connectivity and prioritise pedestrians and cyclists</p>	<p>Minor</p>	<p>Policy T.4: Managing development and its transport impacts to promote sustainable transport choices, facilitate local connectivity and prioritise pedestrians and cyclists</p> <p>Through its planning powers, the Legacy Corporation will <u>(1) promote sustainable transport choices and minimise reliance on the private car to ensure that the development of the area is optimised; <del>To</del> (2) promote the Healthy Streets Approach set out in the draft New London Plan and in order increase journeys through walking, cycling and public transport, in line with the Mayor’s target of 80% by 2041.</u> In doing so, the Legacy Corporation will:</p> <ol style="list-style-type: none"> <li>1. Implement a street network that prioritises pedestrians and cyclists as the most important travel modes, followed by public transport and then the private car</li> <li>2. Expect new development to maximise the opportunities to improve connectivity across, within and through the Legacy Corporation area and, where opportunities arise, with the wider Lower Lea Valley and east London</li> <li>3. Ensure that the amount of new development and growth across its area is related to the capacity of existing or currently planned improvements to transport infrastructure and services</li> <li>4. Expect new development to be designed to include measures that will minimise its impact on public transport and the highway network, and to have no or minimum levels of car parking which do not exceed <u>draft New London Plan</u> parking standards</li> <li>5. Require new development to provide appropriate facilities for the full range of transport users, including pedestrians, rail, bus, car and cycles</li> <li>6. Require new developments to include on site spaces or satisfactory arrangements for car clubs, facilities for electric vehicle charging and stands for cycle hire, where appropriate</li> <li>7. Require major new development to demonstrate how its construction impact will be managed through a Construction Management Plan and how, once operational, servicing and deliveries will be managed through Delivery and Servicing Plans</li> </ol>

	Type of change	Proposed change
		<ol style="list-style-type: none"> <li>8. Require new developments to use target-based Travel Plans to encourage smarter travel, incentivised through S106 Agreements</li> <li>9. Encourage the use of the waterways in the area for transport and leisure and the towpaths as routes for pedestrians and cyclists, as appropriate, managing any potential conflict through design.</li> </ol>
Paragraph 7.15 (7.23)	Minor	<p>Planning policy in London has generally followed a pedestrian, cycle and public transport priority based approach for the last 15 to 20 years. These policies are now leading to dramatic changes in Londoners' behaviour and attitudes to their cars. Car ownership per household across London has decreased dramatically in the last ten years according to the 2001 and 2011 censuses, and road vehicle traffic in London has been falling over the last decade, with vehicle kilometres in 2012 10.9 per cent lower than in 2000, and at their lowest level since 1993. Managing development and its transport impacts through Policy T.4 above will ensure that the development being planned for can be brought forward without significant new public transport (i.e. new railway lines) or new strategic roads, as well as ensuring that pedestrian, cycle and public transport use increases and car use declines. As set out in the context section above, evidence concludes that this is the approach the Legacy Corporation should take to new development in its area. This approach will also facilitate Sustainability Objective 5 of this Local Plan, and help implement policies that seek to improve air quality and reduce emissions from vehicles. <u>This approach also supports the Healthy Streets approach taken in the Draft New London Plan and the Mayor's target of 80% of all journeys being made by walking, cycling or public transport by 2041.</u></p>
T.5: Street network	Minor	<p>Policy T.5: Street network</p> <p>The structure and hierarchy of streets within the Legacy Corporation area as set out in Table 7 helps to determine the most appropriate routes within and through the area, and where and how property and development proposals should best connect to <del>that those</del> <u>networks to increase journeys undertaken through walking, cycling and public transport.</u></p> <ol style="list-style-type: none"> <li>1. In implementing the street network and in considering development proposals that will impact on the network, the Legacy Corporation will prioritise pedestrians and cyclists as the most</li> </ol>

	Type of change	Proposed change
		<p>important travel modes, followed by public transport and then, as appropriate, the private car</p> <ol style="list-style-type: none"> <li>The Legacy Corporation will use its powers as Local Planning Authority to ensure that development is appropriately located in terms of its traffic-generation impacts, with the aim of ensuring that, if major traffic-generating uses are proposed (and are considered acceptable against other policies in this Local Plan), they are located in places that have good and appropriate connections to the strategic routes</li> <li>The Legacy Corporation, in its function as Local Planning Authority, will consult with Boroughs and Transport for London as appropriate in respect of their role as Highways Authorities within the area to ensure that effects of proposals are properly considered.</li> </ol>
T.6: Facilitating local connectivity	No change	No change proposed.
T.7: Transport Assessments and Travel Plans	No change	No change proposed.
T.8: Parking and parking standards in new development	Minor	<p>Policy T.8: Parking and parking standards in new development</p> <p>In considering development proposals that include off street parking, the Legacy Corporation will apply the <u>Draft New London Plan Parking Standards</u>. In considering whether the proposed provision of car parking is appropriate, the Legacy Corporation will require <u>proposals where car parking is being considered</u> to:</p> <ol style="list-style-type: none"> <li><u>Be at a low level appropriate to its location, within the LLDC area, with minimum levels of provision in locations with the highest levels of public transport accessibility. In the first instance aim as a starting point for car-free development, where development is well connected, or planned to be, by public transport in the Legacy Corporation area.</u> In all other areas car parking should be at low level, restricted in line with levels of existing and future public transport accessibility and connectivity.</li> <li><u>Be provided in a way that is appropriate to the existing and proposed character and form of the built environment, and acceptable in design terms. Submit a Car Park Design and Management Plan as</u></li> </ol>

	Type of change	Proposed change
		<p><u>part of their application indicating how the car parking will be designed and managed to be acceptable, referencing Transport for London guidance on car parking management and design.</u></p> <ol style="list-style-type: none"> <li>3. <u>Be provided in a way that is appropriate to the existing and proposed character and form of the built environment and, where provided off-street, does not compromise other potential street-level uses or dominate street frontages</u></li> <li>4. Not take precedence over the incorporation of open space, public realm or amenity space within and around the development</li> <li>5. Incorporate new car club spaces secured through Section 106 Agreements as part of a sustainable approach to parking</li> <li>6. Incorporate the provision of electric charging points and parking bays for electric vehicles as part of any car parking provision</li> <li>7. Provide designated on- or off-street parking bays for Blue Badge holders, <del>appropriate to the size, nature and location of the development</del> <u>in line with the Draft New London Plan Parking Standards.</u></li> <li>8. <u>In cases where the application is for redevelopment of an existing site with parking provision the level of parking provision should be reduced or adjusted to reflect the current approach and standards.</u></li> </ol> <p>Where a scheme requires a Transport Assessment to be submitted, in accordance with Policy T.7 of this Local Plan, the appropriate level of parking should be determined through the assessment process <u>in line with Draft New London Plan Parking Standards.</u></p> <p>For venues which generate a significant level of attendance by members of the public during events, there should be no provision for parking of private vehicles during events, except to meet requirements in the standards for Blue Badge parking bays and for parking for vehicles required for operational purposes.</p> <p>Where low levels of parking are proposed, the Legacy Corporation, in consultation with the appropriate Borough may require that contributions are made towards the setting of new or changes to existing controlled parking</p>



	Type of change	Proposed change
		schemes and/or that future residents are not eligible for parking permits.
Paragraph 7.23 (7.31)	Minor	This policy will allow for the actual level of parking provision proposed as part of a development to take into account local factors such as the ease of access to public transport services and to town and other Centres. The <u>Draft New London Plan</u> sets out maximum parking standards for different types of new development, currently within <del>the Parking Addendum to Section 6 and within Table 6.2: Parking standards, Chapter 10 of the Draft New London Plan.</del>
T.9: Providing for pedestrians and cyclists	Minor	<p>Policy T.9: Providing for pedestrians and cyclists</p> <p>The Legacy Corporation will promote and support the provision of safe routes for walking and cycling within its area that connect well with local destinations (including schools and community facilities) as well as transport nodes within and outside of its area <u>to support the Mayor's target of cycling, walking and public transport making up 80% of all journeys being undertaken by 2041:</u></p> <ol style="list-style-type: none"> <li>1. Walking and cycling provision should be safe, direct, coherent and attractive and be designed to be in accordance with the best practice guidance in place at the time</li> <li>2. Walking and cycling provision should integrate well with the street environment, minimising conflict with other modes wherever possible</li> <li>3. Parking provision for cyclists should meet or preferably exceed minimum standards set out in the <del>current</del> <u>Draft New London Plan standards</u>. Provision should be in a safe and secure and overlooked location, preferably under shelter. Work place cycle facilities should also include adequate levels of showering and changing facilities</li> <li>4. The provision of wayfinding and signage (such as Legible London) should be consistently applied across the Legacy Corporation area, in order to ensure continuity for users.</li> </ol>
T.10: Using the waterways for transport	Minor	'Where appropriate, and to help facilitate projects such as the Leaway, the Legacy Corporation will require development proposals to provide new or improved <u>safe access along the waterways...</u> '
Para 7.30 (7.39)	Minor	'Improvements to the waterways, including <u>appropriate safety measures</u> , and increasing of intensity of use will help promote more sustainable transport choices...'

## Section 8 – Creating a sustainable place to live and work

	Type of change	Proposed change
Paragraph 8.1	Minor	'Ensuring environmental sustainability' is one of the Legacy Corporation's Key Priorities set out in its vision. This Local Plan as a whole seeks to achieve the sustainable development of the Legacy Corporation area that results in a place that is economically, socially and environmentally sustainable. This is reflected in Policy SD.1: Sustainable development, following the vision <u>set out in the Draft New London Plan in Section 3 of the Local Plan</u> . That section, 'Our area', outlines the key sustainability challenges and opportunities within the Legacy Corporation area.
Paragraph 8.2	Minor	To ensure that this Local Plan embraces all aspects of sustainability, this section supplements the other policies in this Local Plan relating to sustainability issues by: <ul style="list-style-type: none"> <li>• addressing health and wellbeing, and</li> <li>• focusing on those matters that will help to make the Legacy Corporation area and the development within it resilient to climate change (including relevant mitigation measures) and addressing the requirement to contribute towards the national and London Plan targets for securing reductions in carbon dioxide emissions, <u>and</u></li> <li>• <u>the expansion of electronic communication networks, including telecommunications and high-speed broadband.</u></li> </ul>
Paragraph 8.3	Minor	In applying the policies of this Local Plan together as a whole (see paragraph 3.7 and 3.8 of this Local Plan), the Legacy Corporation's aim is to ensure that a significant contribution is made to achieving a healthy place to live and work, that the Legacy Corporation's area is a place that achieves a high level of wellbeing and that the positive impacts of securing sustainability will reach beyond the Legacy Corporation boundary to those who live and work in the surrounding areas of east London. <u>To avoid repetition only Objective 5 is included within this section, however to reflect the overarching aims that the Legacy Corporation have around sustainability both Objectives 4 and 5 should be referenced when reading this section.</u>
Objective 5	Minor	Objective 5: Deliver a <u>smart</u> , sustainable and healthy place to live and work
SP.5: A sustainable and healthy place to live and work	Minor	Strategic Policy SP.5: A sustainable and healthy place to live and work  The Legacy Corporation will work with its partners to achieve a sustainable future for those who live and work in

	Type of change	Proposed change
		<p>its area and contribute to a sustainable future for east London and London, by:</p> <ol style="list-style-type: none"> <li>1. Ensuring that development contributes to the health and wellbeing of those living and working in the area</li> <li>2. Ensuring that development meets the needs of the present without compromising the ability of future generation to meet their own needs</li> <li>3. Contributing to the Mayor’s <del>target for reducing London’s carbon dioxide emissions by 60 per cent (below 1990 levels) by 2025, including through energy efficiency measure in buildings and increasing the use and availability of decentralised energy</del> <u>objective of London becoming a zero-carbon city by 2050 utilising all measures of efficiency and energy reduction available informed by the energy hierarchy.</u></li> <li>4. Reducing water use and encouraging the utilisation of rainwater harvesting, grey water recycling and use of non-potable water sources</li> <li>5. Minimising construction, commercial and householder waste</li> <li>6. Encourage a reduction in materials use and increase in materials reuse, recycling and composting, <u>ensuring that there is zero biodegradable or recyclable waste to landfill by 2026 and meeting or exceeding the recycling targets set out in the Draft New London Plan</u></li> <li>7. <u>Requiring retention of existing waste management facilities as set out in policy S7</u></li> <li>8. Minimising the travel demand and increasing opportunities for walking and cycling <u>to support in the delivery of the Mayor’s strategic target of 80 per cent of all trips in London to be made by foot, cycle or public transport by 2041</u></li> <li>9. Requiring developments to mitigate and manage the effects of climate change</li> <li>10. Avoiding overheating and excessive heat generation as a result of new development</li> <li>11. Urban greening through planting in the public realm and private spaces and green and brown roofs</li> <li>12. Requiring Sustainable Urban Drainage measures (SuDs), restriction of surface water run-off rates and interception of pollutants prior to discharge, where appropriate</li> <li>13. Protecting existing and encouraging the provision of new public and private open spaces and an</li> </ol>

	Type of change	Proposed change
		<p>increase of tree coverage in streets and open spaces</p> <p>14. Facilitating sustainable lifestyles for residents by considering the performance of buildings and spaces in operational use</p> <p>15. <u>By supporting the provision and expansion of digital infrastructure within the Legacy Corporation area and enabling development for future infrastructure.</u></p>
Case Study 12: Chobham Manor exemplar homes	Minor	<u>Update figures and other information to reflect current context.</u>
S.1: Health and wellbeing	Minor	<p>Policy S.1: Health and wellbeing</p> <p>Applications for major development schemes will be required within their Design and Access Statement to describe how the scheme will contribute to the health and wellbeing of those who will live and/or work within the development proposed and would not significantly adversely affect those who live and/or work within the vicinity of the proposed development. This should include information on access to schools, health services, community facilities, leisure activities, local shops and services, parks and publicly accessible open spaces. Major development schemes should also demonstrate <u>how they will deliver improvements that support the ten Healthy Streets Indicators as set out in the Draft New London Plan and Transport for London guidance, as well as access to the development via public transport and permeability through walking and cycling and how these link up with existing networks. They should also aim to reduce the dominance of vehicles on streets and demonstrate that they meet the requirements of all other relevant policies in this Local Plan.</u> <del>that they are located and designed to encourage active and healthy lifestyles and that they meet the requirements of all other relevant policies in this Local Plan, including those that encourage walking and cycling.</del></p>
Paragraph 8.5	Minor	Each major development scheme will, by its nature, play a significant part in achieving the aims of this policy. It is important that these development proposals demonstrate that they are helping to achieve this in a manner that is appropriate to their specific circumstances. Physical development will set the scene for existing and new communities to develop and change, with community organisations, local groups and service providers, including faith groups, having a major role in ensuring that these are communities that thrive. It is important that the Design and Access Statements for major schemes identify, by reference to policies within this Local Plan and the <u>Draft</u>

	Type of change	Proposed change
		<p><u>New</u> London Plan, those elements of the proposed scheme that will positively contribute to achieving positive outcomes for those who will live, work or otherwise use the development proposed.</p>
S.2: Energy in new development	Minor	<p>Policy S.2: Energy in new development</p> <p>Developments will be expected to minimise carbon dioxide emissions to the fullest extent possible by application of the Energy Hierarchy as set out below:</p> <ol style="list-style-type: none"> <li>1. Reducing energy requirements</li> <li>2. Supplying the energy that is required more efficiently and where possible generating, storing and using <u>renewable energy on-site</u></li> <li>3. Meeting remaining energy requirements through renewable energy sources where viable and <u>exploiting local energy resources</u>.</li> </ol> <p>Major development proposals should <del>as a minimum meet the regulated carbon dioxide emissions standards outlined within the London Plan</del> <u>be net zero-carbon, with carbon dioxide emissions reduced from both construction and operation. The Draft New London Plan sets this out as a minimum on site reduction of at least 35 per cent beyond Building Regulations is to be expected.</u> The Draft New London Plan requires a minimum on-site reduction of carbon emissions of at least 35 per cent beyond Building Regulations 2013. Residential development should aim to <u>achieve 10 per cent, and non-residential development should aim to achieve 15 per cent through energy efficiency measures.</u> <del>For residential buildings:</del></p> <ul style="list-style-type: none"> <li>• <del>2015–2016: 40 per cent improvement on the 2010 Building Regulations Target Emission Rate</del></li> <li>• <del>2016–2031 zero carbon (including allowable solutions or equivalent contribution to the Carbon Off-setting Fund).</del></li> </ul> <p><del>Non-domestic proposals should achieve a 35 per cent improvement up to 2016, meet building regulation requirements from 2016, and be zero carbon from 2019 onwards.</del></p>

	Type of change	Proposed change
		<p>Where these targets cannot be met on site, <del>and until any nationally recognised Allowable Solutions system is in place,</del> a financial contribution to the Legacy Corporation Carbon Offsetting Fund will be required. <del>A supplementary planning document will be prepared, setting</del> <u>The Legacy Corporation Carbon Offset Supplementary Planning Document (adopted August 2016) sets out the rate per tonne of carbon dioxide and the scheme for applying the funds raised.</u></p> <p>Major applications will be required to provide an Energy <del>Strategy Statement</del> <u>Statement</u> that sets out how the development has addressed the Energy Hierarchy and meets or exceeds the targets above and the source and method of proposed energy supply <u>and will be expected to monitor and report on energy performance.</u> <del>Energy statements should be prepared in accordance with Part D of London Plan Policy 5.2 and provide an estimation of unregulated emissions for development in use, alongside calculated emissions associated with building regulations.</del> <u>Energy Strategies should be prepared in accordance with Policy SI2 of the Draft New London Plan.</u></p>
Paragraph 8.6	Minor	<p>In 2009, carbon emissions from buildings accounted for 43 per cent of the total (Department for Communities and Local Government). In his <u>Draft London Plan</u>, the Mayor sets out his ambition <u>for London to be net zero-carbon to reduce carbon emissions in London to 60 per cent below their 1990 levels, by 2025.</u> This reduction is a strategic priority that has the potential to make a significant contribution in a London context to minimising the projected levels of climate change. <del>This fits with the wider national target set in the 2008 Climate Change Act and government targets for achieving new zero-carbon homes by 2016 and for zero-carbon non-domestic buildings by 2019.</del> Achieving reductions in carbon emissions from these sources is an essential element of a London-wide strategy to contribute towards reducing the rate of identified climate change and improving air quality.</p>
Paragraph 8.7	Minor	<p>The Legacy Corporation <del>will prepare a</del> <u>has an adopted supplementary planning document to set</u> <del>which sets out</del> the rate to be applied to its area-wide carbon off-setting scheme and identifies <del>the</del> <u>syng</u> the mechanism for collecting and allocating the funds raised to ensure that funds are applied in a way that adequately mitigates the carbon dioxide emissions from the contributing development. The scheme for allocating funds raised <del>will take</del> <u>takes</u> into account <del>the</del> local opportunities to aid appropriate retrofitting of carbon saving measures to existing buildings</p>

	Type of change	Proposed change
		<p>and structures within and around the Legacy Corporation area. The rate set <del>will need to</del> ensures that development within the Legacy Corporation area remains viable. The Legacy Corporation's carbon off-setting scheme, designed to meet these carbon targets for the Legacy Corporation's Legacy Communities Scheme development, <del>will</del> provides the basis for this wider scheme. Application of the London Plan Energy Hierarchy (<del>London Plan Policy 5.2</del> set out in Chapter 9 of the Draft New London Plan) will be important:</p> <ol style="list-style-type: none"> <li>1. <b>Be lean:</b> use less energy</li> <li>2. <b>Be clean:</b> supply energy efficiently</li> <li>3. <b>Be green:</b> use renewable energy</li> <li>4. <b>Offset</b> (to be highlighted by new Figure image)</li> </ol> <p>Maximisation of energy efficiency should take account of all aspects of a scheme, including, for example, street lighting and communal lighting using LED or other efficient lighting technology. Outdoor lighting should also minimise losses of light to the sky.</p>
S.3: Energy infrastructure and heat networks	No change	No change proposed
S.4: Sustainable design and construction	No change	No change proposed
S.5: Water supply and waste water disposal	No change	No change proposed
Case Study 14	Replacement	This case study should be replaced with a more up to date example of a scheme in the LLDC area.
IN.1: <u>Increasing digital connectivity, safeguarding existing communications provision and enabling future infrastructure</u> Telecommunications infrastructure and impact of development on broadcast and telecommunications services	Major	<p><del>Policy S6: Telecommunications infrastructure and impact of development on broadcast and telecommunications services</del> <u>Increasing digital connectivity, safeguarding existing communications provision and enabling future infrastructure</u></p> <p><del>In considering proposals for new telecommunications infrastructure, the Legacy Corporation will consider proposals against other policies in this Local Plan. In particular, the Legacy Corporation will need to be satisfied that new telecommunications equipment does not have a significant adverse impact on the appearance of existing buildings and amenity of the area in which it is proposed to be located. Where possible, the Legacy Corporation will require new telecommunications and radio equipment to</del></p>

	Type of change	Proposed change
		<p>be located on existing masts, buildings and other structures to minimise the number of installations, unless the need for a new site has been justified.</p> <p>Where development is proposed that may have significant adverse impact on and interfere with existing telecommunications or broadcast services, the Legacy Corporation will require that an impact statement is prepared and mitigation plan implemented as necessary through a Section 106 Agreement prior to occupation of a proposed development.</p> <p><u>Digital communications and connectivity is a key element in enabling growth and facilitating innovation within the Legacy Corporation area, new development will seek to should:</u></p> <ol style="list-style-type: none"> <li>1. <u>As set out in the Draft New London Plan to achieve greater digital connectivity than set out in part R1 of the building regulations</u></li> <li>2. <u>Safeguard existing communications infrastructure, or where a significant adverse impact has been identified, prepare an impact statement and implement a mitigation plan as necessary through a Section 106 agreement prior to occupation of the proposed development, especially in relation to mobile connectivity</u></li> <li>3. <u>Co-locate services where possible on existing structures, such as masts, or within appropriate street furniture, such as street lighting, to ensure the effective use of public realm to accommodate well-designed and located mobile digital infrastructure that mitigates the impact of new infrastructure on the existing built environment</u></li> <li>4. <u>Ensure that sufficient ducting space is provided for future digital connectivity infrastructure</u></li> </ol> <p><u>As technologies continue to develop that improve efficiency and help reduce resource usage development proposals should consider the latest innovations to help support requirements around initiatives such as the zero-carbon target and the Healthy Streets Approach. The Legacy Corporation supports the usage of innovations and new technologies that go above the expectations already set in policy to make new development sustainable in the long term. Where it is identified that space should be left for future technology or innovations, such as the requirement for space for ducting already set out above, the Legacy Corporation supports these inclusions and making the most of opportunities to enable future</u></p>



	Type of change	Proposed change
		<u>infrastructure where there is evidence to show their benefit.</u>
IN.1 – Reasoned justification	Minor	Provision of high-quality communications infrastructure is essential for economic growth. The Legacy Corporation therefore supports provision of improved infrastructure <u>and enabling space for new infrastructure.</u> However, it is also important to ensure that new <u>communications and smart infrastructure integrates well into the area and has a satisfactory appearance, and does not have a detrimental impact on the area within which it is proposed.</u>
IN.1 – Policy application	Minor	The Legacy Corporation will require that new infrastructure is located as far as possible on existing buildings and structures to minimise the proliferation of stand-alone <u>telecommunications structures</u> which could have a detrimental impact on the area. Where the policy requires a mitigation plan for new development to address potential effects on <u>telecommunications or broadcast communications networks services</u> , the scope of this, including any assessment methodology, should be agreed in advance with the Local Planning Authority. <u>New development should, as far as possible, support innovation and enable communications networks and future infrastructure.</u>
IN2. Planning for waste	Minor	<p>Policy <u>S.7: Planning for waste</u></p> <p>In carrying out its function as a Local Planning Authority, the Legacy Corporation will cooperate with the four Boroughs in matters of strategic waste management and planning. In doing so and in making planning decisions, it will take full account of:</p> <ol style="list-style-type: none"> <li>1. The waste apportionment targets sets for each Borough within the London Plan</li> <li>2. The adopted local waste plans or waste planning policy for that Borough</li> <li>3. The development of new or review of existing adopted waste plans for that Borough.</li> </ol> <p>Proposals that would result in the loss of an existing waste management facility would only be permitted where it can be demonstrated that:</p> <ol style="list-style-type: none"> <li>4. An additional waste management facility has been secured, and is deliverable, which will meet the maximum waste throughput of that existing site, or</li> <li>5. An existing site can provide an additional capacity equivalent to that maximum waste throughput, and</li> </ol>

	Type of change	Proposed change
		<p>6. The new site can serve the same waste management needs of the original site, and</p> <p>7. The new site is <u>located within London and continues to meet London’s strategic need</u> <del>in the same waste authority area or waste authority group area as the original site.</del></p> <p>Proposals for new waste management facilities will be permitted where:</p> <p>8. It is located within an area designated as Strategic Industrial Land (SIL) which has been identified as a Preferred Industrial Location (PIL), or where appropriate within an Industrial Business Park (IBP) or a Locally Significant Industrial Site (LSIS)</p> <p>9. The proposal does not compromise or otherwise make unviable the existing adjacent employment or transport functions</p> <p>10. Its design and operation will not adversely affect the wider amenity of the proposed location</p> <p>It can be demonstrated to have met the tests within the Appendix B of the (draft) National Planning Policy: Planning for Sustainable Waste Uses.</p>
IN.2 – Reasoned justification (Paragraph 7.8)	Minor	<p>The Legacy Corporation is the waste planning authority for its area by virtue of its role as a planning authority. The Four Boroughs have responsibility for waste planning within the remainder of their area. Each borough has, or will have within the lifetime of this Local Plan, an adopted waste plan or waste planning policies. The adopted East London Waste Plan includes and has effect within the Newham part of the Legacy Corporation area. <del>Tower Hamlets is its own waste authority and relevant policies within the Tower Hamlets Core Strategy (2010) will remain relevant.</del> In each case it will be necessary for the Legacy Corporation to cooperate and work closely with each Borough where adopted plans and policy are reviewed or specific proposals that affect waste management or waste sites arise. The Boroughs of Hackney and Waltham Forest each belong to the North London Waste Planning group which is in the early stages of preparing its Waste Local Plan. The Legacy Corporation will work closely with these two Boroughs, the North London Boroughs and other key stakeholders to make sure that the North London Waste Plan continues to take account of any waste capacity, sites and related evidence within that part of the Legacy Corporation area. <u>The Legacy Corporation will also work with the Borough and relevant authorities in relation to waste arrangements in new development to ensure that these are efficient, well designed and appropriate, where</u></p>

	Type of change	Proposed change
		<u>appropriate through encouraging consideration of innovative solutions such as vacuum systems and compactors on site.</u>
IN.2 – Reasoned justification (Paragraph 7.9)	Minor	The <u>Draft New</u> London Plan identifies waste apportionment targets that each Borough should meet for its area. The <u>Draft New</u> London Plan does not include a waste apportionment target for the Legacy Corporation area. However, the Legacy Corporation will cooperate with the four Boroughs, the GLA and TfL in seeking to meet the Borough apportionment targets and strategy for waste. When determining planning applications, these targets will remain the appropriate policy context against which to judge the acceptability of proposals that have an effect on any existing management site or operation, including proposals for new or expanded facilities.
IN.2 – Policy application (Paragraph 7.10)	Minor	With existing waste management facilities located within the area, the policy identifies the criteria that would be necessary for proposals to meet if they would result in the loss of an existing facility, in order for them to be acceptable in terms of the effect on the waste apportionment targets set for each Borough and in order to meet the <u>requirement of London Plan Policy 5.17 Waste Capacity requirements set out in policies in Chapter 9 of the Draft London Plan.</u>
S.6: Waste reduction	Minor	<p><b>Policy S.8: Waste reduction</b></p> <p>The Legacy Corporation will, in making planning decisions, require that new development proposals contribute to the reduction of waste during construction and once operational, by minimising the amount of waste produced and maximising reuse, recycling and composting <u>and promoting a more circular economy.</u></p> <p>Proposals for new development should demonstrate how they have adopted the Waste Hierarchy in their design and how they will enable their residents to minimise waste and maximise both reuse and recycling. Development proposals should demonstrate how they have: designed out waste through lean design, maximised the reused and recycled content within the materials used for construction, minimised the production of excess or waste material during construction and maximised the opportunities for reuse or recycling of materials remaining from construction. Excavated materials should, where practical, be retained and reused on site. Planning applications for major development schemes should include a statement within the Design and Access</p>

	Type of change	Proposed change
		<p>Statement that sets out how the scheme will comply with the requirements of this policy.</p> <p>Proposals for all development will, where relevant, be required to demonstrate that adequate provision has been made for domestic and commercial waste storage and for collection that allows for a range of future collection options which include separate collection of general waste, recyclable materials and other waste streams.</p>
S.7: Overheating and urban greening	Minor	<p>Policy <u>S.9</u>: Overheating and urban greening</p> <p>Proposals for new development should ensure that buildings and spaces are designed to avoid overheating and excessive heat generation internally and externally, while minimising the need for internal air conditioning systems, taking into account <u>Draft New London Plan Policy S14 and the Mayor's zero carbon target of 2050</u> <del>the guidance within the Mayor of London's Climate Change and Adaptation Strategy.</del></p> <p>Outside the existing parks and open spaces within the Legacy Corporation area, opportunities to introduce planting of trees in private and public spaces, including streets, along with those for including green roofs, green walls and other planting opportunities, should be taken to maximise the contribution that urban greening can make in creating a liveable environment and maximising local biodiversity and encouraging local food growing.</p> <p>Planning applications for major development schemes should set out within the Design and Access Statement the measures included to avoid overheating (including overheating analysis against a mid-range climate scenario for the 2030s) and excessive heat generation and, where appropriate, to maximise urban greening.</p>
Paragraph 8.18	Minor	<p>All development proposals should consider the opportunities to avoid overheating within buildings and also introduce additional greening to the site environment. Where feasible, and not in conflict with achieving high levels of building fabric efficiency, passive ventilation should be favoured. Applications for major development schemes will be expected to demonstrate that these issues have been addressed within the scheme design. This policy should be read alongside <u>Draft New London Plan Policy 5.9 Overheating and Cooling Policy S14 Managing heat risk</u>, in particular taking into account the cooling hierarchy:</p> <ol style="list-style-type: none"> <li>1. Minimise internal heat generation</li> </ol>

	Type of change	Proposed change
		<p>2. Reduce the amount of heat entering a building in summer through orientation, shading, albedo, fenestration, insulation and green roofs and walls</p> <p>3. Manage the heat within the building through exposed internal thermal mass and high ceilings</p> <p>4. Passive ventilation</p> <p>5. <u>Providing mechanical ventilation</u></p> <p>6. Active cooling systems (assuming that they are the lowest carbon option).</p>
<p>S.8: <del>Flood risk and sustainable drainage measures</del> <u>Flood risk measures</u></p>	<p>Minor</p>	<p>Policy <u>S.10</u>: Flood risk and sustainable drainage measures</p> <p>The Legacy Corporation will take into account the most up-to-date flood risk information when carrying out its relevant functions and seek to reduce risk to life and property in doing so.</p> <p>Where development is proposed within Flood Zones 2 or 3 and is outside a Site Allocation within this Local Plan, planning applications should be accompanied by evidence that the proposed development is capable of meeting the Sequential Test and, where appropriate, that the Exceptions Test as defined within the National Planning Policy Framework has been applied to demonstrate that no alternative location for the proposed development is available.</p> <p>Where no alternative location is available, a flood risk assessment should be submitted demonstrating that the proposal does not increase flood risk to third parties and, wherever possible, reduces flood risk. A site specific flood risk assessment may be required within Local Flood Risk Zones identified in Surface Water Management Plans (subject to location and degree of flood hazard) to ensure that the development will remain safe and will not increase flood risk to others. Where deemed necessary, the development proposals must be supported by a detailed integrated hydraulic modelling within the Flood Risk Assessment.</p> <p>Development proposals must be designed to reduce vulnerability to climate change, apply the sequential approach on site, be flood resilient and resistant, setting living accommodation finished floor levels 300mm above the predicted flood level for the 1 in 100 chance in any year flood event including an allowance for climate change, and must provide an appropriate means of escape to a higher level within the building or a safe route to a location above the predicted flood level. No basement development will be permitted within Flood Zones 2 or 3.</p>

	Type of change	Proposed change
<p><u>S.9 Sustainable drainage measures and flood protections</u></p>	<p><u>New policy</u></p>	<p><u>S.11 Sustainable drainage measures and flood protections</u></p> <p>The rate of surface water run-off from development sites should be restricted to no greater than the equivalent for a Green Field site of an equivalent for a Green Field site of an equivalent size.</p> <p><u>It should be managed as close to its source as possible in line with the drainage hierarchy set out in policy SI143 of the Draft New London Plan.</u> Using sustainable drainage techniques as a first choice and only using other methods of flow restriction where it can be shown that sustainable drainage methods are not feasible in that location, particularly in areas where a localised surface water drainage problem has been identified within a Surface Water Management Plan (including potential flooding from sewers). <u>The Legacy Corporation will support developments which do not include proposals for impermeable paving.</u> Sustainable drainage systems that have benefits for water quality and storage, <u>efficiency</u>, habitat and landscapes and <u>amenity and recreation</u> should be fully considered before other options. All drainage systems discharging to a watercourse must include appropriate anti-pollution measures that can be easily accessed and maintained.</p> <p>Development proposals that create an obstruction within a watercourse or obstruct existing flood flow paths across land which cannot be mitigated through compensatory works or provision of additional flood storage capacity will not be permitted. The design and layout of proposed development should incorporate appropriate buffer strips adjacent to watercourses to allow access for flood risk maintenance and biodiversity and adequate space for sustainable drainage techniques.</p> <p>Where development is proposed on a site that includes an existing flood defence structure, development proposals should be designed to maintain the integrity of existing structure. Where the need for new or improved flood defences have been identified, relevant planning applications should demonstrate that allowance has been made for the relevant works to take place, including sufficient access for construction. Where a development proposal is dependent on the provision, improvement or repair of a river wall or other flood defence structure, these works should be included within the development applied for within the planning application.</p>

	Type of change	Proposed change
<p><u>S.10 Resilience, safety and security</u></p>	<p>New policy</p>	<p><u>Policy S.12 Resilience, safety and security</u></p> <p><u>The Legacy Corporation works with a range of development partners and stakeholders in order to ensure and maintain a safe and secure environment within the area, that is resilient against emergencies and threats such as fire, flood and terrorism. Part of this includes working closely with agencies such as the Metropolitan Police Service and the London Fire and Emergency Planning Authority to identify relevant needs, policies and other necessary infrastructure required to maintain a safe and secure environment. To continue maintaining this environment it is key that:</u></p> <ol style="list-style-type: none"> <li>1. <u>Development proposals should maximise building resilience and minimise potential physical risks through good design, use of the building regulations, Draft New London Plan policies and policies within this Local Plan, such as S.810 and S.911 in relation to flood risk</u></li> <li>2. <u>New development should consider measures at the start of the design process which aim to design out crime and deter terrorism, assist in the detection of terrorist activity and help mitigate its effects</u></li> <li>3. <u>Where required, physical security and safety measures should support and enhance the Healthy Streets vision and create spaces that people can enjoy, where they feel safe and relaxed and encouraged to choose to walk, cycle or use public transport.</u></li> </ol> <p><u>Supporting text:</u></p> <p><b><u>Reasoned Justification</u></b></p> <p><u>The Legacy Corporation’s key aims around LIVE, WORK, VISIT and INSPIRE rely on the development of a safe and accessible built environment. The Draft New London Plan puts an increased emphasis on people feeling safe in their environment and includes policies in Chapter 3 around Safety, security and resilience. The Legacy Corporation area includes strategic infrastructure such as stations, key routes and iconic facilities in the Queen Elizabeth Olympic Park, as well as large leisure facilities including Westfield Stratford City. There are also a rapidly increasing population, a range of businesses and new development in the area and events that generate large numbers of visitors. This policy therefore supports the Legacy</u></p>

	Type of change	Proposed change
		<p><u>Corporation in to delivery a suitable safe, accessible and relaxed environment as supported by the Draft New London Plan.</u></p> <p><b><u>Policy Application</u></b></p> <p><u>The London Risk Register sets out hazards and threats across London and sets out controls in place in order to prepare for these, this is maintained and managed by the London Resilience Group and provides an evidence base for wider challenges that Planning Authorities in London might face. This, combined with information around more localised needs, such as protective security relating to crowded spaces within the Legacy Corporation’s area, where there are predictable and high levels of crowding, and in relation to As well as information about schemes that have or will create a higher level of risk than existed previously, provides the background for the Legacy Corporation’s approach to resilience, safety and security in the area.</u></p> <p><u>The Metropolitan Police (Designing Out Crime Officers and Counter Terrorism Security Advisers) should be consulted to ensure that major developments in the Legacy Corporation area contain appropriate design solutions from the outset, which respond to the potential level of risk whilst maintaining high quality of place and reducing the need to retrofit solutions in future. Any proposed physical mitigations should give due regard to their impact on the look and feel of the design of the wider area and public realm and how they encourage walking and cycling and play their part in help reducing the dominance of motor vehicles. Where appropriate the Legacy Corporation may also seek to consider protective security or safety measures at other major developments, that do not create a higher level of risk than existed previously at these locations, that whilst they do not increase the risk at that location, have been identified by the police as having a significant risk, however these will be considered on a case by case basis.</u></p>



## Section 10: Sub area 1 – Hackney Wick and Fish Island

Policy, para.	Type of change	Proposed Change
Vision	Minor	<p>Hackney Wick and Fish Island will become a more vibrant, diverse and well-connected series of mixed and balanced neighbourhoods with places of social, cultural and economic activity. The established residential areas in the north, historic character in the centre, and industrial areas to the south, will have been complemented by a mix of new homes, employment floorspace and community facilities around and within buildings of historic interest, a new Neighbourhood Centre and <del>an</del> <u>the</u> upgraded railway station.</p> <p>These will be served by and have direct access to the open spaces and world-class sporting facilities of Queen Elizabeth Olympic Park. A new digital quarter of hi-tech, media, broadcasting and education activities will be clustered within and around Here East, <u>with potential designation of Hackney Wick &amp; Fish Island as a Creative Enterprise Zone</u> this will be <del>and</del> complemented by a significant presence of creative and cultural industries producing bespoke and artistic products west of the Lee Navigation.</p>
Page 149 Area analysis	Deletion	Proposed to remove section and leave this level of analysis to the background papers and evidence base reports, for example the Characterisation study.
Page 151 – Development Potential	Deletion	Proposed to delete. Will ensure consistency in context of recognising that the numbers and potential is dynamic and so will change over the lifetime of the plan and as a result of permissions and implementation of specific schemes.
Page 151 – Para 10.3 – Area Priorities	Minor	<p><b>Creative and productive employment: Protecting creative and cultural industrial uses that support the continuation of Hackney Wick and Fish Island’s entrepreneurial and enterprising work culture.</b> The Legacy Corporation in its role as Local Planning Authority will support the creative and cultural industries that combine to give Hackney Wick and Fish Island its distinctive sense of place. It will also promote development that incorporates a range of employment floorspace including <del>‘starter’ and ‘move on’ units</del> <u>affordable workspace, low cost business space, managed workspace, incubator, accelerator and co-working space suitable</u> for small and medium enterprises. Hackney Wick and Fish Island contains nearly 40 per cent of the employment land within the Legacy Corporation area, <u>and a significant proportion of more than 60 per cent of its individual businesses.</u></p>

Policy, para.	Type of change	Proposed Change
Page 152 – Para 10.3 – Area Priorities	Minor	<p><b>Clusters of activity: Promoting places where public and employment uses animate the private and public realm.</b> To the east, across the Lee Navigation, there is an employment cluster <u>to support the ongoing development of a technology cluster digital and associated</u> creative businesses at Here East. This provides flexible business/studio floorspace, <del>a data centre</del>, retail uses, and conference and education facilities in the former International Broadcast Centre (IBC) and Main Press Centre (MPC). Here East will provide an incubator and accelerator space for start-up businesses, and will create more than 5,000 jobs within a new quarter for London that supports the growth of the digital and creative industries.</p>
Page 152 – Para 10.3 – Area Priorities	Minor	<p><b>Waterways and open spaces: Enhancing the waterside environment and facilitating the provision of publicly accessible open spaces and the activation of the Blue Ribbon Network.</b> A significant proportion of the Sub Area lies within the Lee Valley Regional Park and is situated at the southern extent of a continuous area of open spaces and waterways. Opportunities for informal leisure on and along the waterways should be provided. As part of the public realm improvements for Hackney Wick and Fish Island, a Canal Park runs along the entire western edge of Queen Elizabeth Olympic Park adjacent to the Lee Navigation. This is a local park and a critical piece of public realm for Queen Elizabeth Olympic Park and Hackney Wick and Fish Island. The park accommodates a varied set of landscapes and routes, providing a consistent and active edge to the canal. The design for the Canal Park responds to the ecology, history and diverse heritage of the Lower Lea Valley and helps to meet the needs of <u>existing and</u> future communities <u>including</u> East Wick and Sweetwater.</p>
Page 153	Minor (non-policy amendment)	<p><b>Insert page to be included describing Here East (text for page below to be supplemented by images to illustrate):</b></p> <p><b><u>Here East – SIL (Strategic Technology Cluster)</u></b></p> <p><u>The 2012 Games Press and Broadcast Centres in the north-west of the Queen Elizabeth Olympic Park have been transformed into Here East providing a mix of studio, business, office, accelerator, education and cultural <del>and</del> accelerator space, with 5,000 jobs when fully occupied. See Policy B.1 and Table 2 (B.1a1). Current occupiers include:</u></p> <ul style="list-style-type: none"> <li>• <u>Broadcast and production, including BT Sport</u></li> <li>• <u>Plexal Innovation Centre</u></li> <li>• <u>V&amp;A research and learning hub and storage facility</u></li> </ul>

Policy, para.	Type of change	Proposed Change
		<ul style="list-style-type: none"> <li>• <u>Higher Education and research (including Loughborough University and University College London)</u></li> <li>• <u>Ford Innovation Office</u></li> <li>• <u>Studio Wayne McGregor</u></li> <li>• <u>The Trampery on the Gantry – 21 free standing affordable workspace studio’s (focused on local creative businesses),</u></li> <li>• <u>Event and conference facilities</u></li> </ul>
Policy 1.1: Managing change in Hackney Wick and Fish Island	Minor	<p>Amend Policy 1.1 as follows:</p> <p>Proposals for development within Sub Area 1 will be considered acceptable where they:</p> <ol style="list-style-type: none"> <li>1. Maintain the overall amount of existing employment floorspace (B Use Class), including that used by creative and cultural industries and operating as low-cost and managed workspace (in accordance with the provisions outlined under Policies B.1 and B.4)</li> <li>2. Propose employment floorspace falling within B1 (a), B1 (b), B1 (c) and <del>B2 Use Classes</del> inside the <del>Hackney Wick Station Area allocation boundary</del> <u>Neighbourhood Centre boundary</u>, and B1 (b), B1 (c), B2 and B8 Use Classes outside it</li> <li>3. <u>Within the Neighbourhood Centre boundary, includes floorspace for local retail, cultural and other leisure use (within Use Classes A1-A5 and D1-D2). Outside of the boundary retail and leisure uses should be small scale and serve an immediately localised need.</u></li> <li><del>3</del> 4. Restore and reuse heritage assets for employment or other uses.</li> </ol>
Policy 1.2 Promoting Hackney Wick and Fish Island’s unique identity	No change	No change to policy proposed.
Policy 1.3: Connecting Hackney Wick and Fish Island	No change	No change to policy proposed.
Policy 1.4: Preserving or enhancing heritage assets in Hackney Wick and Fish Island	Deletion	Policy has been deleted and requirements included within Policy BN.16 and its supporting text (see proposed revision text in Section 6 Built and natural environment).

Policy, para.	Type of change	Proposed Change
Policy 1.5: Improving the public and private realm in Hackney Wick and Fish Island	No change	<b>Becomes Policy 1.4.</b> No change to policy proposed.
Policy 1.6: Building to an appropriate height in Hackney Wick and Fish Island  (including para's 10.14 and 10.15)	Deletion	Proposed to delete Policy and paragraphs 10.14 and 10.15 and replace with explanatory text and heights table equivalent to that currently shown within the Sub Area 4 section.  <b><u>Application of Policy BN.5 within the sub area</u></b> <u>The prevailing height of development within Sub Area 1 has been established at 20 metres above ground level, equating to approximately 4-6 stories of development. This is represented through a range of intermittent building heights that together form a unique arrangement that contributes to the area's townscape. Policy BN.5 sets out the approach that will need to be applied both in designing new development and in assessing planning applications where this is proposed to exceed this height. Aside from limited variations, it is expected that new development will remain at or below this level. Within the boundaries of Hackney Wick Neighbourhood Centre, Policy BN.5 identifies the centre function and transport connections as being a potential justification for achieving an element of greater height and density provided the character of the area, particularly within the Hackney Wick Conservation Area, is not harmed. Table 9 below specifically sets out the height above which the policy test set out in Policy BN.10 will apply.</u>
Page 162, para 10.16	Deletion of text	Delete paragraph 10.16 as SPD has been prepared and adopted.
SA1.1 Hackney Wick Station Area	Minor	Add the following to supporting development principles:  <u>The site allocation is expected to yield a minimum of 800 new homes with an affordable housing threshold of 35% in accordance with Policy H2.</u>
SA1.2 Hamlet Industrial Estate	Minor	Add the following to supporting development principles:  <u>The site allocation is expected to yield a minimum of 100 new homes with an affordable housing threshold of 50% in accordance with Policy H2.</u>
SA1.3: Hepscott Road	Minor	Amend the sixth site allocation policy bullet point as follows:  "Any proposal that does not safeguard the existing waste capacity should be resisted unless it can be demonstrated

Policy, para.	Type of change	Proposed Change
		<p>this capacity can be more efficiently re-provided elsewhere within London and otherwise meets the requirements of Policy IN.2 of this Plan. Any such proposal must ensure that such an approach counts towards the Borough's overall waste apportionment target (to the satisfaction of the Borough and the GLA)."</p> <p>The site allocation is expected to yield a minimum of 475 new homes with an affordable housing threshold of 35% in accordance with Policy H2 and extant planning permission.</p>
SA1.4: Bream Street	Site allocation to be deleted	Delete site allocation.
SA1.5: 415 Wick Lane	Site allocation to be deleted	Delete site allocation.
SA1.6 Neptune Wharf	Minor	<p><b>Becomes Site Allocation 1.4.</b> The proposed option is to retain the current site allocation to reinforce delivery of policy requirements within the consented scheme, including delivery of the school.</p> <p>Additional text to be added to "Supporting Development Principles":  <u>The site allocation is expected to yield a minimum of 520 new homes with affordable housing being delivered in accordance with the current planning permission.</u></p>
SA1.7: East Wick and Here East	Major	<p><b>Becomes Site Allocation 1.5.</b> Amend site allocation to exclude Here East and the school and school playing field as follows:</p> <p><b>Site Allocation SA1.5: East Wick and Here East and Here East</b>  <u>Employment cluster and c-Employment, technology and education cluster and c-comprehensive, phased mixed-use development, including residential, employment, retail, leisure and community floorspace next to the Here East Technology Hub and East Wick Primary School, focusing non-residential uses within the boundary of the Neighbourhood Centre around the Cooper Box Arena (incorporating a new primary school and two nurseries).</u></p> <ul style="list-style-type: none"> <li>• <u>Development should include two new nurseries</u></li> <li>• Development should relate well to Hackney Wick Neighbourhood Centre and Canal Park</li> <li>• Development should provide a gateway to Queen Elizabeth Olympic Park and establish a high-quality frontage that engages with both the Lee Navigation and Queen Elizabeth Olympic Park</li> </ul>

Policy, para.	Type of change	Proposed Change
		<ul style="list-style-type: none"> <li>• Development should support the continuation of direct east-west connections from Hackney Wick and Fish Island to the primary school and Queen Elizabeth Olympic Park</li> <li>• Routes between East Wick and the Neighbourhood Centre should provide frontages to support the generation of active ground-floor uses.</li> <li>• <u>Development should support the employment, media, education, technological and creative functions of Here East, including the intensification and redevelopment of under-utilised areas and subsidiary retail, leisure or other 'walk to' services (Table 2, B.1a1).</u></li> </ul> <p><b>Phasing and implementation</b> <del>2015/16</del> 2018/19 onwards.</p> <p>Add to supporting development Principles:</p> <p><u>Where development is phased, introduction of appropriate interim uses is encouraged in accordance with Policy B.3.</u></p> <p><u>Routes connecting East Wick and Hackney Wick Neighbourhood Centre should be attractive and legible.</u></p> <p><u>The site allocation is expected to yield a minimum of 880 new homes with an affordable housing being delivered in accordance with the current planning permission</u></p> <p>Update Phasing and Implementation:</p> <p><del>2015/16 onwards</del> <u>2018/19 onwards</u></p> <p>Update Relevant Planning History to delete reference to Here East and modify site allocation boundary and drawing:</p> <p>Here East:  <del>Here East was granted planning permission on 25 February 2014, subject to conditions and a Section 106 (S106) Agreement (LPA ref 13/00534/FUM, 13/00536/COU and 13/00537/FUL)</del></p> <p><del>—An employment cluster including digital, creative, media and broadcasting businesses and further/higher education uses (i.e. 115,755 sqm of commercial floorspace including data centre, business/studios, education, conference and retail floorspace).</del></p>
SA1.8: Sweetwater	Minor	<p><b>Becomes Site Allocation 1.6.</b></p> <p>Amend site allocation text as follows:</p>

Policy, para.	Type of change	Proposed Change
		<p>“Comprehensive, phased mixed-use development, including residential, employment, retail and community floorspace (<u>served by the existing adjacent primary school and incorporating a new primary school</u>, nursery, health centre and library).”</p> <p>Amend site allocation plan to remove primary school area.</p> <p>Add to Supporting Development Principles:</p> <p><u>The site allocation is expected to yield a minimum of 650 new homes with an affordable housing being delivered in accordance with the current planning permission.</u></p> <p><b>Phasing and implementation</b> 2015/16 2021 onwards.</p>
SA1.9: Bartrip Street South	No change	<b>Becomes Site Allocation 1.7.</b>

**Table 9, Prevailing Building Heights in Hackney Wick and Fish Island**

Location	Height
Hackney Wick and Fish Island (Sub Area 1 as a whole)	20 metres

## Section 11: Sub area 2 – North Stratford and Eton Manor

	Type of change	Proposed Change
Vision	Minor	North Stratford and Eton Manor <del>will</del> <u>has become</u> a thriving <u>neighbourhood</u> and an area of new high-quality housing, and with generous new Local Open Spaces set alongside the parklands of Queen Elizabeth Olympic Park. This <del>will become</del> <u>is a family focus</u> area of vibrant new communities, a place for families to grow and stay. It <del>will have</del> <u>is an area with</u> unrivalled access to public transport, along with excellent schools, community facilities, local shops and services, <del>and be</del> within easy reach of the employment and business opportunities at Stratford and Hackney Wick. It benefits from access to <u>a world-class</u> affordable community sport and leisure activities and <del>become</del> it is a location in which people <del>will</del> aspire to live.
Page 178, Figure 32	Minor	Remove developed parcels from Figure 32.
Page 179 Area profile, Para 11.1	Minor	This area <del>is set to become</del> the earliest established place of change in the Legacy Corporation area. Substantial progress has been made on all site allocations. Much of East Village has been constructed and new delivered homes are already <del>being</del> occupied. <del>Planning permission is in place for the Chobham Manor residential development, with construction of the first phase taking place from mid-2014. Planning permission is also in place for the development of Chobham Farm bordering Leyton Road, with early implementation planned for this scheme providing new homes, open space and local retail use. Chobham Manor and Chobham Farm developments are also starting to become occupied. Chobham Manor residential development phase one is now complete, phase two is well advanced, while the final two phases have secured reserved matter approval. The Chobham Farm development, providing new homes, open space and local retail use, is equally well underway, the first phase is completed with Zone four being currently under construction, and the central section yet to come forward. Chobham Academy accepted its first pupils in 2013 and the Sir Ludwig Guttman Health Centre has opened to serve the wider area.</del>
Para 11.2	Minor	<del>The retail units within the designated Local Centre at East Village are now beginning to thrive, serving local communities. Coupled with other non-residential uses such as the school and health centre combine to provide a heart to the new and expanding community. East Village provides an area mainly focused around Victory Park open space for new local retail space which is identified as a new Local Centre within this Local Plan. Stratford</del>



	Type of change	Proposed Change
		Metropolitan Centre and its retail, leisure, employment opportunities and excellent local and regional transport connections are close by. The employment opportunities at Here East and Hackney Wick are a walk or cycle ride away from this Sub Area.
Para 11.3	Minor	This concentration of new homes is set alongside the north-western-most part of Queen Elizabeth Olympic Park and Lee Valley Hockey and Tennis Centre, which provides a gateway to Queen Elizabeth Olympic Park from the north. Together, the Lee Valley Hockey and Tennis Centre at Eton Manor and Lee Valley VeloPark provide a full range of cycling facilities and a 3,000-seater hockey stadium. <u>These are important leisure and sporting assets hosting local, regional and international events. Five-a-side football pitches are planned and the Eton Manor war memorials (Eton Manor or Villiers Memorial and the Churchill Memorial) are to be relocated to the west of Lee Valley Hockey and Tennis Centre.</u> With the River Lea running along the western edge of the area, it provides a potentially unparalleled living and working environment within east London.
Para 11.4		
Page 180 Area profile	Minor	<ul style="list-style-type: none"> <li>• Continued delivery of the approved housing type, size and tenure mix to ensure a balanced range of homes and a focus on family housing</li> <li>• Continued delivery of high development quality and sustainability standards</li> <li>• <u>Maintaining</u> and achieving quality public spaces and public realm</li> <li>• <del>Allowing for development of existing and future routes between this area and areas to the east and north</del></li> <li>• Ensuring that the Local Centre <del>develops</del> <u>maintains</u> a local function, distinct to the Metropolitan Centre at Stratford</li> <li>• <u>Support the on-going operation and viability of the Lee Valley Hockey and Tennis Centre and the Lee Valley VeloPark.</u></li> </ul>
Page 180	Minor	Insert page to be included to describe the role of the LVRPA owned and operated Velopark and Hockey and Tennis Centre (including map and photographs). Text for page as follows:

	Type of change	Proposed Change
		<p><u>The Lee Valley Hockey and Tennis Centre at Eton Manor and the Lee Valley VeloPark are world class sports facilities. Originally developed for the London 2012 Olympic Games and subsequently transformed they are both important leisure and sporting venues hosting local, national and international events and support the Legacy Corporation’s aspiration to deliver a sporting legacy for local communities. The two venues are owned by the Lee Valley Regional Park Authority and are managed through its leisure trust.</u></p> <p><u>The Legacy Corporation continues to work closely with and support the Park Authority as it seeks to improve and grow the offer associated with the venues, including the further development of the land and facilities associated with the Hockey and Tennis Centre at Eton Manor.</u></p>
Page 185, Table 9	Major	<b>Table 10</b> <u>SA2.4 Chobham Farm North (see Site Allocation SA.2.4) 20 meters</u>
Policy 2.1 – Housing typologies	No change	No change proposed to policy.
Policy 2.2: Leyton Road – improving public realm	No change likely	No change proposed to policy.
Policy 2.3 – Improving connections	Delete	Delete policy as route framework has been implemented. Retain indication of key routes as shown in Figure 33.
Policy 2.4 – Local Centre and non-residential uses	Major	<b>Becomes Policy 2.3.</b> Non-residential uses, including Use Class A1–A5 and B1a, within Sub Area 2 should be small-scale, serve localised need and be concentrated within the designated Local Centre. <u>The Local Centre boundary has been extended to include the mix of established shop frontages along West Park Walk and Prize Walk, as defined on the Policy Map. All non-residential floorspace within the Local Centre boundary is designated as Primary Frontage, as identified on Figure 32. Future uses within the primary frontage should support its local retail function and add to the vitality and attractiveness of the primary frontage area.</u> Outside of the Local Centre, proposals for these uses should be located along key routes and/or in relation to public spaces and should be of a scale that will serve the needs of its immediate surroundings or be ancillary to a main use with which it is associated.
Page 183, para 11.12 Reasoned justification	Major	<b>Reasoned justification 11.10</b>

	Type of change	Proposed Change
		<p>A new Local Centre is identified at East Village, recognising that the amount of retail and other non-residential floorspace concentrated in the identified area provides a focus around the public open spaces here to provide this Centre function. It is important that role of this Centre is established and then protected.</p> <p><u>Designated Local Centre at East Village, has now emerged as a fully functioning local centre. The area is also emerging as a strong independent retail destination, serving localised catchment and complementing the offer at nearby Stratford. The Primary Frontage has been designated to support and maintain its local retail function, vitality and attractiveness. No Secondary Frontage has been designated. It is recognised that the key routes also provide an opportunity for small-scale non-residential uses that, provided they remain of a small scale and ancillary to the main uses of the development blocks, can be appropriate and not undermine the Local Centre function.</u></p>
Page 183, 11.13 Policy application	Major	<p><b>Policy application 11.11</b></p> <p>Any planning applications for new non-residential uses within the Sub Area should be located within the Local Centre boundary or, where they are of a small enough scale, be located along key routes, particularly where these are active frontages as identified in Figure 32. <u>Further uses within Local Centre boundary should maintain appropriate retail uses and support the role and function identified in Table 3.</u></p>
Site Allocation SA2.1: Chobham farm		Site allocation retained unchanged.
SA2.1 Site Allocation Supporting development principles	Minor	<u>The site allocation is expected to yield a minimum of 1,030 new homes (gross) with affordable housing being delivered in accordance with the current planning permission.</u>
SA2.2: East Village		Preferred option - retain current site allocation.
SA2.2 Supporting development principles	Minor	<u>The site allocation is expected to yield a minimum of 1950 new homes (gross) with affordable housing being delivered in accordance with the current planning permission.</u>
Site Allocation SA2.3: Chobham Manor		Site allocation retained unchanged.

	Type of change	Proposed Change
SA2.3 Supporting development principles	Minor	<u>The site allocation is expected to yield a minimum of 860 new homes (gross) with affordable housing being delivered in accordance with the current planning permission.</u>
SA2.4 Chobham Farm North	Major	<p><b>SA2.4 Chobham Farm North</b></p> <p><del>Family focused</del> <u>Mixed-use development, including family housing, external private or shared amenity space and provision of a link through the site into the Chobham Farm public open space to the south of Henrietta Street.</u></p> <p>Supporting development principles:</p> <ul style="list-style-type: none"> <li>• Minimise impacts on residential amenity from railway line to the west and adjoining community building to the north</li> <li>• Provision of safe pedestrian crossing route over Henrietta Street between site and Chobham Farm site allocation development (SA2.1)</li> <li>• Improve public realm and street scene of Leyton Road through design of the development and improvements to the street</li> </ul> <p><u>The site allocation is expected to yield a minimum of 200 new homes (gross) with an affordable housing threshold of 50% on public sector land in accordance with Policy H.2.</u></p>
Address	Major	<p><u>Site address: Land bounded by Liberty Bridge Road, Leyton Road and the railway.</u></p> <p><u>Existing uses: Part- D1 use and the reminder of site is currently in B1 and B8 use class employment uses.</u></p> <p><u>Size: 1.11 ha</u></p> <p><u>PTAL rating: 6a/6b</u></p> <p><u>Flood Zone: 1</u></p>

## Section 12: Sub Area 3 – Central and Southern Queen Elizabeth Olympic Park

Policy, para	Type of change	Proposed Change
Vision	Minor	Central Stratford and Southern Queen Elizabeth Olympic Park will <del>become</del> <u>continue to develop</u> as a diverse area of new high-profile <u>culture</u> , education and sporting facilities....
Sub Area 3 Key Diagram	Minor	Image to identify: <ul style="list-style-type: none"> <li>• Northern zone to Stratford High Street Policy Area</li> <li>• Stratford Waterfront</li> </ul>
Para 12.2	Minor	.....This reinvigorated part of Stratford, a Metropolitan Centre of international importance, combined with Queen Elizabeth Olympic Park, will be the main engine driving growth and regeneration in this part of east London. <u>As a Cultural Quarter the Queen Elizabeth Olympic Park will also be promoted, enhanced and protected.</u>
Para 12.3	Minor	Sub Area 3 will continue to see substantial pressure for new development through the Plan period. A significant amount of new residential, <u>retail, education</u> and office floorspace has planning permission. <del>However,</del> Changing circumstances <del>may</del> mean that <u>consented schemes around Stratford Waterfront are in the process of review.</u> <u>Reviewed proposals will help facilitate and contribute towards the future potential for International Centre status of at Stratford by providing a range of cultural and educational uses alongside the extensive retail, office and residential expansion already planned.</u> <del>of proposals may take place. Where these are outline planning permissions, the final form of new development will become apparent over time but may also present the opportunity for these proposals to change more significantly where economic and other circumstances suggest that this is desirable or necessary. There may also be circumstances where a comprehensive scheme could cover parts of more than one site allocation.</del> Other sites and locations, while not benefiting from a planning permission, do have potential for new development....
Para 12.4	Minor	The Sub Area has potential to deliver many of the strategic requirements of east London as a whole, particularly housing development. <del>As at March 2014, there were approximately 8,700 units within the development pipeline in Sub Area 3, and capacity for about a further 630 units.</del> <u>Results of the 2017 SHLAA suggest capacity for approximately 11,000 homes within the Sub Area up to 2036.</u>
Para 12.5	Minor	<del>Unless specific building heights are stipulated within Policy 3.1, within this Sub Area where any development is proposed above the prevailing height of 30m from ground level, the Tall Buildings policy (BN.10) will apply.</del>

Policy, para	Type of change	Proposed Change		
		<p><b><u>Application of Policy BN.5 within the sub area</u></b></p> <p><u>The prevailing height of development within Sub Area 3 has been established at 30 metres above ground level, equating to approximately 10 storeys of development. This is represented through a range of intermittent building heights that together form a unique arrangement that contributes to the area’s townscape. Policy BN.5 sets out the approach that will need to be applied both in designing new development and in assessing planning applications where this is proposed to exceed this height. Table X below specifically sets out the height above which the policy test set out in Policy BN.5 will apply.</u></p> <p>New Table 11 inserted</p> <table border="1" data-bbox="687 835 1383 875"> <tr> <td data-bbox="687 835 1059 875">Sub Area 3</td> <td data-bbox="1059 835 1383 875">30 metres</td> </tr> </table>	Sub Area 3	30 metres
Sub Area 3	30 metres			
New policy	Major	<p><b><u>Becomes Policy 3.1 Metropolitan Centre</u></b></p> <p><u>Stratford will be promoted to function as a potential future International Centre through:</u></p> <ol style="list-style-type: none"> <li><u>1. Directing large-scale town centre uses to within the centre boundary in accordance with Policy B.2</u></li> <li><u>2. Supporting growth in office floorspace, with the Metropolitan Centre boundary also forming the location for the potential Central Activities Zone reserve</u></li> <li><u>3. Supporting Stratford Waterfront as a new culture and education district and as a future location of town centre boundary extension</u></li> <li><u>4. Focussing higher order comparison retail floorspace, providing at least 80 per cent of the identified retail requirements over the plan period</u></li> <li><u>5. Supporting and enhancing the range of cultural and night time economy uses</u></li> <li><u>6. Delivering new residential accommodation in appropriate locations throughout the centre</u></li> </ol>		
New para	Major	<p><b><u>Reasoned justification</u></b></p> <p><u>12.6 The draft New London Plan identifies potential for Stratford to form a future International Centre. In order for this aspiration to be realised the amount and range of town centre uses should be expanded at this location. The draft New London Plan also identifies Stratford as a potential Central Activities Zone reserve. Given limited land availability within the existing centre boundary, potential locations for expansion also need identifying (as shown within Figure 7).</u></p>		
New para	Major	<p><b><u>Policy application</u></b></p>		

Policy, para	Type of change	Proposed Change
		<p><u>12.7 Any proposals for large-scale town centre uses should be focussed within the existing town centre boundary, or where identified as a potential location for expansion. Other edge of centre locations, such as Stratford High Street Policy Area (see Policy 3.1) also play a role in supporting the diversification of the function of the Centre. Site Allocation 3.1 also guides development of the land falling within the existing Centre boundary, and Site Allocation 3.2 sets out how proposals within Stratford Waterfront East, the potential location for extension of the town centre boundary, should develop.</u></p> <p><u>Residential development will be supported in appropriate locations away from the key shopping areas of the Metropolitan Centre. Where located outside the plots identified within SA3.1 mixed use development including residential should support the town centre designation including that of culture and the night time economy.</u></p>
Policy 3.1	Major	<p><b>Becomes Policy 3.2</b> <del>3.1</del>: Stratford High Street Policy Area</p> <p>Proposals for mixed-use development along Stratford High Street will be required to demonstrate that it will enhance the character, townscape and function as a lively main street., <u>by ensuring that</u> <del>Appropriate proposals for innovative mixed-use products including shared living and where residential and non-residential components are provided as an integrated product in particular focussing on culture and night time economy uses will be supported at the northern zone of the Stratford High Street Policy area (see Figure 34). All other non-residential elements of mixed-use schemes will be acceptable where they maximise flexibility of function and are vertically and horizontally integrated with residential..</del></p>
Policy 3.1	Minor	<p><b>Becomes Policy 3.2</b> Proposals for development greater than <del>27</del> <u>30</u> metres above ground will be subject to Policy BN.10.</p>
Para 12.6	Minor	<p><b>12.8.</b> <u>The introduction of employment-generating uses or cultural and night time economy uses through mixed use development will add to the vitality of the area and contribute to the rejuvenation of the High Street.</u></p>
Para 12.7	Minor	<p><b>12.9</b> <u>Provision of a range of different town <del>Town</del> centre uses and a focus upon culture and night time economy uses within this location should support <del>not be in competition with</del> the further development of the Stratford Metropolitan Centre and have regard to traffic and safety issues. Shared living and other mixed-use developments where new town centre uses are proposed as an integral part of an innovative mixed-use</u></p>

Policy, para	Type of change	Proposed Change
		<u>development product will be considered positively on edge of centre sites at the northern end of the high street where the proposal meets all the requirements of the policy. Mixed use developments proposing flexible....</u>
Policy 3.2	Minor	<b>Becomes Policy 3.3</b> <del>3.2</del> : Improving connections around central Stratford
Figure 35 (And Figure 24)	Minor	Amend to include new crossings of Montfitchet Road
Para 12.14	Minor	<b>12.16</b> Where major development schemes are proposed outside the Site Allocations within this Sub Area, proposals will have regard to all policies within this Local Plan and shall not lead to the loss of planned residential, business or community provision. The introduction of minor uses ancillary to the large-scale venues within the area may be appropriate. Proposals covering more than one Site Allocation, or parts of one, <u>should facilitate, through a portfolio approach the delivery of the aims of each of</u> <del>will be considered against</del> the principles <del>within</del> <u>of</u> the relevant site allocations <u>as a whole.</u>
SA3.1	Minor	A range of town centre uses and residential accommodation appropriate to the scale and form of the Metropolitan Centre designation. <del>The site will form an extension to the Metropolitan Centre Boundary of Stratford with the eastern parcel providing access to the town centre by a Link Bridge. Active uses shall be on the ground floor along enhanced key connections.</del>
SA3.1 Supporting development principles	Major	<ul style="list-style-type: none"> <li>• Provide an overall mix of town centre uses respecting the existing character, scale, and massing within the allocation area</li> <li>• <u>The site allocation is expected to yield a minimum of 2,000 new homes with an affordable housing threshold of 35%, or 50% on public sector land, in accordance with Policy H.2.</u></li> <li>• Suitable for main town centre uses appropriate to the Metropolitan Centre designation</li> <li>• <del>Northern development</del> <u>Development parcel 1</u> should provide a mix of uses, including residential, office and <del>with</del> ground-floor local service retail providing a transition to the residential area to the north</li> <li>• <del>Eastern</del> <u>Development parcel 2</u> should provide a large-scale town centre use with supporting elements, <u>with a link bridge</u></li> </ul>



Policy, para	Type of change	Proposed Change
		<ul style="list-style-type: none"> <li>• <u>Southern Development parcel 3</u> should largely be a <u>mixed use of retail and residential</u> with a transition from retail and other uses, and containing Local Open Space</li> <li>• <u>South-western Development parcel 4 parcels</u> should provide <u>employment uses including offices</u> and residential with the localised retail functions on the ground floors</li> <li>• <u>Development parcel 5</u> should provide <u>residential accommodation, with supporting ground floor uses</u></li> <li>• Key connections shall be enhanced: to the north to East Village; from <del>existing</del> Stratford town centre to the east; <u>from Montfitchet Road across to the Chobham Farm South site (Development Plot 2)</u> from the south through to London Aquatics Centre; and from the west along Westfield Avenue to Queen Elizabeth Olympic Park.</li> <li>• <u>Existing and proposed</u> connectivity routes in private ownership should maintain and enhance the format and appearance of public space <u>and the public realm</u></li> <li>• Points where key connections meet the allocation shall be gateways for enhancement.....</li> </ul>
SA3.1 Phasing and implementation	Minor	<p><del>Residential within The International Quarter will be delivered from 2015</del></p> <ul style="list-style-type: none"> <li>– Other northern and south-western parts of The International Quarter to be delivered from 2020</li> <li>– The housing development at Cherry Park will be delivered from <del>2015</del> <u>2020</u></li> <li>– Delivery of the Chobham Farm South shall depend on access to the site via the town centre Link Bridge and is anticipated to be post-2020.</li> </ul>
SA3.1 Planning history	Minor	<p>Has permission under the Stratford City scheme for 450,000 sqm of office; approximately 1,440 residential units (TIQ- 333, Cherry Park-1,105); 25,500 sqm hotel; 3,000 sqm retail; 2,000 sqm leisure</p> <ul style="list-style-type: none"> <li>– Permission under the Manhattan Loft Gardens scheme for 248 residential units to the north of the International Station</li> </ul>

Policy, para	Type of change	Proposed Change
		<u>Outline permission has also been granted for retail anchor store and 1,200 homes at Cherry Park.</u>
SA3.1	Minor	Amend site allocation image to include new plots, numbering and new connections across Montfitchet Road
SA3.2	Minor	Comprehensive, phased mixed-use development providing <del>edge-of-centre retail</del> , cultural, education, leisure, <u>retail</u> or community functions and incorporating residential to provide for strategic housing requirements. .
SA3.2 Supporting development principles	Major	<ul style="list-style-type: none"> <li>• <u>Provide a mix of uses to support the Metropolitan Centre function (with future potential for inclusion within the town centre boundary) appropriate to the edge-of-centre location</u> in accordance with SP.1, B.2 and B.6</li> <li>• <u>The site allocation is expected to yield a minimum of 500 new homes.</u></li> <li>• <u>Provide affordable housing across the portfolio sites (site allocations SA3.2, SA3.5, SA3.6 and SA 4.3) based on an affordable housing threshold of 50% on public sector land in accordance with Policy H.2.</u></li> <li>• Building form should avoid the ‘canyonisation’ of Carpenters Road. Tall buildings that may be acceptable in this location will be subject to Tall Buildings Policy (BN.5 10)</li> <li>• High residential development density to reflect location and public transport accessibility</li> <li>• Design to take into account waterside setting and the positioning of the London Aquatics Centre and enhance these as focal points</li> <li>• Provision and protection of key connections to and within the site from The International Quarter <u>London</u> to Stratford Waterfront West and beyond via the northern edge of London Aquatics Centre; and a new pedestrian/cycle connection between The International Quarter and Stratford Waterfront East. This should align with the existing urban grain to support permeability and access to Queen Elizabeth Olympic Park and the visitor and sporting facilities within it</li> <li>• Protection of the view through the above connection in line with the Views Policy (BN.10 9)</li> </ul>

Policy, para	Type of change	Proposed Change
		<ul style="list-style-type: none"> <li>• Active uses shall be on the ground floor along key connections to the north-west of London Aquatics Centre and river frontage.</li> </ul>
SA3.2 Phasing and implementation	Minor	<p><b>Phasing and implementation</b> – Delivery on site is expected from 2020 onwards</p> <p>– Delivery of the different uses along the Stratford Waterfront should be phased to ensure a coordinated delivery</p> <p><del>– Phasing should ensure that residential components are delivered alongside other non-residential components</del></p> <p><del>– Where residential is in support of other uses, this should be delivered in tandem.</del></p> <ul style="list-style-type: none"> <li>• <u>Proposals for the site allocation which are linked to other sites should facilitate the delivery of the principles of this site allocation through a portfolio approach.</u></li> </ul>
SA3.2 Address	Minor	<p>Address</p> <p><b>Site address:</b> Land <del>on</del> <u>between waterfront and Carpenters Road between railway line and Waterworks River to the south</u></p> <p><b>Existing use(s):</b> Vacant land</p> <p><b>Size:</b> 8.3 ha</p> <p><b>PTAL rating:</b> 1a–5</p> <p><b>Flood Zone:</b> <u>Zone 3 (mostly) (parts)</u></p>
SA3.3	Minor	<p>Comprehensive, phased mixed-use development providing <del>edge-of-centre retail, education, workspace, edge-of-centre retail, cultural, leisure or community functions potentially incorporating and</del> residential to provide for strategic housing requirements.</p>
SA3.3 Supporting development principles	Major	<ul style="list-style-type: none"> <li>• Provide a mix of uses appropriate to the location in accordance with SP.1, B.2, <u>B.5</u> and B.6</li> <li>• Development should ensure the openness of the Metropolitan Open Land <del>to the east of</del> <u>including within</u> the site allocation</li> <li>• Density reflecting location and Public Transport Accessibility Levels</li> <li>• Tall buildings may be acceptable in this location subject to Tall Buildings Policy (BN.<u>5</u> <del>40</del>)</li> </ul>

Policy, para	Type of change	Proposed Change
		<ul style="list-style-type: none"> <li>• Active uses shall be on the ground floor along key connections including frontages adjacent to the ArcelorMittal Orbit</li> <li>• Design to take into account the waterside setting and open space character of Queen Elizabeth Olympic Park and enhance the setting of the ArcelorMittal Orbit and Queen Elizabeth Olympic Park</li> <li>• Proposals should be designed to take account of, and mitigate, any noise impacts of the rail lines to the south</li> <li>• Protect key connections adjacent and through the site.</li> <li>• <u>Where student housing is provided this should be directly linked to the adjacent education uses in accordance with Policy H4 and will be monitored on a 3:1 basis (3 bedspaces are equivalent to one residential unit)</u></li> <li>• <u>The site allocation is expected to yield a quantum equivalent to a minimum of 600 new homes with an affordable housing threshold of 50% on public sector land in accordance with Policy H.2.</u></li> </ul>
SA3.3 Phasing and implementation	Minor	<ul style="list-style-type: none"> <li>– Delivery on site from <del>2020</del> <u>2018</u> onwards</li> <li>– Delivery of the different uses <del>along the Stratford Waterfront</del> should be phased to ensure a coordinated delivery.</li> <li>• <u>Proposals for the site allocation which are linked to other sites should facilitate the delivery of the principles of this site allocation through a portfolio approach.</u></li> <li>• <u>Where development is phased, introduction of appropriate interim uses is encouraged in accordance with Policy B.3.</u></li> </ul>
SA3.3 Planning history	Minor	<p>The site benefits from planning permission under the LCS scheme for: <del>up to 878 residential units; 1,438 sqm retail; 165 sqm leisure; and 440 sqm community, up to a maximum of 77,043 sqm in total.</del></p> <p><u>Permission granted for mixed use development for academic development and commercial research space, student accommodation and small scale retail space.</u></p>

Policy, para	Type of change	Proposed Change
SA3.3 Address	Minor	<p>Address</p> <p><b>Site address:</b> Land to the south of the ArcelorMittal Orbit, <del>between Waterworks River to the north and City Mill River to the south,</del> bounded by Pool Street and Loop Road</p> <p><b>Existing use(s):</b> Vacant land</p> <p><b>Size:</b> 3.5 ha</p> <p><b>PTAL rating:</b> 1b-2</p> <p><b>Flood Zone:</b> Zone 3 <del>(limited)</del> <u>(parts)</u></p>
SA3.4 Supporting development principles	Major	<ul style="list-style-type: none"> <li>• <u>Proposals should seek to facilitate a net increase in residential accommodation, optimising delivery in accordance with</u> <del>optimise and increase the residential capacity of the area subject in particular to Policy SP.2 and H.1</del> of this Local Plan</li> <li>• <u>The site allocation is expected to yield a minimum of 2,300 new homes (gross) with an affordable housing threshold of 35% or 50% on public sector land, in accordance with Policy H.2.</u></li> <li>• <u>Proposals should maximise affordable housing delivery through the Viability Tested Route re-providing equivalent affordable housing floorspace through equivalent tenures as a minimum</u></li> <li>• <u>Development densities and uses should reflect location, and public transport accessibility and the town centre boundary</u></li> <li>• <u>In accordance with Policy BN.5 any tall buildings should be directed towards the town centre boundary</u></li> <li>• Where provided, commercial and other active uses shall be on the ground floor along key connections, related to the station, Metropolitan Centre and Stratford High Street</li> <li>• Maximise and reflect in any new development or public realm improvement the potential arising from pedestrian movement to and from a new western entrance to Stratford Regional Station and improvements to the Jupp Road bridge</li> <li>• The identified option for the new western entrance to Stratford Regional Station should be incorporated into redevelopment proposals for this site</li> </ul>

Policy, para	Type of change	Proposed Change
		<ul style="list-style-type: none"> <li>• Improve connections from the site to the north and to Stratford Metropolitan Centre; to south-west to the Greenway via Bridgewater Road</li> <li>• Improve connections within the site along Warton Road, Carpenters Road, Gibbins Road and Jupp Road; and to within Queen Elizabeth Olympic Park</li> <li>• Subject to the above, proposals should be in accordance with the provisions of other Local Plan policies including B.1 in relation to employment floorspace, H.1 for housing mix, Cl.1 for community facilities and BN.7 in relation to Local Open Space</li> <li>• Safeguarding of land for DLR North Route Double Tracking phase 2.</li> <li>• Consider retention of existing low-rise family housing where this does not prevent the achievement of wider regeneration objectives</li> <li>• Ensure early community consultation where specific development proposals or regeneration plans are brought forward <u>and take account of the requirements of the Good Practice Guide for Estate Regeneration including residents' ballots</u></li> <li>• Support the <u>Greater Carpenters Neighbourhood Forum in its preparation of a Neighbourhood Plan</u> where this conforms to the requirements of <u>the Local Plan</u> and this site allocation and involves cooperation with the Council in its roles as landowner and housing authority.</li> </ul>
SA3.4 Planning history	Minor	<p><u>Has Permission for student accommodation, residential accommodation, education facility and affordable workspace at Duncan House</u></p>
SA3.5 Supporting development principles	Minor	<ul style="list-style-type: none"> <li>• <u>The site allocation is expected to yield a minimum of 300 new homes.</u></li> <li>• <u>Provide affordable housing across the portfolio sites (site allocations SA3.2, SA3.5, SA3.6 and SA 4.3) based on an affordable housing threshold of 50% in accordance with Policy H.2</u></li> </ul>
SA3.5 Phasing and implementation	Minor	<ul style="list-style-type: none"> <li>• <u>Proposals for the site allocation which are linked to other sites should facilitate the delivery of the principles of this site allocation through a portfolio approach.</u></li> </ul>

Policy, para	Type of change	Proposed Change
SA3.6	Minor	Comprehensive, mixed use development of residential with education uses including provision of <del>an all-age a</del> <u>primary school or equivalent education provision</u> and open space.
SA3.6 Supporting development principles	Major	<ul style="list-style-type: none"> <li>• <u>The site allocation is expected to yield a minimum of 750 new homes.</u></li> <li>• <u>Provide affordable housing across the portfolio sites (site allocations SA3.2, SA3.5, SA3.6 and SA 4.3) based on an affordable housing threshold of 50% in accordance with Policy H.2</u></li> <li>• <u>Development should ensure an active frontage onto Stratford High Street and the junction should be safe and welcoming</u></li> <li>• <u>Where development is phased, introduction of appropriate interim uses is encouraged in accordance with Policy B.3.</u></li> <li>• Development should maintain the openness of the Metropolitan Open Land along the western boundary of the site</li> <li>• <u>Meet any identified demands for school places through provision of all-age school alongside residential development respecting the existing character, scale and massing of the site and its surrounding area</u></li> <li>• Residential capacity could be increased alongside the introduction of business space and significant open space, should the <u>primary school no longer be required all-age school or its secondary school component be delivered within a location suitable to requirements elsewhere within the Legacy Corporation area.</u></li> <li>• Unless school place demand has been <u>or will be</u> demonstrably met elsewhere, retention of sufficient land for delivery of an additional primary school in the later part of the Plan period will be <u>sought</u> <del>required within the</del> site</li> <li>• Development should plan for the associated costs of remediation of the site</li> <li>• Design to reflect the close proximity of industrial and other uses <u>and the potential for wider place-making</u></li> </ul>

Policy, para	Type of change	Proposed Change
		<ul style="list-style-type: none"> <li>• Development will preserve or enhance the listed cottages and the setting of the Conservation Area to the south</li> <li>• Enable safe access <del>to the secondary school</del> <u>to and across the site</u> for pedestrians and cyclists</li> <li>• Development shall respect the existing character, scale and massing of the site and its surrounding area</li> <li>• Cycling and walking access improvements along the Greenway <u>including links to the Channelsea Path beyond the site</u></li> <li>• Proposals to include Local Open Space including play space and BAP habitat</li> <li>• <del>Building heights generally less than 36 metres above ground level, grading down to the south-east</del></li> <li>• <u>Proposals for development above 30 metres from ground level will only be acceptable subject to the provisions of Policy BN.5</u></li> </ul>
SA3.6 Phasing and Implementation	Minor	<ul style="list-style-type: none"> <li><del>– School provision to be provided prior to, or in parallel with, the residential elements</del></li> <li>– Development shall take place once remediation of the land and removal of equipment has taken place including revocation of Hazardous Substance Consent</li> <li>– Delivery on site from <del>2020-2021</del> onwards.</li> <li><u>– Comprehensive delivery across the whole of the site, phased to allow for the timely delivery of housing, for land availability, and to ensure infrastructure requirements are met</u></li> <li><u>- Proposals for the site allocation which are linked to other sites should facilitate the delivery the principles of this site allocation through a portfolio approach.</u></li> </ul>
SA3.6 Planning History	Minor	<p><u>A secondary school associated with the Legacy Communities Scheme has subsequently been provided on Stadium Island (The secondary school associated with the Legacy Communities Scheme has subsequently been built and opened on a different site, adjacent to the London Stadium)</u></p>



Policy, para	Type of change	Proposed Change
Evidence base references	Minor	<u>Retail and Town Centre Needs Study (London Legacy Corporation, 2018)</u>

**Table x**

<u>Location</u>	<u>Height</u>
<u>Whole of sub area 3</u>	<u>30m</u>

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## Section 13 – Sub Area 4 Bromley-by-Bow, Pudding Mill, Sugar House Lane and Mill Meads

Policy, para.	Type of change	Proposed change
Page 211 – Vision	Minor	<p>Vision</p> <p>This will become an area of new business and residential communities that find a focus at a new District Centre at Bromley-by-Bow and a new Local Centre at Pudding Mill, with a secondary hub of employment and leisure uses in the north part of Sugar House Lane. The District Centre at Bromley-by-Bow will provide a new primary school, community facilities and public open spaces. A new DLR station at Pudding Mill and an enhanced Bromley-by-Bow Station will provide excellent public transport links to nearby work and leisure opportunities and good access to the rest of London. New and improved local foot and cycle paths will provide accessible and safe routes to the stations and local shops and services. The many new homes in Bromley-by-Bow, Sugar House Lane and Pudding Mill will meet a wide range of housing needs, while the new homes, business and other premises will have been sensitively and excellently designed, taking account of the historic waterside settings and the heritage assets within and around the Conservation Areas. By <del>2031</del><u>2036</u>, the Sub Area will have become a distinct series of new urban communities, well connected to their surroundings.</p>
Page 213 – Area Analysis	Deletion	Section to be deleted.
Page 215 - 4.1: A potential District Centre	Minor	<p>Policy 4.1: A potential District Centre</p> <p>The Legacy Corporation supports the future designation of a new District Centre at Bromley-by-Bow, in accordance with Table <del>A2.2A1.1</del><u>A1.1</u> of Annex <del>12</del><u>11</u> of the London Plan <del>2011</del>. Proposals for development will be required to demonstrate that they:</p> <ol style="list-style-type: none"> <li>1. Achieve, or are part of, a comprehensive development of the Bromley-by-Bow Site Allocation area</li> <li>2. Include an appropriate mix and balance of uses that together have the potential to function as a District Centre. This mix should include retail, employment, community uses, a primary school, open space</li> <li>3. Respond positively to the adjacent waterways and listed buildings at Three Mills</li> </ol>

Policy, para.	Type of change	Proposed change
		<p>4. Provide appropriate on-site infrastructure that will be secured through a Section 106 Agreement needed to make the development acceptable, such as appropriate access arrangements to the site for pedestrians, vehicles and transport</p> <p>5. Contribute to the wider aspirations for design and regeneration of the wider Bromley-by-Bow area.</p>
<p>Page 216 - 4.2: Bringing forward new connections to serve new development</p>	<p>Minor</p>	<p>Policy 4.2: Bringing forward new connections to serve new development</p> <p>Development proposals within Sub Area 4 should not prejudice and, where relevant, should contribute towards the improvement of existing and the delivery of new connections necessary to serve the anticipated needs of development within the Sub Area.</p> <p>The improvements to existing and new connections considered necessary for the delivery of the development anticipated within this Sub Area are:</p> <ol style="list-style-type: none"> <li>1. Accessibility improvements including a new junction on the A12 at Bromley-by-Bow that serves that potential new District Centre by improving access for pedestrians, cyclists, buses and general traffic</li> <li>2. Improving the pedestrian underpass adjacent to Bromley-by-Bow Station and linking pedestrian and cycle routes to allow access to the new District Centre and the Lee Valley Regional Park beyond</li> <li>3. New and improved vehicle, pedestrian and cycle bridges across the River Lea; a new all-movements junction on the A118 to improve access to and from Sugar House Lane for pedestrians, cyclists, buses and general traffic</li> <li>4. A new bridge across the Bow Back River linking to the all-movements junction and connecting with Marshgate Lane</li> <li><del>5. Delivery of a cycle superhighway route along Stratford High Street</del></li> <li>5. Delivery of a west-east pedestrian and cycle route, parallel with Stratford High Street, through</li> </ol>

Policy, para.	Type of change	Proposed change
		<p>Pudding Mill, across the Greenway and through the Greater Carpenters area parallel to Stratford Metropolitan Centre</p> <p>6. Pedestrian and cycle improvements at Bow Interchange</p> <p>7. New and improved pedestrian and cycle links from Pudding Mill Lane DLR Station to Queen Elizabeth Olympic Park via the Greenway and Southern Loop Road.</p>
Page 218 - 4.3: Station Improvements	Minor	<p>Policy 4.3: Station improvements</p> <p>In considering proposals to improve Bromley-by-Bow Station, <u>to further enhance the existing improvements that have been made</u>, the Legacy Corporation will support proposals that improve accessibility to and within the station and enhance its visual presence within the area. The Legacy Corporation will also require proposed development on adjacent sites to provide walking routes within their sites that are orientated towards the station and help provide legible and direct access to it.</p>
Policy 4.4 and para's 13.9-13.10 (Page 219) -: Protecting and enhancing heritage assets at Three Mills Island and Sugar House Lane	Deletion	<p>Policy deleted along with supporting text and replaced by a site allocation (see below).</p>
Page 220 - SA4.1: Bromley-by-Bow	Minor	<p>A new mixed-use area including:</p> <ul style="list-style-type: none"> <li>• New and reprovided retail floorspace that is capable of functioning alongside a mix of uses, as a new District Centre</li> <li>• A primary school</li> <li>• A new 1.2 hectare park</li> <li>• Riverside walk</li> <li>• Community facility (e.g. library)</li> <li>• New homes with a significant element of family housing</li> <li>• New employment-generating business space in a range of sizes and formats.</li> </ul>

Policy, para.	Type of change	Proposed change
		<p>Proposals for development will be required to demonstrate that they contribute to the comprehensive development of the Bromley-by-Bow Site Allocation area, <u>taking into account the Bromley-by-Bow SPD (adopted 2017) which sets out specific policies for the area.</u> To do this, applications will need to demonstrate:</p> <ul style="list-style-type: none"> <li>• that a masterplan approach for the Site Allocation as a whole is followed</li> <li>• that phasing of development across the overall site is appropriate and secured by condition or through Section 106 Agreements attached to future planning permissions</li> <li>• that there is certainty of timely delivery for the key elements of social and physical infrastructure and land uses identified as required within this site allocation</li> <li>• Proposals will need to include an appropriate mix and balance of uses that together have the potential to function as a District Centre, including retail, employment floorspace, community uses, a primary school, a new park and improved public realm</li> <li>• New development should respond positively to the adjacent waterways and Listed Buildings and Conservation Area at Three Mills. Specifically, development should be lower in scale by the canal and the new park should be located adjacent to the River Lea, <u>particular reference should be made to policy T.10 and access to the canal</u></li> <li>• <u>Where development is phased, introduction of appropriate interim uses is encouraged in accordance with Policy B.3.</u></li> <li>• <del>Proposals for development greater than 18 metres above ground level will only be acceptable subject to the provisions of Policy BN.10</del></li> <li>• Safe pedestrian and cycling access should be provided, particularly to the primary school.</li> <li>• Landing for bridges from Sugar House Lane will need to be incorporated into development proposals.</li> <li>• <u>The site allocation is expected to yield a minimum of 1700 new homes with an affordable housing threshold of 35% in accordance with Policy H2.</u></li> </ul>

Policy, para.	Type of change	Proposed change
Page 221 SA.4.1 Planning History	Minor	<p><b>Relevant planning history</b></p> <p><del>1. There are no listed buildings or Conservation Areas on the site</del></p> <p><del>2. Planning permission was granted in 2010 for a comprehensive mixed-use scheme, on the southern part of the Site Allocation but this permission has now lapsed</del></p> <p><del>3. 1. Permission was granted in 2012 for a predominantly housing development to the northern part of the Site Allocation (PA/11/02423/LBTH) and it is understood that this will be being implemented.</del></p> <p><u>2. Planning permission 17/00334/FUL for mixed use development, including 407 residential units.</u></p> <p><u>3. Planning permission 17/00364/FUL for mixed-use development, including 491 residential units.</u></p>
Page 222 - SA4.2: Sugar House Lane	Minor	<p>Update will be needed to relevant planning history,</p> <ul style="list-style-type: none"> <li><u>The site allocation is expected to yield a minimum of 1200 new homes with affordable housing being delivered in accordance with the current planning permission.</u></li> </ul>
Page 223 – SA.4.2 Sugar House Lane	Minor	<p>Delete the following text:</p> <p><del><b>Existing uses:</b> The area currently accommodates a food store and associated car parking, industrial and distribution activities and vacant land and buildings. Bromley by Bow Station is located to the south west of the Site Allocation area, on the other side of the A12</del></p>
Page 224 - SA4.3: Pudding Mill	Minor	<p>A new medium-density, mixed-use area, including a significant and diverse element of new and replacement business floorspace, including spaces suitable for small- and medium-sized businesses; a new Local Centre adjacent to Pudding Mill Lane DLR Station and Pudding Mill Lane; new homes including a significant element of family housing; new Local Open Space, playspace and public realm. Cumulatively across the Pudding Mill Site Allocation, 25 per cent non-residential floorspace should be achieved, with <u>intensified a predominantly industrial floorspace use mix</u> in the area to the west of Cooks Road and around the Crossrail portal. <u>This is in line with the Pudding Mill SPD (adopted 2017) which sets out local policies around development in this area.</u></p> <p><del>• Proposals for development above 21 metres above ground level will only be acceptable subject to the provisions of Policy BN.10</del></p>

Policy, para.	Type of change	Proposed change
		<ul style="list-style-type: none"> <li>• Non-residential uses should be focused along a new central east-west street</li> <li>• The form of development should allow for improved east-west connections through the site</li> <li>• Provision should be made for key connections, including a new bus/cycle/ pedestrian connection from Stratford High Street to Marshgate Lane and a new pedestrian/cycle connection from Wrexham Road over the A12 and River Lea</li> <li>• <u>Where development is phased, introduction of appropriate interim uses is encouraged in accordance with Policy B.3.</u></li> <li>• <del>Land should be safeguarded for DLR North Route Double Tracking phase 2.</del></li> <li>• Regard will need to be had to not prejudicing the operation of the safeguarded rail freight site to the west (for example by ensuring that noise sensitive uses are located away from the site).</li> </ul> <p><b>Supporting development principles</b></p> <ul style="list-style-type: none"> <li>• Landowners will need to work together to bring forward comprehensive schemes that are capable of achieving the ambitions for development of the site allocation and delivering identified infrastructure needed for the site as a whole.</li> <li>• Open Space/playspace needs to be provided alongside development and located within pockets across the site</li> <li>• A new Local Centre should be brought forward adjacent to Pudding Mill <u>Lane</u> DLR Station</li> <li>• A significant proportion of family homes should be provided</li> <li>• It would be appropriate to re-align Barbers Road to provide a dual fronted street and screening to the Crossrail site</li> <li>• Other Industrial Location designation maintained along the western edge <del>to form a buffer to A12</del> <u>where industrial</u></li> </ul>

Policy, para.	Type of change	Proposed change
		<p><u>uses should be intensified, and facilitate the co-location with residential uses (see Policy B.1 and Pudding Mill SPD)</u></p> <ul style="list-style-type: none"> <li>• <u>The site allocation is expected to yield a minimum of 2,000 new homes.</u></li> <li>• <u>Provide affordable housing across the portfolio sites (site allocations SA3.2, SA3.5, SA3.6 and SA 4.3) based on an affordable housing threshold of 50% in accordance with Policy H.2</u></li> </ul> <p><b>Phasing and implementation</b></p> <p>– Delivery on site from 2015 onwards should be updated in line with updated timelines</p>
<p>Page 225 SA4.3 Pudding Mill</p>	<p>Minor</p>	<p><b>Relevant planning history</b></p> <p>There are no listed buildings or Conservation Areas on the site.</p> <p>11/90621/OUTODA – Part of the site has <u>outline</u> planning permission under the Legacy Communities Scheme permission granted in 2012 <u>for a substantial mixed use scheme, including a new Local Centre, 118,290 sqm of residential development (Use Class C3) and associated community uses and open space.</u></p> <ul style="list-style-type: none"> <li>– Development of up to 118,290 sqm of residential development (Class C3)</li> <li>– Development of up to 2,345 sqm of retail and food/drink (Classes A1–A5)</li> <li>– Development of up to 169 sqm of leisure development (Class D2)</li> <li>– Development of 23,791 sqm of employment (Class B1a) and up to 12,158 sqm of (Class B1b+B1c)</li> <li>– Development of up to 1,482 sqm of community development (Class D1)</li> <li>– Provision of 1,000 sqm of Open Space, including playspace.</li> </ul>



Policy, para.	Type of change	Proposed change
		<p><u>12/02202/AOD (NEW/2/4/1) – Construction of a new DLR railway station at Pudding Mill Lane under the Crossrail Act 2008.</u></p> <p><u>11/00492/AOD (NEW/2/7) – Construction of various structures associated with the Crossrail portal and new DLR station under the Crossrail Act 2008.</u></p> <p><u>Planning permission 14/00422/FUL for mixed use development of 254 residential units and 4,257 sqm of business space.</u></p> <p><u>Planning permission 15/00392/FUL for mixed use development of 194 residential units and 2,136sqm of commercial floorspace.</u></p>
Page 225 SA4.3 Pudding Mill	Minor	<b>Existing uses:</b> light industrial and varied employment uses <u>with some residential.</u> <del>There is no existing residential use within the site.</del>
<u>SA4.4: Three Mills</u>	<u>New Site Allocation</u>	<p><u>SA4.4: Three Mills</u></p> <p><u>Site renewal, focusing on restoration and conservation of the existing heritage building on the site to preserve the character of the area whilst updating facilities to ensure they are fit for purpose to maintain and enhancing the existing employment usage on site whilst maximising the opportunities of the site, including enabling development to support renewal.</u></p> <ul style="list-style-type: none"> <li><u>Proposals should set out how the historic buildings on site and the conservation area will be protected and enhanced by any development that takes place on site</u></li> <li><u>The group of listed buildings at Three Mills Island, including the Grade I listed House Mill, and the listed buildings at Abbey Mills, provide an important historical context to the southern part of the Sub Area and the proposed district centre. It is important to ensure that any new development sits well alongside the existing heritage assets and does not impact negatively upon them.</u></li> </ul>

Policy, para.	Type of change	Proposed change
		<ul style="list-style-type: none"> <li>• <u>Area included within this site allocation includes a range of uses and facilities, any enabling development within proposals should be proportionate and not dominate the development and therefore change the character of the area.</u></li> <li>• <u>Proposals should preserve and enhance the existing usage on site such as the employment usage, supporting the range of creative and business usage on site.</u></li> </ul> <p><u>Supporting development principles</u></p> <ul style="list-style-type: none"> <li>• <u>Proposals for new development or new uses within existing buildings within Three Mills Island will need to demonstrate that they preserve or enhance the character of the Conservation Area and the setting of the listed buildings</u></li> <li>• <u>Proposals will should complement the range of existing employment, including cultural and creative employment and community uses</u></li> <li>• <u>Proposals for development should also preserve or enhance the character and appearance of the Three Mills and Sugar House Lane Conservation Areas</u></li> <li>• <u>Views from the Three Mills Conservation area will be protected from inappropriate development.</u></li> <li>• <u>Proposals for this site allocation will set out how they contribute to the overall vision for sub area four, making reference to planning documents that relate to the wider area such as the Bromley-by-Bow SPD, the proposed district centre at Bromley-by-Bow and the neighbouring Sugar House Lane conservation area.</u></li> <li>• <u>The site allocation is expected to yield a minimum of 100 new homes with an affordable housing threshold of 50% in accordance with Policy H2.</u></li> </ul>
<u>SA4.5: Bow Goods Yards (Bow East and West)</u>	<u>New Site Allocation</u>	Site Allocation SA4.5: Bow Goods Yards (Bow East and West)

Policy, para.	Type of change	Proposed change
		<p>An area of Strategic Industrial Land providing protected freight rail head facilities, divided by the River Lee but linked by rail infrastructure, appropriate for bulk freight and other uses associated with Strategic Industrial Land. This presents the opportunity for long term intensification and development of rail freight, transport and associated industrial uses.</p> <p>Consolidation and intensification of rail, industrial and other appropriate employment uses would present the opportunity in the long-term for an element of release of land at Bow East for alternative uses, provided that:</p> <ul style="list-style-type: none"> <li>• This formed part of a comprehensive masterplan approach</li> <li>• This provided rail access and freight function to both Bow East and Bow West</li> <li>• Continued to provide at least an equivalent amount of SIL function capacity as the current land area</li> <li>• Significantly increased the overall job density of the site allocation area</li> <li>• Secures the long-term provision of sufficient rail and transport infrastructure to serve the uses planned through the comprehensive masterplan approach</li> <li>• Provides an alternative road access across the site allocation area to enable servicing and access to and from the A12</li> <li>• Does not negatively impact on the surrounding highway infrastructure or road safety for those using that surrounding highway and transport network</li> <li>• Demonstrates an acceptable relationship between the rail and other SIL uses and any non-SIL uses proposed, including noise, air quality and visual impact, applying the 'Agent of Change' principle.</li> </ul> <p>Where these requirements are met, any area released for non-SIL uses can include residential development but must demonstrate a strong relationship and connections to:</p>

Policy, para.	Type of change	Proposed change
		<ul style="list-style-type: none"> <li>• The new Local Centre and DLR Station at Pudding Mill</li> <li>• The emerging residential communities at both Pudding Mill and Sweetwater</li> <li>• The Greenway and River Lea, including open space provision and an enhanced landscaped setting to these features.</li> </ul> <p>Supporting Development Principles</p> <ul style="list-style-type: none"> <li>• Creating a buffer of employment uses between the rail and industrial uses and any residential use</li> <li>• Where any residential use is proposed via intensification and consolidation, affordable housing to be defined in accordance with Local Plan Policy (H2) and defining the site as publicly owned</li> <li>• Provision of a significant biodiverse open space buffer along the waterway edge</li> <li>• Ensuring the eastern edge of the site and any vehicle access connections to Pudding Mill Lane and the Loop Road is designed such that it remains a safe route for pedestrians and cyclists moving between Pudding Mill Station and the Queen Elizabeth Olympic Park, its schools and venues</li> <li>• The comprehensive masterplan approach could be phased in its delivery</li> </ul> <p>Industrial and storage distribution uses should be enclosed in high quality and well-designed enclosing structures which complement the views through and within the site as well as reflecting the wider context of proximity of the Queen Elizabeth Olympic Park and its unique setting.</p>

## Section 14 – Delivery and Implementation

Policy, para	Type of change	Proposed change
Para. 14.6	Minor	<u>An</u> <del>The</del> Infrastructure Delivery Plan (IDP) <del>which was</del> <u>has been</u> prepared by the Legacy Corporation in consultation with the Boroughs and other stakeholders. <del>in 2013</del> <u>It</u> identifies the infrastructure that will be needed to support the planned growth within the Legacy Corporation area, and identifies the costs and funding gap for this infrastructure. The IDP <del>will be</del> <u>is</u> reviewed annually and updated as necessary as part of the Authority Monitoring Report. A CIL Infrastructure List has been published alongside the adopted Community Infrastructure Levy Charging Schedule. This sets out which infrastructure the Legacy Corporation intends to fund from its CIL.
Table 11: Infrastructure Delivery Policies	Minor	<u>See below for updated table</u>
Para 14.9	Minor	The Legacy Corporation CIL Charging Schedule came into effect on 6th April 2015. The money raised will be used to help deliver the infrastructure on the CIL <u>Infrastructure List</u> (regulation 123) list and in the Infrastructure Delivery Plan. Further information on the Legacy Corporation CIL is available from the Legacy Corporation website. At their meeting in June 2013 the Board agreed to establish a ‘Project Proposals Group’ to allocate future CIL receipts and section 106 funds. <del>The group is now in operation and information on the allocations made by the group is available for each preceding year from the Authorities Monitoring Report. CIL regulations provide for a ‘neighbourhood portion’ of the funding to be spent in consultation with the local community. The Legacy Corporation has established a Neighbourhood CIL Fund and a local mechanism established to allow local projects to bid for funding from this. will follow government advice on engaging with local communities in the Legacy Corporation area to agree with them how best to spend the neighbourhood funding portion of CIL funds.</del>
Para 14.11	Minor	<del>An</del> <u>Planning Obligations interim draft</u> Supplementary Planning Document <u>was adopted in November 2016 that has been produced which</u> sets out how Section 106 Planning Obligations will be used in the Legacy Corporation area and how they will be used alongside CIL to secure infrastructure. <u>This will be regularly reviewed and updated when necessary to ensure that it remains relevant.</u>
Para 14.13	Minor	Supplementary Planning Documents (SPDs) will be prepared <u>where it is necessary</u> to provide further guidance to the Local Plan and help deliver its

Policy, para	Type of change	Proposed change
		<p>objectives. <del>At present, the following documents are considered necessary, and will be taken forward for adoption following adoption of this Local Plan. The following SPDs have been prepared and adopted since the Local Plan was first adopted in 2015:</del></p> <ul style="list-style-type: none"> <li>• Planning Obligations SPD</li> <li>• Carbon Off-set Funding SPD</li> <li>• Hackney Wick &amp; Fish Island SPD</li> <li>• Pudding Mill SPD</li> <li>• Bromley-by-Bow SPD.</li> </ul>
Para 14.16	Minor	<p><b>Amend text as follows:</b></p> <p>It may be appropriate, in the interests of the proper planning of the Legacy Corporation area, for the Legacy Corporation to acquire properties using these powers, if this would facilitate the regeneration of its area, and this regeneration could not be achieved without using these powers. The Legacy Corporation will follow the <u>Government guidance current at the time as set out in ODPM Circular '06/2004 and 04/2010 Compulsory Purchase and the Criche Down Rules'</u> in deciding whether use of its powers would be appropriate.</p>
Para 14.20	Minor	<p><b><u>Monitoring and future review of the Local Plan</u></b></p> <p>In order to measure the success of the strategy and policies within this Local Plan and help to identify any potential need for a review of all or part of the Local Plan, the Key Performance Indicators (KPIs) set out in Table 12 will be used. A review of the Plan <u>will be undertaken at least once every five years or earlier</u> <del>is likely to be triggered</del> where this monitoring shows that key <u>strategic</u> elements of the Plan, such as delivery against housing targets, would not be met to a significant or on-going extent, <del>or in 2018/19 in any event</del>. Monitoring of these indicators will be reported within the Legacy Corporation's annual Authority Monitoring Report. This report will also include annual updates of the activities undertaken in relation to the Duty to Cooperate. In addition to Local Plan monitoring the Growth Boroughs produce regular Convergence progress reports which report on performance against the Convergence themes and indicators.</p> <p><u>It is likely that the Legacy Corporation will cease to be the Local Planning Authority at some point during the Plan period which runs to 2036. Responsibility for monitoring and reviewing the Local Plan would then become the responsibility of each borough within its own boundary. Once each borough Local Plan is subsequently reviewed and updated it is assumed that these subsume the area of the borough currently covered by this Local Plan. Local</u></p>

Policy, para	Type of change	Proposed change
		<u>Planning Authority responsibilities for neighbourhood planning would also be passed to each borough while responsibility for collecting and otherwise administering the Community Infrastructure Levy would also be transferred.</u>

**Table 11: Infrastructure Delivery Policies**

Type	Description (from IDP)	Policy	Sub Area and Site Allocation
Social Infrastructure	Education (primary, secondary and early years)	CI.2: Planning for and bringing forward new schools	<p><b>Primary</b> Sub Area 1: SA1.64, SA1.7 and SA1.8 Sub Area 3 SA3.4 and SA3.6 Sub Area 4: SA4.1</p> <p><del>Secondary</del> <del>Sub Area 3 SA3.6</del></p> <p><b>Early years</b> Sub Area 1 SA1.4 SA1.5 and SA1.6 Sub Area 2 SA2.3 Sub Area 3 SA3.2 and SA3.3 Sub Area 4 SA4.3</p>
	Primary healthcare; open space; green infrastructure; child play space	<p>CI.1 Providing new and retaining existing community infrastructure</p> <p>SP.3 Integrating the built and natural environment</p> <p>BN.3 Maximising biodiversity</p> <p>BN.6 Protecting Metropolitan Open Land</p> <p>BN.78: Improving Local Open Space</p> <p>BN.89 Maximising opportunities for play</p> <p>S.79 Overheating and Urban Greening</p>	<p><b>Primary Healthcare</b> Sub Area 1 SA1.1 and SA1.86 Sub Area 3 SA3.5 Sub Area 4 SA4.3</p> <p><b>Open Space</b> Sub Area 1,2,3 and 4 Sub area 1 SA1.1, SA1.3, SA1.5 and SA1.64 Sub area 4 SA4.1 and SA4.2</p> <p><b>Child play space</b> Sub Area, 1,2,3 and 4 Sub Area 3 SA3.6 Sub Area 4 SA4.1, SA4.2 and SA4.3</p>
	Sports facilities (courts and swimming pools)	CI.1: Providing new and retaining existing community infrastructure	<p><b>Sports facilities</b> Sub Area 2 Sub Area 4</p>

	Libraries, community centres and community space	CI.1: Providing new and retaining existing community infrastructure 4.1: A potential District Centre SA1.1: Hackney Wick Station Area	<b>Libraries, community centres and community space</b> <u>Sub Area 1 SA1.1</u> Sub Area 3 SA3.4 Sub Area 4 SA4.1
Transport	Local connectivity and transport improvements	SP.4: Planning for and securing transport infrastructure to support growth and convergence T.2: Transport improvements T.3: Supporting transport schemes T.4: Managing development and its transport impacts T.6: Facilitating local connectivity T.10: Using the waterways for transport 1.3: Connecting Hackney Wick and Fish Island <del>2.3 Improving connections</del> 3.23: Improving connections around central Stratford 4.2: Bringing forward new connections to serve new development 4.3: Station improvements	<b>Local connectivity and transport improvements</b> Sub area 1,2,3 and 4 Sub Area 1 SA1.3 <del>and SA1.5</del> Employment cluster designation B.1a3 Sub Area 2 SA2.1 Sub Area 3 SA3.2, SA3.3 and SA3.4 Sub Area 4 SA4.1, SA4.2, <del>and SA4.3 and SA4.5</del>
	Strategic Transport Improvements	SP.4: Planning for and securing infrastructure to support growth and convergence T.1: Strategic Transport Improvements	<b>Strategic Transport Improvements</b> Sub Area 1 SA1.1 Sub Area 3 Sub Area 4
Utilities and Hard Infrastructure	Energy (electricity, gas and Combined Cooling, Heat and Power [CCHP])	S.2: Energy in new development S.3: Energy infrastructure and heat networks	<b>Energy</b> All sub areas



	Sewerage (waste water)	S.5: Water supply and waste water disposal S.68: Waste reduction, recycling and composting	<b>Sewerage</b> All sub areas
	Waste management and flood defences	IN.2 S.7: Planning for waste S.810: Flood risk and sustainable drainage measures SP.4: Planning for and securing infrastructure to support growth and convergence	<b>Waste management and flood defences</b> All sub areas
	<u>Telecommunications and Digital Technology</u>	<u>S.6: Increasing digital connectivity, safeguarding existing communications provision and enabling future infrastructure</u>	<b><u>Telecommunications and Digital Technology</u></b> All sub areas

**Table 12: Local Plan Key Performance Indicators**

NO.	KEY PERFORMANCE INDICATOR	MONITORING CRITERIA	RELEVANT OBJECTIVE
1	Growth in economic activity	<ul style="list-style-type: none"> <li>Percentage of working-age residents in employment within the four Growth Boroughs compared to the London average</li> <li>Net gain/loss in employment floorspace (B Use Class) compared to 2015 baseline</li> </ul>	1
<del>2-4</del>	Creation of retail centres	<ul style="list-style-type: none"> <li>Net gain/loss in retail and leisure floorspace (A1-5, C1 and D2 Use Classes) within the identified centres</li> <li>Vacancy rates within the identified centres compared to the London average.</li> <li>Number of jobs/local jobs/opportunities within employment training initiatives created.</li> </ul>	1
<del>3-5</del>	Supply of housing	<ul style="list-style-type: none"> <li>Number of homes permitted per annum</li> <li>Number of 'affordable' homes permitted per annum and the tenure breakdown</li> <li>Number of homes completed per annum</li> <li>Number of 'affordable' homes completed per annum and the tenure breakdown</li> <li><u>Number and % of schemes containing residential going through the Fast Track Route (FTR)</u></li> <li><del>Average number of bedrooms per unit</del></li> <li>Number of one-, two- and three-bedroom plus units permitted per annum (% of total)</li> </ul>	2

NO.	KEY PERFORMANCE INDICATOR	MONITORING CRITERIA	RELEVANT OBJECTIVE
		<ul style="list-style-type: none"> <li>• Amount of accommodation provided for students, older persons and gypsies and travellers.</li> </ul>	
4-6	Provision and protection of community facilities	<ul style="list-style-type: none"> <li>• Net gain/loss in community floorspace (D1 Use Class)</li> <li>• On-site community infrastructure secured through S106 Agreement as part of large-scale development.</li> <li>• Number of school places provided and /or granted planning permission</li> <li>• Number and capacity of new health facilities approved</li> </ul>	2
5-7	Protecting heritage assets and improving design quality	<ul style="list-style-type: none"> <li>• Loss of heritage assets</li> <li>• <del>Proportion of relevant approved applications (proposing residential use) that meet the 'Baseline' Quality and Design Standards outlined within Annex 1 of the Mayor's Housing SPG (excluding any elements of the Baseline Standards that are addressed by the Nationally Described Space Standards – Technical Requirements and Policies BN.5 and S.5).</del></li> <li>• Proportion of relevant approved applications (proposing non-residential use) that incorporate all applicable elements of the Legacy Corporation's Inclusive Design Standards.</li> <li>• Proportions of relevant approved applications (proposing residential use) that provide 90% of dwellings in accordance with Optional Requirement M4 (2) Category 2 of Part M of the Building Regulations, and 10% of dwellings in accordance with Optional Requirement M4 (3) Category 3 of Part M of the Building Regulations.</li> <li>• Proportion of relevant approved applications (proposing residential use) meeting the Nationally Described Space Standards – Technical Requirements.</li> <li>• Proportion of relevant approved applications that meet 'Site layout planning for daylight and sunlight' (BRE, 2011) or superseding guidance.</li> </ul>	3,5
6-8	Retaining open space	<ul style="list-style-type: none"> <li>• Quantum of open space gained or lost through development</li> </ul>	3, 5
7-9	Protect biodiversity and habitat	<ul style="list-style-type: none"> <li>• <del>No net loss of SINCS</del></li> <li>• <u>Number of applications approved for development schemes that provide a net gain</u></li> <li>• Number of applications approved for development schemes meeting <u>the Urban Greening Factor target score including urban greening initiatives.</u></li> </ul>	3, 5
8-10	Improving the waterway environment	<ul style="list-style-type: none"> <li>• Number of applications approved for major development schemes next to the waterways including measures to improve the environment of the Blue Ribbon Network.</li> </ul>	3, 5

NO.	KEY PERFORMANCE INDICATOR	MONITORING CRITERIA	RELEVANT OBJECTIVE
<del>9</del> <u>11</u>	Managing transport impacts	<ul style="list-style-type: none"> <li>• Number of Travel Plans entered into through either condition or S106 Agreement</li> <li>• Travel Plan reviews to monitor: 1. Trip generation rates 2. Mode share and change in mode share over time 3. The effectiveness of the Travel Plan measures 4. The effectiveness of delivery and servicing strategies</li> </ul>	4, 5
<del>10</del> <u>12</u>	Reducing car use	<ul style="list-style-type: none"> <li>• Number of car club spaces approved.</li> </ul>	4, 5
<del>11</del> <u>13</u>	Delivering electric-charging infrastructure	<ul style="list-style-type: none"> <li>• Number of electric-charging points approved.</li> </ul>	4, 5
<del>12</del> <u>14</u>	Car parking provision	<ul style="list-style-type: none"> <li>• Number of applications approved for car-free or car-capped development schemes</li> <li>• Net gain/loss of car parking spaces.</li> </ul>	4, 5
<del>13</del> <u>15</u>	Cycle parking provision	<ul style="list-style-type: none"> <li>• Net gain/loss of cycle parking spaces.</li> </ul>	4, 5
<del>14</del> <u>16</u>	Delivering transport infrastructure	<ul style="list-style-type: none"> <li>• Infrastructure provided on site as part of development – e.g. new junctions, new cycle paths, new footpaths.</li> </ul>	4, 5
<del>15</del> <u>2</u>	Improvements in IMD	<ul style="list-style-type: none"> <li>• Changes in Indices of Multiple Deprivation within Wards that fall within the Legacy Corporation area.</li> </ul>	1, 2, 4, 5
<del>16</del> <u>3</u>	Improvements in health	<ul style="list-style-type: none"> <li>• Changes in health indicators for residents within Wards that fall within the Legacy Corporation area.</li> <li>• Changes in life expectancy for residents within Wards that fall within the Legacy Corporation area.</li> </ul>	2, 3, 5
17	Reductions in carbon emissions	<ul style="list-style-type: none"> <li>• Number of applications approved for major development schemes (proposing residential use) that achieve a 40% or greater improvement on 2010 Building Regulations Target Emission Rate, or from 2016 onwards achieve a Zero Carbon target (including any permitted allowable solutions)</li> <li>• Number of applications approved for major development schemes (proposing non-residential use) that achieve a 35% or greater improvement on 2013 Building Regulations TER, meet building regulations requirements from 2016 to 2019, or from 2019 onwards achieve a Zero Carbon target (including any permitted allowable solutions)</li> <li>• Number of applications approved for major development schemes (proposing non-residential use) achieving a minimum of BREEAM 2011 'Very Good', while achieving a maximum score for water use (or an equivalent in any future nationally recognised assessment scheme).</li> </ul>	3, 4, 5

NO.	KEY PERFORMANCE INDICATOR	MONITORING CRITERIA	RELEVANT OBJECTIVE
18	Water efficiency	• Number of applications approved for major development schemes designed to achieve 110 litres of water use per person per day or less	3, 4, 5
19	Coverage of trees and green roofs	<del>• Number of applications approved for major development schemes that include the provision of additional trees.</del>	<del>3, 4, 5</del>
<del>20</del> 19	Planning obligations	• The AMR will include a breakdown of all financial and non-financial obligations secured through S106 Agreement.	All

### AMR Monitoring Criteria

1. Growth in economic activity	Percentage of working-age residents in employment within the four growth boroughs compared to the London average
	Net gain/loss in employment floorspace (B Use Class) compared to 2015 baseline
	Number of new business start-ups compared to closures in the Growth boroughs
	Number of jobs/local jobs/ opportunities within employment training initiatives created
2- Creation of retail centres	Net gain/loss in retail and leisure floorspace (A1-5, C1 and D2) by use within the Centres
	Vacancy rates within the identified centres compared to the London average
	New retail floorspace permitted outside the Centres (units and quantum)
	Number of applications submitted for change of use from A1 to non-A1 floorspace within the Centres and per cent granted
3- Supply of housing	Number of homes permitted per annum
	Number of affordable homes permitted per annum by tenure <u>and the tenure breakdown</u>
	Number of homes completed per annum
	Number of affordable homes completed per annum by tenure <u>and the tenure breakdown</u>
	<u>Average number of bedrooms per unit</u> <u>Number and % of schemes containing residential going through the Fast Track Route (FTR)</u>
	Number of homes permitted per annum by unit size
	Amount of specialist housing provided
	Changes in resident population and household profile
4- Provision and protection of community facilities	Net gain/loss in community floorspace (D1 Use Class)

	On site community infrastructure secured through S.106 Agreement as part of large scale development
	Number of new school places delivered/granted permission
	Number and capacity of new health facilities granted planning permission
5- Protecting heritage assets and improving design quality	Loss of heritage assets
	Proportion of relevant applications approved for development schemes (proposing residential use) that meet 'Baseline' standards
	Proportion of relevant applications approved for development schemes (proposing non-residential use) that incorporate all applicable elements of the Legacy Corporation's Inclusive Design Standards
	Proportion of relevant applications approved for development schemes (proposing residential use) that provide 90% of dwellings in accordance with M4 (2)
	Proportion of relevant applications approved for development schemes (proposing residential use) meeting the Nationally Described Space Standards
	Proportion of relevant applications approved for development schemes that meet daylight and sunlight guidance
6- Amount of open space	Quantum of open space gained or lost through development
7- Protect biodiversity and habitat	<u>No net loss of SINCS (net gain or loss)-Net gains</u>
	Number of applications approved for development schemes meeting Urban Greening Factor (UFG)-including urban greening initiatives
	<u>Surface cover type provided to meet the Urban Greening Factor target score/number of application approved for major development schemes that include:</u> <ul style="list-style-type: none"> <li>- <u>Green roofs</u></li> <li>- <u>Trees</u></li> <li>- <u>Green walls</u></li> </ul>
8- Improving the waterway environment	Number of applications approved for major development schemes next to the waterways including measures to improve the environment of the Blue Ribbon Network
9- Managing transport impacts	Number of green travel plans entered into through condition or S106 agreement
10- Reducing car use	Number of car club spaces approved
11- Delivering electric charging infrastructure	Number of electric charging points approved
12- Car parking provision	Number of applications approved for car-free or car-capped development schemes

	Net gain/loss of car parking spaces
13- Cycle parking provision	Net gain/loss of cycle parking spaces
14- Delivering transport infrastructure	Infrastructure provided on site as part of development e.g. new junctions, cycle paths
<del>15</del> - 2 Improvements in IMD	Changes in Indices of Multiple Deprivation within wards
<del>16</del> - 3 Improvements in health	Changes in health indicators for residents within relevant wards
	Changes in life expectancy for residents within relevant ward
	Physically active children
	Personal well-being by Borough
17- Reductions in carbon emissions	Number of applications approved for major development schemes (proposing residential use) that achieve a 40% or greater improvement on 2010 Building Regulations
	Number of applications approved for major development schemes (proposing non-residential use) that achieve a 35% or greater improvement on 2010 Building Regulations
	Number of applications approved for major development schemes (proposing non-residential use) that achieve a minimum of BREEAM 'Very Good'
18- Water efficiency	Number of applications approved for major development schemes designed to achieve 110 litres of water use per person
<del>19</del> - Coverage of trees and green roofs	<del>Number of applications approved for major development schemes that include the provision of trees</del>
	<del>Number of applications approved for major development schemes that include green roofs</del>
20- Planning Obligations	Breakdown of all financial and non-financial obligations secured through S106 Agreement

## Appendix 1: The Policy Context

Replace current section with an updated version taking account of new policy context including the new NPPF and the draft New London Plan.

## Appendix 2- Key Housing Locations

Replace current Tables 14 & 15 with new Table 14 below:

	Pre-adoption phase	Adoption phase				Total
	2018-2019	2020-2024	2025-2029	2030-2034	2035-2036	
<b>Allocations</b>						
Sub Area 1	125	580	329	25	0	1059
Sub Area 2	0	0	0	0	0	0
Sub Area 3	25	331	1159	924	29	2468
Sub Area 4	115	698	1020	268	0	2101
<b>TOTAL</b>	<b>264</b>	<b>1610</b>	<b>2509</b>	<b>1217</b>	<b>29</b>	<b>5628</b>
<b>Permissions</b>						
Sub Area 1	489	869	29	0	0	1387
Sub Area 2	918	1840	251	0	0	3010
Sub Area 3	1108	3302	2532	168	0	7110
Sub Area 4	574	2257	1347	0	0	4178
<b>TOTAL</b>	<b>3089</b>	<b>8268</b>	<b>4159</b>	<b>168</b>	<b>0</b>	<b>15684</b>
<b>Additional capacity</b>						
Sub Area 1	35	767	678	359	7	1846
Sub Area 2	0	83	331	0	0	414
Sub Area 3	0	1188	948	176	130	2442
Sub Area 4	0	30	136	64	2	232
<b>TOTAL</b>	<b>35</b>	<b>2068</b>	<b>2093</b>	<b>600</b>	<b>139</b>	<b>4934</b>
<b>AREA TOTAL</b>	<b>3389</b>	<b>11945</b>	<b>8760</b>	<b>1985</b>	<b>168</b>	<b>26246</b>

## Appendix 3- Schedule of Designated (Nationally Listed) and Non-Designated Heritage Assets

Reference	Name	Designation
1	61 to 79 Eastway	Non-Designated
2	The Old Baths	Non-Designated
3	Trafalgar Mews	Non-Designated
4	St Mary of Eton Church	Designated
5	32a Eastway	Non-Designated
6	Gainsborough School	Designated
7	Sewer vent pipe	Non-Design
8	Warehouse at corner of Wallis Road and Berkshire Road	Non-Design
9	Central Books and Rubber Works	Non-Design
10	Oslo House	Non-Design

11	Lion Works	Non-Design
12	Former Carless Institute	Non-Design
13	Spegelstein buildings/Daro Works	Non-Design
14	Eton Mission Rowing Club	Non-Design
15	88 Wallis Road	Non-Design
16	86 Wallis Road	Non-Design
17	Railway bridge over Lee Navigation	Non-Design
18	Hope Chirical Works Wall	Non-Design
19	Lord Napier public house and adjacent housing	Non-Design
20	Everett House	Non-Design
21	14 Queen's Yard	Non-Design
22	Queens Yard and Kings Yard/Energy Centre (former Clarnico Works)	Non-Design
23	Carpenter's Road Bridge	Non-Design
24	92 White Post Lane	Non-Design
25	Boundary wall to the Hertford Union Canal	Non-Design
26	McGrath House and Outbuildings	Non-Design
27	Carlton Chimney	Non-Design
28	Bottom lock of Hertford Union Canal	Non-Design
29	Former Timber Yard Gatehouse	Non-Design
30	Broadwood's Piano Factory	Non-Design
31	Algha Works	Non-Design
32	Wick Lane Rubber Works (East)	Non-Design
33	Wick Lane Rubber Works (West)	Non-Design
34	Britannia Works	Non-Design
35	Swan Wharf	Non-Design
36	Old Ford Lock	Non-Design
37	Northern Outfall Sewer Bridge	Non-Design
38	Former Christ Church Mission and Sunday School	Non-Design
39	Former Glass Bending Factory	Non-Design
40	Former Ammonia Works Warehouse	Non-Design
41	Public House, 421 Wick Lane	Non-Design
42	Tide Gate	Non-Design
43	Pedestrian Bridge, Greenway	Non-Design
44	City Mill River Footbridge	Non-Design
45	Warton House, Box Factory, perfume/soap makers	Non-Design
46	Parish Boundary Marker between no. 231 and 233	Designated
47	The Log Cabin	Designated
48	Burford Road	Non-Design
49	Stratford (Market) Station, High St	Non-Design
50	306-308 High St	Non-Design
51	116-130 Abbey Lane	Designated
52	Former Superintendent's House	Designated
53	Bases of Pair Former Chimney Stacks	Designated
54	Gate Lodge	Designated
55	Gates and Gatepiers at Entrance to Abbey Mills	Designated
56	Pumping Station	Designated
57	Abbey Mills Pumping Station	Designated
58	Stores Building at Abbey Mills	Designated
59	B Station at Abbey Mills Pumping Station	Designated



60	C station with Associated Valve House	Designated
61	Offices Opposite Clock Mill	Designated
62	Paved roadway extending from west side of House Mill to wall and gate on East side of Clock Mill	Designated
62	Clock Mill	Designated
63	The Still, Three Mills Distillery	Non-Design
64	Old River Lee Narrows	Non-Design
65	Carpenter's Lock Bridge	Non-Design
66	Carpenter's Lock	Non-Design

#### Appendix 4- Schedule of Local Open Spaces and its identified function(s)

Reference Number	Primary and Secondary Function(s)
1	Amenity Open Space/Pocket Park
2	Local Park/ Outdoor Sports Facilities
3	Outdoor Sports Facilities
4	Pocket Park
5	Natural/Semi-Natural Open Space
6	Natural/Semi-Natural Open Space
7	Linear Open Space
8	Linear Open Space
9	Green Corridor
10	Green Corridor
11	Outdoor Sports Facilities
12	Natural/Semi-Natural Open Space
13	Natural/Semi-Natural Open Space
14	Linear Open Space
15	Linear Open Space
16	Green Corridor
17	Natural/Semi-Natural Open Space
18	Amenity Open Space
19	Amenity Open Space
20	Linear Open Space
21	Linear Open Space
22	Amenity Open Space
23	Linear Open Space
24	Outdoor Sports Facilities
25	Children and Young People (5 - 11 years)
26	Amenity Open Space
27	Linear Open Space
28	Natural/Semi-Natural Open Space
29	Linear Open Space
30	Allotments
31	Natural/Semi-Natural Open Space
32	Amenity Open Space
33	Amenity Open Space
34	Amenity Open Space
35	Regional Park
36	Amenity Open Space

37	Natural/Semi-Natural Open Space
38	Children and Young People (5 - 11 years)
39	Children and Young People (5 - 11 years)
40	Amenity Open Space
41	Linear Open Space
42	Allotments
43	Green Corridor
44	Linear Open Space
45	Children and Young People (>11 years)

## Appendix 5 – Strategic Policies Table

The Local Plan policies set out in the table below are Strategic Policies for the purposes of paragraphs 20-23 of the National Planning Policy Framework (2018). The Strategic Policies are necessary to address the strategic priorities of the Legacy Corporation's area (as described in Objectives 1 to 5 this Local Plan).

Policy Number	Policy Name
Policy SD.1	Sustainable development
Policy SP.1	A strong and diverse economy
Policy B.1	Location and maintenance of employment uses (including Table 2 Employment clusters)
Policy B.2	Thriving town, neighbourhood and local centres (including Table 3, Retail centre hierarchy)
Policy B.3	Creating vitality through interim uses
Policy B.4	Providing low-cost business space, affordable and managed workspace
Policy B.5	Increasing local access to jobs, skills and employment training
Policy B.6	Higher education, research and development
Policy SP.2	Maximising housing and infrastructure provision within new neighbourhoods
Policy H.1	Providing for and diversifying the housing mix
Policy H.2	Affordable housing
H.3	Meeting accommodation needs of older people
H.4	Providing student accommodation
Policy H.5	Location of gypsy and traveller accommodation
Policy H.6	Houses in Multiple Occupation (HMOs)
Policy H.7	Shared living accommodation
Policy H.8	Innovative housing models
Policy CI.1	Providing new and retaining existing community infrastructure
Policy CI.2	Planning for and bring forward new schools
Policy SP.3	Integrating the natural, built and historic environment
Policy BN.1	Responding to place
Policy BN.2	Creating distinctive waterway environments
Policy BN.3	Maximising biodiversity
Policy BN.4	Designing development
BN.5	Proposals for tall buildings
BN.6	Requiring inclusive design

BN.7	Protecting Metropolitan Open Land
BN.8	Improving Local Open Space
Policy BN.9	Maximising opportunities for play
BN.10	Protecting key views
BN.11	Air quality
BN.12	Noise
BN.13	Protecting archaeological interest
BN.14	Improving the quality of land
BN.17	Conserving or enhancing heritage assets
Policy SP.4.	Planning for and securing transport infrastructure to support growth and convergence
Policy T.1	Strategic transport improvements
Policy T.2	Transport improvements
Policy T.3	Supporting transport improvements
Policy T.4	Managing development and its transport impacts to promote sustainable transport choices, facilitate local connectivity and prioritise pedestrians and cyclists
Policy T.5	Street network
Policy T.6	Facilitating local connectivity
T.7	Transport assessments and travel plans
T.8	Parking and parking standards in new development
T.9	Providing for pedestrians and cyclists
T.10	Using the waterways for transport
Policy SP.5	A sustainable and healthy place to live and work
Policy IN.2	Planning for waste
Policy S.1	Health and wellbeing
Policy S.2	Energy in new development
Policy S.3	Energy infrastructure and heat networks
Policy S.4	Sustainable design and construction
Policy S.5	Water supply and waste water disposal
Policy S.6	Increasing digital connectivity, safeguarding existing communications provision and enabling future infrastructure
Policy S.7	Planning for waste
S.8	Waste Reduction
S.9	Overheating and urban greening
Policy S.10	Flood Risk
policy S.11	<u>Sustainable drainage measures and flood protections</u>
Policy S.12	Resilience, safety and security
Site Allocation SA.1.1	Hackney Wick Station Area
Site Allocation SA.1.2	Hamlet Industrial Estate
Site Allocation SA.1.3	Hepscott Road
Site Allocation SA.1.4	Neptune Wharf
Site Allocation SA.1.5	East Wick and Here East
Site Allocation SA.1.6	Sweetwater
Site Allocation SA.1.7	Bartrip Street South
SA.2.1:	Chobham farm
SA.2.2	East Village
SA.2.3	Chobham Manor
SA.2.4	Chobham Farm North

SA.3.1	Stratford Town Centre West
SA.3.2	Stratford Waterfront East
SA.3.3	Stratford Waterfront West
SA.3.4	Greater Carpenters District
SA.3.5	Bridgewater Road
SA.3.6	Rick Roberts Way
SA.4.1	Bromley-by-Bow
SA.4.2	Sugar House Lane
SA.4.3	Pudding Mill
SA.4.4	Three Mills
SA.4.5	Bow Goods Yards (Bow East and West)

## Appendix 6 – Policies Map Schedule of Proposed Changes

Correction – designation of two Local Open Spaces (LOS) north of Hackney Wick as Metropolitan Open Land, in accordance with the previous extent in LB Hackney Local Plan
Designation of five new LOS in accordance with the Open Space and Play Space Assessment 2018 recommendations
Correction to the positions of the two LOS south of the London Aquatic Centre
Deletion of SA1.4 to respond to the changes in the draft revised Local Plan
Deletion of SA1.5 to respond to the changes in the draft revised Local Plan
Addition of new site allocations SA2.4, SA4.4 and SA4.5
Amendments to the boundaries of Site Allocations SA3.2 and SA3.3
Amendment to the East Village Local Centre boundary
Amendments to the Metropolitan Centre boundary as being the location for the potential Central Activities Zone (CAZ) reserve

**Appendices 3 (Glossary), 4 (Use Class Order) 5 (Abbreviations) & 6 (Index of policies) will become 7, 8, 9 & 10.**

Minor amendments and updates to be made.

London Legacy Development  
Corporation

**Integrated Impact Assessment  
of the Local Plan**

**Non-Technical Summary**

NTS/248753/2018-0021

Issue | September 2018

This report takes into account the particular instructions and requirements of our client.

It is not intended for and should not be relied upon by any third party and no responsibility is undertaken to any third party.

Job number 248753-00

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**ARUP**



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# 1 Introduction

## 1.1 Background

Ove Arup and Partners Limited (Arup) has been appointed by the London Legacy Development Corporation (LLDC) to conduct an Integrated Impact Assessment (IIA) of the revised LLDC Local Plan. The Local Plan provides a framework that will guide growth and development throughout the area in a sustainable way.

This non-technical summary identifies the component assessments undertaken for the Local Plan and that are presented in the full Integrated Impact Assessment report. It also describes the key findings of the IIA process.

## 1.2 The area

The London Legacy Development Corporation has powers that allow it to make planning decisions within a 'Planning Area' in the vicinity of the Queen Elizabeth Olympic Park (Figure 1). The Planning Area crosses the boundaries of four London boroughs: Tower Hamlets, Hackney, Newham and Waltham Forest.

## 1.3 Sustainability

The LLDC area is densely populated and dynamic, where development is driving growth and regeneration. The Local Plan process aims to make sure that this growth is sustainable and appropriate for the local context. However, to understand if the Local Plan is promoting sustainable development, a full understanding of the barriers to achieving sustainability in the local area was needed.

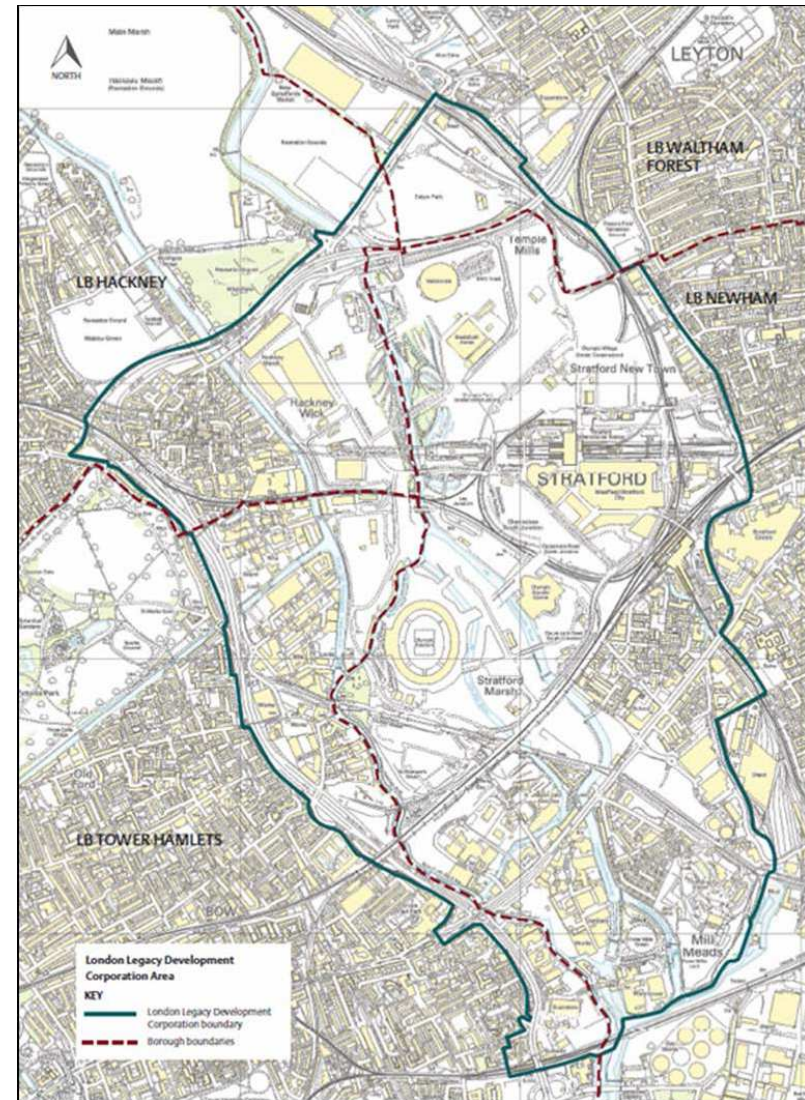


Figure 1: The LLDC Planning Area

## 2 Integrated Impact Assessment

### 2.1 How assessment is undertaken

An IIA combines five different assessments to assess the Local Plan:

**‘Sustainability Appraisal**, which refers to the overall sustainability of



policies and their effects on the environment and is required by the Planning and Compulsory Purchase Act 2004;

**‘Strategic Environmental Assessment’**, which refers to the assessment of the environmental effects of plans and programmes required by European Union Legislation (Directive 2001/42/EC);



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**‘Equalities Impact Assessment’**, which focuses on ensuring that different groups of people, including those defined as ‘protected characteristics’ (such as age, gender, disability, race etc.) are not adversely affected by changes and is required by the Equality Act 2010;



**‘Health Impact Assessment’**, which seeks to ensure that the overall health of the population is protected and enhanced, and is recommended under the Greater London Authority Act 1999; and



**‘Community Safety Impact Assessment’**, which considers the effects of policies on the perceived and actual levels of safety and crime in communities and the wider area and is a requirement under Section 17 of the Crime and Disorder Act 1998 and the Police and Justice Act 2006.



Ensuring sustainable growth across the area is a central aim of the Local Plan. To demonstrate that the Local Plan is achieving this aim, it was tested against certain criteria that were developed by understanding the sustainability issues, or barriers to achieving sustainability, of the area. To understand these sustainability issues, the latest information and statistics relating to the London Legacy Development Corporation area were reviewed.

The sustainability issues were examined and 24 objectives were created to address them. During the review process, each policy in the Local Plan was scored against these 24 objectives using the following scale:

Very negative	--
Somewhat negative	-
Neutral	0
Somewhat positive	+
Very positive	++
Unknown	?

This identified where policies will have positive, negative or neutral effects, or where the effects were unknown at this stage. Where appropriate, recommendations were then made to improve the policy outcomes.

This Integrated Impact Assessment has only reviewed those policies which are new, or which have significantly changed since the 2015 LLDC Local Plan. Section 3 provides a summary of each objective and how well the amended and new policies scored against this objective.

### 3 Assessment findings

#### 3.1 Objective 1 – Equality and inclusion

**To make London a fair and inclusive city where every person is able to participate, reducing inequality and disadvantage and addressing the diverse needs to the population.**

Research looking at equality and inclusion in the London Legacy Development Corporation area revealed a growing and ageing population which could result in social isolation. As a result of redevelopment in the area, residents are at risk of the effects of gentrification which can threaten community cohesion. This objective promotes an equal and respectful culture, providing opportunities for people to lead an active and fulfilling life and ensures that locals can participate in decision-making.

A large number of policies affect equality and inclusion in the area. Around two thirds of the assessment ratings were positive or very positive, with **Transport** policies having a particularly positive effect on equality and inclusion. This is because these policies aim to make transport easier, more efficient and more accessible by improving public transport and promoting sustainable travel. Policies relating to **Health and wellbeing** and **Housing** also perform well against this objective. However, there are opportunities to improve the **Air Quality** and **Noise** policies to benefit equality in the area. Improvements could increase the provision for, and encourage the use of, quiet outdoor community spaces away from places with poor air quality to promote a sense of community openness, inclusion and ownership of the area.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

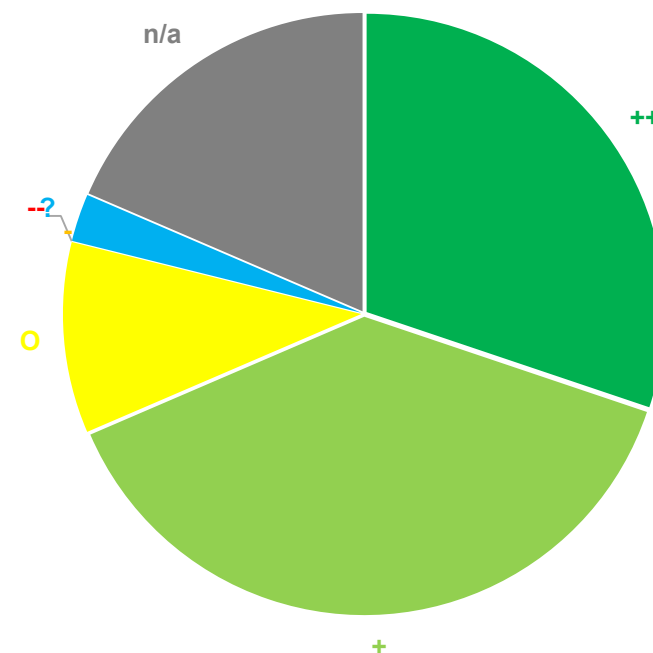


Figure 2: Overall policy performance against Objective 1.

### 3.2 Objective 2 – Social integration

**To ensure London has socially integrated communities which are strong, resilient and free of prejudice.**

Research into levels of social integration in the London Legacy Development Corporation area revealed that issues of population growth and development without consideration of the community could result in feelings of social isolation. This is particularly the case in pockets of deprivation throughout the area. The objective aims to create socially integrated communities by managing the effects of population growth and designing developments, especially housing, in a way which maximises opportunities for community engagement and integration.

A large number of the policies will impact upon social integration. Overall, around two thirds of the assessment ratings were either positive or very positive. Local Plan policies relating to **Housing** will address problems arising from a population that is both growing and ageing, providing more housing that is affordable and appropriately designed. **Transport** policies will increase social integration by providing and improving infrastructure to make housing and development accessible, support economic growth by connecting housing with town and city centres, and facilitate local connectivity through promoting sustainable transport choices. Site allocations for development in the **Sweetwater** and **Chobham Farm North** intend to provide housing and transport links within the London Legacy Development Corporation area. This supports social integration by providing residents with access to facilities like shops, schools and healthcare and improving transport links.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

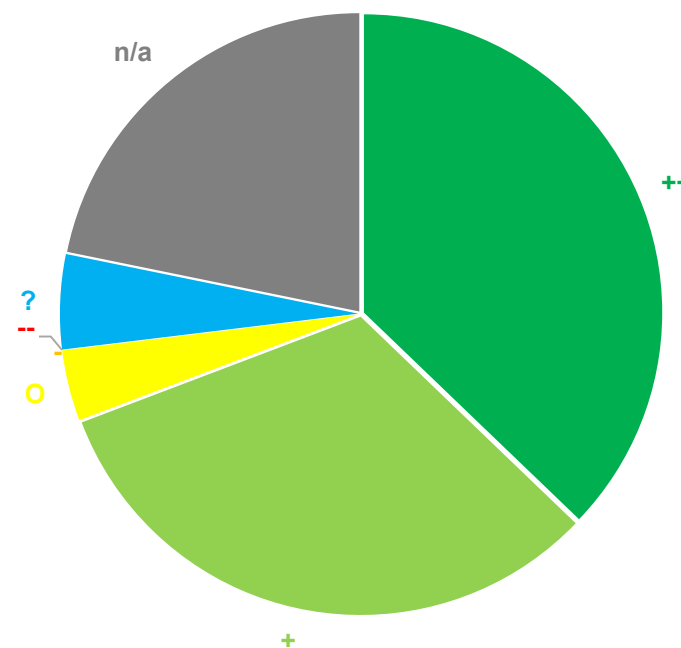


Figure 3: Overall policy performance against Objective 2.

### 3.3 Objective 3 – Health and health inequalities

**To improve the mental and physical health and wellbeing of Londoners and to reduce health inequalities across the City and between communities.**

Research into the health across the population of the area showed that people are living longer, which is introducing a need for more complex and long-term healthcare treatments. This is putting pressure on local health services and is creating inequalities around health, as different groups within the population are less able to access appropriate healthcare. These health inequalities are also seen through low levels of engagement in physical activity. The objective targets new developments in the area that meet the needs of the whole population, provide services without discrimination and encourage sustainable and efficient development.

A large number of policies will have an impact upon health and health inequalities in the area. A small proportion of policies would not impact on the current situation; these policies relate to the **Metropolitan Centre**, the **Stratford High Street Policy Area**, meaning that there are opportunities to improve the policies to benefit the health of the population in specific areas. Two thirds of the assessments rated the policies either positive or very positive. **Transport** policies work to connect developments and residents to key services, while **Housing** and **Design** policies are ensuring that modern homes are built to new and sustainable design standards to reduce fuel poverty and encourage healthy lifestyles. The **Health and Wellbeing** policy is very positive in encouraging healthy lifestyles to reduce obesity and improve access to healthcare services and facilities in the London Legacy Development Corporation area.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

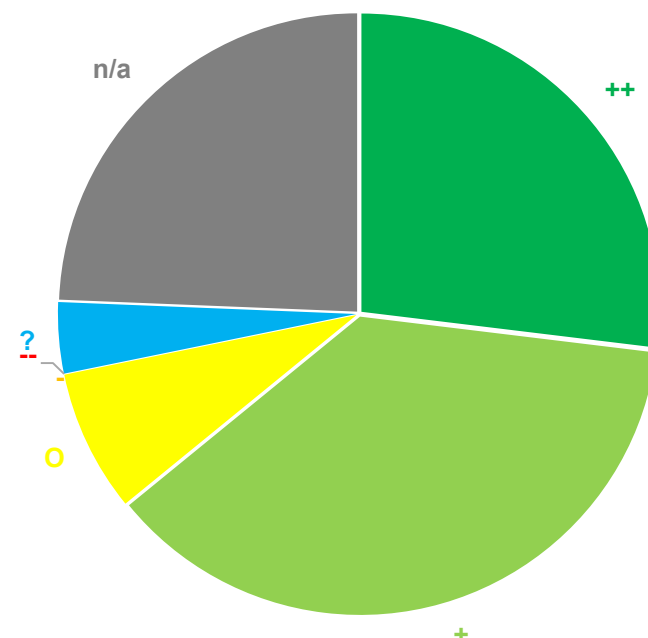


Figure 4: Overall policy performance against Objective 3.

### 3.4 Objective 4 – Crime, safety and security

**To contribute to safety and security and the perceptions of safety.**

Research into the crime, safety and security across the area revealed that there are many ‘high value’ assets in the area, which draw large numbers of visitors. This increases the risk to safety in the event of an incident. More generally, there are a number of unlit vacant and derelict sites that reduce the perceived safety of the area. This objective aims to ensure crime and safety are considered at the design stage of developments, and to reduce crime more widely and improve safety.

This objective was only relevant for around one third of the assessments. Of the assessments that were relevant, just less than one half required further information to determine the impact of the policies. This included assessments for **Transport** policies, including the policy relating to **Metropolitan Centre**, the **Street Network**, and managing development and infrastructure for sustainability and serving new developments. Policies often lacked specific detail relating to how design could influence perceived and actual safety, especially while maintaining accessibility. Some policies do not specifically refer to safety but it is recognised that the aim of the policy would increase safety, security or resilience and further information is requested. The assessments for policies in **Resilience, safety and security**, the **Stratford High Street Policy Area**, and **Providing for pedestrians and cyclists** were overall very positive and stand to improve safety and resilience in the area. The challenges faced by the Local Plan to achieve this objective are related to fire and safety within the night time economy.

Overall, the Local Plan is successful in targeting some specific crime and safety issues. However, this approach is often inconsistent between policies and certain issues are not referenced.

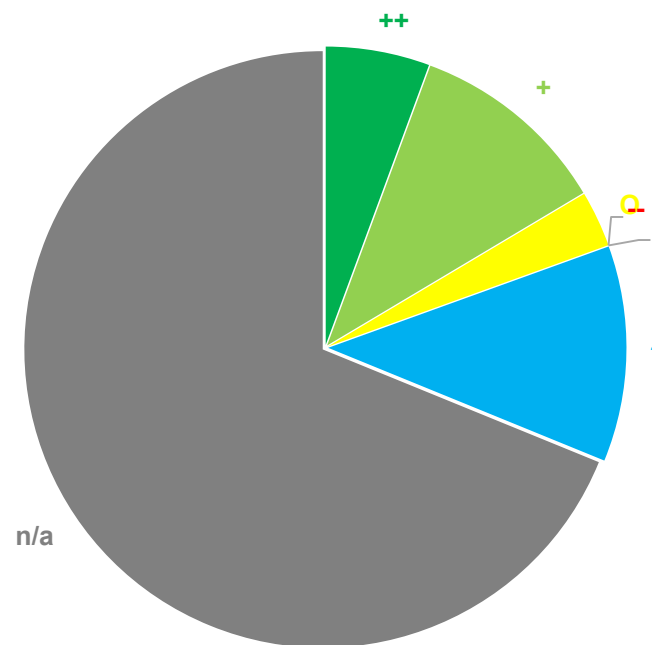


Figure 5: Overall policy performance against Objective 4

### 3.5 Objective 5 – Housing supply, quality, choice and affordability

**To provide a quantum, type, quality and tenure of housing (including specialist and affordable provision) to better meet demographic change and household demand.**

Research into housing in the London Legacy Development Corporation area shows a significant projected population increase up to 2036. This will put increasing pressure on the existing housing stock, and will increase the need for adaptable homes to support a range of needs. The most common type of housing in the London Legacy Development Corporation area are flats and apartment blocks, whilst the area as a whole is similar to the rest of London and has experienced an increase in house price to earnings, making the area less affordable. As a result, levels of home ownership are low, and there is a risk of increased levels of homelessness. The objective seeks to ensure that the provision and quality of housing better meets demographic change and household demand. This is particularly in terms of facilitating the delivery of house building, reducing homelessness and overcrowding, increasing the range and affordability of housing and promoting accessible and adaptable homes.

A large number of policies will impact on housing. Around three quarters of the policies scored positively or very positively against the assessment criteria, with **Housing** policies surrounding delivery, diversifying affordability and inclusive design performing particularly well. Those surrounding shared living and alternative accommodation also performed well, as a result of the focus on appropriate locations and supported by communal facilities, along with ongoing management arrangements.

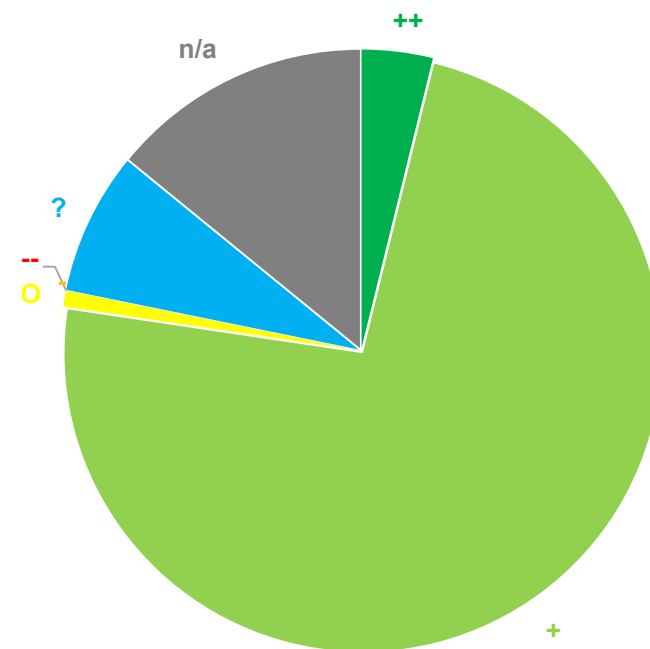


Figure 6: Overall policy performance against Objective 5

### 3.6 Objective 6 – Sustainable land use

**Make the best and most efficient use of land so as to support sustainable patterns and forms of development.**

Research into land use showed that there are a large amount of brownfield sites being promoted for development. This objective aims to ensure that the risks associated with contaminated land are considered, and that existing and new communities are supported by appropriately located developments, which integrate the existing transport network and are designed in such a way that is inclusive for different groups of people.

This objective was relevant to around two thirds of the assessments undertaken. Overall, the majority of these assessment ratings were either very positive or positive. Policies which performed very well were not specific to one chapter and related more broadly to the integration of land use and transport infrastructure. For example, the **Maximising housing and infrastructure provision within new neighbourhoods** policy ensures land for housing is acquired from a range of sources, promotes regeneration and provides benefits for existing communities. The site allocations of the **Metropolitan Centre** and **Bow Goods Yards** support developments which are accessible and integrated within the existing transport network, and would be appropriate in size to ensure different groups of people and their communities would not be negatively affected by development.

Overall, the Local Plan policies performed well against this objective.

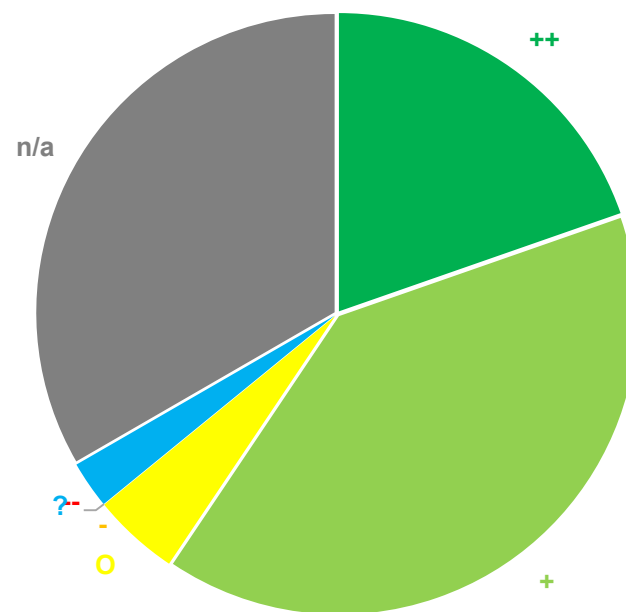


Figure 7: Overall policy performance against Objective 6.



### 3.7 Objective 7 – Design

**To create attractive, mixed use neighbourhoods, ensuring new buildings and spaces are appropriately designed that promote and enhance existing a sense of place and distinctiveness reducing the need to travel by motorised transport.**

Research into the area revealed several opportunities to improve the design of developments and neighbourhood areas to retain community cohesion. The objective aims to use design to improve the quality and sustainability of neighbourhoods. It encourages good design principles which can create safe, accessible, high quality and sustainable developments. This will create a sense of place and promoting vibrant communities and diversity within the area. It will also make the area friendlier for those with sensory and cognitive impairments.

Over half of the assessments undertaken against this objective were positive or very positive. Policies that performed very well were either referring to a specific issue in the area, such as **Providing for pedestrians and cyclists** and **Designing development**, where design is at the heart of the development process. **Transport** and **Housing** policies are also promoting design through accessible and inclusive design of buildings and infrastructure. There are opportunities for **Air Quality** and **Affordable Housing** policies to refer to good design principles to ensure that clean air is properly considered in development construction and its ongoing use.

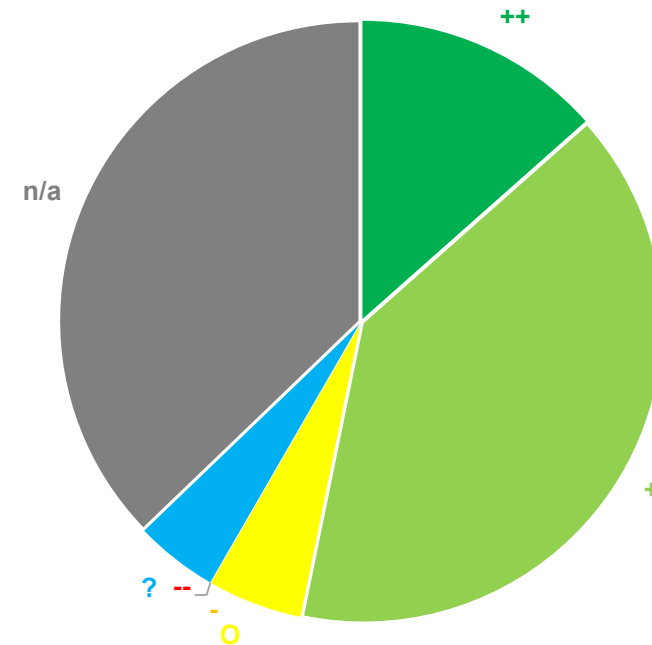


Figure 8: Overall policy performance against Objective 7.

### 3.8 Objective 8 – Accessibility

**To maximise accessibility for all in and around London.**

Research into accessibility identifies that as the population within the area continues to grow, there are concerns over accessibility in and around London. This objective seeks to ensure that policies improve access to public transport, increase equality of access to services and facilities, and improve links between areas, neighbourhoods and communities.

A large number of the assessments were relevant to accessibility. The majority of the assessment ratings were positive or very positive. Local Plan policies relating to **Housing** and **Transport** policies had particularly positive effect on accessibility due to the promotion of a 'Healthy Streets Approach.' This aims to improve accessibility, maximise inclusivity and reduce transport issues relating to noise, air pollution, dangerous roads, social isolation and severance. The Site Allocations of **Hackney Wick and Fish Island, North Stratford and Eton Manor, Central and Southern Queen Elizabeth Olympic Park, Bromley-by-Bow, Pudding Mill, Sugar House Lane and Mill Meads** perform well against this objective by identifying local improvements to improve accessibility.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place

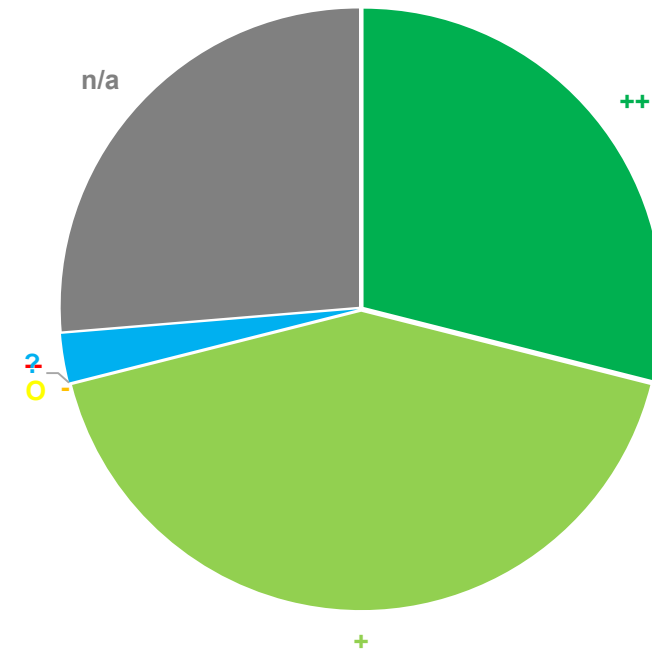


Figure 9: Overall policy performance against Objective 8.

### 3.9 Objective 9 – Connectivity

**To enhance and improve connectivity for all to, from, within and around London and increase the proportion of journeys made by sustainable and active transport modes.**

Research into connectivity in the London Legacy Development Corporation area revealed that there are issues relating to the integration of new developments with existing neighbourhoods. There are concerns surrounding access to new and existing transport hubs, as well as public transport. This objective aims to address these issues by improving public transport links across London, reducing congestion on roads as well as public pavements and footpaths, and reducing unequal access to public transport.

Around one quarter of policies affect connectivity in the area. Two thirds of the assessment ratings were positive or very positive impact. Many **Transport** policies have a particularly positive effect on connectivity by ensuring growth and integration of transport infrastructure and reducing congestion in the wider area. However, there are opportunities to improve some transport to benefit connectivity in the area. Improvements could increase sustainable and active travel to increase the proportion of journeys made by sustainable and active transport modes.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

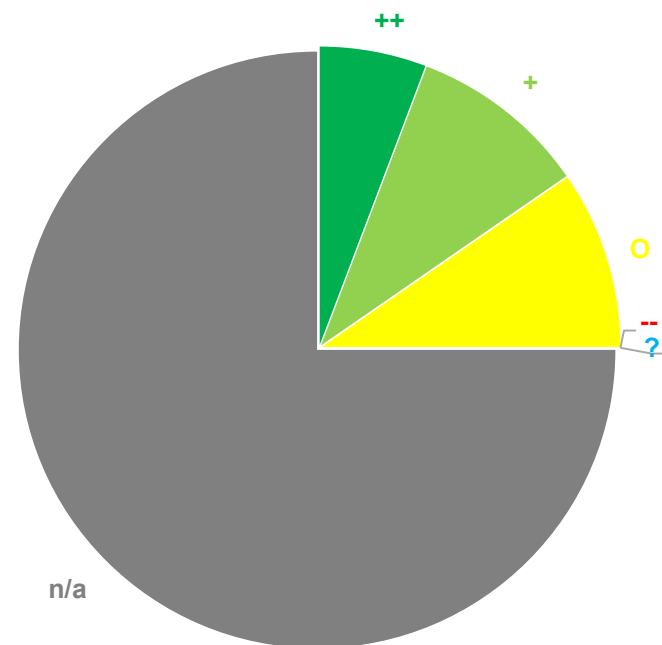


Figure 10: Overall policy performance against Objective 9.

### 3.10 Objective 10 – Economic competitiveness and employment

To maintain and strengthen London's position as a leading, connected, knowledge based global city and to support a strong, diverse and resilient economic economy structure providing opportunities for all.

Research into economic competitiveness and employment has identified specific economic issues that could impact growth and prosperity in the London Legacy Development Corporation area. Specific local issues such as housing pressures, unemployment and the difference between income and the cost of living put the economy at risk of continuous change and the impacts of the decision to leave the European Union. The objective seeks to address these issues through increasing productivity and delivering economic benefits across the London Legacy Development Corporation area.

A large number of Local Plan policies will impact on economic competitiveness and employment. Overall, around two thirds of the assessment ratings were either positive or very positive. **Employment** policies have a very positive impact as they aim to ensure that employment is appropriately located, to improve accessibility and develop resilience in the main economic centres within the area. Policies relating to **Transport, Housing, Development and Health and wellbeing** all positively impacted on this objective. These groups of policies supported redevelopment, resilience and social mobility to improve economic growth in the London Legacy Development Corporation area.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

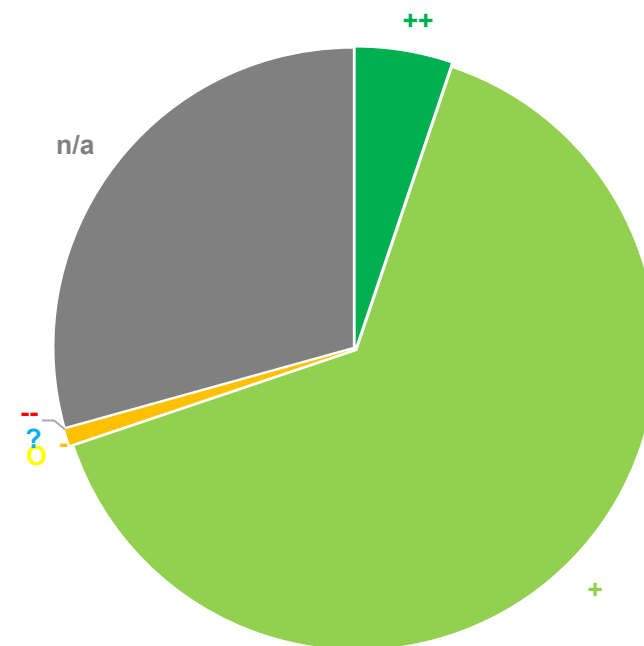


Figure 11: Overall policy performance against Objective 10.

### 3.11 Objective 11 – Infrastructure

**To ensure that provision of environmental, social and physical infrastructure is managed and delivered to meet population and demographic change in line with sustainable development and to support economic competitiveness.**

Research into infrastructure in the London Legacy Development Corporation area shows a significant projected population increase up to 2036, which will increase demands on infrastructure throughout the Local Plan timeframe. Similarly, the baseline demonstrates that a number of new centres will emerge through the Local Plan period. The substantial level of planned residential and economic growth will require extensive supporting infrastructure. This objective seeks to ensure that the provision of environmental, social and physical infrastructure is managed and delivered to meet population and demographic change in line with sustainable development and economic competitiveness more broadly.

A large number of policies will impact on infrastructure. Around three quarters of the policies scored positively or very positively. The majority of policies affect the provision of social, environmental and physical infrastructure to some extent. Policies surrounding **Transport** and **Employment** played a particularly important role in supporting economic competitiveness and inclusivity, whilst policies surrounding **Energy** and **Flood Risk** playing a greater role in unlocking housing growth.

Policies surrounding **Town, Neighbourhood and Local Centres** support the objectives from a community’s perspective, providing a range of cultural, leisure, sport and retail infrastructure to improve the amenity of existing and future residents and visitors, and as a result performed well against the Integrated Impact Assessment criteria.

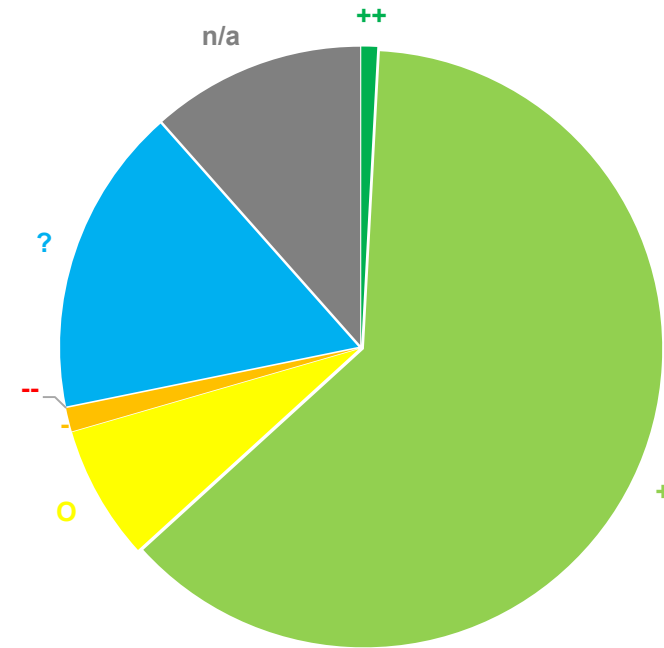


Figure 12: Overall policy performance against Objective 11.

### 3.12 Objective 12 – Education and skills

**To ensure the education and skills provision meets the needs to London's existing and future labour market and improves life chances for all.**

Research into education and skills in the London Legacy Development Corporation area revealed a demand for schools, the need to address the low percentage of qualified individuals within ethnic minorities, and to ensure that adults are sufficiently skilled to enter employment. This objective seeks to address these issues through the provision of sufficient school places to meet the growing needs across London and supporting adult education opportunities.

This objective was only relevant to less than a quarter of assessments. Of the assessments that were relevant, the majority were positive or very positive. **Housing** policies have a positive effect on the objective as they indirectly support the development of education infrastructure, and the wellbeing of students, particularly through the provision of appropriate accommodation for students. Additionally, policies relating to **Digital Connectivity** positively impact the objective through improving skills of residents and supporting London's reputation as an international city of learning.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

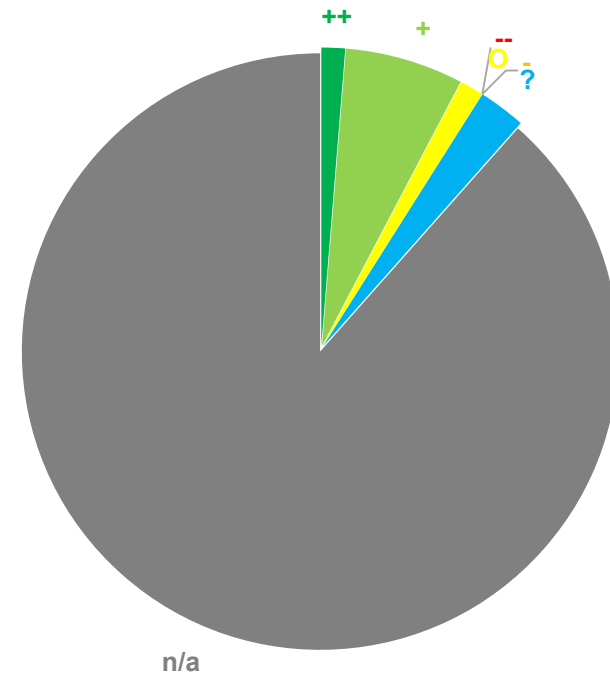


Figure 13: Overall policy performance against Objective 12.

### 3.13 Objective 13 – Culture

**To safeguard and enhance London’s rich cultural offer, infrastructure, heritage, natural environment and talent to benefit all Londoners while delivering new activities that strengthen London’s global position.**

Research into cultural assets within the London Legacy Development Corporation area revealed a need for the retention of historical and cultural heritage within developments and improved management of any development pressures, with specific regard to the protection of key historical assets within the London Legacy Development Corporation area. The objective seeks to address these issues through improving accessibility to cultural venues and affordable cultural activities.

Over a quarter of the policies will impact upon cultural aspects. Overall, the majority of the assessment ratings were positive. Local Plan policies relating to **Heritage** ensure the conservation of heritage assets, supporting the wider protection of cultural assets. **Housing** policies support improved participation through the provision of mixed and inclusive neighbourhoods which provide wider supporting social and physical infrastructure. This will increase accessibility to cultural activities, particularly for marginalised groups and older persons.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

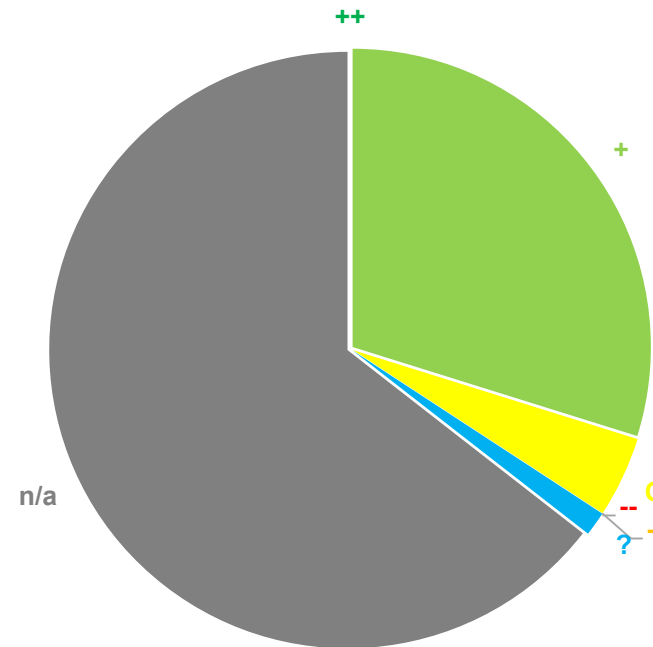


Figure 14: Overall policy performance against Objective 13.

### 3.14 Objective 14 – Air Quality

**To reduce emissions and concentrations of harmful atmospheric pollutants, particularly in areas of poorest air quality and reduce exposure.**

Research into air quality within the London Legacy Development Corporation area revealed high levels of nitrogen air pollution in key hotspots and a need for continuing air quality improvements across the boroughs. The objective seeks to address these issues by encouraging cleaner air through a reduction in emissions, focusing on areas which have higher concentrations of vulnerable people.

One third of the policies will impact upon air quality. Overall, just over a quarter of the assessment ratings were positive. Policies on **Health and Wellbeing** and **Transport** support a ‘Healthy Streets Approach’ which aims to improve accessibility, reduce congestion and manage transport issues relating to poor air quality. The policies will also reduce the reliance on private car use, resulting in reduced emissions. **Energy** policies have a positive effect as focus is placed on efficient and renewable forms of energy which can reduce pollutant emissions.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

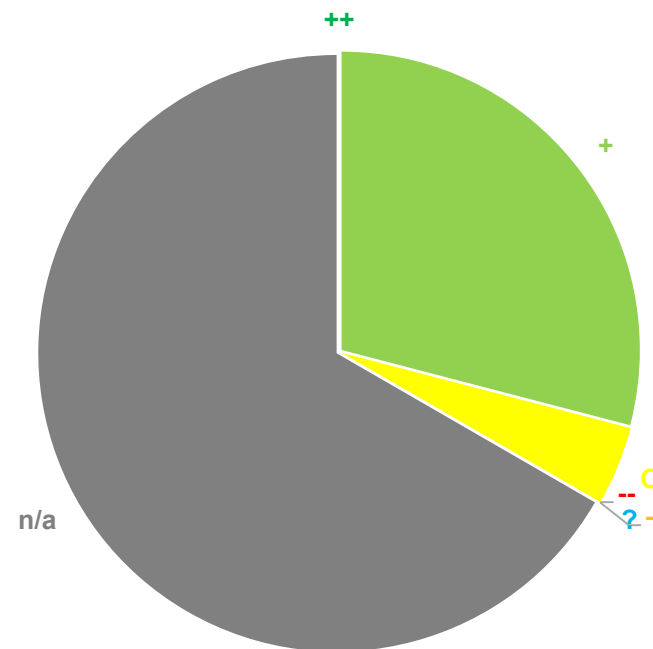


Figure 15: Overall policy performance against Objective 14.



### 3.15 Objective 15 – Climate change adaptation

To ensure London adapts and becomes more resilient to the impacts of climate change and extreme weather events such as flood, drought and heat risks.

Research into climate change adaptation revealed a risk to property from flooding. There is a need for the protection of drainage areas as well as improved management of extreme weather events. The objective seeks to help reduce climate change impacts on vulnerable groups and to help the local area function during extreme weather events such as flooding or extreme periods of drought.

This objective was only relevant to a small number of the assessments. Of the assessments that were relevant, almost all were positive or very positive. **Health and Wellbeing** policies will have a very positive effect on the objective as they support green spaces which aim to minimise the impact of increased heat through measures such as increasing shade from trees. Additionally, **Flooding and Drainage** policies positively affect the objective by requiring developers to assess and mitigate flood risks within the area.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

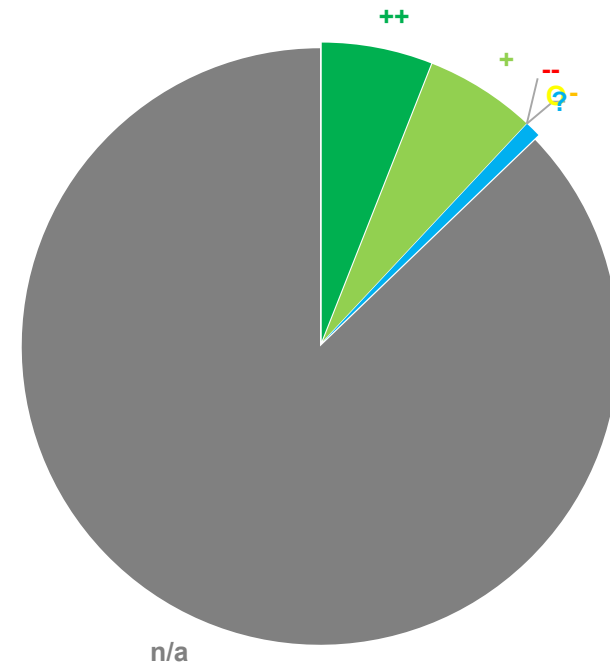


Figure 16: Overall policy performance against Objective 15.

### 3.16 Objective 16 – Climate change mitigation

**To help tackle climate change through reducing greenhouse gas emissions and moving towards a zero carbon London by 2050.**

Research into climate change mitigation revealed a need for focus on energy use and demand in the London Legacy Development area and improve energy efficiency and the use of renewable or local energy. The objective seeks to address this by promoting low-carbon developments and the use of new technologies to reduce pollution.

The objective was relevant to approximately one third of the assessments. Of the assessments that were relevant, all were positive or very positive. **Transport** policies were overall very positive through the support for the ‘Healthy Streets Approach’ which encourage greener streets with more cycling, walking and public transport. This supports a reduction in private car use, reducing pollution in the London Legacy Development Corporation area. **Digital** policies also positively impact the objective through the use of digital tools to reduce carbon emissions and improve energy efficiency.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

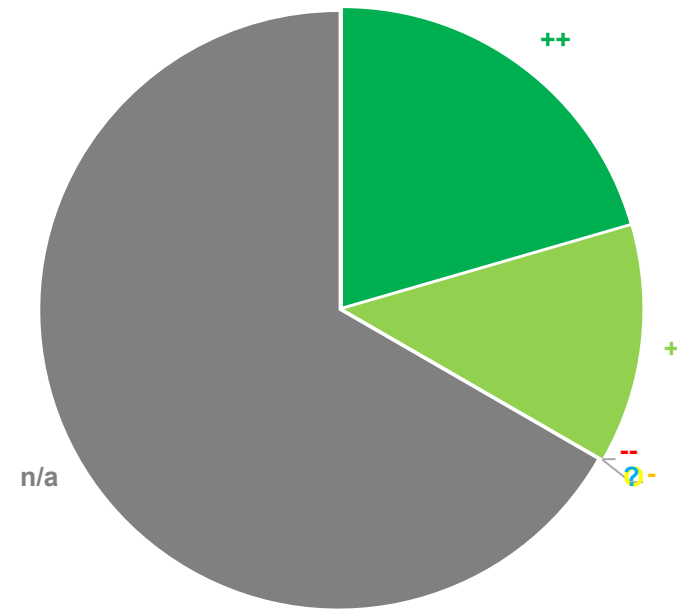


Figure 17: Overall policy performance against Objective 16.

### 3.17 Objective 17 – Energy use and supply

**To manage and reduce demand for energy, achieve greater energy efficiency, utilise new and existing energy sources effectively, and ensure a resilient smart and affordable energy system.**

Research into energy use and supply in the London Legacy Development Corporation area revealed that energy is not produced locally or from renewable resources. This objective seeks to address these issues by increasing the proportion of energy both purchased and generated from renewable and sustainable resources. It also aims to reduce the impacts of fuel poverty, particularly for vulnerable groups.

This objective was relevant to less than a quarter of the assessments. Of the assessments that were relevant, the majority were positive or very positive. **Housing** and **Sustainability** policies in particular supported energy efficiency. These policies aim to improve energy efficiency of new housing, resulting in reduced energy use and therefore reduced costs for residents. Policies relating to **Transport** also perform positively against this assessment. However, there are opportunities to use digital and infrastructure policies to benefit energy efficiency in the area.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

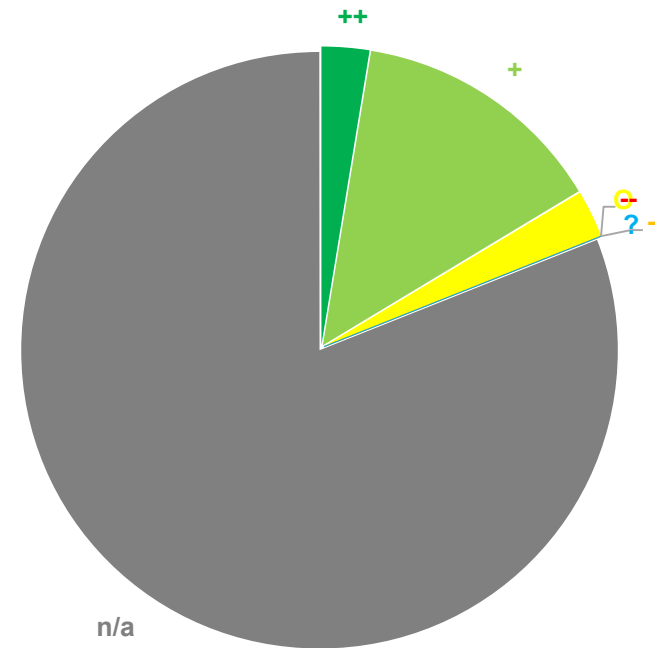


Figure 18: Overall policy performance against Objective 17.

### 3.18 Objective 18 – Water resources and quality

**To protect and enhance London’s water bodies by ensuring that London has a sustainable water supply, drainage and sewerage system.**

Research into water resources and quality in the London Legacy Development Corporation area identified importance of key waterbodies in the local area. This objective ensures that policies improve and protect London’s rivers and water bodies, and supports necessary improvements to the water systems infrastructure.

This objective was only relevant for around one third of assessments. Of the assessments that were relevant, all were positive or very positive. **Sustainability** policies have a particularly positive impact on water resources and quality. This is because these policies support natural flood protection measures and green landscaping to manage flooding in the local area. The Site Allocations of **Hackney Wick and Fish Island, North Stratford and Eton Manor, Central and Southern Queen Elizabeth Olympic Park, Bromley-by-Bow, Pudding Mill, Sugar House Lane and Mill Meads** all also perform well against this objective by supporting improvements to local areas.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

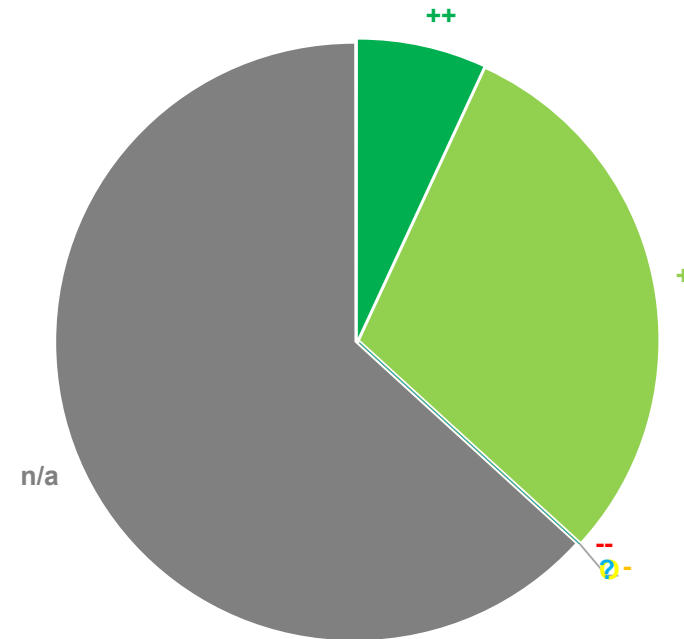


Figure 19: Overall policy performance against Objective 18.

### 3.19 Objective 19 – Flood risk

**To manage the risk of flooding from all sources and improve the resilience of people and property to flooding.**

Research into water resources and quality in the London Legacy Development Corporation identified the potential flood risk in the area. This objective seeks to minimise and manage the risk of flooding and promote sustainable drainage.

This objective was relevant for less than half of the assessments. Of the assessments that were relevant, all were positive or very positive.

**Sustainability** policies have a particularly positive impact improving the resilience of people and property to flooding. These policies aim to reduce sources of flood risk through the maintenance of existing flood defences and promote new defences where necessary. The Site

Allocations of **Hackney Wick and Fish Island, North Stratford and Eton Manor, Central and Southern Queen Elizabeth Olympic Park, Bromley-by-Bow, Pudding Mill, Sugar House Lane and Mill Meads** all also perform well against this objective by supporting improvements in local areas.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

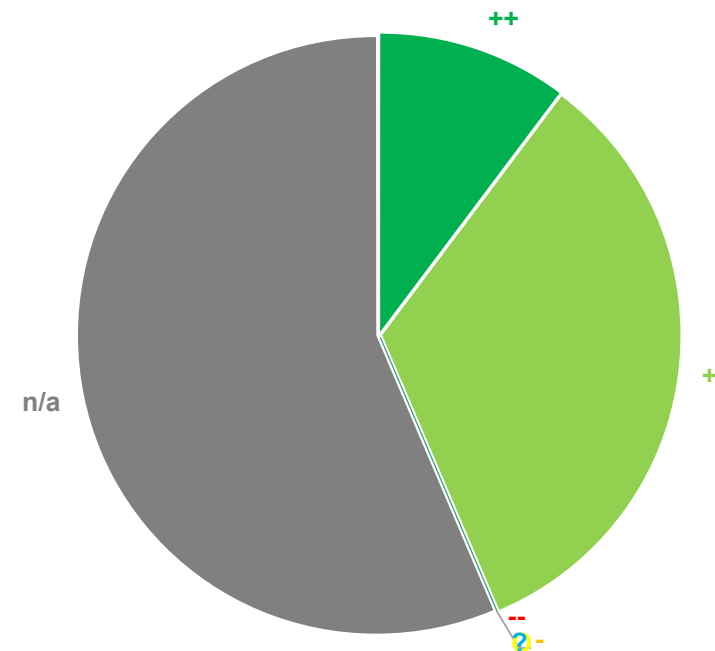


Figure 20: Overall policy performance against Objective 19.

### 3.20 Objective 20 – Natural capital and natural environment

**To protect, connect and enhance London’s natural capital (including important habitats, species and landscapes) and the services and benefits it provides.**

Research into natural capital and the natural environment in the London Legacy Development Corporation area identified biodiversity and protected areas. This objective seeks to protect and enhance London’s green spaces and waterways, thereby bringing nature closer to people and avoiding damage to protected species and habitats.

This objective was only relevant for less than half of the assessment. Of the assessments that were relevant, the majority were positive.

**Design and Sustainability** policies have a particularly positive impact on enhancing London’s natural capital. These policies aim to improve the natural environment by reducing emissions and water use, encouraging low-carbon development and increasing tree coverage and the provision of open and green spaces. Policies relating to **Transport and Health and Wellbeing** support ‘Healthy Streets Approach’ which aim to reduce private vehicle use and associated emissions. However, there are opportunities to improve the development and flood risk policies to benefit the natural environment in the area.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

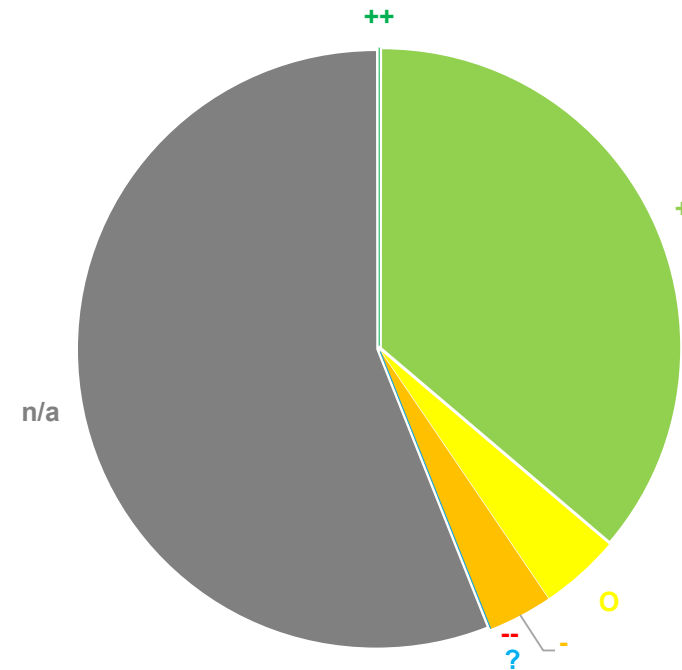


Figure 21: Overall policy performance against Objective 20.

### 3.21 Objective 21 – Historic environment

**To conserve and enhance the existing historic environment, including sites, features, landscapes and areas of historical, architectural, archaeological and cultural value in relation to their significance and their settings.**

Research into the historic environment revealed that there is a need for the protection of key historical assets within the London Legacy Development Corporation area. This objective seeks to address these issues through the conservation and enhancement of historic assets and their setting, and promote improved accessibility for all to heritage environments.

This objective was relevant to approximately one third of assessments. Of the assessments that were relevant, the majority were positive.

**Heritage Conservation** policies were very positive benefit by ensuring the conservation and enhancement of heritage assets. **Transport** policies also have a particularly positive effect on the historic environment as they aim to increase accessibility through improving transport links to areas of historic significance. Policies related to **Health and Wellbeing** also benefit through the increase in social connectivity and inclusion through the shared sense of place offered by historic environments.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

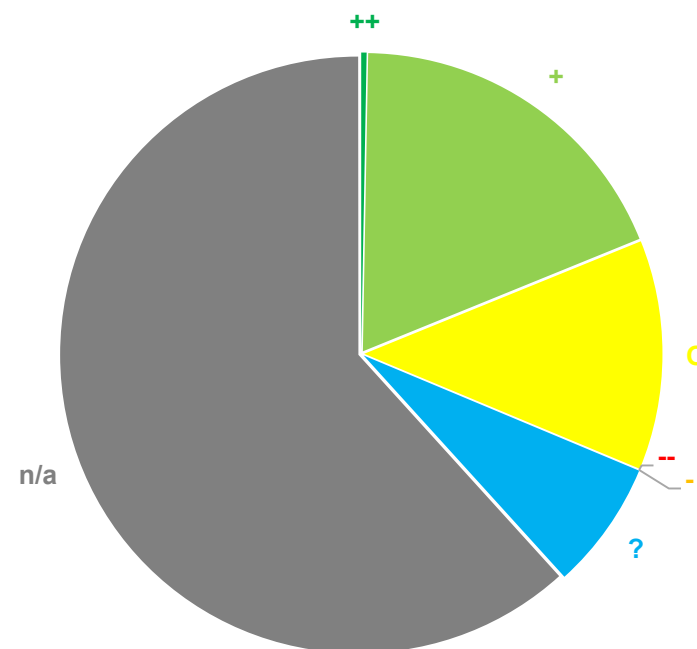


Figure 22: Overall policy performance against Objective 21.

### 3.22 Objective 22 – Geology and soils

**To conserve London’s geodiversity and protect soils from development and over intensive use.**

Research into geology and soils revealed a potential need to prevent or remediate the risk of soil contamination. Modification of the landscape and remediation are required for large areas of brownfield land. This objective seeks to address these issues through the promotion of the use of brownfield land, prevention of further soil degradation, soil restoration.

This objective was relevant to very few assessments. However, of the assessments that were relevant, all were positive. **Housing** policies had a positive effect on this objective as they promote redevelopment or brownfield sites and enhancement of sites. The **Sustainable Drainage Measures** policy also specifically effects this objective through restricting surface water run-off and reducing land contamination and pollution of watercourses.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

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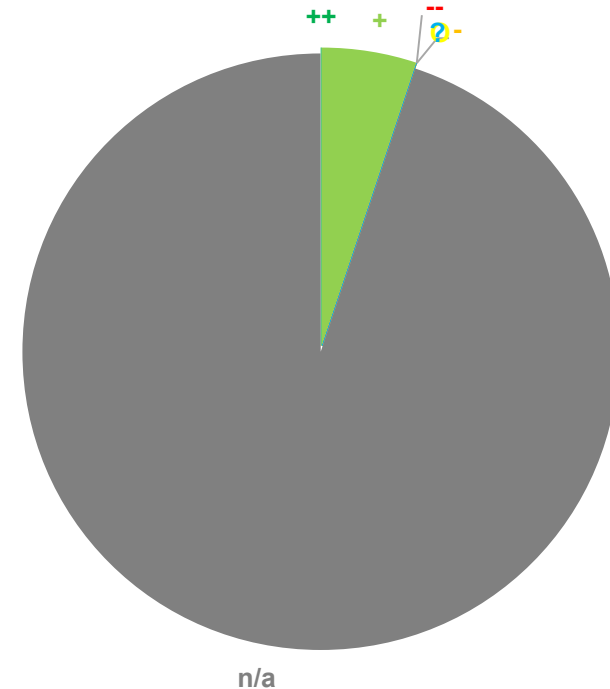


Figure 23: Overall policy performance against Objective 22.



### 3.23 Objective 23 – Materials and waste

**To keep materials at their highest value and use for as long as possible. To significantly reduce waste generated and achieve high reuse and recycling rates.**

Research into materials and waste revealed that there is an increasing pressure on waste sites and infrastructure to meet demand from new developments. This objective seeks to address these issues through promoting of waste reduction, reuse, re-manufacturing and recycling.

The objective was only relevant to a small number of policies. Of the assessments that were relevant, the majority were positive or very positive. The policy **Creating Vitality Through Interim Uses** has a particularly positive impact on materials and waste as it aims to reduce waste through extending the lifetime of assets and providing means by which people can share resources. Additionally, **Housing** and **Digital Connectivity** policies have a positive impact by encouraging materials re-use and reduced waste generation.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

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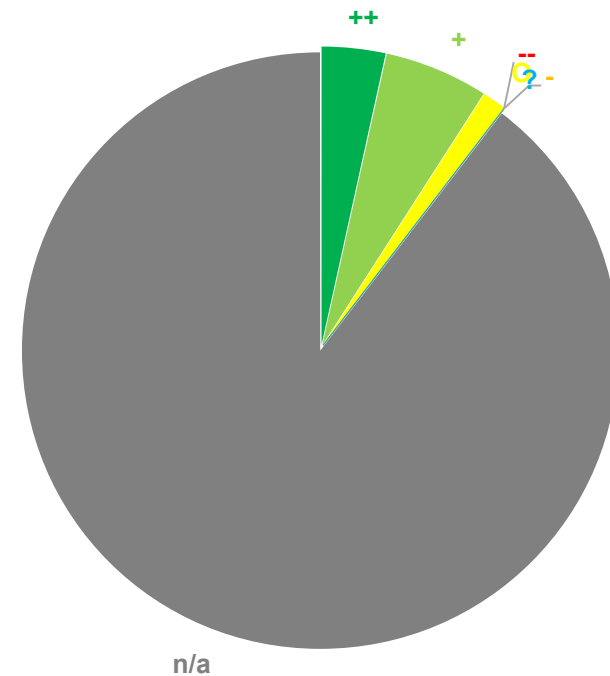


Figure 24: Overall policy performance against Objective 23.

### 3.24 Objective 24 – Noise and vibration

**To minimise noise and vibration levels and disruption to people and communities across London and reduce inequalities in exposure.**

Research into noise and vibration revealed that there is a need for balance between the night-time economy and residential housing in mixed-use developments due to the impact of noise levels on existing residents during large-scale redevelopment. This objective seeks to address these issues through reducing the level of exposure to high levels of noise including night time noise in residential areas, road, rail and aviation noise and disruption from vibrations.

This objective was only relevant to just under one quarter of assessments. Of the assessments that were relevant, the majority were positive. The **Noise** policy supports actions to reduce noise levels and increase access to quiet and tranquil spaces. **Land Use** policies have a particularly positive impact on noise and vibration due to their provision for temporary interim uses of land premises and mitigation of noise disturbances in these areas. There are opportunities for **Housing** and **Business** policies to improve the current noise landscape, particularly when considering the location of new developments and proposals to support the night time economy.

Overall, this objective is strongly supported by the Local Plan policies that will be put in place.

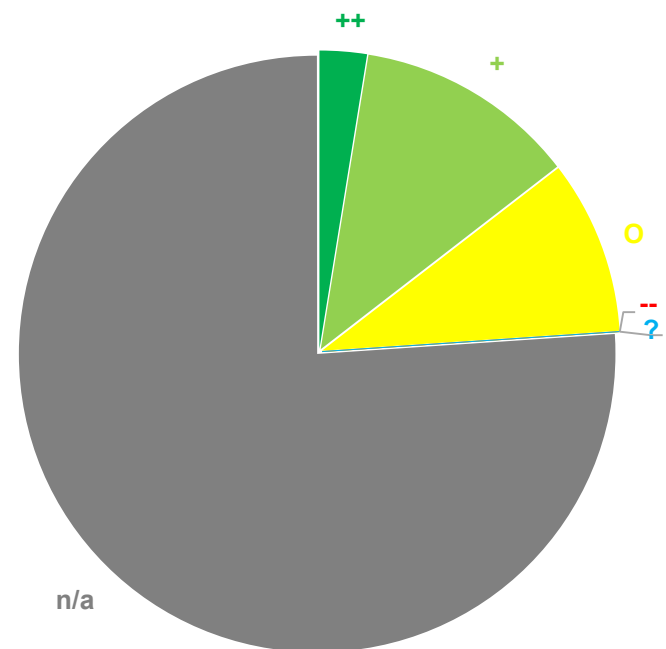


Figure 25: Overall policy performance against Objective 24.

## 4 Next Steps

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This Integrated Impact Assessment is open for consultation alongside the draft Local Plan. Upon completion of the consultation period, the consultation responses will be collated and analysed. The findings will be used in preparation of, and where appropriate revision of, the final Local Plan. The review of consultation feedback is due to be undertaken in early 2019.

If you would like further information about, or would like to comment on, the London Legacy Development Corporation Local Plan or this Integrated Impact Assessment, please contact:

LLDC Planning Policy  
Level 10  
1 Stratford Place  
E20 1EJ  
  
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## Appendix 7 - CIL Preliminary Draft Charging Schedule



**September 2018**

This Charging Schedule has been issued, approved and published in accordance with The Community Infrastructure Levy Regulations 2010 and Part 11 of PA 2008.

## Contents

1	Name of the Charging Authority.....	3
2	The CIL Rates.....	3
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5	Instalment Policy .....	6
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7	Neighbourhood Funding.....	7
8	Review and Monitoring Arrangements.....	7

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## 1 Name of the Charging Authority

1.1 The Charging Authority is the London Legacy Development Corporation.

## 2 The CIL Rates

2.1 The Legacy Corporation as a charging authority is required to have regard to the Mayoral CIL, as set by the Mayor of London, when setting its own CIL rate(s). In April 2019 the new Mayoral CIL 2 rate comes into place replacing the current Mayoral CIL rate and sets out a single rate for the whole of the Legacy Corporation area. The Mayoral CIL 2 rate for the Legacy Corporation area will be £60 per square metre from April 2019. The Legacy Corporation is a collecting authority for Mayoral CIL, and will therefore collect this charge and pass it to Transport for London.

2.2 The Legacy Corporation CIL rates are shown in the table below. For clarity, the table shows rates exclusive of Mayoral CIL.

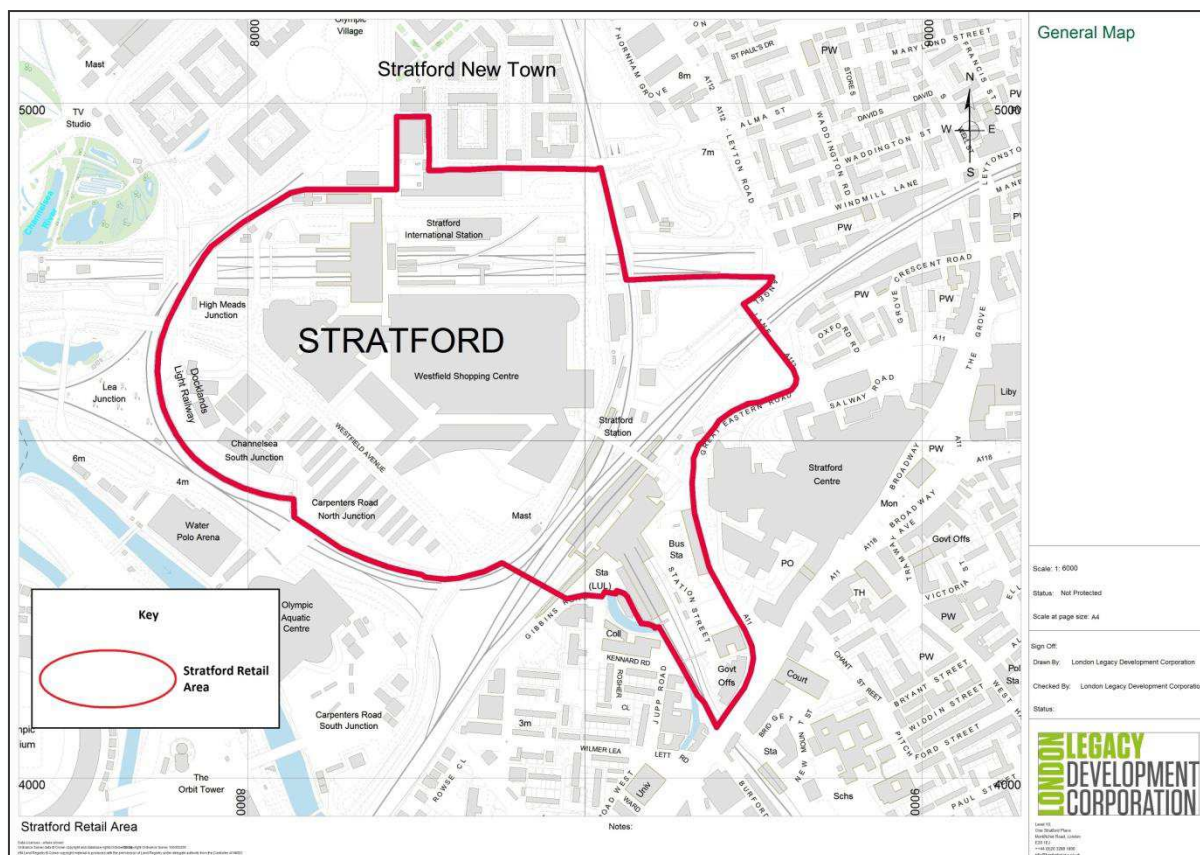
	Exclusive of Mayoral CIL
Development Type	Proposed Legacy Corporation CIL Charge (£/m <sup>2</sup> )
Residential (C3 and C4), residential institutions except hospitals (C2), shared-living/co-living (Sui Generis) but excluding student accommodation	£73.90
Student accommodation (Sui Generis)	£123.17
Convenience supermarkets and superstores and retail warehouses (over 1000 sq m)	£123.17
Hotels (C1)	£123.17
Comparison and all other retail (A1-A5) in 'Stratford Retail Area'	£123.17
Office (B1a) within the 'Stratford Retail Area'	£123.17
All other uses except education, healthcare and affordable workspace	£20
Education, healthcare and affordable workspace	Nil

### 3 Definitions

- 3.1 **Superstores / supermarkets:** Shopping destinations in their own right where food and convenience shopping needs are met and which can also include non-food floorspace as part of the overall mix of the unit.
- 3.2 **Retail warehousing:** Large stores specialising in the sale of household goods (such as carpets, furniture and electrical goods), DIY items and other ranges of comparison goods, catering for a significant proportion of car-borne customers.
- 3.3 **Convenience goods:** Food and non-alcoholic beverages, tobacco, alcoholic beverages, newspapers and periodicals and non-durable household goods.
- 3.4 **Comparison goods:** Any other goods, including clothing, shoes, furniture, household appliances, tools, medical goods, games and toys, books and stationery, jewellery and other personal effects.
- 3.5 **Shared-Living/Co-Living:** Large-scale, purpose-built shared living developments which in planning terms are Sui Generis non-self-contained market housing, this type of accommodation is seen as providing an alternative to traditional flat shares and includes additional services and facilities, such as room cleaning, bed linen, on-site gym and concierge service.
- 3.6 **Affordable workspace:** Workspace with a below market rental values secured through Section 106 agreements in line with Legacy Corporation Local Plan Policy.

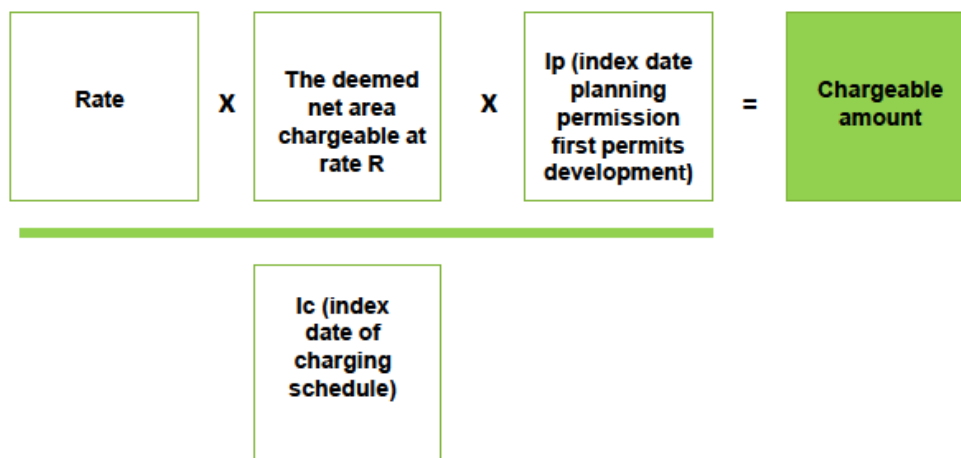


### Map 1: Stratford Retail Area



## 4 How the chargeable amount will be calculated

4.1 The chargeable amount will be calculated in accordance with the CIL regulations 2010 (as amended). The Legacy Corporation will use the following formula, as required by the regulations:



- 4.2 In most cases the deemed net chargeable area will be the gross internal area of the chargeable development<sup>1</sup>. However, the regulations allow for the gross internal area of retained parts of in-use buildings to be deducted and not charged. There are also provisions for deducting parts of existing buildings that will be demolished. Applicants are advised to look at the CIL regulations (regulation 40) and the government guidance for detailed advice.
- 4.3 Index linking will be applied as set out in the formula above, using the All in tender price index published by BCIS.
- 4.4 The regulations also allow for social housing relief, and exemptions for residential annexes or extensions. These are explained fully in the regulations and government guidance.
- 4.5 Applicants should note however that if they commence development without serving a commencement notice as required by the regulations and / or without having applied for social housing relief, then they will lose their ability to pay by instalments and / or will not be eligible for relief for social housing.
- 4.6 Legacy Corporation officers will liaise with applicants and agents to confirm the correct CIL charges and will issue liability notices as soon as practicable after planning permission first permits developments (as required by the regulations).

## 5 Instalment Policy

- 5.1 The Legacy Corporation has decided not to introduce its own instalment policy; therefore the Mayor of London’s instalment policy will apply in the Legacy Corporation area. This is set out below.

Amount of CIL liability	Number of Instalment Payments	Amount or proportion of CIL payable in any instalment/time at which payments are due
£100,000 or less	No instalments	Total amount payable within 60 days of commencement of development
£100,001 or more	Two	<ul style="list-style-type: none"> <li>• The greater of £100,000 or half the value of the total amount payable within 60 days of commencement of development</li> <li>• The remainder within 240 days of commencement of development</li> </ul>

<sup>1</sup> Almost all development is chargeable development under the regulations, except for a) a building into which people do not normally go; b) a building into which people go only intermittently for the purpose of inspecting or maintaining fixed plant or machinery. The change of use of a single dwelling house to two or more separate dwelling houses is also not chargeable development. See regulation 6 for more information.

## **6 Draft Infrastructure List**

- 6.1 Infrastructure that the Legacy Corporation intends to fund through CIL is set out on its Draft Infrastructure List.

## **7 Neighbourhood Funding**

- 7.1 The Neighbourhood Funding element represents 15% of all Legacy Corporation CIL collected, under the regulations the neighbourhood funding element does not need to be spent in accordance with the Draft Infrastructure List.
- 7.2 In 2016 the Legacy Corporation ran a consultation to establish neighbourhood priorities for use of this funding, the results of which have been published on the Legacy Corporation website. Following this in 2018 a first bidding round has been undertaken to enable the community in the Legacy Corporation area, and related partners, to bid for monies from the Neighbourhood Priorities Fund, in line with the established Neighbourhood Priorities, to fund projects that will mitigate the impacts of development on the local community. It is hoped that this will be an annual opportunity for local projects, however this will be based on CIL revenue going forward.

## **8 Review and Monitoring Arrangements**

- 8.1 During the process of consultation for the Legacy Corporation's first Preliminary Draft Charging Schedule views were invited on a review mechanism, responses were not conclusive. Therefore, the Legacy Corporation proposed to apply the rates first set out for at least three years, with the potential for a review to be undertaken sooner if circumstances changed materially. This Preliminary Draft Charging Schedule has been developed after this initial three year deadline and is in response to proposed changes to national policy, market changes, and in the types of development coming forward as well as new market products.
- 8.2 The Legacy Corporation is committed to ensuring the use of CIL is open and transparent and will publish an annual report which will clearly set out how much CIL money has been received and the infrastructure to which that money has been applied, as required by regulation 62 Reporting.

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## Appendix 8



### Draft Infrastructure List

(as required by Regulation 123 of the Community Infrastructure Levy Regulations 2010 (as amended))

Infrastructure List: Infrastructure Projects it is intended to fund through Legacy Corporation CIL (extracted from IDP – see IDP for further information on projects).

#### ***Specific Project - Child play space***

- 1.1 Eastway Community Facility including children's play areas

#### ***Specific Projects - Local transport schemes***

- 1.2 Upgrade of existing bridge over Old River Lea (south of Old Ford Lock) for use by cycles and wheelchairs by adding ramp
- 1.3 New pedestrian/cycle connection from Crown Close/Wick Lane to Greenway (ramp/stairs)
- 1.4 Upgraded pedestrian and cycle facilities at Wansbeck Road crossing

#### ***Specific Projects - Strategic transport schemes***

- 1.5 Western Overbridge for major capacity upgrade at Stratford Station
- 1.6 TfL Cycle Hire

#### ***Specific Projects - Flooding***

- 1.7 Strategic flood mitigations options

Note: see S106 and CIL Supplementary Planning Document for guidance on on-site infrastructure to be provided and secured through S106 Planning Obligations

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**Appendix 9: Draft Infrastructure Delivery Plan Project List**

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
1.	A 2FE primary school at Bromley by Bow South	Education - Primary Schools	No planning permission, no funding	Alongside development	Unknown	No	No	Developer	LB Tower Hamlets, tbc	SA4.1	Sub Area 4, SA4.1	Tower Hamlets
2.	A 3FE primary school, Neptune Wharf (Fish Island)	Education - Primary Schools	Has funding or planning permission	Unknown	Unknown	No	Yes	Developer	Developer / LB Tower Hamlets	Neptune Wharf Planning permission s106 agreement	Sub Area 1, SA1.6 (current planning permission includes permission for D1 floorspace as well as land for primary school)	Tower Hamlets
3.	Nursery - 220sqm (GEA) - PDZ 1 - Stratford Waterfront East	Education - Early years / Nurseries	Has funding or planning permission	Delivery on site from 2021 onwards	Unknown	No	No	Private sector provision - space / use secured through planning permission	Provision of space through LCS s106 agreement	LCS s106 agreement - Nine nurseries, 50 places each	Sub Area 3, SA3.2	Newham
4.	Nursery - 180sqm - PDZ 2 - Stratford Waterfront West	Education - Early years / Nurseries	Has funding or planning permission	Delivery on site from 2021 onwards	Unknown	No	No	Private sector provision - space / use secured through planning permission	Provision of space through LCS s106 agreement	LCS s106 agreement - Nine nurseries, 50 places each	Sub Area 3, SA3.3	Newham
5.	Nursery - 220sqm - PDZ 4 - Sweetwater	Education - Early years / Nurseries	Has funding or planning permission	Delivery on site from 2021 onwards	Unknown	No	No	Private sector provision - space / use secured through planning permission	Provision of space through LCS s106 agreement	LCS s106 agreement - Nine nurseries, 50 places each	Sub Area 1, SA1.8	Tower Hamlets
6.	Two Nurseries - 360sqm (GEA) which can be provided as one or two premises - PDZ 5 - East Wick and Here East	Education - Early years / Nurseries	Has funding or planning permission	Delivery on site from 2021 onwards	Unknown	No	No	Private sector provision - space / use secured through planning permission	Provision of space through LCS s106 agreement	LCS s106 agreement - Nine nurseries, 50 places each	Sub Area 1, SA1.7	Hackney
7.	Two Nurseries - 378sqm (GEA) which can be provided as one or two premises - PDZ 6 - Chobham Manor	Education - Early years / Nurseries	Has funding or planning permission	Alongside development	Unknown	No	No	Private sector provision - space / use secured through planning permission	Provision of space through LCS s106 agreement	LCS s106 agreement - Nine nurseries, 50 places each	Sub Area 2, SA2.3	Newham
8.	Two Nurseries - 360sqm (GEA) which can be provided as one or two premises - PDZ 8 - Pudding Mill	Education - Early years / Nurseries	Has funding or planning permission	Delivery on site from 2021 onwards	Unknown	No	No	Private sector provision - space / use secured through planning permission	Provision of space through LCS s106 agreement	LCS s106 agreement - Nine nurseries, 50 places each	Sub Area 4, SA4.3	Newham

Appendix 9: Draft Infrastructure Delivery Plan Project List

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
9.	A 1FE Nursery at Neptune Wharf Fish Island	Education - Early years / Nurseries	Has funding or planning permission	Unknown	Unknown	No	Yes	Developer		Neptune Wharf Planning Application (Options 2 or 3)	Sub Area 1, SA1.6 (current planning permission includes permission for D1 floorspace)	Tower Hamlets
10.	A Children's Centre at Bromley By Bow new District Centre	Education - Early years / Nurseries	No planning permission, no funding	Unknown	Unknown	No	Yes	Unknown		Bromley by Bow Masterplan; Planning Application	Sub Area 4, SA4.1 (specifies community facility rather than being specific to children's centre)	Tower Hamlets
11.	LCS - Primary Care Centre (PDZ 4, 2,554m <sup>2</sup> , six GPs and six dentists)	Primary healthcare	Has funding or planning permission	2014 - 2026	Unknown	Yes	No	Developer		LCS Planning Application	Sub Area 1, SA1.8	Tower Hamlets
12.	Walk-in centre (PDZ 8 645m <sup>2</sup> , two GPs and two dentists)	Primary healthcare	Has funding or planning permission	2016 - 2026	Unknown	Yes	No	Developer		LCS Planning Application	Sub Area 3 and 4, SA4.3 and SA3.5	Newham
13.	New community facilities in Hackney Wick Neighbourhood Centre	Primary healthcare	No planning permission, no funding	Med / Long term	Unknown	No	Yes	To be confirmed		LLDC	Sub Area 1, SA1.1 (specifies community facilities rather than being specific to a health centre)	Hackney/ Tower Hamlets
14.	1,000 – 2,500m <sup>2</sup> community and health space in the Greater Carpenters Estate area	Sports and Leisure	No planning permission, no funding	Unknown	Unknown	No	Yes	Unknown		Stratford Metropolitan Masterplan	Sub Area 3	Newham
15.	Additional indoor tennis courts at Eton Manor	Sports and Leisure	No planning permission, no funding	Medium term	Unknown	No	Yes	Unknown	LTA/Tennis Foundation	LLDC	Sub Area 4	Waltham Forest



**Appendix 9: Draft Infrastructure Delivery Plan Project List**

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
16.	Visitor moorings facilities (Provision of better boater facilities Visitor mooring in the Park with creation of pontoons north of the existing commercial moorings. Could be on a bookable basis)	Sports and Leisure	No planning permission, no funding	Short term	Unknown	Unknown	Unknown	Canal & River Trust, S106, LLDC	Canal & River Trust, LLDC	Canal & River Trust, Olympic Legacy Waterways Framework	Sub Area 2	Newham
17.	LCS - 12.4ha open space within the red line boundary	Open Space	Has funding or planning permission	2015 - 2031	Unknown	Yes	No	Developer / LLDC (part of permitted scheme)		LCS Planning Application	Sub Area 1, 2, 3 and 4	All
18.	Public open space at Bromley by Bow North, Sugar House Lane, Bromley by Bow South, Hackney Wick	Open Space	Has funding or planning permission	Alongside development	Unknown	Yes	No	Developer (part of permitted schemes)		LLDC consultation / relevant s.106 agreements	Sub Area 1 and 4, SA4.1, SA4.2, SA1.1	Newham, Hackney, Tower Hamlets
19.	New public spaces created alongside canal edge providing public access to the water and integrating informal connections	Open Space	No planning permission, no funding	Unknown	Unknown	No	Yes	Unknown	Canal & River Trust	LLDC	Sub Area 1, SA1.1	Tower Hamlets
20.	1.2 ha local park, Fish Island / north of Hertford Union Canal	Open Space	Has funding or planning permission	Alongside development	£1.1M	Yes	£1.1M	s.106 / CIL contributions / capital funding	Canal & River Trust	Fish Island AAP	Sub Area 1, SA1.3	Tower Hamlets
21.	Opportunities for public space south of Hertford Union Canal	Open Space	Has funding or planning permission	Alongside development	Unknown	No	Yes	Unknown		Local Plan and Neptune Wharf planning permission and s106 agreement	Sub Area 1, SA1.6	Tower Hamlets
22.	Local / open square with connections to and from the Greenway in Fish Island south adjacent to 417 Wick Lane	Open Space	No planning permission, no funding	Alongside development	Unknown	No	Yes	Unknown		Fish Island AAP	Sub Area 1, SA1.5	Tower Hamlets
23.	LCS - 29 children's play spaces / areas totalling 14,210m <sup>2</sup>	Child Play Space	Has funding or planning permission	2015 - 2031	Unknown	Yes	No	Developer	LLDC	LCS s.106 agreement		All

**Appendix 9: Draft Infrastructure Delivery Plan Project List**

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
24.	LCS children's play space PDZ1 - 900sqm comprising 2x doorstep play and 1x neighbourhood play	Child Play Space	Has funding or planning permission	Delivery from 2021 onwards	Unknown	Yes	No	Developer	LLDC	LCS s.106 agreement	Sub Area 3	Newham
25.	LCS children's play space PDZ2 - 1145sqm comprising 3x doorstep play and 1x neighbourhood play	Child Play Space	Has funding or planning permission	Delivery from 2021 onwards	Unknown	Yes	No	Developer	LLDC	LCS s.106 agreement	Sub Area 3	Newham
26.	LCS children's play space PDZ4 - 1344sqm comprising 3x doorstep play and 1x local play	Child Play Space	Has funding or planning permission	Delivery from 2021 onwards	Unknown	Yes	No	Developer	LLDC	LCS s.106 agreement	Sub Area 1 (SA1.7)	Hackney
27.	LCS children's play space PDZ5 - 2501sqm comprising 4x doorstep play, 1x local play and 1x Youth play	Child Play Space	Has funding or planning permission	Delivery from 2021 onwards. Local Play and Youth play delivered (Canal Park)	Unknown	Yes	No	Developer	LLDC	LCS s.106 agreement	Sub Area 1(SA1.8)	Tower Hamlets
28.	LCS children's play space 1821sqm PDZ6 - comprising 3x doorstep play and 1x neighbourhood play	Child Play Space	Has funding or planning permission	Delivery from 2016 onwards. Neighbourhood play delivered (Tumbling Play)	Unknown	Yes	No	Developer	LLDC	LCS s.106 agreement	Sub Area 2	Newham
29.	LCS children's play space PDZ8 - 1605sqm comprising 2x doorstep play and 2x local play	Child Play Space	Has funding or planning permission	Delivery from 2021 onwards	Unknown	Yes	No	Developer	LLDC	LCS s.106 agreement	Sub Area 4 SA4.3	Newham
30.	LCS children's play space 1365sqm PDZ12 - comprising 1x local play	Child Play Space	Has funding or planning permission	Delivery from 2021 onwards	Unknown	Yes	No	Developer	LLDC	LCS s.106 agreement	Sub Area 3 SA3.6	Newham
31.	Bromley by Bow South	Child Play Space	No planning permission, no funding	Alongside development	Unknown	Yes	No	Developer		Bromley by Bow SPD	Sub Area 4	Tower Hamlets

Appendix 9: Draft Infrastructure Delivery Plan Project List

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
32.	Pudding Mill children's play space	Child Play Space	No planning permission, no funding	As development comes forward - 2015 onwards	Unknown	No	Yes	S106 agreement	Developer	Pudding Mill Land Use and Design Framework	Sub Area 4	Newham
33.	Eastway Community Facility including children's play areas	Child Play Space	To be confirmed	Med / Long	Unknown	No	Yes	Unknown		Hackney Wick AAP	Sub Area 1	Hackney
34.	Trowbridge Village Green renovation project	Child Play Space	To be confirmed	Unknown	Unknown	No	Yes	Play Pathfinder		OLSPG Infrastructure Delivery Study	Sub Area 1	Hackney
35.	LCS - 2,423m <sup>2</sup> flexible community space, 1,258m <sup>2</sup> flexible cultural space, 3,606m <sup>2</sup> of flexible leisure space and an Idea Store (2,460m <sup>2</sup> )	Libraries, Multi Use Community space and Cultural Facilities	Has funding or planning permission	Unknown	Unknown	Yes	No	Developer funding	LLDC	LCS s.106 agreement		All
36.	Stratford City - multi-use community facility of 1,572m <sup>2</sup> on the eastern side of the site	Libraries, Multi Use Community space and Cultural Facilities	Has funding or planning permission	Unknown	Unknown	Yes	No	Unknown	Developer	Stratford City S106 agreement	Sub Area 2	Newham
37.	Bromley-by-Bow South community facility	Libraries, Multi Use Community space and Cultural Facilities	No planning permission, no funding	Alongside development	Unknown	Yes	No	Developer (provision as part of scheme)	Developer	Site allocation SA4.1	Sub Area 4	Tower Hamlets
38.	Local theatre run by 'The Yard Theatre' charity with main stage, public areas and rooms for community use	Libraries, Multi Use Community space and Cultural Facilities	No planning permission, no funding	Unknown	Unknown	Unknown	Yes	Developer with the Yard Theatre	The Yard Theatre	The Yard Theatre	Sub Area 1	Tower Hamlets
39.	1,000 - 2,500m <sup>2</sup> new community and health space in the Greater Carpenters Estate	Libraries, Multi Use Community space and Cultural Facilities	No planning permission, no funding	Unknown	Unknown	No	Yes	Unknown		Stratford Metropolitan Masterplan	Sub Area 3, SA3.4	Newham

Appendix 9: Draft Infrastructure Delivery Plan Project List

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
40.	Improving pedestrian and cycle links across the A12 south of Bow Roundabout and improved pedestrian and cycle environment along the A12 corridor.	Local Transport Schemes	No planning permission, no funding	Medium-Long term	£7.0- 12M	£7.0- 12M	Yes	s.106 /278 agreement from forthcoming development at Bromley by Bow South and TfL funding	Developer / TfL	LLDC/TfL	Sub Area 4, SA4.1	Tower Hamlets
41.	A12 subway by Bromley by Bow Station	Local Transport Schemes	No planning permission, no funding	Medium term	Unknown	Unknown	Unknown	Unknown	TfL/Developer	TfL/Developer	Sub Area 4	Tower Hamlets
42.	Improve pedestrian and cycle route under the A12 from Eastway to Mabley Green	Local Transport Schemes	No planning permission, no funding	Long term	Unknown	No	Yes	TBC s106 / TfL / LLDC		Hackney Wick & Fish Island Supplementary Planning Document	Sub Area 1	Hackney
43.	Further upgraded pedestrian / cycle connection over the A12 from Wallis Road to Cadogan Terrace.	Local Transport Schemes	No planning permission, no funding	Long term	Unknown	No	Yes	s.106/CIL / TfL / LLDC		Hackney Wick & Fish Island Supplementary Planning Document	Sub Area 1	Tower Hamlets
44.	A new link between Fish Island North and Fish Island Mid to provide a more direct route between the hub at Hackney Wick and Fish Island Mid (includes Bridge over Hertford Union Canal)	Local Transport Schemes	Has funding or planning permission	Short term	£4.6M	No	Yes	s.106 / CIL	Canal & River Trust	Fish Island AAP	Sub Area 1, SA1.3	Tower Hamlets
45.	New link across Hertford Union Canal bottom lock, from Smeed Garden through McGrath Site	Local Transport Schemes	No planning permission, no funding	Medium term	Unknown	No	Yes	Developer/S.106/CIL	Developer	LLDC	Sub Area 1	Tower Hamlets
46.	Upgrade of existing bridge over the Old River Lea (south of Old Ford Lock) for use by cycles and	Local Transport Schemes	No planning permission, no funding	Long term	£232,000	No	£232,000		Canal & River Trust/LLDC	Canal Park project scoping	Sub Area 1	Tower Hamlets

Appendix 9: Draft Infrastructure Delivery Plan Project List

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
	wheelchairs by adding ramp											
47.	New pedestrian/cycle connection from Crown Close/Wick Lane to Greenway (ramp/stairs)	Local Transport Schemes	No planning permission, no funding	Short term	£600,000	300,000	Yes	s.106/LLDC	Developer	LBTH/OPLC connections study and HWFI public realm strategy	Sub Area 1, SA1.5	Tower Hamlets
48.	New rail bridge connection across the River Lea at Autumn Street or Riverside Wharf (dependent on the future of Bow Midland East rail yard in Newham).	Local Transport Schemes	No planning permission, no funding	Long term	Unknown	No	Yes		Canal & River Trust	Fish Island AAP	Employment cluster designation B.1a3 (Bow Goods Yard East)	Tower Hamlets
49.	A12 underpass improvements Wick Lane	Local Transport Schemes	No planning permission, no funding	Short term	£500,000	No	Yes	LLDC / TfL / S106 / CIL	LLDC	HWFI Connectivity Study	Sub Area 1	Tower Hamlets
50.	Upgraded pedestrian and cycle facilities at Wansbeck Road crossing	Local Transport Schemes	No planning permission, no funding	Short term	£250,000	No	£250,000	LLDC / LBTH / S106 / CIL	LLDC/LBTH	Hackney Wick & Fish Island Supplementary Planning Document	Sub Area 1	Tower Hamlets
51.	Bromley by Bow Project: new junction	Local Transport Schemes	No planning permission, no funding	Medium term	TBC	TBC	Yes	Funding TBC, identified as part of Bow Vision and A12 Study*	TfL (Bow Vision)	Local Plan/TfL Bow Vision/TfL A12 Study	Sub Area 4	Tower Hamlets
52.	A12 Streetscape outside BBB Station	Local Transport Schemes	No planning permission, no funding	Medium term	Unknown	no	Yes	TfL and developer s106	TfL (Bow Vision)	Local Plan/BBB Station Public Realm works/Forthcoming Bromley by Bow SPD	Sub Area 4	Tower Hamlets
53.	A12 30mph Speed limit	Local Transport Schemes	No planning permission, no funding	Medium term	Unknown	no	Yes	TfL, A12 RTF Study*	TfL	TfL A12 RTF Study	Sub Area 4	Tower Hamlets, Hackney,
54.	Franklin Street pedestrian crossing	Local Transport Schemes	No planning permission, no funding	Short term	Part of £7-£12M junction improvement	Yes part TfL part BBB developer	Yes	Estimate based other A12 surface crossings*	TfL	Local Plan	Sub Area 4	Tower Hamlets
55.	Environmental Enhancement of Hancock Road	Local Transport Schemes	No planning permission, no funding	Medium Term	£1.8M	no	Yes	Funding TBC*	TfL and Developer. Related to the Bow South development and Bow Vision	TfL A12 Study	Sub Area 4	Tower Hamlets

Appendix 9: Draft Infrastructure Delivery Plan Project List

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
56.	Bow Interchange (Flyover removal)	Local Transport Schemes	No planning permission, no funding	Medium term	£54M	yes	Yes	Funding TBC, identified as part of Bow Vision*	TfL (Bow Vision)	TfL Bow Vision	Sub Area 4	Tower Hamlets
57.	Upgrade of subway connection from Olympic Park at U07 under the Green Way on City Mill River	Local Transport Schemes	No planning permission, no funding	Short / Med term	Unknown	No	Yes	CIL/s106/LLDC	Crossrail/Canal & River Trust /LLDC/Newham	Canals and Rivers Trust	Sub Area 4	Newham
58.	Upgrade of access point from Green Way to towpath at the cross of the River Lea and upgrade to towpath	Local Transport Schemes	No planning permission, no funding	Short / Med term	Unknown	No	Yes	CIL/s106/LLDC	Canal & River Trust /LLDC/Newham	Canals and Rivers Trust	Sub Area 4	Newham
59.	New bus/ped/cycle bridge over the Bow Back river at Marshgate Lane, Pudding Mill Lane	Local Transport Schemes	No planning permission, no funding	Short / Medium term	£3.8M	No	Yes	s106	Developer - part provision made through s106 for Porsche garage site. Linked to the Marshgate Lane junction proposals/ Canal & River Trust	Stratford Metropolitan Masterplan Transport Study	Sub Area 4, SA4.3	Newham
60.	Highway improvements in the Chobham Farm Area improving East-West local connectivity	Local Transport Schemes	No planning permission, no funding	Short / Med term	Unknown	Yes	No	Developer	Chobham Farm s106 provides funding for study	Stratford Metropolitan Masterplan Transport Study	Sub Area 2, SA2.1	Newham
61.	Bridge from IQL to Stratford Waterfront over rail tracks	Local Transport Schemes	No planning permission, no funding	Short term	£9M	No	Yes	Developer	Passive safeguarding in Stratford City zone 2 s106 (TIQ). Potential to be brought forward as part of Stratford Waterfront development.	LLDC 10 Year Plan	Sub Area 2, SA3.2, SA3.3	Newham
62.	LCS – Bridgewater Road bridge – Pudding Mill	Local Transport Schemes	Has funding or planning permission	Assumed 2021	Unknown	No	Unknown	Developer	Developer/ Canal & River Trust	LCS planning permission	Sub Area 4	
63.	Downscaling of Warton Road junction	Local Transport Schemes	No planning permission, no funding	Med term	£800,000	No	£300,000	Unknown	Newham	Stratford Metropolitan Masterplan Transport Study	Sub Area 3, SA3.3	Newham
64.	E38 Carpenters Road Underpass and surrounding environment	Local Transport Schemes	No planning permission, no funding	2020	Unknown	Unknown	Unknown	Unknown	Newham, LLDC & Network Rail	LLDC Connectivity Study	Sub Area 3	Newham

**Appendix 9: Draft Infrastructure Delivery Plan Project List**

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
	65. Direct access into Stratford Station from Carpenters area - including new entrance to Stratford Station	Local Transport Schemes	Has funding or planning permission	Short term	£6.2M	£4.2M	Yes	£3.22M from LLV pooled funds and s106 contributions and £1M from LLDC		Stratford Metropolitan Masterplan Transport Study	Sub Area 3, SA3.4	Newham
	66. Direct access into Stratford Station from Carpenters area - new Jupp Road Bridge	Local Transport Schemes	No planning permission, no funding	Medium term	£7M	No	Yes	CIL/S106/Developer	LLDC/Newham	Stratford Metropolitan Masterplan Transport Study	Sub Area 3, SA3.4	Newham
	67. Improved pedestrian and cycle connections between Carpenters area and Stratford Town Centre	Local Transport Schemes	No planning permission, no funding	Short / Med term	£800,000	No	£800,000	Unknown		Stratford Metropolitan Masterplan Transport Study	Sub Area 3, SA3.4	Newham
	68. Stratford Station western overbridge	Local Transport Schemes	No planning permission, no funding	Long term	£80-120million	£1million	£79-119million	TfL, LLDC, Section 106, CIL, Newham, GLA	TfL/NR/LLDC	TfL LU Outcome Definition Study, Crossrail Legion Model, Overbridge Feasibility Study	Sub Area 3	Newham
	69. Angel Lane entrance Stratford Station – direct access into Stratford Station via a disused ticket hall to Eastern subway onto the NR platforms	Local Transport Schemes	Passive Provision within Westfield YHA/Office planning application, no funding	Short term	£4million	£2million	£2million	TfL, LLDC, Section 106, CIL, Newham, GLA	TfL/Developer	TfL LU Outcome Definition Study, and South-eastern Ticket Hall Feasibility Study	Sub Area 3	Newham
	70. Lift change at western subway of Stratford Station	Local Transport Schemes	No planning permission, no funding	Med/ long term	£3million	£3million	No	TfL/LLDC/GLA	TfL/LLDC	TfL LU Outcome Definition Study, Crossrail Legion Model	Sub Area 3	Newham
	71. Events Entrance at platform 1 Stratford Station	Local Transport Schemes	No planning permission, no funding	Med/ long term	Unknown	No	Unknown	Unknown	Unknown	Transport Study 2018	Sub Area 3	Newham
	72. New Event Day Entrance to Stratford Station via Northern Ticket Hall	Local Transport Schemes	No planning permission, no funding	Short term	Unknown	No	Unknown	Unknown	TfL	TfL Outcome Definition Study, Event Day Entrance Legion Modelling	Sub Area 3	Newham

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	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
73.	Stratford Station Platform 13 entrance	Local Transport Schemes	No planning permission, no funding	Med/ long term	Unknown	No	Unknown	Unknown	Unknown	Transport Study 2018	Sub Area 3	Newham
74.	Stratford station southern ticket hall expansion	Local Transport Schemes	No planning permission, no funding	Short term	Unknown	Unknown	Unknown	Unknown	TfL	TfL LU Outcome Definition Study	Sub Area 3	Newham
75.	Pedestrian and cycle bridge between the Bisson Road and Sugar House Lane areas, via Three Mills	Local Transport Schemes	Being implemented	Med / Long term	£700,000	No	£700,000	Unknown	Sugar House Lane s106, Canal & River Trust	Stratford Metropolitan Masterplan Transport Study	Sub Area 4, SA4.2	Newham
76.	Bridges between Bromley by Bow and Sugar House Lane, including bus bridge at Culvert Drive	Local Transport Schemes	Has funding or planning permission	Short / Med term	Unknown	Yes	Unknown	Developer	Sugar House Lane s106, Bromley by Bow North s106, Canal & River Trust	s106 agreements	Sub Area 4, SA4.1, SA4.2	Newham , Tower Hamlets
77.	Direct link between Sugar House Lane and Marshgate Lane / Marshgate Lane /Stratford High Street junction	Local Transport Schemes	Has funding or planning permission	Short term	£3.5M	Yes - £2.65M	£850,000	Landprop to fund and deliver SHS junction		Stratford Metropolitan Masterplan Transport Study, Bow Vision	Sub Area 4, SA4.3	Newham
78.	Cooks Road / Stratford High Street junction - left turn required from SHS to Cooks Road	Local Transport Schemes	No planning permission, no funding	Alongside development	Unknown	None	Yes	s.106 as part of development	TfL / LB Newham / developer	Bow Vision	Sub Area 4	Newham
79.	Cooks Road Bridge widening over Bow Back River	Local Transport Schemes	No planning permission, no funding	Med / Long term	£100,000 to £400,000	No	Yes	s106	Developer/LLDC	Pudding Mill Land Use and Design Framework, Canal & River Trust	Sub Area 4, SA4.3	Newham
80.	Downgrading of Stratford High Street, including improved public realm and narrowing to northeast of Warton Road, or	Local Transport Schemes	No planning permission, no funding	Med term	Unknown	No	Yes	Unknown		Stratford Metropolitan Masterplan Transport Study	Sub Area 3	Newham



Appendix 9: Draft Infrastructure Delivery Plan Project List

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
	addition of bus lanes.											
81.	Pudding Mill Lane west-east bridge over A12 at Five Bells Wrexham Road	Local Transport Schemes	No planning permission, no funding	Long term	£6million	None	Yes	s.106/CIL / TfL / LLDC	TfL / LB Tower Hamlets, LB Newham	LLDC	Sub Area 4	Newham , Tower Hamlets
82.	A12 Road Bridge at Crown Close and Old Ford Road	Local Transport Schemes	No planning permission, no funding	Med term	£20M	No	£20M	CIL and TfL funding	LLDC / TfL	Local Plan	Sub Area 1, none new - shown on Figure 29 in Local Plan	Tower Hamlets
83.	Leaway 2 - Link between Twelvetrees Bridge and Lea Valley Walk - new stairs, lift and ramps, and associated public realm - new local connection between Bromley by Bow to Stratford and QEOP to River Thames	Local Transport Schemes	Being implemented	Short term	£5M	No	£5M	LLDC/TfL/LBN/GLA	LLDC/LBN/ Canal & River Trust	Lea River Park Primer, Fatwalk Stage E	N/A	Newham
84.	Upgraded pedestrian link (new ramp) from Dace Road to the Greenway	Local Transport Schemes	No planning permission, no funding	Med term	£600,000	No	Yes	s.106 /CIL / LLDC		Hackney Wick & Fish Island Supplementary Planning Document	Sub Area 1	Tower Hamlets
85.	Warton Road pedestrian/cycle route and associated public realm works – to add new pedestrian footway underneath railway tracks on western side	Local Transport Schemes	Has funding or planning permission	Medium term	£900,000	Yes	£200,000	Unknown	Newham / LLDC/ Canal & River Trust	Stratford Transport Implementation Group (STIG), S106 agreements, Newham	Sub Area 3	Newham
86.	LCS planning permission projects/upgrades	Local Transport Schemes	Has funding or planning permission	Long Term	Unknown	Yes	No	LCS S106		LCS Planning Permission		All
87.	TfL Cycle Hire within Hackney Wick & Fish Island	Local Transport Schemes	Partially complete	Med Term	£500,000	Yes	Unknown	TfL/S106	TfL	TfL internal work	Sub Area 1	Tower Hamlets, Hackney

Appendix 9: Draft Infrastructure Delivery Plan Project List

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
88.	TfL Cycle Hire - Chobham Manor	Local Transport Schemes	No planning permission, no funding	Med Term	£200,000	No	Unknown	TfL/S106	TfL	TFL internal work	Sub Area 2	Newham
89.	TfL Cycle Hire - Other LCS PDZs and remaining LLDC area £2,450,000. The £200k for Here East would be considered part of this area	Local Transport Schemes	Partially complete	Med Term	£2.45M	£200k	£2.25M	£200k Here East S106	TfL	TFL internal work	Sub Area 1 and 4	Hackney
90.	Montfichet Road/Westfield Avenue highway/public realm works, Stratford City bus station access. Carriageway reduction and reallocation to urban realm and cycle/pedestrian space. Provision of segregated cycle facilities.	Local Transport Schemes	No planning permission, no funding	Medium Term	£3.4million approx.	£540,000	Unknown		LLDC/LBN	LLDC feasibility study, Mott McDonald & urban initiatives	Sub Area 2	Newham
91.	Montfichet Road West – Link to Pool Street/Loop Road pedestrian connection	Local Transport Schemes	No planning permission, no funding	2020	Unknown	Unknown	Unknown	Unknown	Newham/LLDC	LLDC Connectivity Study	Sub Area 3	Newham
92.	Electric Vehicle Charging Infrastructure	Local/Strategic Transport Schemes	No planning permission, no funding	2018-19	Unknown	Unknown	Unknown		LBH/TfL	Hackney Transport Strategy	Sub Area 1	Hackney
93.	Hackney Wick Zero Emissions Network (ZEN) and associated Neighbourhood Programme	Strategic Transport Schemes	No planning permissions, no funding	2020 onwards	Unknown	Unknown	Unknown	LBH	LBH	Hackney Transport Strategy	Sub Area 1	Hackney

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	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
94.	Pedestrian and Cycle link between Leyton and Olympic Park between Ruckholt Road and Temple Mills Lane	Local Transport Schemes	No planning permission, no funding – Waltham Forest are currently undertaking early stage feasibility on this, including understanding land ownership	Medium term	Unknown	Unknown	Unknown	LBWF	LBWF	Lea Bridge and Leyton Vision	Sub Area 2	Waltham Forest
95.	Cycle Lane provision between Eastway/Ruckholt Road and A12	Local Transport Schemes	No planning permission, no funding	Medium term	Unknown	Unknown	Unknown	LBWF	LBWF	Lea Bridge and Leyton Vision	Sub Area 2	Waltham Forest
96.	Bus network enhancements – to match the increasing travel demands resulting from the growth in the LLDC area and provide links to neighbouring communities and facilities	Local Transport Schemes	No planning permission, no funding	Medium term	Unknown	Unknown	Unknown		TfL	TfL		All boroughs
97.	Downgrade of Great Eastern Road/Stratford Gyratory – removal of gyratory – TfL Major Schemes	Strategic Transport Schemes	Being Implemented	Med term – completion 2019	£17.8M	£998k committed for design phase	£16.8M	S106/CIL/TfL/LBN	LBN	TfL Major Schemes Programme	N/A	Newham
98.	Electricity sub-station upgrades in Bow and West Ham	Electricity	To be confirmed	Unknown	Unknown	Unknown	Unknown	National Grid		LB of Tower Hamlets Core Strategy		Tower Hamlets and Newham
99.	132kV power lines replacement in Stratford and West Ham	Electricity	To be confirmed	2015-2019	Unknown	Yes	No	EDF		LB of Newham Core Strategy and LB of Newham CIL Infrastructure Planning and Funding Gap Report		
100.	132kV network between West Ham and Brunswick Wharf	Electricity	To be confirmed	2015-2019	Unknown	Yes	No	EDF		LB of Newham Core Strategy and LB of Newham CIL Infrastructure Planning and		

**Appendix 9: Draft Infrastructure Delivery Plan Project List**

	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
										Funding Gap Report		
101.	132kV network laid from West Ham to Orchard Place	Electricity	To be confirmed	2015-2019	Unknown	Yes	No	EDF		LB of Newham Core Strategy and LB of Newham CIL Infrastructure Planning and Funding Gap Report		
102.	Provision of 26.3 MVA across the borough through various projects	Electricity	To be confirmed	2009- 2026	Unknown	Unknown	Unknown	Unknown		LB of Waltham Forest Core Strategy		
103.	Beckton pressure reduction Station rebuild	Gas	To be confirmed	2014-2015	Unknown	Yes	No	National Grid		LB of Newham Community Infrastructure Study Future Needs Report		
104.	Point of Contact A, LB of Waltham Forest heat export point	Combined Cooling, Heating and Power networks	To be confirmed	Unknown	Unknown	Unknown	Unknown	Unknown		OLSPG Energy Study		
105.	Point of Contact B, LB of Newham heat export point	Combined Cooling, Heating and Power networks	To be confirmed	Unknown	Unknown	Unknown	Unknown	Unknown		OLSPG Energy Study		
106.	Point of Contact D, connection to Fish Island and Hackney Wick	Combined Cooling, Heating and Power networks	To be confirmed	Unknown	Unknown	Unknown	Unknown	Unknown		OLSPG Energy Study		
107.	Newham Local Heat Network – connection from West Ham (Manor Road) to Greenway	Combined Cooling, Heating and Power networks	To be confirmed	Unknown	Unknown	Unknown	Unknown	Unknown		LB of Newham Community Infrastructure Study Future Needs Report, Royal Docks Infrastructure Study (Ramboll), Heat Network Local Development Order (March 2013)		

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	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
108.	Thames Tideway Tunnel combined sewer overflow improvement projects	Sewage	To be confirmed	Approx. 2015-2020	£4.1 billion	Yes	No	Thames Water and Ofwat		LB of Tower Hamlets Infrastructure Delivery Plan and Core Strategy; LB of Newham Core Strategy; LB of Hackney Infrastructure Assessment		
109.	Deephams STW	Sewage	To be confirmed	Approx. 2015	As part of an allocated £675M for STW upgrades	Yes	No	Unknown		Thames Water Investment Programme: Our plans for 2010-2015		
110.	Beckton STW upgrade	Sewage	To be confirmed	Approx. 2015	As part of an allocated £675M for STW upgrades	Yes	No	Unknown		Thames Water Investment Programme: Our plans for 2010-2015		
111.	Additional works to upgrade sewage works capacity as a result of residential growth	Sewage	To be confirmed	Up to 2031	Unknown	No	Yes	Unknown		Thames Water Investment Programme: Our plans for 2010-2015		
112.	INF3 waste site at Beckton Riverside	Waste	To be confirmed	2012-2015	Unknown	Unknown	Unknown	Unknown		LB of Newham CIL Infrastructure Planning Report and East London Joint Waste		
113.	<u>North London Waste Plan</u>	<u>Waste</u>	<u>Currently at draft stage. See Appendix 2 "Hackney Site and Areas" of the draft plan:</u> <a href="http://www.nlwp.net/consultation/1-DraftPlanDocuments.html">http://www.nlwp.net/consultation/1-DraftPlanDocuments.html</a>	<u>Medium term</u>	<u>Unknown</u>	<u>Unknown</u>	<u>Unknown</u>	<u>North London Boroughs</u>	<u>North London Waste Plan</u>	<u>North London Waste Plan</u> <a href="http://www.nlwp.net">http://www.nlwp.net</a>	<u>Currently identified sites are outside the LLDC area.</u>	
114.	Hackney Wick and Hackney Marshes Flood alleviation and habitat enhancement	Flooding	To be confirmed	Med / Long term	£7-11M	Yes	£7-11M	Multi-agency and cross borough	LB of Hackney, Environment Agency, LB of Tower Hamlets, Canals and Rivers Trust	Consultation with Environment Agency and Hackney Level 2 Strategic Flood Risk Assessment		

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	Infrastructure Project	Type of project	Status	Phasing	Cost (£M)	Committed funding (£M)	Funding gap	Funding / source arrangements	Delivery / other responsible agencies	Information Source	Local plan sub area and / or site allocation	Borough
115.	Strategic flood mitigation options	Flooding	To be confirmed	Short / Med term	£800–10M	No	£800–10M	Multi-agency	LB of Hackney, LB of Tower Hamlets, Environment Agency	Hackney Wick AAP		
116.	Surface water flooding mitigation	Flooding	To be confirmed	Short / Med / Long term	Unknown	No	Yes	Defra, S106 /	LB of Hackney, Environment Agency	Hackney Wick AAP		

Status Key	
	Being implemented
	Has funding or planning permission
	No planning permission, no funding
	No longer proposed / necessary
	To be confirmed



**Subject:** Land adjacent to Eastcross Bridge, Queen Elizabeth Olympic Park, London, E20; ref: 18/00340/FUL

**Meeting date:** 25 September 2018

**Report to:** Planning Decisions Committee

**Report of:** Josh Hackner, Planning Development Manager

FOR DECISION

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**This report will be considered in public**

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## **1. EXECUTIVE SUMMARY**

- 1.1. The application seeks planning permission for the construction of a high ropes adventure course, with a reception, store and associated landscape. The proposed course consists of a series of columns connected by various rope-based activities.
- 1.2. Two structures at the eastern end of the site would provide a reception kiosk, storage space and WC. Step access to the course would also be provided within these structures. A total of 23 tree-like structure columns are proposed across the site, with platforms provided on each. The columns would vary in height, the tallest being 14.8 metres from ground level and the smallest being 6.8 metres (refer to Appendix 5 for proposed site elevations). The platforms would be a minimum of 3 metres above ground and the area around the columns at ground level would remain open and accessible at all times.
- 1.3. The application site (0.28ha) is situated within North Park area of Queen Elizabeth Olympic Park (QEOP), within the London boroughs of Hackney and Newham. The site runs parallel with the Eastcross Bridge, without restricting any public access. The majority of the site is contained on the east of the River Lea, and comprises a small parcel of undeveloped grassland to the west of the River Lea.
- 1.4. The proposed scheme is considered to meet the relevant policies in the Local Plan and the London Plan which promote QEOP as a major visitor and tourist destination in accordance.
- 1.5. The site is within Metropolitan Open Land (MOL) and the proposals are not considered to harm the openness of the MOL in accordance with Policies BN.6 of the Local Plan, 7.17 of The London Plan and the NPPF.
- 1.6. The River Lee runs through the centre of the site, which is designated as a Site of Importance for Nature Conservation (SINC). The proposed scheme is not considered to have an adverse impact on the biodiversity value of the site or the SINC, compliant with Local Plan Policy BN.3, London Plan Policy 7.19 and Paragraph 118 of the NPPF.
- 1.7. Officers consider the design to be sympathetic to the site context and reflect the character of the North Park through its scale, massing and material selection. The development would optimise the function and enhance the local distinctiveness of the waterway environment by creating opportunities for recreational activities along the

waterway. It would provide an accessible recreational facility, for users to enjoy this part of the Park.

- 1.8. The proposals are not considered to have an adverse impact on the residential amenity of existing or future residents by way of noise or visual amenity, nor would it compromise the open space of the area, which contributes to healthy and sustainable communities.
- 1.9. As such, Officers consider the development to be in accordance with national, London and local plan policies and represent sustainable development.

It is therefore recommended that the Committee resolve to grant planning permission for the development proposed subject to the following conditions.

## **2. RECOMMENDATIONS**

### **2.1. The Committee is invited to:**

**2.1.1 APPROVE the applications, for the reasons given in the report and grant planning permission and advertisement consent subject to the conditions set out in this report.**

**2.1.2 AGREE TO DELEGATE AUTHORITY to the Director of Planning Policy and Decisions to finalise the recommended conditions as set out in this report including such refinements, amendments, additions and/or deletions as the Director of Planning Policy and Decisions considers reasonably necessary.**

## **3. FINANCIAL IMPLICATIONS**

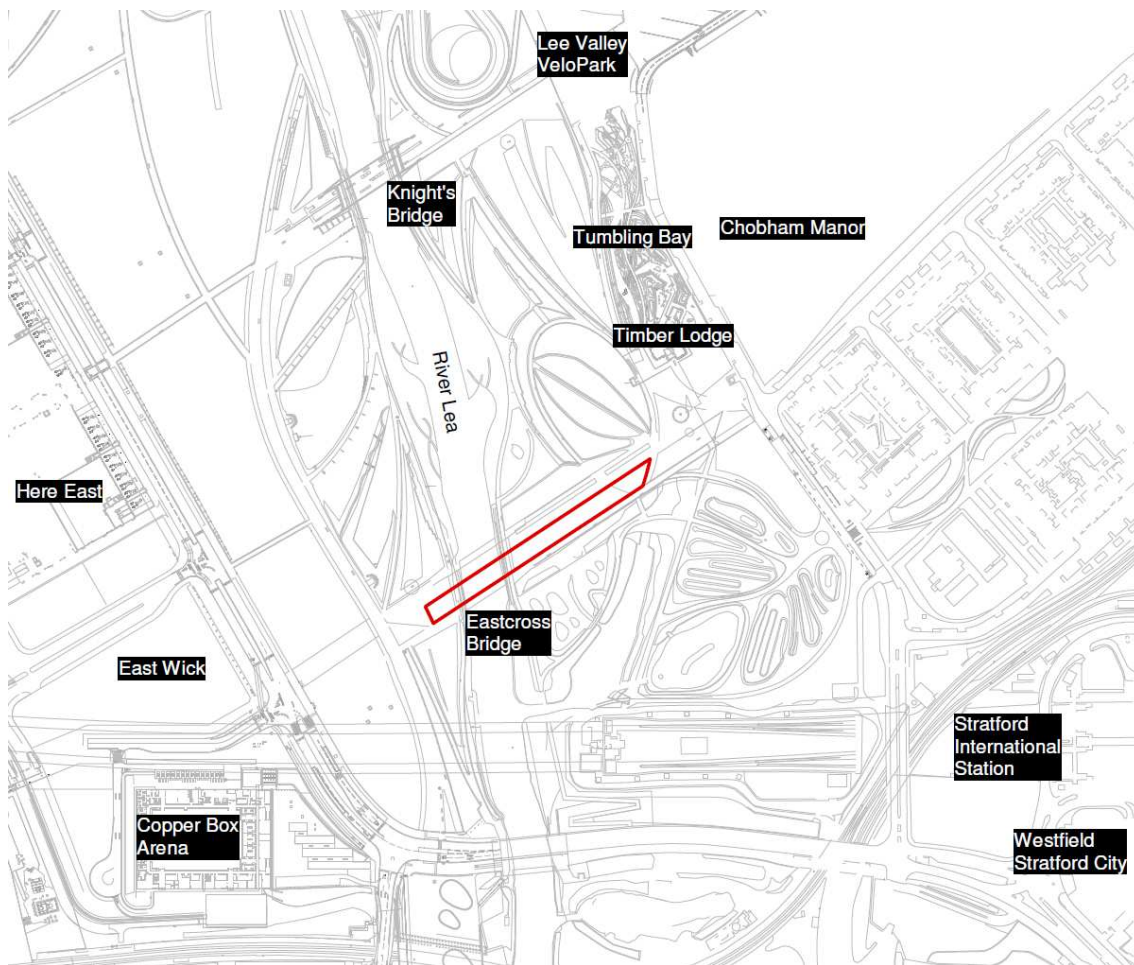
- 3.1. There are no financial implications as a result of this application.

## **4. LEGAL IMPLICATIONS**

- 4.1. The recommendation is that planning permission be granted, subject to conditions.



## Site Plan



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<b>Location:</b>	Land adjacent to Eastcross Bridge, Queen Elizabeth Olympic Park, London, E20
<b>London Borough:</b>	Newham and Hackney
<b>Proposal:</b>	Full planning permission for the construction of a high ropes adventure course, with two single storey buildings containing reception, store and accessible W.C; and associated landscaping and infrastructure.
<b>Applicants:</b>	Skywalk Adventure
<b>Agent:</b>	Nexus Planning

## 5. SITE & SURROUNDINGS

5.1. The application site (0.28ha) is situated within the North Park area of Queen Elizabeth Olympic Park (QEOP) (within the boroughs of Hackney and Newham), located between two existing pedestrian footbridges that span the River Lea between Planning Delivery Zones (PDZs) 5 and 6 in the core wetland area of the QEOP.

- 5.2. The site runs parallel with the Eastcross Bridge. The majority of the site is contained on the east of the River Lea, and comprises a small parcel of undeveloped grassland to the west of the River Lea.
- 5.3. The Timber Lodge Café is situated to the north-east of the site (approximately 80m from the eastern side of the application site), which is a single storey multi-functional Park Hub building containing a café and flexible space, with a reception area and WCs.
- 5.4. The proposals are within close proximity of the phased East Wick development, which was consented as part of the Legacy Communities Scheme (LCS), with phase 1 scheduled to be delivered between 2020 and 2021.
- 5.5. The Copper Box Arena is also situated approximately 160 metres to the south-west of the site. Existing neighbourhoods, known as Chobham Manor and East Village are situated approximately 100 metres to the east of the easternmost edge of the site.
- 5.6. The application site is within Metropolitan Open Land (MOL) and Sites of Importance for Nature Conservation (SINC) designations as identified within the Local Plan.
- 5.7. The site is also situated within the Opportunity Area, Stratford as identified in The London Plan (2016).
- 5.8. The Environment Agency Flood Zone Maps demonstrate that land either side of the River Lea is classed as Flood Zone 3.

## **6. APPLICATION PROPOSAL**

- 6.1. The application seeks planning permission for the construction of a high ropes adventure course, with two single storey buildings containing a reception, store and accessible W.C comprising a total floor area of 89sqm, along with associated landscape and infrastructure works.
- 6.2. The course would consist of a series of columns connected by various rope-based activities. Steel wires would also be provided between the columns to enable users to be safely attached onto the course.
- 6.3. A total of 23 structural columns are proposed across the site, with platforms provided on each. The columns would vary in height, the tallest being 14.8 metres from ground level and the smallest being 6.8 metres (refer to Appendix 5 for proposed site elevations). The platforms would be a minimum of 3 metres above ground and the area around the columns at ground level would remain open and accessible at all times.
- 6.4. Access to and egress from the course would be provided from within the proposed single storey buildings. The space in between the two structures would be used as a safety briefing area, and gates are proposed to close this area off during operational hours. Outside operational hours, the structures would be closed off and the space between opened up.
- 6.5. The course would start at the eastern end of the site, with rope connections linking the columns up to the river bank. At this point, a zip wire is provided across the River Lea onto a platform on the western river bank. Another zip wire allows access back to the eastern side of the river, and further rope connections provide a route back to the finish structure at the eastern end of the course.
- 6.6. For the proposed site plan, please refer to appendix 2 and for floor plans and elevational drawings refer to appendix 3 and 4.

## **7. RELEVANT PLANNING HISTORY**

- 7.1. The 2007 applications for the Site Preparation works (ref. 07/90011/FUMODA) and for the Olympic and Paralympic facilities and their Legacy Transformation (known as the OLF permission ref. 07/90010/OUMODA) were granted in September 2007 with an associated s.106 agreement.
- 7.2. 08/90287/REMODA: Reserved Matters pursuant to Condition OD.0.19 (submission of details for bridges) and Condition OD.0.59 (foundation details) of Olympic and Legacy Facilities Planning Permission Ref: 07/90010/OUMODA were approved on 13th January 2009 for the construction of Bridges F02 and F03 (Eastcross Bridge). This application granted approval for the permanent bridge structures and the temporary bridge abutment foundations, which included temporary piles and pile caps.
- 7.3. 12/90239/FUMODA: Full Planning Permission was granted on 14<sup>th</sup> August 2012 for the construction of a single storey multi-functional North Park Hub building (now known as Timber Lodge), landscape works comprising ecological themed Parklands incorporating a Neighbourhood Play Space and associated hardstanding.

## 8. POLICIES & GUIDANCE

### 8.1. National Planning Policy Framework (July 2018)

- 8.2. The National Planning Policy Framework sets out national planning policy and is a material consideration in planning decisions. It sets out a presumption in favour of sustainable development which plans and decisions should apply. This requires that in order to achieve this, development proposals that accord with an up to date development plan should be approved without delay. Where there are no relevant development plan policies or the policies that are most relevant are out of date, permission should be granted unless the application of policies within the NPPF that protect areas or assets of particular importance provides a clear reason for refusing development; or any adverse impacts of granting permission would significantly or demonstrably outweigh the benefits, when assessed against the policies in the NPPF as a whole. The NPPF should be read in conjunction with the Planning Practice Guidance, a web-based resource for all users of the planning system. This set out detailed guidance in support of the policy areas in the NPPF, including the importance of good design and how this can be achieved through planning decisions.
- 8.3. The following sections of the NPPF are considered relevant to this planning application:
  - Section 2: Achieving sustainable development
  - Section 6: Building a strong, competitive economy
  - Section 8: Promoting healthy and safe communities
  - Section 9: Promoting sustainable transport
  - Section 11: Making effective use of land
  - Section 12: Achieving well designed places
  - Section 15: Conserving and enhancing the natural environment
- 8.4. The adopted 'Development Plan' is the London Legacy Development Corporation Local Plan 2015 – 2031 (July 2015) and The London Plan (2016).

- 8.5. The most relevant policies are listed below:

#### The London Plan (March 2016)

Policy 2.4                      The 2012 Games and their legacy

Policy 2.9	Inner London
Policy 2.14	Areas for regeneration
Policy 4.1	Developing London's economy
Policy 6.1	Car Parking
Policy 6.3	Assessing the effects of development on transport capacity
Policy 6.9	Cycling
Policy 6.10	Walking
Policy 6.13	Parking
Policy 7.2	An inclusive environment
Policy 7.17	Metropolitan Open Land

London Legacy Development Corporation Local Plan (July 2015)

SD.1	Sustainable development
SP1	Building a strong and diverse economy
B.2:	Thriving town, neighbourhood and local centres
BN.1	Responding to place
BN.2	Creating distinctive waterway environments
BN.3	Maximising biodiversity
BN.5	Requiring inclusive design
BN.6	Protecting Metropolitan Open Land
BN.8	Maximising opportunities for play
BN.9	Protecting key views
BN.11	Reducing noise and improving air quality
T.4	Managing developments and its transport impacts
T.8	Parking and parking standards in new development
T.9	Providing for pedestrians and cyclists
SP.5	A sustainable and healthy place to live and work
S.1	Health and wellbeing
S.4	Sustainable design and construction
S.5:	Water supply and waste water disposal
S.6	Waste reduction
S.8	Flood risk and sustainable drainage measures

Other material considerations:

Mayor of London – Olympic Legacy SPG (2012)

LLDC's Park Management Plan (adopted January 2017)

The Draft London Plan (December 2017)

- 8.6. The Mayor of London published, for the purpose of public consultation, a draft new London Plan on 29th November 2017. The policies in the draft new London Plan

currently have only very limited material weight when making planning decisions. That weight will increase once the new Plan is submitted for its Examination in Public. This report may make reference to policies within the new London Plan where they are directly relevant to the assessment of the application proposal. However, the relevant development plan policies remain those within the current London Plan (March 2016) and the LLDC Local Plan (July 2015).

8.7. Relevant policies include:

SD1	Opportunity Areas
D2	Delivering good design
D3	Inclusive design
G1	Green Infrastructure
G3	Metropolitan Open Land
G6	Biodiversity and access to nature
SI12	Flood risk management
SI14	Waterways – strategic role
SI17	Protecting London’s waterways
T5	Cycling
T6	Car Parking

## 9. PUBLIC CONSULTATION

### Statement of Community Involvement

- 9.1. A Statement of Community Involvement (SCI) has been submitted in support of the application. The SCI outlines the pre-application discussions, and public and stakeholder engagement undertaken in advance of the submission of the planning application by the applicant.

#### Pre-application discussions with the London legacy Development Corporation

- 9.2. Preapplication discussions took place with LLDC, to advise on the proposal, including what the key policy considerations and planning issues would be, and as to the requirements of the application submission.

#### Public Exhibition Events

- 9.3. The applicant carried out two public exhibition events which were held at Timber Lodge Café on 21<sup>st</sup> March 2018 and 14<sup>th</sup> April 2018, where boards were provided outlining the proposed high ropes course. The events were advertised by way of posters provided on Queen Elizabeth Olympic Park and Chobham Life website, and on the Timber Lodge Café notice boards. A comments box was provided at the event to enable attendees to provide feedback on the proposals. The applicant has informed that the comments and general feedback received was very positive. Examples of comments received included:

- “Great scheme! Would love more leisure by the river”;
- “Good to see ‘development’ which isn’t going to swallow up more of the park with new buildings”; and
- “This idea is very creative – for adults and children being able to go up will build togetherness. It’s very local for people in E20. Having birthday parties here is astonishing. It will still keep the greenery with using materials like wood.”

#### Consultation with other organisations

9.4. The applicant has carried out consultations with local organisation consisting of meetings to present the scheme to stakeholders and workshops that encourage stakeholders to input into the design of the scheme. The organisations the applicant has consulted include:

- Get Living London and East Village Management;
- Lee and Stort Boat Tours;
- Legacy Youth Voice;
- Here East;
- Canal and River Trust;
- Environment Agency;
- Metropolitan Police;
- LLDC Park Manager;
- MACE Construction

9.5. A summary of key response provided below:

Get Living London and East Village Management

9.6. Supportive of the scheme and would like to discuss potential co-marketing opportunities/wayfinding.

Lee Stort Boat Tours

9.7. Support the scheme and would also like to work with the applicant, Skywalk Adventure.

Legacy Youth Voice

9.8. Would like to know more about the cost and offers for young people. Some of the Legacy Youth Voice wanted to work on the course and other provided a number of visual ideas for the course elements which some have been incorporated in to the course design.

Here East

9.9. Support the principle of the proposal and the design, however would like to understand more around the expected visitor numbers, volume and parking arrangements and whether this will impact Parkes Street.

9.10. *Officers response: Submission of an Event Management Plan has been recommended, which would provide further details on this.*

Canal River Trust (CRT)

9.11. CRT were consulted and confirmed that the proposed development is outside of their statutory consultee area and therefore did not need to comment.

Environment Agency (EA)

9.12. Confirmed they have no major concerns.

Metropolitan Police

9.13. The Metropolitan Police provided comments in order for the development to be secured by design, which the scheme has positively responded to.

**Consultation**

9.14. Details of responses received from consultees are set out in the table and paragraphs below:

## External Consultees

<b>Consultee</b>	<b>Response</b>
<b>London Borough of Newham Planning</b>	No comments received
<b>London Borough Hackney Planning</b>	No objection
<b>London Borough Tower Hamlets Planning</b>	No comments received
<b>Lee Valley Regional Park Authority</b>	No comments received
<b>Environment Agency (EA)</b>	No comments received
<b>Natural England</b>	No comments received
<b>London Wildlife Trust</b>	No comments received
<b>Sport England</b>	No comments received
<b>Canal River Trust (CRT)</b>	No comments received
<b>Lea Rivers Trust</b>	No comments received
<b>Thames Water</b>	No comments received
<b>Greater London Archaeology Advisory Service (GLAAS)</b>	No comments received
<b>Transport for London (TfL)</b>	No comments received
<b>London Cycling Campaign</b>	No comments received
<b>Sustrans</b>	No comments received
<b>Metropolitan Police</b>	No comments received
<b>London Fire and Emergency Planning Authority (LFEPA)</b>	No comments received
<b>Greater London Authority (GLA)</b>	No comments received
<b>LLDC Inclusive Design</b>	No comments received
<b>LLDC Sustainability</b>	No comments received

9.15. The application was advertised in the Hackney Today newspaper on 23<sup>rd</sup> July 2018, and a site notice was placed near to the application site on 18<sup>th</sup> July 2018.

9.16. One letter of objection has been received (address of objector not provided). Their objection is summarised below:

- The tree structures detract from the beauty of the natural trees; and

- Dominance of the skyline by the course would adversely impact upon the peace felt by park users who currently cycle and walk along the Eastcross Bridge.

9.17. Officer response: The issues raised are considered in the assessment section of the report.

#### Internal Consultees

##### **PPDT's Environmental Consultant (Arup)**

9.18. PPDT's Environmental Consultants have reviewed the submitted Noise Assessment, Flood Risk Assessment (FRA), Preliminary Ecological Appraisal and Sustainability Statement and confirm that they are all acceptable.

##### **PPDT's Transport Consultant (CH2M)**

9.19. PPDT's Transport Consultants have undertaken a review of the submitted Transport Statement.

9.20. Consultants advised that further details should be provided about the management of construction, with reference to its impacts on QEOP. Officers recommend a condition (see condition 5) is imposed securing the submission of a Construction Management Plan prior to the commencement of the development to the Local Planning Authority for approval, which would adequately address these matters.

9.21. Consultants also recommended that post construction further details are provided regarding staff and customer Travel Plan. Officers recommend a condition (see condition 9) is imposed securing the submission of a Travel Plan prior to the operation of the proposed development.

9.22. With regard to cycle parking, consultants raised concern over the principle of utilising cycle provision at Timber Lodge. Timber Lodge has a surplus of 23 cycle spaces (above policy requirements) and the applicant seeks to utilise this surplus to off-set the 2 cycle spaces which would be needed to meet London Plan policy for the proposed scheme. As detailed in the assessment section of the report, the existing spaces are considered to be underused and Officers therefore consider the principle of this arrangement to be acceptable in this instance, particularly given that it would avoid any unnecessary clutter of the public realm at this sensitive location.

##### **Quality Review Panel (QRP)**

9.23. The proposed scheme was presented to the QRP on 31<sup>st</sup> May 2018. The summary of their response is as follows (refer to Appendix 8 for the full report):

- "The attraction could contribute positively to the visual interest of the park";
- "The choice of cumaru timber as the primary material is considered appropriate";
- "The Panel is also pleased that the ground plane beneath the course - an attractive wild flower meadow - will be retained and accessible to the public throughout operation of the attraction";
- "The Panel commends the building's design - its division into two parts is an ingenious solution"; and
- "The form of the uprights has the potential to be elegant".

##### **Built Environment Access Panel (BEAP)**

9.24. The proposed scheme has been presented to the BEAP. Their comments are summarised below:

#### General design



- Installation of reasonable adaptations were encouraged to accommodate a range of individuals with disabilities to utilise the course;
- overtaking points were welcomed on the course;
- seating with arm rests is welcomed in areas where there could be prolonged waiting;
- building should be fully accessible for disabled staff and customers;
- temporary/ retractable canopy for inclement weather should be considered;
- no sharp edges; and
- 1 DDA accessible toilet should be considered.

9.25. Officer response: *The scheme has responded positively to the recommendation made at BEAP. There would be no restrictions to inclusive access and would be available at all time of course operation. The scheme incorporates a hoist and lowering system known as a Niko Rail system, which facilitates access to and from the course. The Niko Rail system allows for the customers to manoeuvre to the zip starting point and safe transition to the zip wire. The scheme has been designed to accommodate a buddy/chaperone to instructor to zip over the customers on a tandem basis.*

9.26. *Overtaking points have been included in response to recommendations from BEAP. These are included on a number of trees on the basis that someone who is slower does not feel pressured by holding others up.*

9.27. *The building is fully accessible for disabled staff and customers and a DDA compliant toilet has been incorporated. Three DDA compliant toilets are also available at the Timber Lodge.*

9.28. *A canopy in the reception area has also been designed into the scheme in response to BEAP.*

#### Hearing impairment

- allow for hearing loops at ticket desk and in briefing area; and
- have available written safety instructions.

9.29. Officer response: *A hearing loop at the ticket desk/reception and briefing area have been incorporated, along with written safety instructions.*

#### Visual impairment

- ensure some colour delineation on footings and areas where heads might bang against apparatus;
- demonstrate you have considered a secure area where guide dogs can be left; and
- consider audio descriptive technology on the course.

9.30. Officer response: *In response to recommendation made by BEAP, the scheme has been amended to aid visually impaired customers. Colour delineation on footings and elements of the course have been included.*

*As advised, an area for guide dogs has been incorporated into the scheme along with storage facilities.*

#### Physical disabilities

- ensure there are areas where wheelchairs and mobility aids can be stored; and
- demonstrate if someone with a severe physical disability can use the experience by considering:
  - a buddy system;

- being attached to the harness;
- possibility of being able to pull yourself around; and
- people accessing the zip.

9.31. Officer response: *As explained in paragraph 9.16, the scheme has incorporated facilities to ensure the less able bodied can experience the course through the installation of apparatus as listed above.*

#### Mental health

- consider what support Skywalk Adventures can provide;
- how information is relayed;
- allowance for carers;
- avoidance of queues; and
- integrated and dedicated sessions.

9.32. Officer response: Skywalk Adventures (applicant), has informed that customers with mental health issues are encouraged to engage with staff and dedicated sessions can be arranged.

#### **Planning Decisions Committee Briefing**

9.33. The scheme was presented to Members on 22 May 2018 and the following points were made:

- The entrance/ exit structure could look more exciting when closed;
- Better illustrations of how the entrance/exit structure would look would be helpful;
- Thought should be given to how the facility would be managed on match/event days; and
- How would the attraction work for spectators?

9.34. The matters raised have been addressed by the applicant and are discussed within the assessment section of this report.

## **10. ASSESSMENT OF PLANNING ISSUES**

### **Principle of Development**

10.1. The application site is situated within Sub Area 2 as designated by the LLDC Local Plan, which identifies the location suitable for open space, sports and leisure activities. Objective 1 and Policy SP.1 of the Local Plan, inter alia, seeks to promote QEOP as a visitor and tourist destination. Furthermore, Policy 2.4 of the London Plan 2016 and SD1 of the Draft London Plan 2017 promotes the QEOP, its venues and surrounding attractors as an international visitor designation for sport, recreation and tourism.

10.2. Policy BN.2 of the Local Plan also seeks to optimise the functions and enhance the local distinctiveness of waterway environments by encouraging development proposals that create opportunities for recreational activities along the waterway, providing the movement of passengers and freight along the waterways are not disrupted.

10.3. The NPPF provides guidance which promotes healthy communities. Officers consider that the proposal to be well aligned with the NPPF's guidance that planning decisions should aim to achieve places which enable and support health and well-being needs through the provision of sports facilities.

- 10.4. The North Park is primarily intended to be a quieter landscape, for existing and future residents. However, the proposed use, given its scale and intensity, is not considered to be inappropriate to this emerging area, and would contribute to the delivery of Objective 1 and Policy SP.1 of the Local Plan and the vision for Sub Area 2 by drawing visitors to explore the North Park.
- 10.5. Furthermore, the LLDC's Park Management Plan (adopted January 2017) sets out the vision for the Park, in particular it notes that the Park should provide –
- A highly active network of waterways, parks and public spaces, combining natural heritage with active programming for sporting, cultural and other events, and
  - Visitor attractions to form the basis of a growing visitor economy and to make the Olympic Park a compelling and popular destination.
- 10.6. The principle of the development in the proposed location is also considered to accord with policies in the London Plan, particularly Policy 2.4 (2012 Games and their legacy), which seeks to promote the QEOP as an international visitor designation and Policy BN.2 'Creating distinctive waterway environments' of Local Plan.

#### Metropolitan Open Land (MOL)

- 10.7. The application site is situated within MOL and this is a primary consideration in the determination of this application. Policy BN.6 of the Local Plan states that development should preserve the openness of MOL. This is further emphasised in Policy 7.17 of The London Plan, which gives the same level of protection as in the Green Belt. The policy guidance of paragraphs 133-147 of the NPPF on Green Belts apply equally to MOL.
- 10.8. Paragraph 145 of the NPPF states that a local planning authority should have regard to the construction of new buildings as inappropriate in the Green Belt, however it also outlines an exception toward the provision of appropriate facilities for outdoor sport and outdoor recreation providing the facility preserves the openness of the Green Belt, and does not conflict with the purpose of including land within it. Officers consider the proposed use to fall within the exception outlined with the NPPF; as it would provide recreation. The matter of openness is discussed below. In this policy context, consideration relates to the start/finish kiosks and related structures necessary to support the proposed outdoor recreation use and whether the openness of the MOL would be adversely affected.
- 10.9. With regard to the single storey buildings, these would comprise a total floor area of 89 square metres, which in the context of the wider site (0.28ha/2,800 sqm) is considered to be modest in scale. The buildings have been appropriately minimised and represent ancillary structures, supporting the proposed outdoor recreational facility.
- 10.10. The 23 columns proposed across the site would vary in height. The tallest column would measure 14.8 metres from ground level and the lowest, 6.8 metres. The columns have been designed to appear 'tree-like' with vertical timber slats to create visual permeability and minimising perceived density. The area around the columns would remain open and accessible at all times.
- 10.11. In summary, it is considered that the proposed development would not harm the openness of the MOL and does not conflict with the purpose of including land within it, in accordance with Policies BN.6 of the Local Plan, 7.17 of The London Plan and the NPPF. The principle of the development is therefore acceptable.
- 10.12. The design and appearance of the overall proposal has been assessed in more detail under the relevant headers below.

#### **Design**

- 10.13. Strategic Policy SP.3 of the Local Plan relates to how development should integrate with the natural, built and historic environment. It states that LLDC will create a high-quality built and natural environment, by ensuring development contributes to place making, enhances its surroundings, maintains and promotes local distinctiveness, supports delivery of the priorities for the various sub-areas and respects LLDC's Design Quality Policy.
- 10.14. The proposed scheme has been subject to a Quality Review Panel on the 31<sup>st</sup> May 2018 (QRP Report is provided in Appendix 8).

#### Height, scale and massing

- 10.15. The proposed development has been designed to be sympathetic to the surrounding context and to relate well to the character of the North Park. The development has been designed to appear subservient to the adjacent Eastcross Bridge and the Timber Lodge Café, which is situated in close proximity. The single storey buildings containing the reception, store and accessible WC would have a combined floor area of 89 square metres, which is considered to be minor in scale, helped by splitting the required floor area into two structures to minimise the perception of massing and allowing the retention of views through QEOP. QRP... 'commends the building's design – its division into two parts is an ingenious solution'.
- 10.16. The structures would be clad in vertical cumaru timber slats that would enable visual permeability, contributing to reducing the perceived massing of the buildings.
- 10.17. The course would comprise a total of 23 tree-like structure columns situated across the site ranging between 14.8 metres and 6.8 metres (refer to Appendix 5 for proposed site elevations), with standing/landing platforms provided on each. The column structures are connected by way of different 'elements' that range in complexity for customer enjoyment, with the key part of the proposal being the two zip lines that would sail across the River Lea.
- 10.18. The tree-like structures have been designed to appear as an attractive feature of the North Park, whilst trying to appear minimal and blend in to the setting of the site. To achieve this, the tree structures would be clad in cumaru timber vertical battens to create visual permeability and minimise perceived density. QRP considered the form of the uprights to have, 'the potential to be elegant'. The vertical slats around the columns would increase in both numbers and density as the structures get higher, and the top would form a wider opening reminiscent of a tree canopy. The tree structures are proposed to have different types of pole structures to represent the various growth stages of the trees and provide visual variation across the site and the skyline (refer to Appendix 5 and 6 for further details on proposed column types and site elevation). The design of the platforms has been influenced by the 'History Trees' located around the QEOP by comprising a blackened steel ring around the platform (refer to Appendix 6 for details of the columns).
- 10.19. By request of QRP, the applicant provided a prototype of the uprights of the column structures. Design Officers have reviewed the prototype and offer their support, however recommend the submission of further detailed drawings pertaining to key elements of the columns to ensure the design quality aspirations of the scheme are delivered. In light of this, Officer recommend a planning condition requiring the submission of these details to the Local Planning Authority for approval.

#### Views

- 10.20. Figure 17 of the Local Plan identifies vistas, sightlines and viewing corridors that warrant consideration when determining development proposals. Policy BN.9 of the Local Plan states that proposals for development that impact a key view will only be considered acceptable when the development makes a positive contribution to the characteristics and composition.

- 10.21. In support of the application, analysis that evaluates and justifies the visual impact upon that view has been provided.
- 10.22. The Local Plan identifies a key view corridor which runs along the Lea Navigation towards the Lee Valley VeloPark to the north. Viewing along this corridor (approximately 200 metres south of the application site toward the north to the VeloPark) it is considered that only the structures proposed adjacent to the river alongside the zip wire would be visible from this location. These visible elements are considered to have been designed in a sympathetic way to the surrounding context and would respect the character of the North Park, making a positive contribution in accordance with Policy BN.9 of the Local Plan. Officers consider the key view through QEOP to be retained and the proposed development would appear subservient against the backdrop and immediate surrounding and built forms, such as the Eastcross Bridge and Timber Lodge.
- 10.23. Officers consider the massing to be acceptable and that it would not impact on the amenity of existing or proposed developments; nor would it have an adverse impact on the character and setting of North Park.

#### Materials

- 10.24. Cumaru timber is the proposed primary cladding material, which has been selected to be in-keeping with other developments within the QEOP and would be sustainably sourced. This material would be used in the single storey buildings comprising the reception, store and accessible WC and applied to the steel columns. QRP support the choice of material.
- 10.25. Rope and Cumaru would be used to form the connection between the columns, and steel wires would also be provided between the columns to enable users to be safely attached onto the course.
- 10.26. Officers consider the proposed materials to be acceptable, however to ensure high quality of design and detailing, it is recommended that a condition is imposed (see Condition 3) requiring the submission of material samples as detailed within the condition.

#### Landscaping

- 10.27. London Plan policy 7.5 requires the public realm to be secure, accessible, safe, and easy to understand and maintain and relate well to local context and incorporate the highest quality design. Policy BN.1 of the Local Plan requires careful consideration to be given to landscape and water, ensuring proposals relate well to the local area's defining natural and man-made landscape features, in particular the linear form of the waterways and parklands.
- 10.28. The proposed course would 'float' above the existing wildflower lawns and therefore would impede access beneath the course, maintaining free access. QRP are supportive that the ground plane beneath the course, an attractive wild flower meadow, would be retained and accessible to the public throughout operations of the attraction.
- 10.29. The proposed landscaping would include making good any damage caused as a result of the installation of the columns. Existing trees are to be maintained as part of the proposal, however the applicant has advised that should the removal of any smaller stock trees be required these would be relocated within the park. A planning condition has been recommended, requiring the applicant to submit details of any such trees that are to be removed and where they would be relocated.

#### Ecology/Biodiversity

- 10.30. The River Lee runs through the centre of the site, which is designated as a Site of Importance for Nature Conservation (SINC). It is important for its groupings of plants, fish, birds and invertebrates, and as a corridor for mobile species such as bats.

- 10.31. The NPPF and Local Plan Policy BN.3 seeks the protection and enhancement of biodiversity within open space, parks and built-up neighbourhoods. The policy requires developments (relevant to the proposed) to maximise opportunities to protect and enhance biodiversity; Integrated habitat and other measures that will support biodiversity; ensure measures are taken to conserve and promote SINC; retain trees and contribute to tree-planting. This is re-enforced by Policy 7.19 of The London Plan, which also requires developments to make positive contributions to the protection, enhancement, creation and management of biodiversity where possible.
- 10.32. The proposed development would retain existing trees on the site and would reinstate any areas of grassland that may be disturbed by the installation of the foundations for the tree columns with new species-rich meadow turf. Also, the use of peat free compost and species-rich meadow turf to make good any areas that are disrupted, would result in a net ecological gain through the proposed development. QRP welcomed the retention of the existing immature trees, however raised concerns that no thorough analysis of likely growth over the 20 years lifespan of the attraction had been completed. QRP advised that trees would need some pruning where they intersect with the proposed attraction's platform and lines. To ensure this is fully considered in the future, Officers recommend a planning condition requiring the applicant to submit an Arboricultural Report and details of any trees that may need to be removed (including where they would be relocated) to the Local Planning Authority for approval.
- 10.33. Whilst the proposed 23 columns would result in a total loss of 4.5sqm of wild meadow typology where the columns enter the ground, the proposal seeks to extend the existing north facing edge of wild meadow grassland by 21sqm in the same axis by converting part of the existing gold top bridge tarmac. Officers are satisfied that the targets set within the approved BAP would not be compromised. The proposal would offset the loss and result in a net gain of 16.5sqm, and has the added benefit of providing further security distance from the existing trees. The proposed structure for the staff and briefing area would be situated on existing hard standing, so would not affect BAP figures for the park.
- 10.34. PPDT Environmental Consultants have reviewed the Ecological Appraisal and consider it to be acceptable.
- 10.35. Officers do not consider the proposed development to have an adverse impact on the biodiversity value of the site or the SINC, compliant with Local Plan Policy BN.3, London Plan Policy 7.19 and Paragraph 118 of the NPPF.

#### Lighting

- 10.36. Lighting and signage is not proposed outside of the proposed start/finish buildings. Directional signage would be provided onto existing finger posts, but this is not considered to require planning permission or advertisement consent.
- 10.37. The proposed landscaping is considered to relate well to the wider landscaping of QEOP.

#### Inclusive Design

- 10.38. Policy BN.5 requires non-residential proposals to respond to the needs of all users and to provide accessible and inclusive environments.
- 10.39. The scheme was presented to BEAP and their comments have helped ensure that the proposed development would be accessible.
- 10.40. The course would provide inclusive access at all times during hours of operations, alongside specific time slots for group or individual participation. This was a concern also raised by QRP, who recommended that the operator make the accessible course available whenever the attraction is open.

- 10.41. The course would incorporate a hoist and lowering system, which would allow inclusive access to the course for all. Whilst on the course, overtaking points would be included on a number of columns, to allow someone who is slower in enjoying the course to not feel pressured by holding up others.
- 10.42. Furthermore, in response to BEAP advice, the design of the course has been modified to aid visually impaired customers by introducing colour delineation on footings/elements on the course. BEAP also recommended that an area for guide dogs be designed in to the scheme, which has been incorporated, as well as storage for mobility aids and wheelchairs.
- 10.43. The building would also have hearing loops at the ticket desk and in the briefing area, along with written safety instructions.
- 10.44. A DDA compliant toilet would also be incorporated, with three more DDA compliant toilets available at the Timber Lodge Café (located approximately 80 metres away from the application site).
- 10.45. In summary, Officers consider that the scheme would achieve a good standard of accessibility and meets the requirements of Policy BN.5 of the Local Plan.

### **Transport**

- 10.46. London Plan policy 6.3 'Assessing effects of development on transport capacity' states that development should not adversely affect safety on the transport network and that development effects on transport capacity should be fully assessed. The applicant has submitted a Transport Statement to support the planning application.

### Accessibility

- 10.47. Policy T.4 of the Local Plan requires development to promote sustainable transport choices and minimise reliance on the private car to ensure that the development of the legacy area is optimised.
- 10.48. The site has a public transport accessibility level (PTAL) rating of 2, although the easternmost end of the site is classed as PTAL 3 (where 1 is generally considered to be very poor and 6 is excellent). The site is well served by bus stops located on Olympic Park Avenue and Waterden Road. Stratford International Station is located approximately 450 metres south-east of the site, providing rail services on the Southeastern and Dockland Light Rail network. Stratford national rail station is approximately 1km south-east of the site, which is served by Greater Anglia, TfL Rail, London Overground and Underground. The Jubilee and Central Lines operate through this station. Hackney Wick Station is situated approximately 550 metres to the west of the site, which is served by the London Overground.
- 10.49. Given the development is well served by public transport, many visitors to the proposed facility are expected to arrive and depart via public transport and/or by walking in accordance with Policy T.4 of the Local Plan.
- 10.50. Officers are satisfied that the site is accessible and served well by sustainable modes of transport.
- 10.51. An events management plan condition is recommended (condition 8) which would, amongst other things, require details to be submitted for approval for the hours of operation of events and relationship with the timing of events at the London Stadium; as well as crowd management arrangements for visitors both arriving or leaving the site.

### Cycle Parking

- 10.52. Policy T.9 requires parking provision to meet or preferably exceed minimum standards set out in The London Plan, specifically Policy 6.13. The London Plan requires 1 cycle parking space per 8 staff members and 1 visitor space per 100 square metres. Provision should also be in a safe and secure location and integrate well with

the street network. The site is accessible by foot via the existing footpath network within the QEOP.

- 10.53. The application seeks approval for 89sqm of floorspace (kiosk buildings), with 8 full time employees. This generates a policy requirement of 2 cycle parking spaces.
- 10.54. The applicant has informed that an agreement is in place for the proposed development to share the existing cycle parking provision at the Timber Lodge Café, which is situated approximately 80 metres to the north-east of the site.
- 10.55. Timber Lodge has a floorspace of 483sqm with 10 full time employees, which generates a cycle parking requirement of 7 spaces in-line with Policy 6.13 of The London Plan. As part of the Timber Lodge permission, 20 cycle spaces were secured exceeding policy requirements however, 30 spaces have been provided for users of the Timber Lodge and other facilities within the park.
- 10.56. As such, there is a surplus of 23 cycle spaces for the wider park use which can also be utilised by the proposed High Ropes attraction. The applicant has also demonstrated that these spaces are currently being underused during peak times. As such, Officers consider the current provision to be sufficient and if more cycle spaces were to be installed, would clutter the public realm.
- 10.57. Officers consider this arrangement to be acceptable in this instance and would support the aspirations of Policy T.9 of the Local Plan.

#### Car Parking

- 10.58. No car parking is proposed as part of this development, however if staff and participants so choose they could use car parking facilities in and around the QEOP.
- 10.59. No blue badge spaces are to be provided as part of the scheme, however spaces are available on Olympic Park Avenue, approximately 100m east of the site. A mobility service is also available to those arriving through the South Park. PPDT Transport Consultants consider the proximity of the spaces to be acceptable and provide a reasonable justification for the scheme not to provide blue badge spaces.
- 10.60. Given the range of public transport connections available within walking and cycle distance of the site, the principle of no car parking provision is considered to be acceptable in this instance. The provision of no additional car parking would also encourage staff and participant to use sustainable transport modes and minimise reliance on the private car in accordance with Policy T.4 of the Local Plan, Policy 6 of the London Plan and Paragraph 103 of the NPPF (July 2018).
- 10.61. A planning condition is recommended requiring the applicant to monitor customer journeys and to submit these details to the Local Planning Authority for review on an annual basis.

#### Servicing and Delivery

- 10.62. The proposed scheme would utilise the existing QEOP refuse system. A service charge would be paid by the applicant, which would be secured through the lease agreement. Existing waste and recycling is available at Timber Lodge, which the proposed development would utilise. The amount of waste generated by the proposed scheme is considered to be minimal. As such, Officers consider this arrangement to be acceptable.
- 10.63. Overall, Officers are satisfied that the proposals meet London Plan policies 6.9, 6.10 and 6.13 and LLDC Local Plan policies T.4, T.8 and T.9 in proposing a development which supports sustainable transport.

#### **Flooding**

- 10.64. The applicant provided a Phase 1 Flood Risk Assessment (FRA) in support of the application. The assessment concludes that the site is not at risk in a 1 in 100 year



fluvial flooding event, as the River Lea flood waters are predicted to be confined in-channel.

- 10.65. The eastern 100m of the site is within Flood Zone 2 and as such fluvial flooding poses a risk to site users. The site is within an EA Flood Alert and as such site users can be evacuated prior to a flooding event.
- 10.66. PPDT's Environmental Consultants have reviewed the FRA and confirm that the proposed development is suitable within Flood Zones 2 and 3, providing the flood risk management measures are implemented and visitors are made aware of the evacuation plan. These submitted details are secured by condition 2 which lists the Flood Risk Assessment as an approved document.
- 10.67. Officers therefore consider the proposed development to be acceptable within a Flood Zone 2 and 3, in accordance with Policy S.8 of the Local Plan.

#### **Noise and Disturbance**

- 10.68. Policy BN.11 of the Local Plan requires developments to minimise the effect of noise on amenity.
- 10.69. In support of the application a Noise Assessment has been submitted, which assess the impact of the proposed development on amenity in the locality. The assessment focuses on the impact at the location of the residential developments at East Wick and Chobham Manor.
- 10.70. The assessment demonstrates that the noise level associated with the proposed development would be significantly lower than the pre-existent residual noise level. PPDT Environmental Consultants reviewed the noise assessment and confirm its acceptability.
- 10.71. With regard to opening hours, the facility would have varied operating hours depending on the time of year. Between 1<sup>st</sup> October to 31<sup>st</sup> March, the facility would be open to the public from 10:00 to 18:00, Monday to Saturday; and 10:00 to 17:00 on Sundays and Bank Holidays. Between 1<sup>st</sup> April to 31<sup>st</sup> September, the facility would be open to the public from 10:00 to 20:00, Monday to Saturday; and 10:00 to 18:00 on Sundays and Bank Holidays. The proposed hours maximise daylight hours and ensure the facility does not operate after dark. Officers consider these operating hours to be reasonable and would ensure the surrounding amenity is protected; and that noise and disturbance is minimised. A planning condition has been recommended securing these operating hours to the public.
- 10.72. It therefore concludes that the proposed development would have no unacceptable impact on the residential amenity of existing or future residents near the site with respect to noise, meeting the requirements of Policy BN.11 of the Local Plan.

#### **Sustainability**

- 10.73. In support of the application the applicant provided a Sustainability Statement, which demonstrates sustainability measures that would be incorporated into the proposed development. The statement includes details of bio-toilets, sustainably sourced building materials, ventilated and naturally illuminated internal floorspace and measures to encourage sustainable transport modes.
- 10.74. Policy S.4 'Sustainable design and construction' of the Local Plan requires major developments to take the following matters in to account:
  - Resource efficiency;
  - Carbon dioxide emissions reduction;
  - Natural heating and ventilation;
  - Utilisation of decentralised energy sources;
  - Living roofs; and

- Sustainable drainage systems.
- 10.75. Although the scheme being considered does not comprise 'Major Development' in accordance with the Town and Country Planning (Development Management Procedure) (England) Order 2015, the applicant has demonstrated consideration towards these matters in the design of the development.
- 10.76. Local Plan Policy S.5 encourages building-specific measures to reduce potable water demand and use. Policy S.6 requires proposals to contribute towards the reduction of waste during construction and once operational.
- 10.77. London Plan Policy 5.3 requires developments to demonstrate that sustainable design standards are integral to the proposals.
- 10.78. PPDT Environmental consultants have reviewed the Sustainability Statement and consider it to be acceptable, meeting the requirements of Policies S.4, S.5 and S.6 of the Local Plan; and Policy 5.3 of the London Plan where relevant.

## **11. HUMAN RIGHTS & EQUALITIES IMPLICATIONS**

- 11.1. Members should take account of the provisions of the Human Rights Act 1998 as they relate to the proposal and the conflicting interests of the applicants and any third party opposing the application in reaching their decision. The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report. In particular, Article 6 (1), of the European Convention on Human Rights (ECHR) in relation civil rights and a fair hearing; Article 8 of the ECHR in relation to the right to respect for private and family life and Article 1 Protocol 1 of the ECHR in relation to the protection of property have all been taken into account.
- 11.2. In addition, the Equality Act 2010 provides protection from discrimination in respect of certain protected characteristics namely: age, disability, gender reassignment, pregnancy and maternity, race, religion, or beliefs and sex and sexual orientation. It places the Local Planning Authority under a legal duty to have due regard to the advancement of equality in the exercise of its powers including planning powers. Officers have taken this into account in the assessment of the application and Members must be mindful of this duty when determining all planning applications. In particular Members must pay due regard to the need to:
- Eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Equality Act;
  - Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and
  - Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.
- 11.3. Officers are satisfied that the application material and their assessment has taken into account these issues as relevant in the Assessment section of this report. Officers consider that the effects of the proposal would not be so adverse as to cause harm and justify a refusal of consent or permission.

## **12. CONCLUSION**

- 12.1. The proposed scheme is considered to support the aspirations of the QEOP by promoting the park as a visitor and tourist destination in accordance with Local and Regional Policy.
- 12.2. Officers consider the design to be sympathetic to the site context and reflects the character of the North Park through its scale, massing and material selection. The development would optimise the function and enhance the local distinctiveness of the

waterway environment by creating opportunities for recreational activities along the waterway.

- 12.3. The proposed scheme is considered to represent an appropriate outdoor recreational use to be located within an MOL, meeting the exception outlined with the NPPF. The openness of the MOL is considered to be maintained. Furthermore, the proposals are not considered to have an adverse impact on the biodiversity value of the site or the SINC.
- 12.4. The proposals are not considered to have an adverse impact on the residential amenity of existing or future residents by way of noise or visual amenity.
- 12.5. As such, Officers consider the development to be in accordance with national, London and local plan policies and represent sustainable development.
- 12.6. It is therefore recommended that the Committee resolve to grant planning permission for the development proposed subject to the following conditions.

### **13. PLANNING CONDITIONS**

#### **1. Time limit**

The development shall be commenced before the expiration of three years from the date of the permission.

Reason: In accordance with Section 91 of the Town and Country Planning Act 1990 (as amended).

#### **2. Works in accordance with approved details**

The development shall be carried out and retained thereafter in accordance with the following drawings and documents:

- (Document titles and drawing references to be inserted ahead of issuing the decision notice)

Reason: To ensure that the development is undertaken in accordance with the approved drawings

#### **3. Material samples**

Prior to above ground works commencing associated with the development hereby permitted material samples of the following have been presented on site and approved in writing by the Local Planning Authority:

- Cumaru Timber;
- Blackened Steel;
- Pavilion 'timber' lining; and
- 'Tree' platforms.

The construction of the development shall be implemented and thereafter retained in accordance with the approved details.

Reason: In order to ensure that these samples will make an acceptable contextual response in terms of materials to be used and achieve a satisfactory quality of design and detailing.

#### **4. Detailed Drawings**

Prior to commencement of the relevant part of the development the following detailed drawings shall be submitted to and approved in writing by the Local Planning Authority.

Detailed drawings including sections (at 1:50 / 1:20) of:

- Bay study of façade/ cladding/ glazing system;
- Entrances complete with canopy;
- Principal features on all facades;
- Pavilion 'timber' lining;
- Tree-like Structures including columns, platforms, junctions/fixings, ties, cladding, 'branches', foundations, depth of soil/reinstatement around the base of the columns etc (the different type of structure needs to be clearly indicated on the location plan showing design and height/mass);
- Parapets / roof edges;
- Gates;
- External lighting;
- Rooflights; and
- Signage.

The development shall not be carried out and retained otherwise than in accordance with any such approval given.

Reason: In order to ensure that a high quality of design and detailing.

Pre-commencement justification: In order to ensure that a high quality of design and detailing.

#### **5. Construction Management Plan**

The development shall not commence until a Construction Management Plan (CMP) has been prepared in consultation with the Local Planning Authority, local highway authorities, and local environmental health teams and such CMP has been submitted to and approved in writing by the Local Planning Authority. The development shall be provided in accordance with the agreed CMP.

The Construction Management Plan shall include information on the following points:

- Delivery of construction materials including details of vehicle movements, traffic routes and how the site entrance would be managed during construction
- Details of a community liaison officer to be appointed and employed throughout the construction period
- Storage of materials
- Hours of construction
- Adherence to the Considerate Constructors Scheme

Pre-commencement justification: To ensure that the Local Planning Authority can assess whether construction of the development would generate any unacceptable environmental impacts upon neighbours that would require appropriate mitigation.

Reason: To ensure that the construction of the development minimises its impacts on local residents.

## **6. Tree Planting**

Prior to the first use of the development full details of all proposed tree planting and the proposed times of planting, shall have been submitted to approved in writing by the local planning authority, and all tree planting shall be carried out in accordance with those details and at those times. Planting shall comply to BS:4428 Code of practice for general landscaping operations.

A scheme for the protection of the retained trees by way of a Arboricultural Method Statement shall also be submitted to and approved in writing by the Local Planning Authority.

If within the lifetime of the development any tree planted or any tree planted in replacement for it, is removed, uprooted or destroyed or dies, [or becomes, in the opinion of the local planning authority, seriously damaged or defective,] another tree of the same species and size as that originally planted shall be planted at the same place, unless the local planning authority gives its written consent to any variation.

Reason: To ensure that the Local Planning Authority is satisfied as to the design and details of the landscaping in the interest of visual amenity and sustainability.

## **7. Hours of Operation**

The premises shall not be open for customers to use outside the following hours between 1st October – 31st March:

- i. 1000 – 1800 Mondays – Saturday
- ii. 1000 – 1700 Sundays and Bank Holidays

The premises shall not be open for customers to use outside the following hours between 1st April – 30th September:

- iii. 1000 – 2000 Monday – Saturday
- iv. 1000 – 1800 Sundays and Bank Holidays

The premises shall not be open for customers beyond the above times unless details of the proposed opening hours on a set date are submitted to and approved by the Local Planning Authority in writing in advance.

Reason: To minimise noise and disturbance to local residents.

## **8. Events Management Plan**

Prior to the opening of the facility to the general public or the occupation of the facility, whichever is first, an Events Management Plan shall be submitted to and approved in writing by the Local Planning Authority in consultation with Strategic and Local Transport Authorities and the Metropolitan Police.

The Events Management Plan shall include details of hours of operation, noise mitigation measures, the proposed arrangements for coordinating the use of the public realm of the site with other events in the area, the proposed set down and pick up of visitors including facilities for bus, coach and taxis and measures to manage spectator travel and the coordination and control of car parking when Events are taking place. A schedule of intended Events and other programmed activities taking place at the facility shall be submitted to the Local Planning Authority prior to any Events taking place. Events shall only be held in accordance with the approved Events Management Plan.

In addition to those topics covered above the Events Management Plan shall include details of the following:

- i. Date, type and scale of events;
- ii. Hours of operation of events and relationship with the timing of events at the London Stadium, such as football matches;

- iii. Predicted noise levels at sensitive receptors (such as the nearest residential properties) and details of noise mitigation measures including for plant and generating equipment. Noise levels from plant shall not exceed those likely to give rise to complaint as assessed under BS4142;
- iv. Details of any proposed licensing arrangements;
- v. The provision of set down and pick up points for visitors;
- vi. Crowd management arrangements for visitors both arriving or leaving the site, getting to public transport hubs including stewarding;
- vii. Queue control measures for the use and their impact upon pedestrian facilities.

Thereafter the approved Events Management Plan shall be implemented.

Reason: In the interests of public safety, and to minimise noise and disturbance to residents.

## **9. Travel Plan**

a) Before the development hereby permitted is first used the applicant shall submit in writing and obtain the written approval of the Local Planning Authority to a Travel Plan setting out the proposed measures to be taken to encourage the use of modes of transport other than the car by all users of the building, including staff and visitors. The approved Travel Plan shall thereafter be implemented.

b) At the start of the second year of operation of the approved Travel Plan a detailed survey showing the methods of transport used by all those users of the building to and from the site and how this compares with the proposed measures and any additional measures to be taken to encourage the use of public transport, walking and cycling to the site shall be submitted to and approved in writing by the Local Planning Authority and the development shall not be carried out otherwise in accordance with any such approval given.

Reason: In order to encourage sustainable means of travel.

## **APPENDICES**

Appendix 1: Site Location

Appendix 2: Proposed Site Plan

Appendix 3: Proposed Floor Plans of Kiosk Buildings

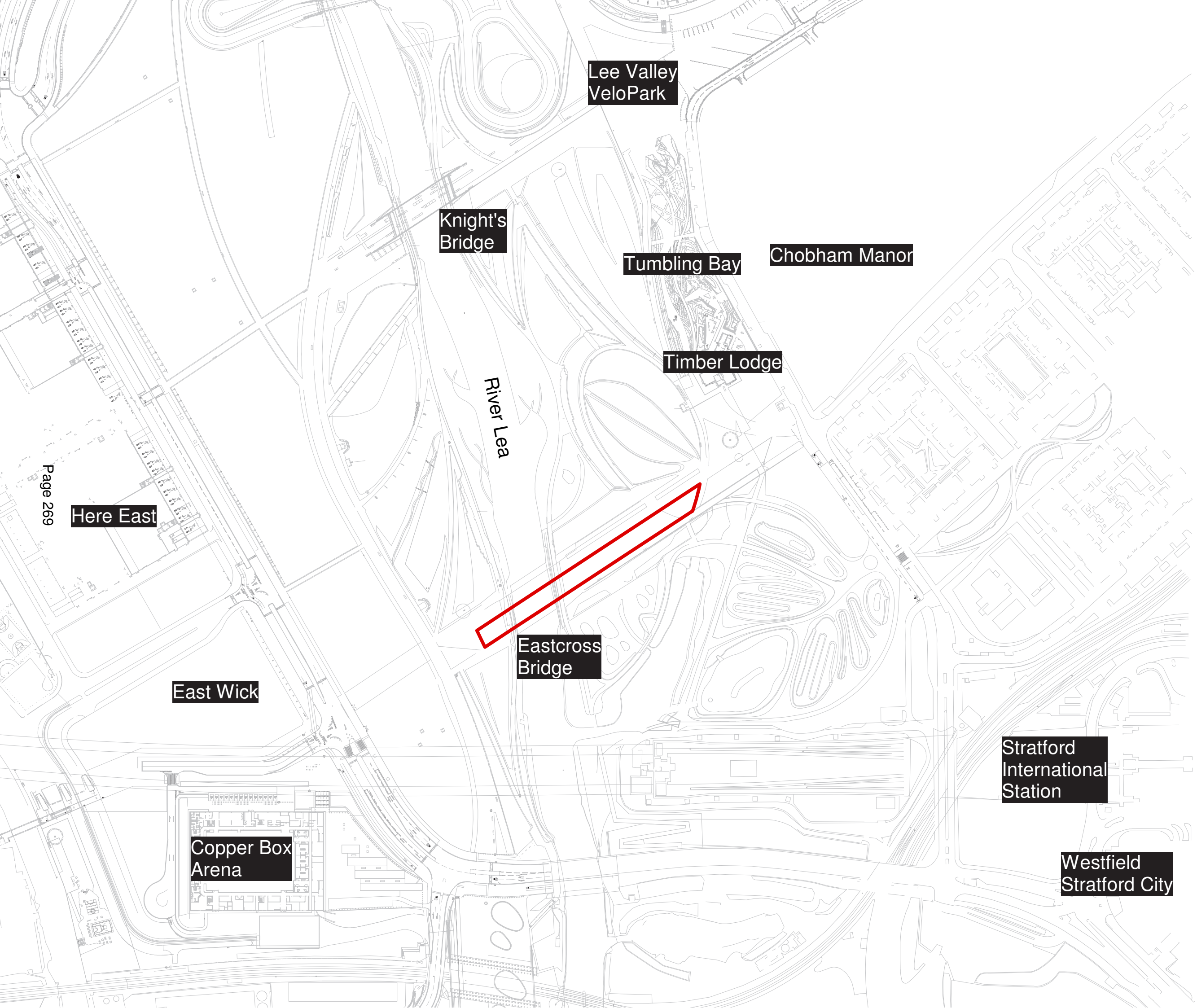
Appendix 4: Proposed Elevations and Sections of Kiosk Buildings

Appendix 5: Proposed Site Elevations

Appendix 6: Proposed Column Structure Types

Appendix 7: CGIs

Appendix 8: Quality Review Panel report



Lee Valley  
VeloPark

Knight's  
Bridge

Tumbling Bay

Chobham Manor

Timber Lodge

River Lea

Here East

Eastcross  
Bridge

East Wick

Stratford  
International  
Station

Copper Box  
Arena

Westfield  
Stratford City

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Skywalk Adventure Ltd  
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Surrey KT10 8AN

Scheme:  
Proposed High Ropes Adventure at  
Queen Elizabeth Olympic Park

# PLANNING

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Location Plan

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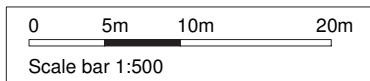


EASTCROSS BRIDGE

RIVER LEA

WETLANDS WALK

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Scheme:  
**Proposed High Ropes Adventure at  
Queen Elizabeth Olympic Park**

# PLANNING

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**Proposed Site Plan**

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**28/06/2018**

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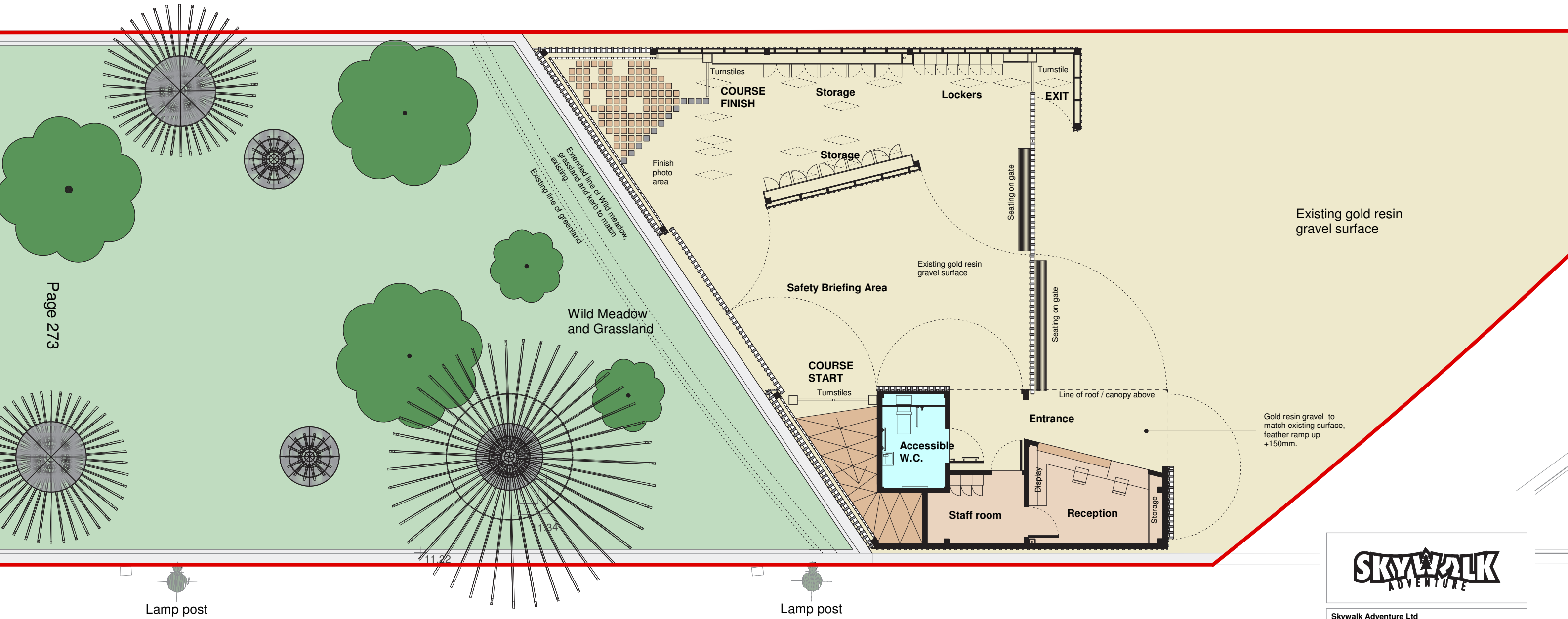
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Existing gold resin gravel surface

Gold resin gravel to match existing surface, feather ramp up +150mm.



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Scheme:  
**Proposed High Ropes Adventure at  
Queen Elizabeth Olympic Park**

# PLANNING

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**Proposed Ground Floor Plan**

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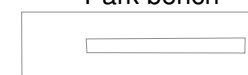
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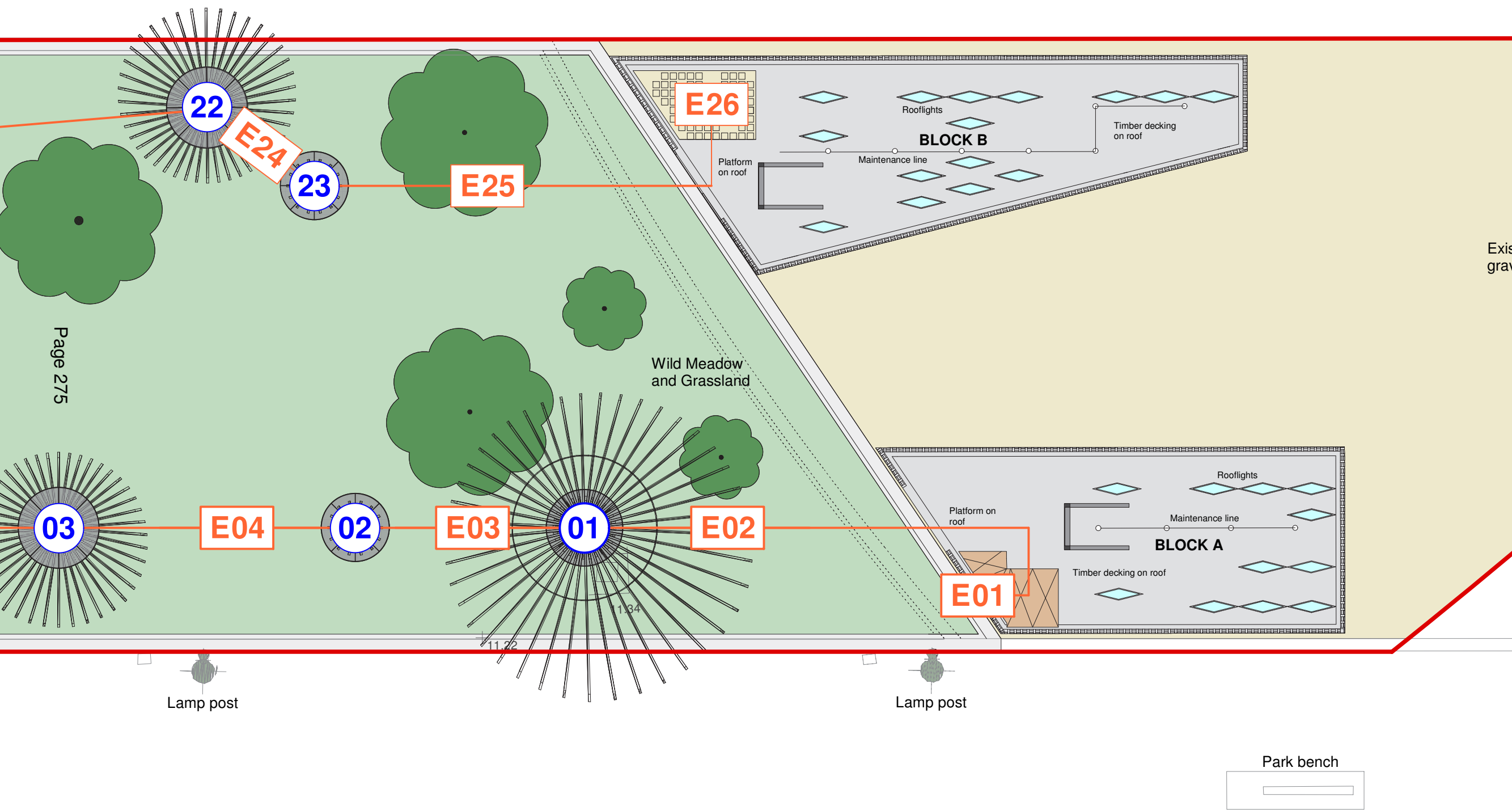
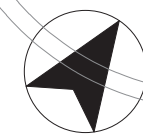
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Park bench

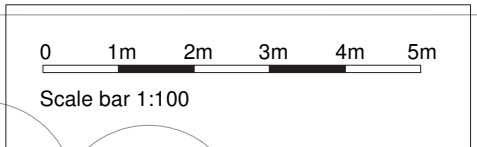



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Existing gold resin gravel surface

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Scheme:  
**Proposed High Ropes Adventure at  
Queen Elizabeth Olympic Park**

## PLANNING

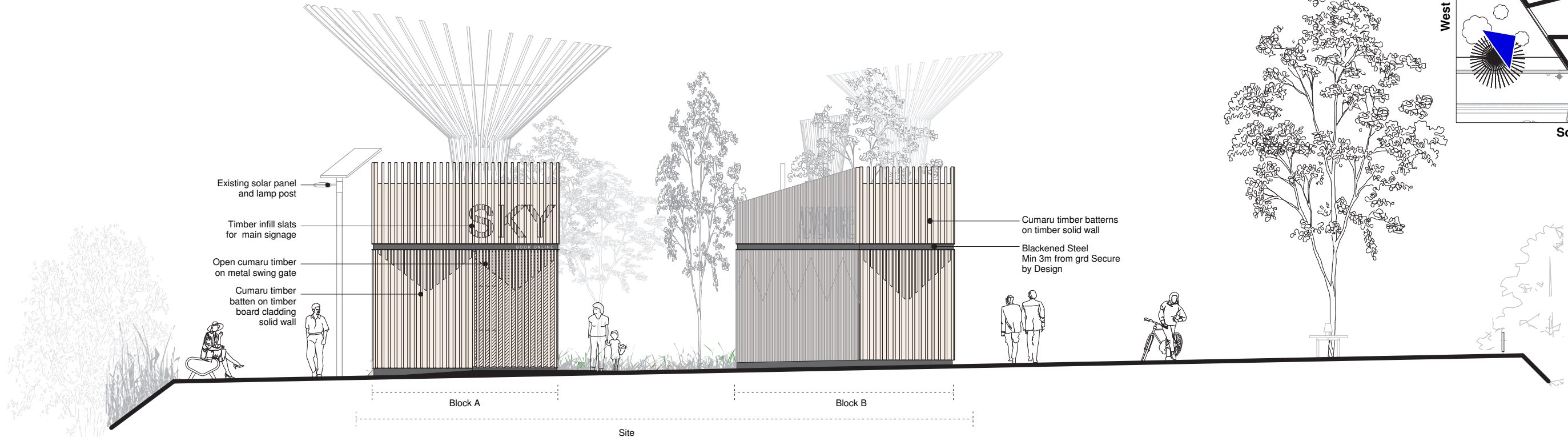
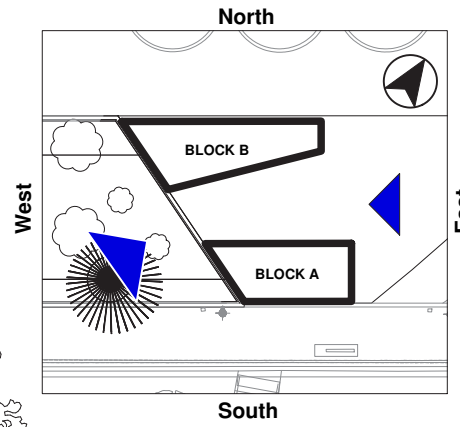
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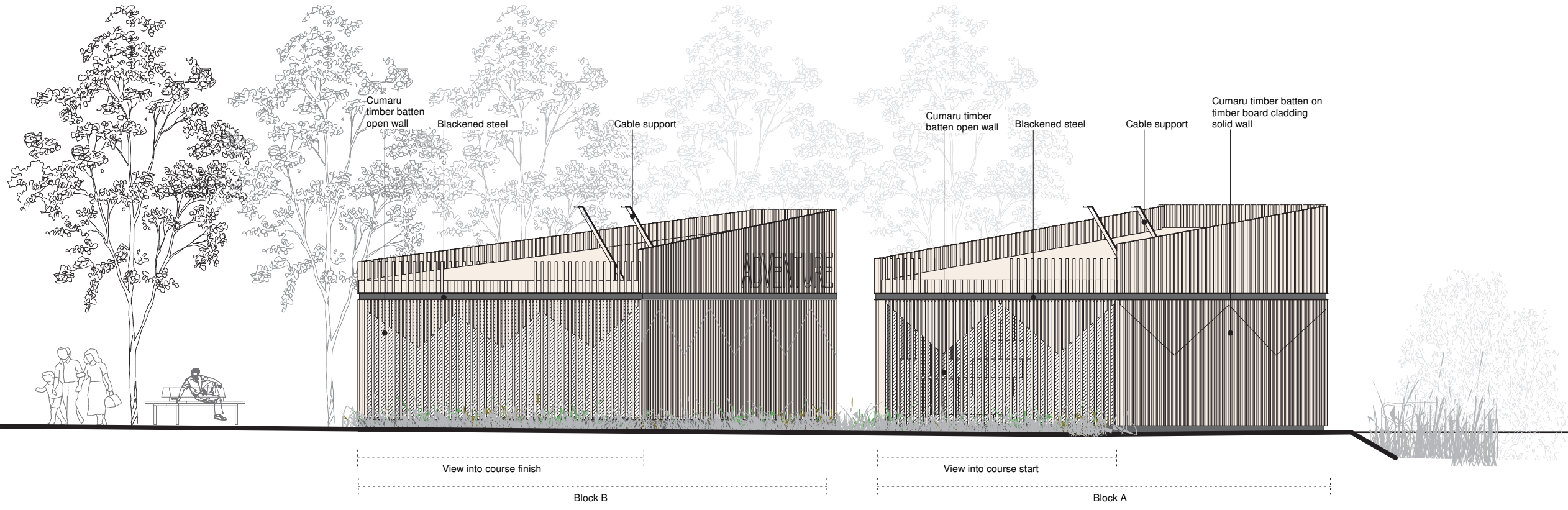
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BLOCK A & B - East Elevation

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BLOCK A & B - South West Elevation



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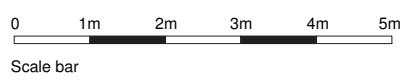
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Queen Elizabeth Olympic Park**

## PLANNING

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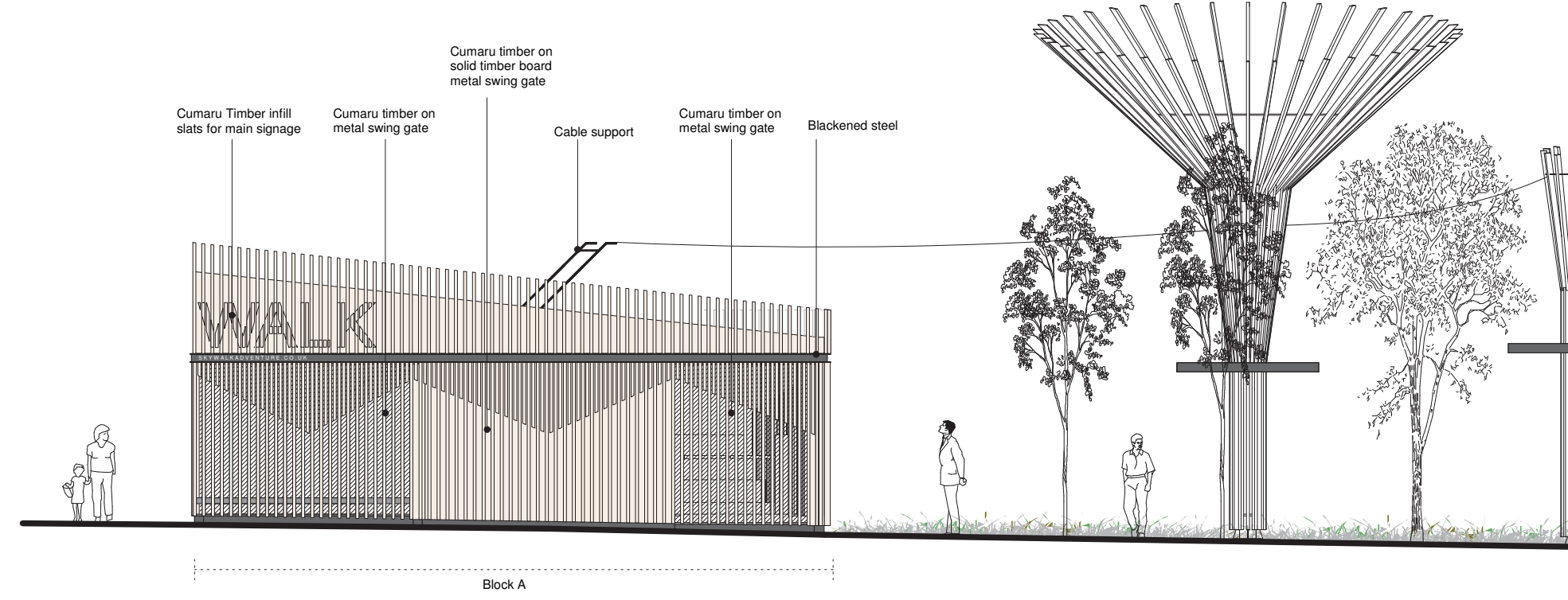
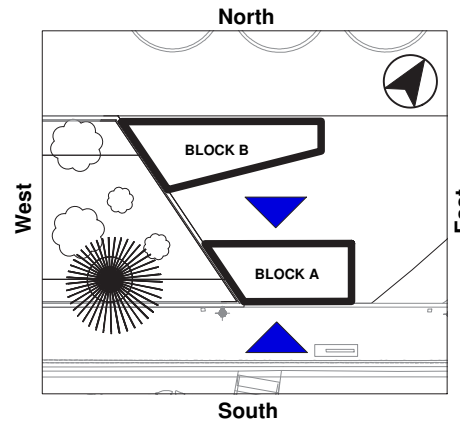
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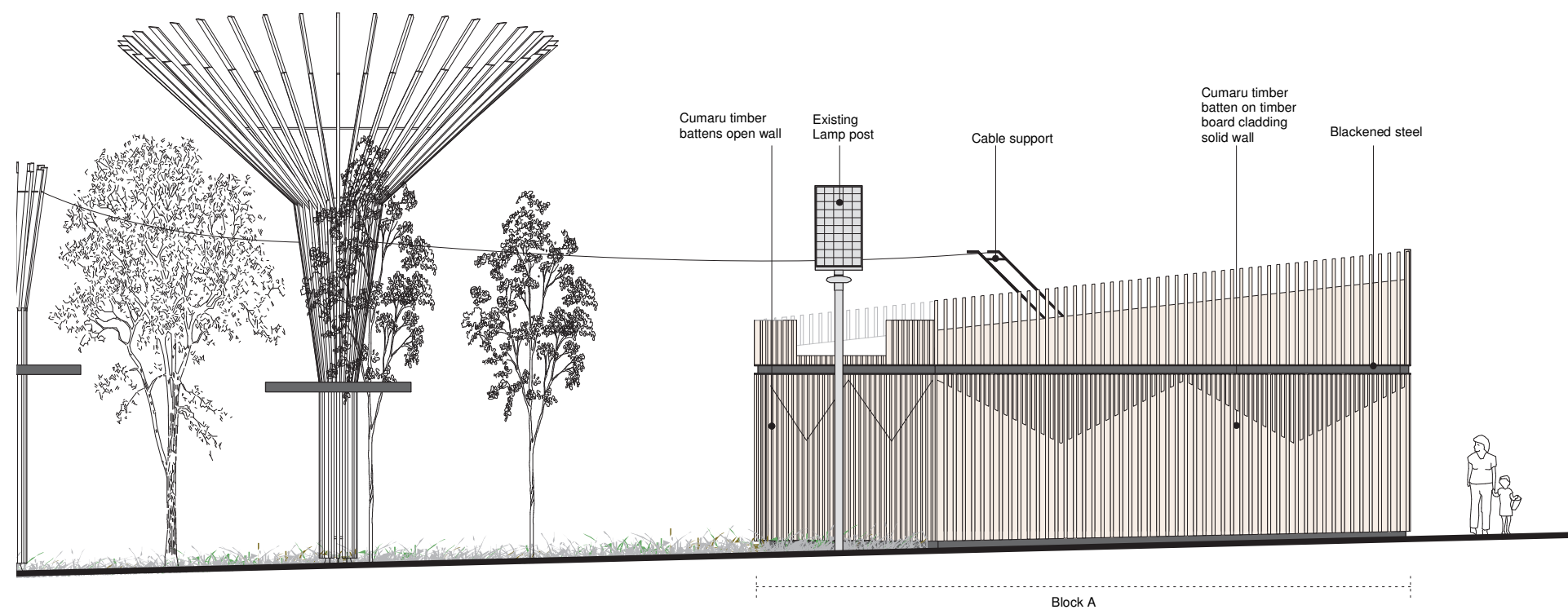
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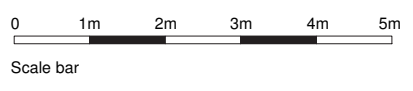





BLOCK A - North Elevation



BLOCK A - South Elevation





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Scheme:  
**Proposed High Ropes Adventure at  
Queen Elizabeth Olympic Park**

## PLANNING

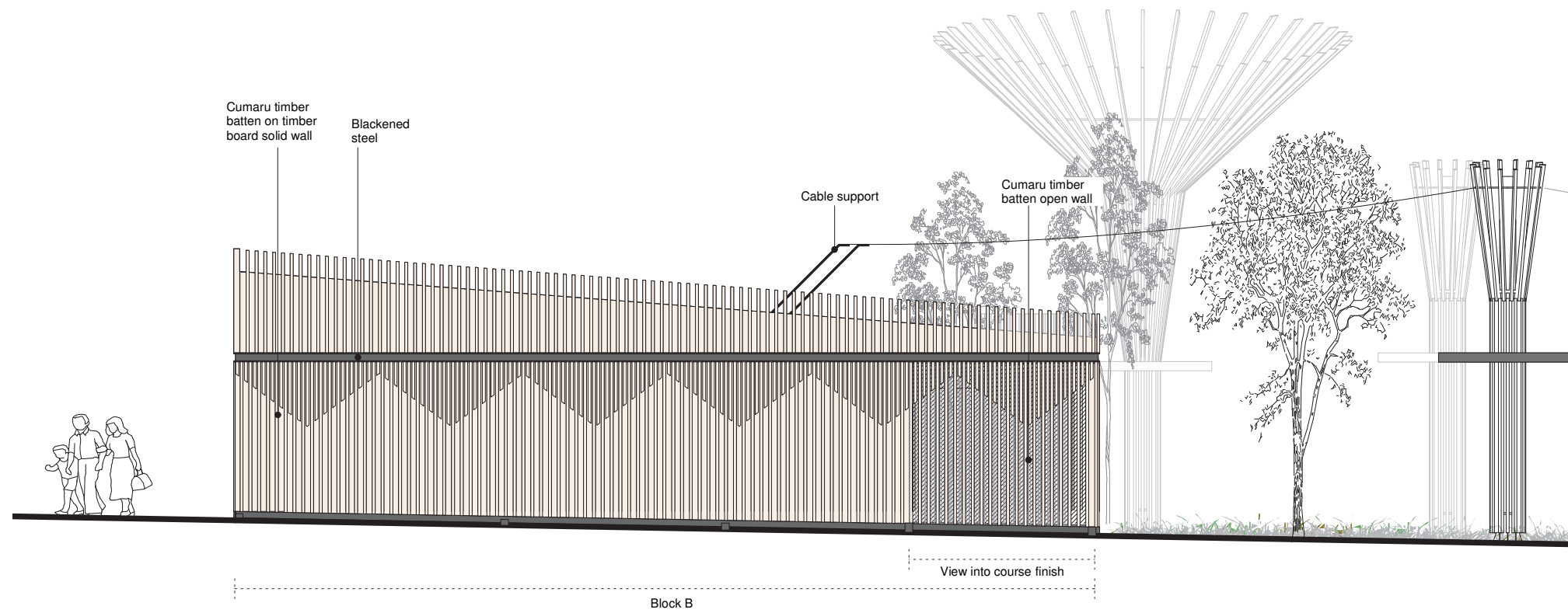
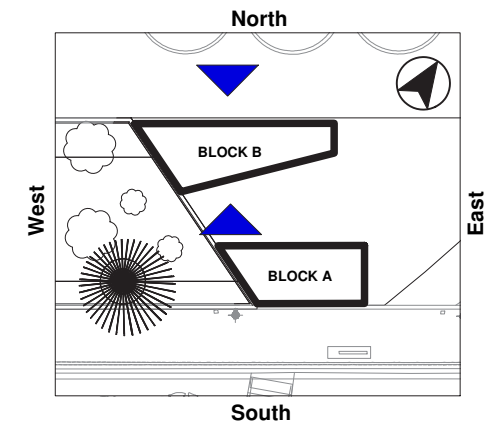
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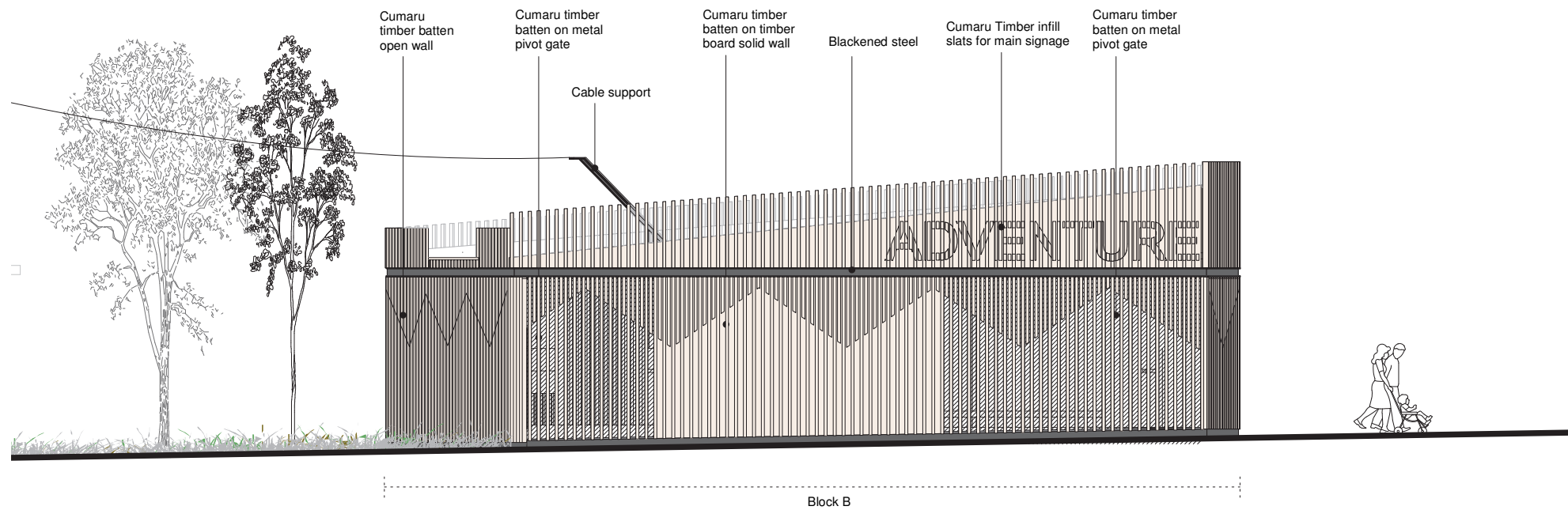
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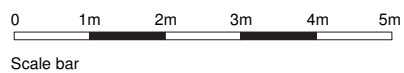
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BLOCK B - North Elevation



BLOCK B - South Elevation



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Scheme:  
Proposed High Ropes Adventure at  
Queen Elizabeth Olympic Park

# PLANNING

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Block B - North and South Elevations

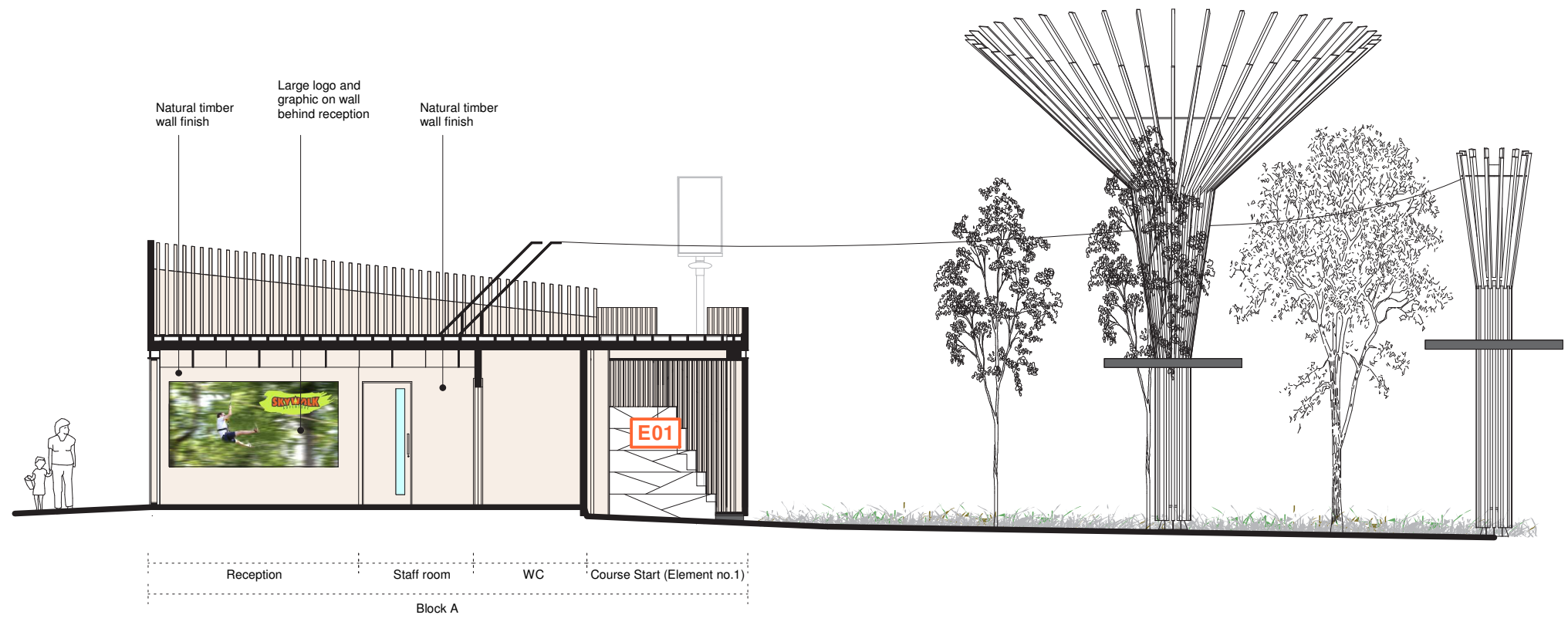
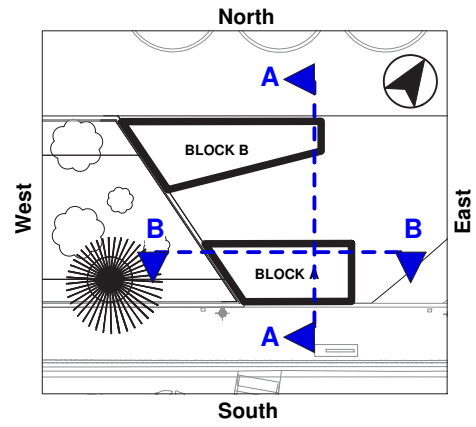
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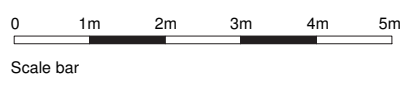
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Section AA



Section BB





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Surrey KT10 8AN

Scheme:  
**Proposed High Ropes Adventure at  
Queen Elizabeth Olympic Park**

## PLANNING

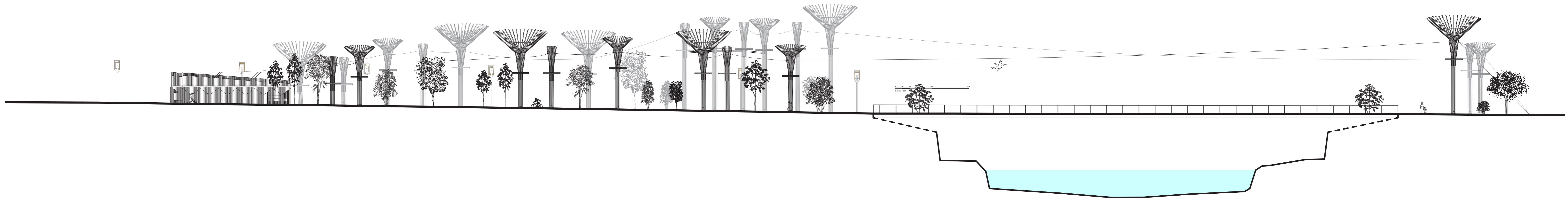
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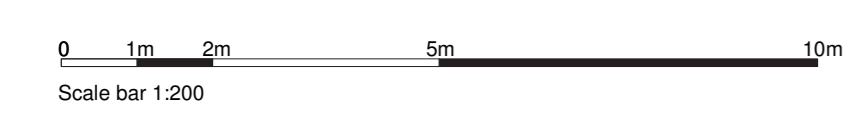


NORTH FACING SITE ELEVATION  
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Site



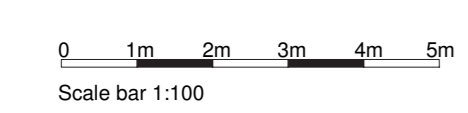
SOUTH FACING SITE ELEVATION  
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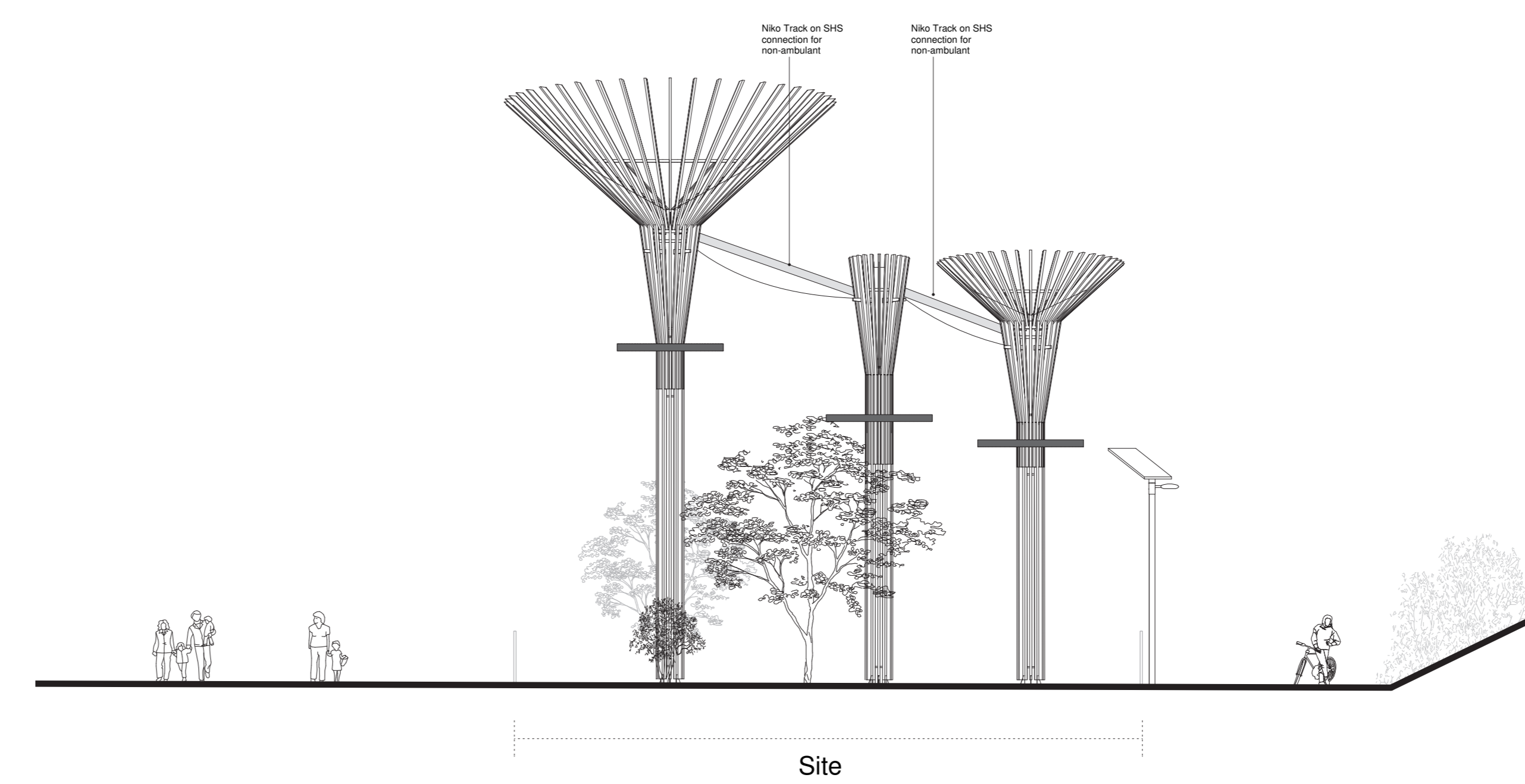
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EAST FACING SITE ELEVATION  
Scale 1:100



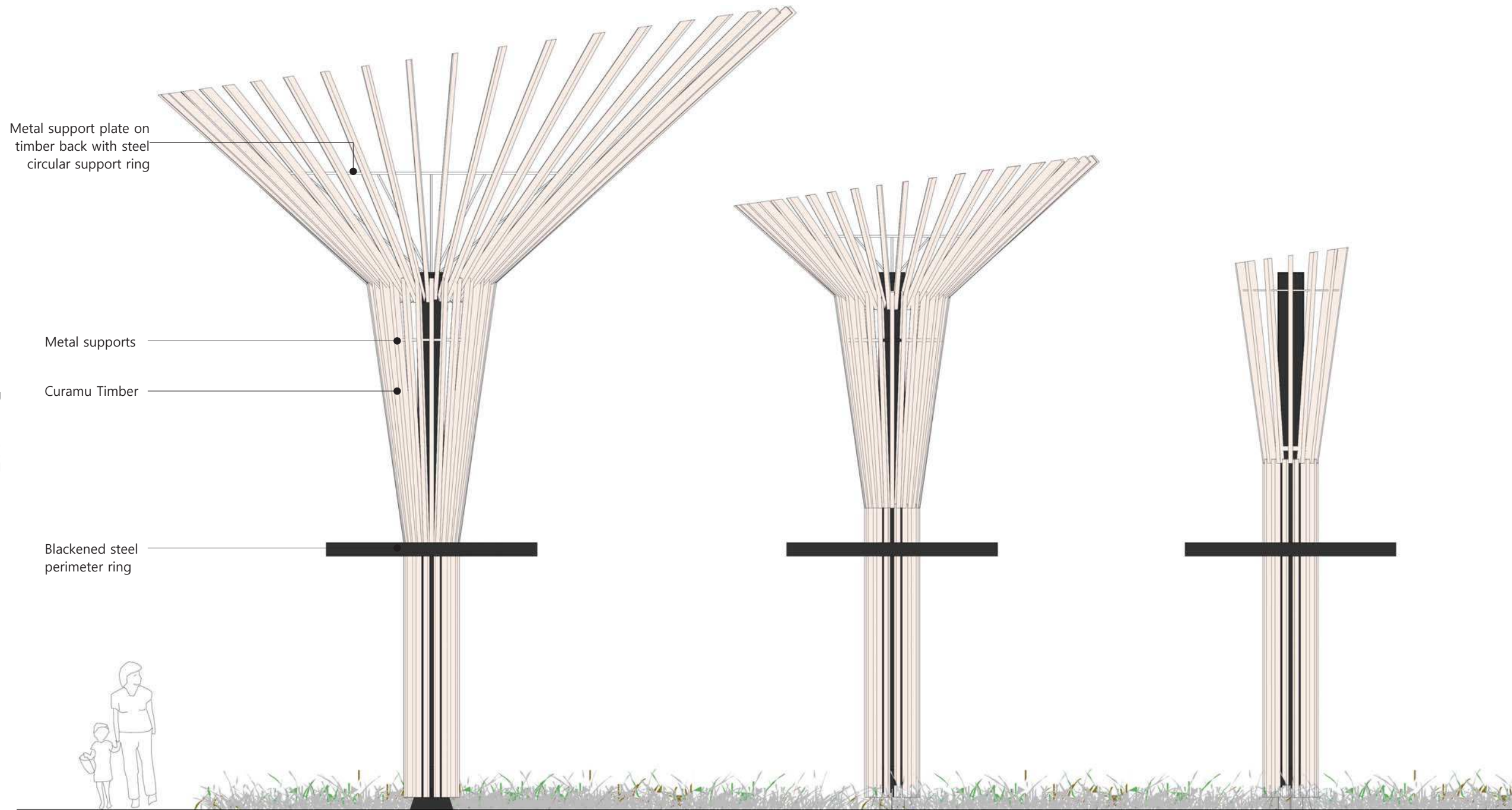
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WEST FACING SITE ELEVATION  
Scale 1:100

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TYPE 1

TYPE 2

TYPE 3



Skywalk Adventure Ltd  
 Sandown Sports Park, More Lane, Esher  
 Surrey KT10 8AN

Scheme:  
 Proposed High Ropes Adventure at  
 Queen Elizabeth Olympic Park

# PLANNING

Title:  
 Tree Structure Types

Date: 28/06/2018      Scale: 1:50@A3      Drawn: myt

Dwg No:  
 PA.120

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**London Legacy Development Corporation Quality Review Panel**

**Report of Formal Review Meeting: High ropes attraction**

Thursday 31 May 2018

Level 10, 1 Stratford Place, Montfichet Road, London E20 1EJ

**Panel**

Peter Studdert (chair)  
Julia Barfield  
Ed McCann  
Jayne Earnscliffe

**Attendees**

Catherine Smyth	LLDC Planning Policy and Decisions Team
Anne Ogundiya	LLDC Planning Policy and Decisions Team
Mark Camley	London Legacy Development Corporation
Craig Roberts	London Legacy Development Corporation
Naz Begum	London Legacy Development Corporation
Adrian Harvey	Frame Projects

**Apologies / report copied to**

Anthony Hollingsworth	LLDC Planning Policy and Decisions Team
Edward Fane	London Legacy Development Corporation
Ben Hull	London Borough of Newham
Andrew Cunningham	London Borough of Hackney
Deborah Denner	Frame Projects

**Confidentiality**

This is a pre-application review, and therefore confidential. As a public organisation the LLDC is subject to the Freedom of Information Act (FOI), and in the case of an FOI request may be obliged to release project information submitted for review.

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## 1. Project name and site address

High Ropes, East Cross Bridge (F03), Queen Elizabeth Olympic Park

## 2. Presenting team

Peter Le Masurier	Skywalk Adventure Ltd
Man Yong Tse	Skywalk Adventure Ltd
Jordan Wang	Skywalk Adventure Ltd
Paul Cope	Fabrik

## 3. Planning authority's views

The high ropes attraction forms part of a complementary offer to the Tumbling Bay playground and Timber Lodge café; the proposal links Tumbling Bay with a proposed youth play space to the west of the site.

The attraction includes a series of new structures, designed to be in keeping with its natural setting and to create minimal visual impact. No existing trees will be removed. The project will be completed in just two materials: Cumaru timber and blackened steel. The ground plane under the course will remain open and accessible both while the attraction is operating and when closed. The new building will sit on the existing hard surface at the eastern end of the footbridge. When open, the exterior of the building becomes gates creating a secure internal gathering / briefing area. Some of the façade is designed to be solid, while other parts are permeable.

It is estimated that the attraction will receive 240 visitors a day when open, with an expected visit duration of 1 hour 45 minutes. Up to 60 visitors will be on the course at any one time.

The planning authority would particularly welcome comments on: scale and appearance, including impact on the landscape; and the design and materials, including details of the crown pieces. A prototype may be essential to provide assurances that the proposed structures are appropriate to the park landscape.

A planning application submission is scheduled for mid-June 2018, with work on site anticipated to begin by the end of September and a planned opening date of February 2019.





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## 4. Quality Review Panel's views

### *Summary*

The Quality Review Panel considers that the high ropes attraction has the potential to be a significant and positive addition to Queen Elizabeth Olympic Park, provided that the LLDC is satisfied that such an attraction is consistent with its wider vision for the North Park. Success will depend on the detail, however, such as how the timber cladding is attached to the steel structure of the upright elements and the delicacy of the crown pieces. The panel strongly recommends that a prototype of the uprights be produced before any final decision to proceed. The attraction could contribute positively to the visual interest of the park, and the panel recommends inclusion of an assessment of long views in the next iteration of the design. While welcoming the intention to make provision for an accessible course, the panel has serious concerns about restricting its availability. Provision for disabled visitors must be made on every day that the attraction is open; failure to do so could be legally challenged. The limited toilet facilities proposed are also a concern, despite the relative proximity of facilities at Timber Lodge; the panel therefore recommends reconsideration of an implied reliance on off-site toilets.

### *Overall design approach*

- The panel questions how the proposed attraction fits within the strategic vision for the North Park, which is intended to be a quieter landscape primarily for current and future residents, rather than visitors. An attraction of this kind marks a departure from that vision. The panel thinks that the LLDC needs to consider this departure carefully, regardless of the quality of the attraction's design, and reassure itself that it is compatible with the wider strategic context.
- The choice of Cumaru timber as the primary material is considered appropriate. Timber cladding is to be used over steel to allow more slender vertical elements. The design team must be certain that the uprights will be stable enough, given the likely oscillation, especially on the tallest elements.
- The panel is pleased that existing immature trees are to be retained but is concerned that no thorough analysis of likely growth over the 20 year lifespan of the attraction has been completed. It is expected that the trees will eventually need some pruning where they intersect with the attraction's platforms and lines.
- The panel therefore recommends that an assessment be made of the next 20 years of growth for the trees currently on site, to ensure that the attraction will function properly throughout its lifetime, without intrusive pruning that could scar the site after the attraction has been dismantled.
- The panel is also pleased that the ground plane beneath the course – an attractive wild flower meadow – will be retained and accessible to the public throughout operation of the attraction.
- No long views were included in the presentation. Despite assurances that, except for north / south views along the river and from the Copper Box, the attraction will be masked by topography and trees, the panel asks that long views of the site be



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tested. Long view visualisations, especially for the tallest uprights, will be critical to the next stage of the scheme's development.

### *Building*

- The panel commends the building's design – its division into two parts is an ingenious solution.
- It questions, however, whether sufficient consideration has been given to how the building will fare when closed and unattended, especially after dark – no additional lighting is envisaged – and also on match days at the stadium, when vandalism and other anti-social behaviour are a possibility. The panel also suggests that thought be given to maintenance of the structure over time, as there is a risk that it could appear quite tired after five to 10 years.
- Further thought also needs to be given to the flow of visitors through the relatively constrained space of the reception building, especially with regard to the location of the toilet and the lockers.
- The proposed single toilet is insufficient, given anticipated visitor numbers and the duration of each visit, although it is acknowledged that this is intended to minimise the footprint of the building. While the attraction is close to existing public toilets, which include accessible toilets, at Timber Lodge, the proposed provision is inadequate. Many visitors are likely to use the facility before embarking on over an hour in a harness. The panel therefore strongly recommends provision of additional toilets.
- The panel also raises the likely interaction between the building and the major cycle route that passes it and, in particular, the risk of accidents at the blind corner where visitors may step into the path of cyclists. The panel suggests that some consideration be given to how this corner is managed, even to the extent of introducing some kind of physical barrier to separate visitors from the cycle route.

### *Other structures*

- The form of the uprights has the potential to be elegant. There is some concern, however, about whether the slender form of the crown pieces within the visualisation would be achieved in practice and that the effect might prove to be heavier and less attractive. Similarly, the panel is concerned that the light colours shown in visualisations might be optimistic, given the likely effects of weathering over time.
- The timbers of the crown pieces are proposed to be 100mm x 50mm, backed by steel, to minimise movement. The panel thinks, however, that movement itself could be a positive feature and, depending on an assessment of fatigue loading, this should not be designed out.



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- Despite some concerns about the resolution of the crown pieces, the panel thinks that their visibility among the trees would be a positive addition. It suggests that the crown pieces in particular should be considered as public art – they serve no practical purpose – and be designed and realised with an appropriate degree of ambition and care.
- The panel strongly recommends that a prototype of the uprights be produced before any final decision to proceed.

### *Sustainability*

- Information on the environmental performance of the building has yet to be provided. The panel considers that an assessment of the environmental aspirations for the scheme, and likely performance against these, is essential.

### *Accessibility*

- The panel welcomes the intention to provide a risk activity for disabled people, in the form of an accessible course (a loop of the two long zip wires, accessed by a hoist from ground level). It questions, however, whether the proposal to make this available only at certain times is sufficient or, indeed, legally compliant. It therefore recommends that the operator reconsider this, and make the accessible course available whenever the attraction is open.

### *Next steps*

- The Quality Review Panel considers that the high ropes attraction has the potential to be a successful addition to Queen Elizabeth Olympic Park, but would like to see a number of issues resolved, including around appropriateness, accessibility, visual impact and detailing, before a final decision to proceed.



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**Subject:** Land at Marshgate Lane, Pudding Mill, Stratford, London E15 2NH,  
Reference 17/00669/VAR

**Meeting date:** 25 September 2018

**Report to:** Planning Decisions Committee

**Report of:** Anne Ogundiya, Principal Planning Development Manager

FOR DECISION

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**This report would be considered in public**

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## **1. EXECUTIVE SUMMARY**

- 1.1. This application for full planning permission was considered at the meeting of the Planning Decisions Committee (PDC) held on 22<sup>nd</sup> May 2018. Having considered the report (as clarified by the update report); received a presentation from the Principal Planning Development Manager; and asked questions of the applicants, Members resolved to defer consideration of the application for the following reasons:
- Further detail to be provided by the applicant regarding the driver for the proposed increased commercial space;
  - Officers to explore with the applicant an enhanced affordable housing offer;
  - Mix of Units - not enough family units and too many studios;
  - Residential Quality and Density:
    - Proposals are poor quality, loss of cores, units too small and are too many per core;
    - Officers to provide further details demonstrating that the quality of the scheme would be high despite the proposal exceeding the density thresholds recommended in the London Plan;
  - Architectural expression:
    - The provision of the additional floor between Buildings 1 and 2 results in a poorer visual appearance.
- 1.2. An updated report was prepared by officers for the 24<sup>th</sup> July PDC but deferred at the applicant's request and therefore not considered by Members.
- 1.3. The applicant has provided further detail as requested by Members on the driver for increased commercial space, which is included within the assessment.

- 1.4. Since the May PDC the applicant has revised the s73 submission. The applicant has reduced the proposed amount of commercial floorspace by 199m<sup>2</sup>, i.e. from 5,671m<sup>2</sup> to 5,472m<sup>2</sup>. The extant consent approved 4,257m<sup>2</sup> of commercial floorspace. In addition, the total number of residential units now proposed on the site is 260 from the originally proposed 275 units. The approved number of residential units on the site is 254, with a 17.7% affordable housing provision.
- 1.5. Affordable housing: The revised s73 proposes 206 market sale units compared to 209 in the extant consent and 221 in the previous s73 application. Therefore, a reduction from both previous proposals. In lieu of this the current s73 proposal contains 54 affordable tenure units (87% London Affordable Rent / 13% London Shared Ownership). This equates to 21% affordable housing (24% by NIA floorspace) an increase over the previous s73 proposition of 19.6% (21% by NIA floorspace). This is a higher provision than the original consent (17.7%), and a proposal which provides a higher proportion of affordable homes as LAR (London Affordable Rent) tenure.
- 1.6. Mix of Units: Following Members concerns regarding the introduction of 10 studios to the development and the proposed reduction in the number of 3 bedroom units, the applicant has removed all the studios, and has increased the number of 3 bedroom units from the originally proposed 50 units (in the original S73 application) to 69 x 3 bedroom units, 4 less than the extant consent position of 73 units. Generally, as revised, the s73 unit mix split is more similarly aligned with the extant consent than previously proposed to Members at the May PDC. The proposed revised unit mix equates to 62% family housing (i.e. 2 bed units or more), the proportion of family housing in the extant consent is 60%.
- 1.7. Residential Quality and Density: The proposals, as submitted and revised, comply with adopted policy and design guidance. A number of the consented residential units were significantly larger than required by the Housing SPG (2016) space standards. As revised Officers are satisfied that all units still meet space standards, and indeed they are typically around 10% larger than the Mayor's Housing SPG standards, and the proposals result in more housing.
- 1.8. As revised, of the approved 8 residential blocks only blocks 1, 2 and 3 are now proposed to be amended.
- 1.9. Architectural Expression: A deeper recess of 2.5m has been introduced to the south elevation. This is compared with 1.15m for the development as previously presented to Members and 2.65m for the approved development. The deeper recess reinforces the concept of the original design as approved, in response to Members' concerns that the recess is not as visible as the original proposals. There would be no change to the materials proposed for the elevation which, as consented, are GRP panels. There would be minor changes to the setting out of the elevation, to allow for the internal changes to the apartment layouts. Balcony locations and sizes remain unchanged.
- 1.10. Following the review of the additional and updated information, Officers remain of the view that the s73 variation to condition 2 (approved drawings) is considered to be acceptable. The development meets the tests of national, regional and local development plan policy in terms of density, urban design, and architecture of the buildings. The development successfully optimises the capabilities of the site and will deliver housing including affordable housing meeting local and strategic need. It is therefore recommended that the application be approved.

- 1.11. The applicant has also submitted a section s96A non-material amendment application, which would change the description of development so that it is consistent with the conditions as altered by the s73, the decision for which would be issued at the same time as this s73 application.

## **2. RECOMMENDATIONS**

### **2.1 The Committee is asked to:**

- a) Approve the application for the reasons given in the report and grant planning permission subject to:**
  - 1. the satisfactory completion of the deed of variation to the original legal agreement under s.106 of the Town and Country Planning Act 1990 and other enabling powers to secure the planning obligations set out in the recommended heads of terms which are set out in this report; and**
  - 2. the conditions set out in this report.**
- b) Confirm that their decision has taken into consideration the environmental information addendum submitted in relation to the application, as required by Regulation 26(1) of the EIA Regulations:**
- c) Agree to delegate authority to the Director of Planning Policy and Decisions to:**
  - 1. Finalise the recommended conditions as set out in this report including such refinements, amendments, additions and/or deletions (including to dovetail with and where appropriate, reinforce, the final planning obligations to be contained in the deed of variation to the original s.106 legal agreement) as the Director of Planning Policy and Decisions considers reasonably necessary;**
  - 2. Finalise the recommended deed of variation to the original legal agreement under s.106 of the Town and Country Planning Act 1990 and other enabling powers as set out in this report, including refining, adding to, amending and/or deleting the obligations detailed in the heads of terms set out in this report (including to dovetail with and where appropriate, reinforce the final conditions and informatives to be attached to the planning permission) as the Director of Planning Policy and Decisions considers reasonably necessary; and**
  - 3. Complete the deed of variation to the original s.106 legal agreement referred to above and issue the planning permission.**
  - 4. Determine the associated S96A application for non-material amendments referred to above.**

## **3. FINANCIAL IMPLICATIONS**

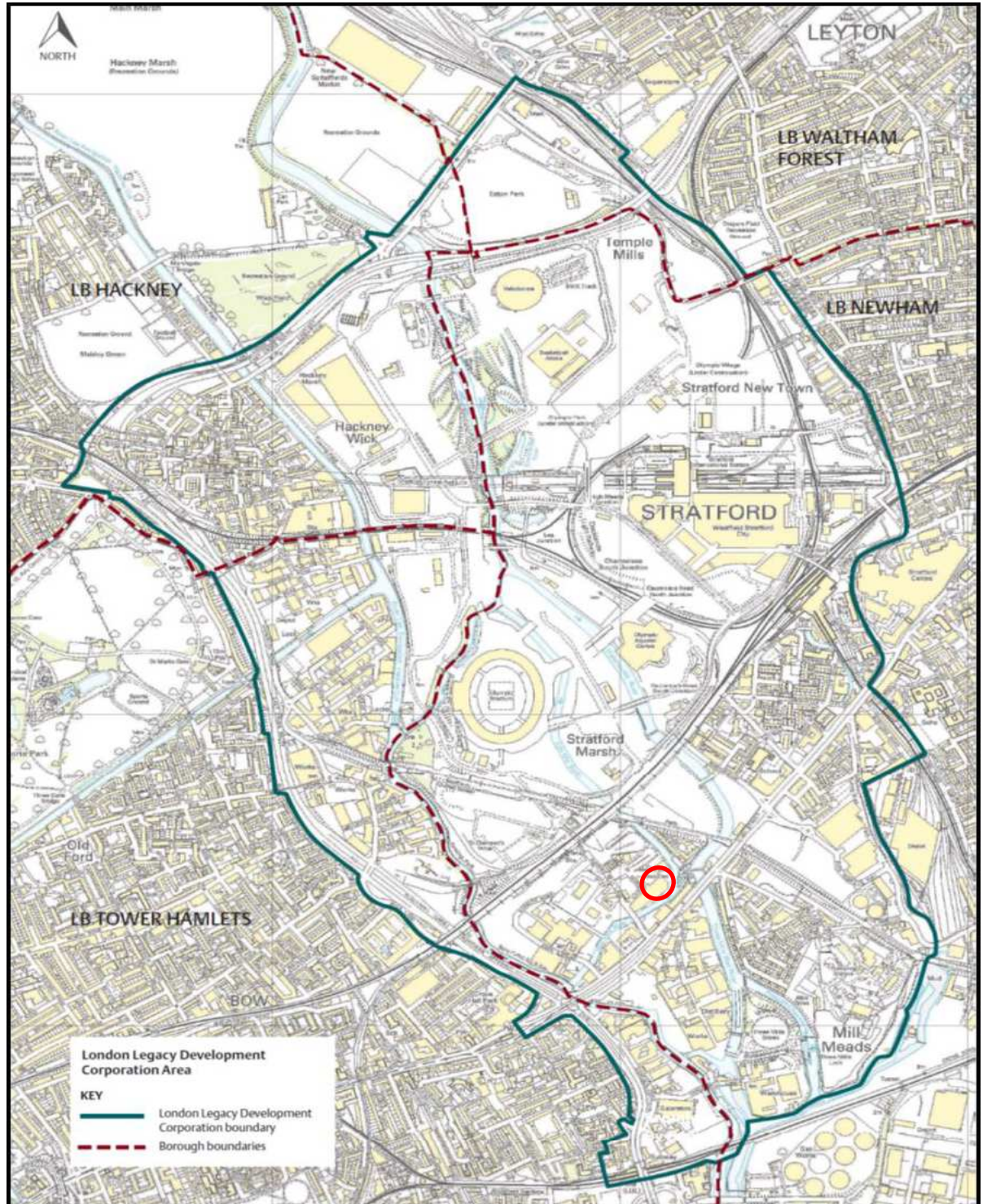
- 3.1 There are no financial implications.

#### **4. LEGAL IMPLICATIONS**

- 4.1. The recommendation is that planning permission is granted, subject to conditions and completion of a deed of variation to the original s.106 agreement in order to tie the originally agreed obligations to the new permission, to ensure adequate mitigation of the impacts of the development. The contents of the deed of variation and summary of the original Heads of Terms is described in paragraphs 6.35 and 6.36 of this report and 10.94 of the report to May PDC.



## SITE PLAN



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**Location:** Land at Marshgate Lane, Pudding Mill Stratford, London E15 2NH

**London Borough:** Newham

**Proposal:** Application under s73 of the Town and Country Planning Act 1990 (as amended) for the variation of Condition 2 (approved drawings) of planning permission ref. 14/00422/FUL dated 5 May 2017 (comprehensive mixed use redevelopment comprising: demolition of

existing buildings, and the erection of 8 buildings ranging from 3 to 12-storeys in height, comprising a total of 254 residential dwellings including affordable housing, and 4,257m<sup>2</sup> Gross Internal Area (GIA) of B1 (business) floorspace, together with basement, access, servicing, car parking, cycle parking, cycle storage, plant, open space and landscaping).

Submission under s73 of the Town and Country Planning Act 1990 (as amended) for variation to condition 2 (approved drawings) of planning permission reference 14/00422/FUL, to make changes to the approved scheme to include: (1) increase of employment use from 4,257m<sup>2</sup> to 5,472m<sup>2</sup> (additional 1,175m<sup>2</sup> (GIA) floorspace); (2) decrease of residential floorspace from 28,003m<sup>2</sup> to 27,514m<sup>2</sup> (decrease of 489m<sup>2</sup> (GIA) floorspace); (3) optimisation of residential floorspace, increasing the number of residential units from 254 to 260; and (4) alterations to façades and associated minor internal amendments to facilitate the proposed changes.

**Applicants:** Workspace 14 Limited  
**Agent:** Bilfinger GVA  
**Architects:** Squire and Partners

## 5. REVISIONS TO SECTION 73

5.1. The revised s.73 seeks the following:

5.1.1. Increase of employment use from 4,257m<sup>2</sup> to 5,472m<sup>2</sup> (additional 1,175m<sup>2</sup> (GIA) floorspace) within building 1 and 2. This has resulted in the loss on the approved 3rd floor of a total of 5 residential units to accommodate the additional commercial floorspace;

5.1.2. Decrease of residential floorspace from 28,003m<sup>2</sup> to 27,514m<sup>2</sup> (decrease of 489m<sup>2</sup> (GIA) floorspace);

5.1.3. Optimisation of residential floorspace, increasing the number of residential units from 254 to 260 (see table 1 below). Unit sizes on the residential floors within building 1 and 2 have been optimised, which has enabled additional residential units to be provided; five additional units in Building 1 (total 47 units net gain of 5 over extant consent (consented as 42)) and one additional unit in Building 2 (total 28 units net gain over extant consent of 1 unit (consented as 27)). See table 2 below;

5.1.4. Reconfiguration of the party walls and marginal movement of the cores in Building 3. There are no changes to the number of units within Building 3, which is as consented i.e. 56, however there are changes to the unit mix as follows: 3 less 1 bedroom units, 2 less 2 bedroom units and 7 extra 3 bedroom units.

5.1.5. General alterations to façades and associated minor internal amendments to facilitate the proposed changes.

**Table 1 - Overall Scheme Optimisation of Units**

Consented Scheme	Number of Units	Proposed S.73	Number of Units	Change
1 bedroom	100	1 bedroom	99	-1
2 bedroom	76	2 bedroom	88	+12
3 bedroom	73	3 bedroom	69	- 4
4 bedroom	5	4 bedroom	4	-1
<b>Consented Total Units</b>	<b>254</b>	<b>Proposed Total Units</b>	<b>260</b>	<b>+6</b>

5.2. The following table 2 is a comparison of the residential element within Buildings 1 and 2 illustrating what is consented and what is proposed.

**Table 2 - Buildings 1 + 2 Mix Comparison**

	Consented Number of units	Proposed s73 number of units	Change in number of units
1 bedroom	33	35	+2
2 bedroom	12	28	+16
3 bedroom	23	12	-11
4 bedroom	1	0	-1
<b>Total</b>	<b>69</b>	<b>75</b>	<b>+6</b>

5.3. The approved landscaping has generally not been revised for this s.73 application. Where there are changes to the approved landscape they are limited to the building entrances of Buildings 1 and 2, and the relocation of the shared landscape area to the new roof above the office levels. The servicing strategy for the site remains as the consented scheme.

## **6. ASSESSMENT OF PLANNING ISSUES**

- 6.1. The assessment of the application presented in the appended Planning Decisions Committee (PDC) report remains applicable. However, references to changes to buildings 4 - 8 is no longer applicable as the layout and façade treatment are now proposed to remain the same as the original permission (14/00422/FUL). Also, since the May PDC the applicant has revised the total number of proposed additional of units which was previously proposed at 21 to the now proposed 6 units. The total number of residential units proposed on the site is now 260 from the originally approved 254.
- 6.2. The following addresses the issues that have been raised as being of concern to Members at their meeting on 22<sup>nd</sup> May 2018.

### **Driver for increased commercial space**

- 6.3. At the May PDC Members requested further details relating to the need for the additional commercial floorspace. The applicant has since revised the amount of additional commercial floorspace proposed. An additional 1,175m<sup>2</sup> (GIA) is proposed within Buildings 1 and 2, which is a reduction of 239m<sup>2</sup> from the quantum reported to Members in May. The total commercial floorspace now proposed is 5,472m<sup>2</sup> from the extant consent approval of 4,257m<sup>2</sup>. The applicant has advised that the revisions are necessary in order to enable delivery of the development and to secure a joint venture partner, which they advise is critical to the delivery of the scheme.
- 6.4. The additional floor of commercial development within Buildings 1 and 2 is required to provide a minimum floorspace for the applicant (Workspace Ltd) to viably occupy the development. They state that they are "...keen to invest in Pudding Mill as a veritable prospect for the future in the next few years and beyond. This project and the additional commercial floorspace proposed will kick start the regeneration of Pudding Mill and encourage future agglomeration of business floorspace and housing in the Pudding Mill Local Centre. In addition, the additional B1 floorspace could equate to an additional 117 full time jobs...".
- 6.5. They also comment that given that the majority of the development earmarked/approved for the immediate Pudding Mill area has not yet come forward the additional commercial floorspace would ensure their minimum business centre requirements as a "destination centre" for this area. As such, the applicant advises that the additional commercial floorspace is essential for Workspace to ensure the business centre is successful.
- 6.6. Officers consider the delivery and occupation of the commercial floorspace by the applicant to be a significant driver to this part of the regeneration of Pudding Mill Lane, which would deliver additional benefits including an employment hub for small and medium enterprises alongside a significant number of new homes (260 units) together with increased connectivity and the provision of high quality public amenity space and playspace, which accords with the policy aspirations for the area.

### **Affordable Housing**

- 6.7. At the May PDC meeting Members raised concerns regarding the applicant's offer of 19.6% by unit of affordable housing, which whilst lower than the Local Plan Policy H.2 requirement of 35% (subject to viability) is higher than the extant consent affordable housing provision of 17.7%.

- 6.8. As revised, the amended s73 proposal would result in an additional 6 units created overall from the optimised floorspace i.e. from the approved 254 units to 260 units, along with the additional floor of commercial floorspace at Buildings 1 and 2; of which 54 units would be affordable housing. Overall this equates to 21% (24% by NIA floorspace) of affordable housing across the site compared to the 17.7% agreed in the consented scheme and the 19.6% (21% by NIA floorspace) presented to Members in May.
- 6.9. It is noted that whilst the overall number of units proposed in the original s73 has reduced from 275 to the now 260 units, the proposed quantum of affordable housing proposed remains the same at 54 units, an increase of 9 affordable units from the extant consent.
- 6.10. In response to the points raised by Members the applicant has revised the affordable offer to include a greater proportion of London Affordable Rent tenure (LAR) family units. All of the additional affordable housing units that would be provided pursuant to the s.73 application would be made available at the LAR tenure, which is considered to be genuinely more affordable than the Affordable Rent tenure approved in in the extant scheme.
- 6.11. As stated in the draft London Plan (Policy H4) ‘...delivering more genuinely affordable housing is a key strategic issue for London...’. The Mayor’s preferred affordable housing tenures are: homes based on social rent levels, including social rent and London Affordable Rent, London Living Rent and London Shared Ownership. LAR and social rent homes are for households on low incomes where the rent levels are based on the formulas in the Social Housing Regulator’s Rent Standard Guidance. The rent levels for social rent homes use a capped formula and LAR homes are capped at benchmark levels published by the GLA. Rents for both are significantly less than 80 per cent of market rents, which is the maximum for Affordable Rent permitted in the NPPF. London Shared Ownership is an intermediate ownership product which enables London households unable to buy on the open market, to purchase a share in a new home and pay a low rent on the remaining, unsold, share.
- 6.12. The revised offer (since May) equates to four more 3 bed units, six more 2 bed units, and four more 1 bedroom units, all at LAR; which is 19 more affordable rented units than the consented scheme; whilst the number of shared ownership units would reduce by 9, from 16 to 7 (see table 5 below). As revised, the overall affordable housing tenure split is 65% LAR, and 35% shared ownership, Members were previously presented with 52% LAR and 48% shared ownership. The additional LAR tenure is welcomed in helping to meet affordable housing need.
- 6.13. The commercial floorspace has been valued at approximately the same value per ft<sup>2</sup> as the London Affordable Rented units, given the site’s PTAL rating of between 3 and 4 and the current lack of amenities in Pudding Mill.
- 6.14. PPDT’s viability consultant, having reviewed the proposed changes to the development / tenure mix now proposed, and the impact this now has on potential value, concludes that ‘...there is no scope on financial viability grounds for an increase from the affordable housing provision contained within the current proposal...’. The viability consultant has run an independent discounted cashflow analysis of the revised proposals and note that there are two fundamental changes compared to their previous review; firstly, the larger average unit sizes, and secondly the change in tenure mix from higher value shared ownership towards lower value London affordable rent. ‘
- 6.15. PPDT’s viability consultant’s analysis is satisfied that the blended value applied by the applicant’s viability consultant for affordable tenures is reasonable, and concludes that the

proposed development generates a residual land value below the land value that would be considered as a competitive return in line with GLA guidance. This represents the land value that would otherwise be generated by the current consented scheme. They go on to state that ‘...it is appropriate to use this land value as a reference point for determining viability for the proposed scheme on the basis that it fairly represents the existing use value of the site...’ Officers are satisfied that the approach is in accordance with the Affordable Housing and Viability SPG.

- 6.16. The applicant has agreed to sign up to the Mayor’s affordable housing review mechanism in its SPG which is not included as part of the consented application, thereby potentially securing provision for more affordable housing. This is highlighted in the revised Heads of Terms (paragraph 6.35).

### **Unit Mix**

- 6.17. The unit mix as originally consented, presented to Committee on 22<sup>nd</sup> May and then compared against the proposals set out in the s.73 application is set out in tables 3, 4 and 5 respectively below.
- 6.18. Following Members’ concerns regarding the proposed unit mix, which was considered to comprise too many studio apartments and not enough family units, the applicant has reviewed the proposals further and has removed all of the previously proposed studio units. These units have been enlarged to provide 1 bedroom units instead. This has been achieved by amending the typical floor plan of Buildings 1 to replace the private studio apartments with one bedroom apartments on each floor from level 5 to level 11, thereby reducing the number of studios from 10 to 0.
- 6.19. When compared with the schemes presented to Members at its May Committee the proposed market housing tenure has been reduced, by 15 units (see table 5 below). In addition (as stated above), ten family shared ownership units (2 and 3-bedroom units) are now offered at London Affordable Rent.
- 6.20. The total number of family units, 2+ bedrooms, is increased from 60% under the extant consent to 62%.
- 6.21. In addition, the typical floor plan of Building 1 has been amended to replace the private two bedroom apartments with three bedroom apartments on each floor from level 5 to level 11, thereby increasing the number of three bedroom units overall from 50 to 69.
- 6.22. Whilst the number of 3 bed units provided as open market units has been reduced when compared with the consented proposals i.e. from 60 to 49, this allows for the provision of more affordable housing for the revised proposals given the impact on viability.

**Table 3 – Unit Mix as originally approved under application reference 14/00422/FUL**

Tenure	Studio	1-bed	2-bed	3-bed	4-bed	All beds Total
Market (82% of total housing)	0	83	62	60	4	209
Shared Ownership / Intermediate (6% of total housing, 36% of total affordable housing)	0	5	7	4	0	16
Affordable Rent (11% of total housing, 64% of total affordable housing)	0	12	7	9	1	28
<b>All tenures Total</b>	<b>0</b>	<b>100</b>	<b>76</b>	<b>73</b>	<b>5</b>	<b>254</b>

**Table 4 – Unit Mix as presented to Committee on 22nd May 2018**

Tenure	Studio	1-bed	2-bed	3-bed	4-bed	All beds Total
Market (80% of total housing)	10	53	119	37	2	221
Shared Ownership (9.5% of total housing, 48% of total affordable housing)	0	5	14	2	0	21
London Affordable Rent (10.5% of total housing, 52% of total affordable housing)	0	13	8	11	1	33
<b>All tenures Total</b>	<b>10</b>	<b>71</b>	<b>141</b>	<b>50</b>	<b>3</b>	<b>275</b>

**Table 5 – Proposed Revised Unit Mix**

Tenure	Studio	1-bed	2-bed	3-bed	4-bed	All beds Total
Market (80% of total housing)	0	82	72	49	3	206
Shared Ownership (2.7% of total housing, 13% of total affordable housing)	0	0	2	5	0	7
London Affordable Rent (18% of total housing, 87% of total affordable housing)	0	17	14	15	1	47
<b>All tenures Total</b>	<b>0</b>	<b>99</b>	<b>88</b>	<b>69</b>	<b>4</b>	<b>260</b>

- 6.23. Officers welcome the revised mix and affordable housing offer which is over and above what is supported on viability grounds as set out in the appended Committee Report (paragraphs 10.18 – 10.43), and is regarded as beneficial in terms of helping to meet housing need.
- 6.24. Officers are of the view that the revised offer is acceptable. It is considered that the proposals offer a unit mix which from the applicant's viability perspective enables the scheme to be deliverable, while contributing to delivering mixed and balanced communities and homes for families in line with Local Plan Policy H1.

### **Architectural Expression, Design Quality, and Density**

- 6.25. London Plan policy 3.5 seeks to achieve housing development of the highest quality – internally, externally and in relation to their context and to the wider environment. As set out in the appended Committee Report (paragraphs 10.55 – 10.58) the Mayor's Housing SPG states that where density ranges are exceeded they should be rigorously tested according to different aspects of 'liveability'. It also states that the scheme should be exemplary with respect to urban design, provision of amenity space, climate change and transport to ensure that the site is not being overdeveloped. Those matters are discussed in this report below, and in officers' report from May. Similar objectives are set out in the Local Plan Policy H.1. All of these issues are discussed in the appended Committee Report (paragraphs 10.44 – 10.85).

Architectural Expression: Following Members concerns around the perceived general degradation of the overall approved scheme, the applicant has further amended the proposals, and of the approved 8 buildings is now seeking to amend only blocks 1, 2 (the mixed residential and commercial block) and 3 (the residential block). Members expressed concern at the reduction in the number of cores per block. As revised the scheme meets the Mayor's Housing SPG which states that each core should be accessible to generally no more than 8 units per floor. As revised the scheme has reverted to the original scheme layout with two cores per block.

- 6.26. The s73 proposals introduce a third full floor of additional commercial accommodation to buildings 1 and 2, which results in a reduction in the recess originally proposed separating the commercial areas from the residential floors above. At its May meeting Members were of the view that the reduced recess presented as 1.15m could have a detrimental impact on original architectural integrity of the two buildings and the development as a whole. Following Members concerns the applicant has re-introduced a deeper recess of 2.5m, which compares to the originally approved recess of 2.65m.
- 6.27. Officers are of the view that the revision successfully reinforces the concept of the original design as approved. The reinstatement of the recess, albeit slightly less deep, would ensure that the initial architectural expression of the consented scheme is retained without compromising the quality of the increased commercial floor plates on this third-floor level. Officers are satisfied that the revision ensures that the scheme retains the high-quality architecture of the consented scheme while enabling changes which the applicant has advised are crucial to the scheme's delivery.
- 6.28. Other alterations now proposed are amendments primarily to the fenestration and balconies, in line with the proposed internal alterations as a result of the optimisation of the residential units, and are not unduly visible within the wider scheme.
- 6.29. Density: With regard to density, the proposed development as submitted and as revised falls within the suggested density parameters of the adopted London Plan (2016) and is comparable with the wider Legacy Communities Scheme (LCS) for Pudding Mill. Furthermore, it should be noted that the direction of travel for the draft London Plan is to increase site densities and to remove prescribed limits for housing in London where design



quality and adequate residential amenity can be provided. The s73 proposals would result in a residential density of 675 habitable rooms per hectares. For context, the LCS residential density range proposed for the adjoining PDZ 8 Development Parcels are between 450-950 and 450-650 habitable rooms per hectare. The London Plan maximum density range for development proposals is 200-700hr/ha.

- 6.30. All of the units as currently proposed are typically at least 10% above the nationally prescribed standards and all would have private balconies that would comply with the Mayor's Housing SPG.
- 6.31. Amenity: There is a reduction in residential amenity space by 70m<sup>2</sup> when compared with the consented scheme. However, the provision is still 2,033m<sup>2</sup> which would exceed the baseline requirement of 1,717m<sup>2</sup> as set out in the Mayor's Housing SPG; this excludes private balconies which are provided in every unit in accordance with the GLA's Housing SPG. In addition, publicly accessible space would be provided to the whole frontage of the Bow Back River, a significant benefit of the scheme. Also, the development offers high quality residential space including defensible space and high-quality entrances and amenity areas.
- 6.32. Furthermore, the reordering of the layout would mean that the proposals would provide additional amenity space (46m<sup>2</sup>) for the office accommodation over the extant consent.
- 6.33. Overall, and subject to the recommended conditions, the proposal meets the policy requirements for residential accommodation and performs well from a 'liveability' perspective. The proposal is considered to represent high quality design in terms of its high performance against the liveability criteria despite the high density. Officers can confirm that the size of units and internal room sizes meet and in many cases, exceed the minimum standards set out in the London Plan and the SPG.
- 6.34. As set out in the appended Committee Report, Officers are satisfied that taking into account the 'liveability' and design factors referred to, the density of the proposed scheme would be of high quality consistent with the density and character of its context. The proposals demonstrate high quality design, and architectural integrity in accordance with Local Plan Policies B.4, BN.1 and BN.4 in line with the extant consent.
- 6.35. **Extant Consent Heads of Terms:** The following are the heads of terms set out in the original s.106 agreement (14/00422/FUL) to mitigate the effects of the development.
- 6.36. As highlighted below the proposed deed of variation to the original s.106 agreement would tie in the originally agreed s.106 obligations to the proposed s73 permission. It would cover the proposed changes to the affordable housing offer, including requiring a review mechanism in accordance with the Mayor's Affordable Housing and Viability SPG. The heads of terms set out below reflect what has been secured in the existing s.106 agreement and the obligations that will need to be amended by the deed of variation are shown in bold and underlining:
- Final details of the treatment of the podium to Block 1 and 2
  - **To secure the Affordable Housing final offer, and to include review mechanisms, in accordance with the Mayor's Affordable Housing and Viability SPG.**
  - Affordable housing delivery linked to market housing
  - **Safeguarding of Land Required for Future Road and Bridge Link**
  - The land that is identified for the alignment of the future road link and bridge from Sugar House Lane to Pudding Mill shall be laid out in accordance with the details contained in the planning application and any subsequent approval of details prior to first occupation of the development and shall be maintained by the Developer in that condition thereafter until such time as the relevant highway

authority request that it be made available for the construction of the road and bridge link.

- Waterway Survey
- Transportation of Construction and Waste Materials by Water Transport
- An obligation in the legal agreement to use reasonable endeavours to meet aspirations for the water transport of waste arising from demolition at the site and for new materials to be delivered by waterway.
- Marshgate Lane Design Works Contribution
- Implement design initiatives / proposals arising from the impact of the development on Marshgate Lane in order to rebalance the street and enhance provision for users including pedestrians and cyclists.
- Relocation Strategy
- Relocation strategy as part of the legal agreement to ensure that the applicant uses reasonable endeavours to relocate the existing tenants within the Growth Boroughs
- Employment Workspace Model
- Delivery of workspace as per the model of Workspace14 Limited
- Workspace to notify the LPA prior to commencement as to whether it would occupy the workspace. If it does not occupy, then it should be offered as low-cost workspace for a period of 2 years
- Requirement for workspace strategy to ensure employment floorspace is designed and managed to meet the needs of small local companies and businesses
- Local Labour Strategy: To ensure a proportion of the employment during construction and operation (end-use) are from the Host Boroughs and to provide training initiatives
- London Living Wage: The minimum hourly wage as published by the GLA for both construction and end jobs.
- Travel Plan: To implement, monitor and review and provide membership to a car club and include electric car charging points
- Car Park Management Strategy
- Provision of Car Club spaces and Contribution to Membership Fees
- Safeguarded Land for TfL Cycle Docking Station
- Restriction on eligibility of future residents and commercial occupiers for on-street parking permits
- Electric Charging Point Provision
- Waterways Strategy
- Design Monitoring Contribution – In order to maintain design quality in the event that the original architect is not retained.
- Sustainability: Use of reasonable endeavours to secure the extension of the QEOP district heating and cooling network to the development site and thereafter an obligation to secure connection of all buildings; and in the event that this cannot be achieved to submit details of alternative measures by which equivalent

carbon savings can be achieved; reasonable endeavours to encourage occupiers to reduce their energy usage

- Management and maintenance of new public realm.
- Public pedestrian and cycle access along the east-west in perpetuity

## **7. Human Rights & Equalities Implications**

7.1. Members should take account of the provisions of the Human Rights Act 1998 as they relate to the application and the conflicting interests of the Applicants and any third party opposing the application in reaching their decisions. The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report. In particular, Article 6 (1), of the European Convention on Human Rights in relation civil rights and a fair hearing; Article 8 of the ECHR in relation to the right to respect for private and family life and Article 1 Protocol 1 of the ECHR in relation to the protection of property have all been taken into account.

7.2. In addition, the Equality Act 2010 provides protection from discrimination in respect of certain protected characteristics namely: age, disability, gender reassignment, pregnancy and maternity, race, religion, or beliefs and sex and sexual orientation. It places the Local Planning Authority under a legal duty to have due regard to the advancement of equality in the exercise of its powers including planning powers. Officers have taken this into account in the assessment of the application and Members must be mindful of this duty inter alia when determining all planning applications. In particular Members must pay due regard to the need to:

1. Eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Equality Act;
2. Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and;
3. Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

7.3. Officers are satisfied that the application material and Officers' assessment has taken into account these issues. Particular matters of consideration have included provision of accessible housing and parking bays, as well as the provision of affordable and family housing.

## **8. CONCLUSION**

8.1. As set out in section 12 of the appended Committee Report the proposals accord with national, regional and local development plan policy. The development forms a mixed use that is respectful of its context and would positively regenerate the area in accordance with Local Plan policy.

8.2. It is considered that the variations proposed as part of the s73 application can be supported, noting that they would be limited in their effect on the originally permitted scheme and would only constitute minor material amendments to the parent permission (14/00422/FUL). The minor material amendments are considered both individually and cumulatively to be acceptable and subject to the necessary alteration to the original s.106 agreement by way of the deed of variation and conditions, the officer recommendation is to grant this s73 variation planning permission.

8.3. The s73 application as revised is recommended for approval subject to the satisfactory completion of a deed of variation to the existing s.106 Legal Agreement.

## 9. PLANNING CONDITIONS

### 1. Time limit

S73 - CONSENT LIMITED TO THAT OF ORIGINAL PERMISSION: The development hereby permitted shall be begun not later than the expiration of [04/05/2020].

Reason: To ensure the commencement timescale for the development is not extended beyond that of the original planning permission granted on [05/05/2017] [ref: 14/00422/FUL]. Furthermore, to comply with the provisions of Section 91(1)(a) of the Town and Country Planning Act 1990

### 2. Works in accordance with approved details

The development, including demolition and construction, shall be carried out in accordance with the following details and plan numbers:

INSERT DRAWING NUMBERS

and the description of development contained in the application and any other plans, drawings, documents, details, schemes or strategies which have been approved by the Local Planning Authority pursuant to these conditions.

Reason: To ensure that all works are properly implemented and retained.

### 3. Section 106 Obligation

No development on the 14ML Land shall commence until all of the parties who own a freehold interest in the 14ML Land have entered into the Supplemental Agreement with the Local Planning Authority.

Reason: To ensure that no development of the 14ML Land can take place until that land is bound into the relevant planning obligations contained within the s.106 Agreement.

### 4. Workspace 14 Build Out

No part of the development permitted on the 14ML Land shall be occupied until at least 84 residential units on the W14 Land have been occupied.

Reason: To ensure the viability review obligations of the s.106 Agreement capture the achieved sales values of the majority of residential units comprised within the development.

### 5. Notice of Commencement

The development shall not be commenced until written notice of intention to commence the development has been given to the Local Planning Authority. The notice required by this condition shall only be given where there is a genuine prospect of development being commenced within 21 days of the notice and the notice shall confirm and provide evidence that this is the case.

Reason and pre-commencement justification: To enable the LPA to monitor development.

6. **Development in accordance with Environmental Statement**

The Development (including other matters submitted for approval pursuant to this permission) shall be carried out in accordance with the mitigation measures set out in the document WIE10067.3.2.1. Marshgate\_Mitigation Table - Final July 2016.

Reason: To ensure the mitigation measures specified in the Environmental Statement are satisfactorily implemented.

**DEMOLITION & CONSTRUCTION**

7. **Demolition and Construction Management Plan (DCMP)**

The development on either the W14 Land or the 14ML Land shall not be commenced until a demolition and construction management plan (CMP) for the respective area of land, has been submitted to and approved by the Local Planning Authority. The Demolition and Construction Management Plan and the updates thereto shall be in accordance with all relevant legislation in force and substantially in accordance with all policy adopted and best practice guidance published at the time of submission. The DCMP shall include as a minimum the following information:

- a. The arrangements for liaison with the relevant highway authorities;
- b. The parking of vehicles of site operatives and visitors;
- c. The loading and unloading of plant and materials including a construction logistics plan;
- d. The storage of plant and materials use in constructing the development;
- e. The erection and maintenance of security hoardings;
- f. Measures to control the emission of dust and dirt during construction;
- g. A scheme for recycling and disposing of waste resulting from demolition and construction work;
- h. Dealing with complaints and community liaison;
- i. measures to control the flow of surface water off the slab (temporary drainage details);
- j. Attendance as necessary at the LLDC Construction Transport Management Group (CTMG) and or Construction Coordination Group;
- k. Details of routes and access for construction traffic. Including lorry holding areas; and
- l. Guidance on membership of the Fleet Operator Recognition Scheme and implementation of vehicle safety measures and driver training including cycle awareness and an on-road cycle module.

The development shall be carried out in accordance with the approved details.

Reason and pre-commencement justification: To ensure that the Local Planning Authority can assess whether the development would generate any unacceptable environmental impacts through demolition and construction that would require appropriate mitigation and to be in accordance with London Plan Policy S.3 and Local Plan 2015 Policy T.4.

**8. Code of Construction Practice**

The development on either the W14 Land or the 14ML Land shall not be commenced until a Code of Construction Practice for the respective area of land has been submitted to and approved by the Local Planning Authority. The Code of Construction Practice shall be in accordance with all relevant legislation in force and substantially in accordance with all policy adopted and best practice guidance published at the time of submission. The Code of Construction Practice shall include proposals for the following:

- Safeguarding of buried services
- Location and height of any proposed stock
- waste generation and materials reuse and recycling
- air quality mitigation measures from demolition and construction activity within the Site
- noise mitigation measures from demolition and construction activity within the Site

The Development shall be carried out in accordance with the approved details.

Reason and pre-commencement justification: To ensure that the Local Planning Authority can assess whether the development would generate any unacceptable environmental impacts through construction that would require appropriate mitigation in accordance with Local Plan Policies BN.11 and S.4.

**9. Demolition and Construction Waste Management Plan**

The development on either the W14 Land or the 14ML Land shall not be commenced until a Demolition and Construction Waste Management Plan (DCWMP) for the respective area of land has been submitted to and approved by the Local Planning Authority. The objectives of the DCWMP shall be to ensure all waste arising from the construction works are managed in a sustainable manner, maximising the opportunities to reduce, reuse and recycle waste materials. The DCWMP shall also detail the compliance and assurance requirements to be maintained on the Site during all phases of construction. The DCWMP shall include as a minimum the following information:

- classification of all waste including hazardous waste according to current legislative provisions;
- performance measurement and target setting against estimated waste forecasts;
- reporting of project performance on quantities and options utilised;
- measures to minimise waste generation;
- opportunities for re-use or recycling;
- provision for the segregation of waste streams on the Site that are clearly labelled;
- licensing requirements for disposal sites;

- an appropriate audit trail encompassing waste disposal activities and waste consignment notes;
- measures to avoid fly tipping by others on lands being used for construction. Returns policies for unwanted materials;
- measures to provide adequate training and awareness through toolbox talks; and
- returns policies for unwanted materials.
- The Development shall be carried out in accordance with the approved details.

Reason and pre-commencement justification: Submission required prior to commencement to ensure that the Local Planning Authority to ensure that the impact of demolition and construction is appropriately mitigated and ensure high standards of sustainability are achieved in accordance with Policy 5.18 of the London Plan and Policy S.6 of the Local Plan 2015.

**10. Approval of road works necessary**

The development on either the W14 Land or the 14ML Land shall not be commenced until details of the following works to the highway for the respective area of land have been submitted to and approved in writing by the Local Planning Authority in consultation with the Highway Authority. Works to the highway shall only be carried out by the Highway Authority:

- The provision of an on-street loading bay.
- The installation of new crossovers and reinstatement of footways where existing crossovers are being blocked up.

The building hereby permitted on the respective area of land shall not be occupied until these works have been consented in accordance with the approved details.

Reason and pre-commencement justification: To ensure that occupiers of the site have adequate facilities to support the use and encourage the uptake of sustainable transport methods to and from the site, and to ensure that all road works associated with the proposed development are to a standard approved by the Local Planning Authority and are completed before occupation and in accordance with Local Plan 2015 Policy T.4.

**11. Piling/Foundation Designs**

Piling or any other foundation designs using penetrative methods shall not be permitted on either the 14ML Land or the W14 Land other than with the express prior written consent of the Local Planning Authority, which may be given for those parts of the respective area of land where it has been demonstrated that there is no resultant unacceptable risk to groundwater. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the proposed activities safeguard human health and do not harm controlled waters and groundwater resources in line with the Environment Agency's approach to groundwater protection March 2017 and in accordance Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015.

**12. Piling method statement**

No piling, including impact piling shall take place on either the 14ML Land or the W14 Land until a piling method statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling would be carried out, including measures to prevent and minimise the potential for impact on ground water, damage to subsurface water infrastructure, and the programme for the works) for the respective area of land has been submitted to and approved in writing by the Local Planning Authority. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: To ensure that the proposed activities safeguard human health and do not harm controlled waters and groundwater resources in line with the Environment Agency's approach to groundwater protection March 2017 and in accordance Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015

**13. Remediation Statement**

No development except demolition to existing slab level shall commence on either the 14ML Land or W14 Land until a Remediation Statement has been submitted to and approved by the Local Planning Authority in writing. The Remediation Statement shall outline and justify the assessment approach required to be completed and approved by the Local Planning Authority. The Remediation Statement shall also define the phases of development for which subsequent submissions are required to be submitted to discharge the contamination assessment, remediation strategy and methodology.

Reason: Submission required prior to commencement to safeguard human health, controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other off-site receptors in accordance with Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015.

**14. Contamination assessment, remediation strategy and methodology**

No phase shall be commenced except demolition to existing slab level until details of remediation for that phase as defined in the submitted Remediation Statement have been submitted to and approved in writing by the Local Planning Authority. The contamination assessment, remediation strategy and methodology approach shall be detailed in either of the following documents:

- remediation protection method statement; or
- site specific remediation strategy and remediation method statement

Each submission shall contain as a minimum:

- A review of relevant previous contamination information in conjunction with additional ground investigations undertaken as part of the Development. Any contamination assessment shall use appropriate assessment criteria.
- Details of the proposed development and general work methodology and programme.
- General health and safety and environmental controls including any details of any required authorisations, permits, licences and consents.
- Discovery strategy to deal with unexpected contamination.
- Details of how the remediation works and materials (including site won and imported) are to be validated.



- The remediation method statement shall include details of design, installation and verification of gas and vapour protection measures in accordance with current guidance and British Standards if required by the Local Planning Authority.

The Development shall be carried out in accordance with the approved details.

Reason: To safeguard human health, controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015.

**15. Remediation, validation and protection**

No occupation of any part of the permitted Development (or stage in development as may be agreed in writing with the Local Planning Authority) shall take place until a verification (or validation) report demonstrating completion of works set out in the approved remediation protection method statement, site specific remediation statement and remediation method statement has been submitted to and approved in writing by the Local Planning Authority.

The verification report shall assess and describe the requirements for long-term monitoring and maintenance (including contingency action) to ensure the effectiveness of the remediation measures implemented. The long-term monitoring and maintenance shall be implemented as approved.

Reason: To safeguard human health, controlled waters, property and ecological systems, and to ensure that the development has been carried out safely without unacceptable risks to workers, neighbours and other offsite receptors and in accordance with Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015

**16. Unexpected contamination**

If during development unexpected contamination is encountered then no further development shall be carried out until an addendum to the remediation protection method statement, site specific remediation statement and remediation method statement (referred to in conditions 13 and 14) has been submitted to and approved in writing by the Local Planning Authority (unless otherwise agreed in writing with the Local Planning Authority). The addendum shall be implemented as approved, in writing by the Local Planning Authority.

Reason: To safeguard human health, controlled waters, property and ecological systems, and to ensure that the development is carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015.

**17. Surface Water Drainage**

Development of either the 14ML Land or W14 Land shall not be commenced except demolition to existing slab level until a detailed surface water drainage scheme for the respective area of land, based on the following agreed documents has been submitted to and approved in writing by the local planning authority.

- Marshgate Business Centre and 14 Marshgate Lane, Flood Risk Assessment, Document Reference: CIV15409 ES 002, October 2014.

- Marshgate Lane, London, E15 2NH, Drainage Report, Job No. 5239, August 2014.

The drainage strategy shall include a restriction in run-off and surface water storage on site as outlined in the FRA. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

Reason and pre-commencement justification: Submission required prior to commencement to the Local Planning Authority to prevent the increased risk of flooding, to improve and protect water quality, and improve habitat and amenity in accordance with Policy 5.13 of the London Plan and Policy BN13 of the Local Plan 2015.

**18. Drainage**

The drainage works referred to in the approved Marshgate Business Centre Drainage strategy prepared by Waterman dated October 2014 shall be implemented in full for each relevant phase of the development, and no discharge of foul or surface water from the site shall be accepted into the public system until the said drainage works have been completed unless minor variations are agreed in writing in advance with the Local Planning Authority.

Reason: The development may lead to sewage flooding; to ensure that sufficient capacity is made available to cope with the new development; and in order to avoid adverse environmental impact upon the community and in accordance with Policy 5.13 of the London Plan and Policy BN13 of the Local Plan 2015.

**19. Archaeological investigation**

- A) For each of the 14ML Land and the W14 Land no development of the respective area of land other than demolition to existing ground level shall take place until (i) a programme of archaeological evaluation for the respective area of land has been submitted to and approved in writing by the Local Planning Authority (ii) the approved archaeological evaluation programme has been implemented and (iii) a report on that evaluation has been submitted to and approved in writing by the Local Planning Authority.
- B) If heritage assets of archaeological interest are identified by the evaluation under Part A, then before development of the respective area of land, other than demolition to existing ground level, commences (i) a Written Scheme of Investigation shall be submitted to and approved in writing by the Local Planning Authority.
- C) No development or demolition of the respective area of land other than demolition to existing ground level shall take place other than in accordance with the Written Scheme of Investigation approved under Part (B) and archaeological works shall be carried out by a suitably qualified investigating body acceptable to the Local Planning Authority.
- D) The development on the respective area of land shall not be occupied until a site investigation and post investigation assessment has been completed in accordance with the programme set out in the Written Scheme of Investigation approved under Part (B), and the provision for analysis, publication and dissemination of the results and archive deposition has been secured.

Reason: To ensure that the archaeological interests are protected and considered appropriately and in accordance with London Plan policy 7.8 of the Local Plan and policy BN.2 of the Local Plan 2015.

## **RESIDENTIAL STANDARDS**

### **20. Residential standard- internal noise levels**

No residential premises shall be occupied unless it has been designed and constructed in accordance with BS8233:2014 'Sound insulation and noise reduction for buildings- Code of Practice' and attains the following internal noise levels:

- Bedrooms- 30dB LAeq,T\* and 40dB LAfmax
- Living rooms- 30dB LAeq, D\*
- \*T- Night-time 8 hours between 23:00-07:00
- \*D- Daytime 16 hours between 07:00-23:00.

Reason: To ensure that the occupiers and users of the development do not suffer a loss of amenity by reason of excess noise from environmental and transportation sources and to be in accordance with Policy 7.15 of the London Plan and Policy BN11 of the Local Plan 2015.

### **21. Sound insulation and noise mitigation details – Residential and Non-Residential**

The development on either of the 14ML Land or the W14 Land shall not be occupied until details of the proposed sound insulation scheme to be implemented between the residential accommodation and any non-residential uses of the respective area of land have been submitted to and approved in writing by the Local Planning Authority. The scheme shall include a glazing specification for all windows to ensure a good standard of internal noise can be achieved during day time and night time in accordance with the guideline levels of BS8233 1999: "Sound insulation and noise reduction for buildings – code of practice" or an equivalent standard. Details should include airborne and impact sound insulation. The Development on the respective area of land shall not be occupied until the noise mitigation measures approved as part of the sound insulation scheme have been installed. The approved scheme is to be completed prior to occupation of the Development and thereafter permanently retained.

Reason: To protect the amenity of future occupants and/or neighbours in accordance with Policy 7.15 of the London Plan and Policy BN11 of the Local Plan 2015.

### **22. Accessible housing**

90% of the residential units hereby permitted shall be designed and constructed in accordance with M4 (2) Category 2 of Part M of the Building Regulations.

10% of the residential units hereby permitted shall be designed and constructed in accordance M4 (3) Category 3 of Part M of the Building Regulations (split proportionately so that 10% of each of the following tenures of residential units shall be so provided: market housing units, intermediate housing units, social rented housing units and affordable rented housing units; and 10% of each of the following sizes of residential units shall be so provided: one, two and three bedroom units).

The actual number of units to be provided in accordance with this condition would be the number of whole units that is as near as arithmetically possible to the specified percentage and 0.5 or above shall be rounded up to the nearest whole.

Reason: To ensure adequate accessible housing is provided and to be in accordance with Local Plan 2015 Policy BN.5: Requiring inclusive design.

23. **Refuse storage**

Before the first occupation of the development hereby permitted on either of the 14ML Land or the W14 Land, the waste and recycling storage, transfer and collection arrangements for the respective area of land shown on the approved drawing/s shall be submitted in a Waste Servicing Strategy and approved in writing by the local planning authority, and made available for use by the occupiers of the Development. The Waste Servicing Strategy shall include the following key pieces of information:

- Details and plans showing where residents and commercial tenants in each building should deposit their waste;
- Description of the waste containers and equipment to be housed in each waste storage area;
- Explanation of how and when waste containers will be transferred between waste rooms, the route the container will take during transfer, and where the necessary equipment for facilitating the operations will be stored; and
- Details of arrangements for collection contractors for municipal and commercial waste, including where the waste will be collected from, where the waste collection vehicles will park, and the route for transferring bins between waste stores and vehicles.

The facilities and management processes provided shall thereafter be retained and maintained for the life of the Development of the respective area of land and neither they nor the space they occupy shall be used for any other purpose unless it can be demonstrated that these facilities are no longer required or can be reduced in size without affecting recycling rates, to be agreed in writing by the Local Planning Authority in accordance with Policy S.6 of the Local Plan.

Reason: To ensure there are refuse storage and management arrangements in place in advance of the use commencing to avoid highway and safety issues and ensure that that the refuse would be appropriately stored within the site in the interest of protecting the amenity of the site and the area in general from litter, odour and potential vermin/pest nuisance.

**PARKING, SERVICING, CYCLES**

24. **Deliveries and servicing management plan**

The development on either of the 14ML Land or the W14 Land shall not be occupied until a delivery and servicing management plan (DSMP) detailing how all elements of the respective area of land are to be serviced has been submitted to and approved in writing by the Local Planning Authority. The DSMP shall be prepared in accordance with TfL's online guidance on delivery and servicing plans found at <http://www.tfl.gov.uk/info-for/freight/planning/delivery-and-servicing-plans#on-this-page-1> or such replacement best practice guidance as shall apply at the date of submission of the DSMP. The approved DSMP for the respective area of land shall be implemented from first occupation of that respective area of land and thereafter for the life of the development on the respective area of land.

Reason: In the interests of highway and pedestrian safety and residential amenity making adequate provision for deliveries and servicing, and encouraging sustainable delivery methods in accordance with Policy 6.11 of the London Plan and Policy T4 of the Local Plan 2015.

25. **Cycle Storage and facilities- details to be submitted**

The development on either of the 14ML Land or the W14 Land shall not be occupied until details (1:50 scale drawings) of the facilities to be provided for the secure storage of cycles (for both residential and commercial elements) and on site changing facilities and showers (for the commercial element) for the respective area of land have been submitted to and approved in writing by the Local Planning Authority. The development of the respective area of land shall be carried out in accordance with the approved details and the facilities provided shall be retained for the life of the development on the respective area of land and the space used for no other purpose.

Reason: In order to ensure that satisfactory secure cycle parking and facilities for cyclists are provided and retained and in accordance with Policy 6.3 of the London Plan and Policy T4 of the Local Plan 2015.

**SUSTAINABILITY**

26. **Renewable energy**

The development on the W14 Land and on the 14ML Land shall be constructed and operated in accordance with the submitted Price/Myers Energy Assessment dated 1st April 2015 and achieve reductions in regulated CO2 emissions through the use of on-site renewable energy generation sources approved as part of this development.

Reason: To ensure a high standard of sustainable design and construction and to ensure sufficient information is available to monitor the effects of the development in accordance with Policy 5.2 of The London Plan and Policies S2 and S5 of the Local Plan 2015.

27. **BREEAM**

Before any fit out works to the commercial premises hereby authorised begins, an independently verified BREEAM report (detailing performance in each category, overall score, BREEAM Design Stage rating and a BREEAM certificate of building performance) which demonstrates that a minimum 'Excellent' rating has been achieved shall be submitted to and approved in writing by the Local Planning Authority and the development of the respective area of land shall not be carried out otherwise than in accordance with any such approval given. Within three months of occupation of any commercial premises hereby permitted, a certified Post Construction Review (or other verification process agreed with the Local Planning Authority) shall be submitted to and approved in writing by the Local Planning Authority, confirming that the agreed standards above have been met.

Reason: To ensure that high standards of sustainability are achieved in accordance with Policies 5.2, 5.3 and 5.6 of The London Plan and Policies S2 and S4 of the Local Plan 2015.

28. **Material samples**

Prior to the construction of the development on either of the 14ML Land or the W14 Land, a schedule detailing brick bonding(s) where appropriate and samples of materials and finishes to be used on the external surfaces of the development of the respective area of land shall be submitted to and approved in writing by the Local Planning Authority. The approved materials shall be used in the implementation of the development. A sample panel(s) of brickwork of not less than 1m x 1m showing mortar mix, bonding and pointing type shall be constructed for the Local Planning Authority to inspect and approve and shall be retained on site until completion of the works, and the brickwork shall be constructed in accordance with the approved panel(s).

Reason and pre-commencement justification: To safeguard the appearance of the buildings and the character of the area generally and to enable the Local Planning Authority to properly consider and control the development in the interest of visual amenity and to accord with Policies BN.1 and BN.4 of the Local Plan 2015.

29. Prior to the construction of buildings 1 and 2 details of the architectural design, including façade treatment, as well as materials and colour of the external surfaces of the podium level (levels 1, 2 and 3) of buildings 1 and 2 (at a scale to be agreed with the Local Planning Authority) shall be submitted to and approved in writing by the Local Planning Authority and shall be constructed in accordance with the approved details.

Reason and pre-commencement justification: To safeguard the appearance of the buildings and the character of the area generally and to enable the Local Planning Authority to properly consider and control the development and to be in accordance with Strategic Policy SP.3 and Policies BN.1 and BN.10 of the Local Plan 2015.

30. **Detailed drawings**

Prior to the construction of the development on either of the 14ML Land or the W14 Land detailed drawings including sections (at a scale to be agreed in writing with the Local Planning Authority) of the respective area of land shall be submitted to and approved in writing by the Local Planning Authority as follows:

- Commercial façade details (ground level entrances and shopfront): elevations and sections through shopfronts, including details of doors, windows and signage and junctions with new pedestrian space as well as canopies, security shutters and areas for signage;
- Residential façade details (including elevations facing the internal courtyard): elevations and sections annotated with materials and finishes of all windows (including reveals and sills), entrances, external bin stores, balconies, and balustrades, pipework and parapets; and all openings adjacent to the highways.

The development of the respective area of land shall not be carried out otherwise than in accordance with the approved drawings.

Reason and pre-commencement justification: To enable the Local Planning Authority to properly consider and control the development and to be in accordance and in order to ensure a high quality of design and detailing is achieved and to be in accordance with Strategic Policy SP.3 and Policies BN.1, BN.4 and BN.10 of the Local Plan 2015.

31. **Green/brown roof**

Prior to the construction of the development on either of the 14ML Land or the W14 Land, details of the biodiverse roofs (including a specification and maintenance plan) to buildings 2, 3, 4, 5, 6, 7 and 8 as relevant to the respective area of land shall be submitted to and approved in writing by the Local Planning Authority and the development of the respective area of land shall not be carried out otherwise than in accordance with any such approval given.

Reason and pre-commencement justification: To ensure the proposed development would enhance the visual amenities of the locality and is designed for the maximum benefit of local biodiversity and to be in accordance with Policy S.4 of the Local Plan 2015.

32. **Landscaping Plan (including roof terrace)**

Prior to the construction of the development on either of the 14ML Land or W14 Land as shown on the appended phasing plan detailed drawings, the following information regarding the respective area of land shall be submitted to and approved in writing by the Local Planning Authority:

- Detailed drawings 1:50 and 1:10 of a hard and soft landscaping scheme showing the treatment of all parts of the site not covered by buildings and roof terrace areas (including wind mitigation measures, boundary treatments, surfacing materials of any parking, access, or pathways layouts, materials and edge details and material samples of hard landscaping);
- Full details of the treatment of site boundaries (including bollards)
- Details of all existing trees on and adjacent to the land, and details of any to be retained, together with measures for their protection, during the course of development;
- Waterside landscaping;
- Planting schedules;
- Sections through street tree pits;
- Species mix;
- Details of biodiversity enhancements (bird and bat nesting boxes etc);
- Details of the increased marginal habitat highlighted in Appendix 14.1 of the 2014 Environmental Statement
- Details including plans, elevations and specifications of any play equipment to be provided and
- Details of parapet/balustrade and planting buffer around the roof terrace

The planting, seeding and/or turfing shall be carried out as approved in the first planting season following completion of building works comprised in the development of the respective area of land and any tree or shrub that is found to be dead, dying, severely damaged or diseased within two years of the completion of the building works OR two years of the carrying out of the landscaping scheme (whichever is later), shall be replaced in the next planting season by specimens of similar size and species in the first suitable planting season. Planting shall comply to BS:4428 Code of practice for general landscaping operations, BS:3996 Nursery stock specification, BS:5837 Trees in relation to construction and BS:7370 Recommendations for establishing and managing grounds maintenance organisations and for design considerations related to maintenance. All other works including hard surface materials and play equipment shall be carried out and completed prior to the occupation of any part of the development.

Reason and pre-commencement justification: To ensure the proposed development would enhance the visual amenities of the locality and in order that the Local Planning Authority may ensure that the design and details are of high quality and to be in accordance with Policies BN.1 and BN.3 of the Local Plan 2015.

33. **Overheating**

Before the construction of the façades for the development on either of the W14 Land or the 14ML Land, an assessment of the internal temperature in summer of the development of the respective area of land shall be submitted to the Local Planning Authority, so as to demonstrate compliance with the requirements of the Building Regulations 2010 (Schedule 1, Part L) such assessment must use the method of calculation set out in the SAP 2012 (Appendix P) (or any subsequent edition of the SAP as may amend or replace the 2012 edition, as published by BRE). The assessment shall include details of any mitigation measures that are proposed to be used to reduce overheating, which shall include without limitation and where appropriate design of the facades; provision of ventilation; and internal layout. The mitigation measures shall be approved in writing by the Local Planning Authority. Following approval of the mitigation measures the building on the respective area of land shall be constructed in accordance with the approved details and maintained in this condition thereafter, unless other minor variations are agreed in writing by the Local Planning Authority.

Reason: To ensure a comfortable level of amenity for residents of the development and in the interests of visual amenity and to be in accordance with Policy S.4 of the Local Plan 2015.

34. **Photovoltaics**

Prior to the commencement of the development on either of the W14 Land or the 14ML Land full details of photovoltaic (PV) panels and a strategy for their installation on the respective area of land shall be submitted to and approved in writing by the Local Planning Authority. The development of the respective area of land shall only be carried out in accordance with the approved details. The approved details shall be implemented prior to the first use of the building on the respective area of land and shall thereafter be permanently maintained to the satisfaction of the Local Planning Authority.

Reason and pre-commencement justification: To ensure that the development incorporates renewable technologies and meets a high standard of sustainable design and construction and is in accordance with Policy 5.2 of the London Plan and Policies S2 and S5 of the Local Plan 2015.

**PERMITTED DEVELOPMENT**

35. **Land Use**

Notwithstanding the provisions of Part 3 of Schedule 2 of the Town and Country Planning (General Permitted Development) Order 2015 (or any Order revoking and/or re-enacting that Order) the commercial premises hereby approved shall only be used for the purposes specified in the application (being use class B1 as defined in the Schedule to the Town and Country Planning (Use Classes) Order 1987 (as amended at the date of this permission).

Reason: To provide control over the loss of employment generating use in accordance with the regeneration objectives for the Legacy Corporation area as set out in its purposes and within the Corporation's Local Plan.

In granting this permission the Local Planning Authority has had regard to the special circumstances of this case and wishes to have the opportunity of exercising control over any subsequent alternative use.



## **CAFÉ / BREAKOUT AREA**

### **36. Café / breakout hours of use**

The café / breakout area shall not operate outside the hours of 0800 to 2300 hours on any day.

Reason: In the interests of residential amenity and to be in accordance with Policy BN.1 of the Local Plan 2015.

### **37. Café / breakout external alterations**

Prior to the use of the café/breakout area, details of the external appearance and specification of the flue at roof top level to Building 1 shall be submitted and approved in writing by the Local Planning Authority and this shall be implemented before first use of the café/breakout area, and also retained in accordance with the approved details. No other external flue, ventilation equipment or any other external alteration shall be installed in relation to the café / breakout use without the prior written approval of the Local Planning Authority.

Reason: In the interest of visual and resident amenity and to be in accordance with Policy BN.1 of the Local Plan 2015.

### **38. Lighting**

No architectural lighting, security lighting or other external means of illumination of the site shall be provided, installed or operated in the development, except in accordance with a detailed scheme which shall provide for lighting that is low level, hooded and directional, and has been submitted to and approved in writing by the Local Planning Authority. The scheme shall be implemented in accordance with the approved details and retained thereafter.

Reason: In the interest of residential amenity and to protect ecological systems in accordance with Local Plan policy BN.3.

## **INFORMATIVES:**

1. This planning application has been assessed against current planning legislation only. The applicant (or any subsequent owner or developer) is therefore reminded that the onus of responsibility to ensure the development meets current fire safety regulations lies fully with them and that they are legally obliged to obtain the relevant Building Regulations consent.
2. In accordance with LFEPA guidance the applicant is strongly recommended to ensure the installation of sprinklers in the new development. The development shall conform with Part B5 of Approved Document B of the Building Regulations.
3. Include Thames Water and English Heritage Greater London Archaeology informatives.

## **Appendices**

Appendix 1 Marshgate PDC Report from 22<sup>nd</sup> May 2018

Appendix 2 Marshgate PDC - Update Report 22<sup>nd</sup> May 2018

Appendix 3 Marshgate PDC Report from 22<sup>nd</sup> May 2018 - original S73 drawings

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**Subject: Land at Marshgate Lane, Pudding Mill, Stratford, London E15 2NH,  
(Ref: 17/00669/AOD)**

**Meeting date: 22 May 2018**

**Report to: Planning Decisions Committee**

**Report of: Anne Ogundiya, Principal Planning Development Manager**

FOR DECISION

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**This report would be considered in public**

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**1. EXECUTIVE SUMMARY**

- 1.1.** The application site comprises two industrial sites, No. 14 Marshgate Lane (14ML), and the Marshgate Business Centre (W14) which would be demolished as part of the proposal. The application site has an area of approximately 1.33 ha and is bounded by Marshgate Lane to the west, Bow Back River and City Mill River to the south and east and the LLDC Legacy Communities Scheme (LCS) redevelopment land (Planning Delivery Zone 8 (PDZ 8)) to the north. The site is located within the Pudding Mill Local Centre.
- 1.2.** This report considers a submission under s73 of the Town and Country Planning Act 1990 (as amended) for variation to condition 2 (approved drawings) of planning permission reference 14/00422/FUL, which granted consent in May 2017 for a mixed-use development comprising 254 residential units within 8 new buildings (ranging from 3-12 storeys) and 4,257m<sup>2</sup> Gross Internal Area (GIA) of commercial floorspace on 3 floors within Buildings 1 and 2. The permission also includes associated landscaped public realm, basement, access, servicing, car and cycle parking, and safeguarded land for the route of a new link road from Stratford High Street across the Bow Back River.
- 1.3.** The proposed s73 variation to condition 2 (approved plans) is submitted as a result of the applicant carrying out a post planning permission viability assessment and a design development appraisal to determine the optimal amount of employment and residential space that the redevelopment could provide. The process revealed that the scheme would benefit from additional commercial floorspace, and that a number of the residential units were significantly oversized when compared to the minimum floorspace requirements set out within the national described space standards (DCLG Technical Housing Standards) and referred to in the Mayor's Housing Supplementary Planning Guidance (SPG) of March 2016.
- 1.4.** The s73 seeks to make changes to the approved scheme, described above, to increase the amount of commercial floorspace (Use Class B1) to 5,671m<sup>2</sup> (an additional 1,414m<sup>2</sup> (GIA)) representing an increase of 33% (within Buildings 1 and

2); the optimisation of the existing residential floorspace resulting in an increase in the number of residential units to 275 from the consented 254 units (an increase of 21 units or 8.3%); and alterations to the approved façades to align with the proposed new plan arrangements. The additional residential units are proposed to be contained within the existing building footprint and height.

- 1.5.** The residential component would comprise 71 x one bedroom units, 141 x two bedroom units, 50 x three bedroom units, 10 x studios, and 3 x four bedroom units. The proposal includes 19.6% (by unit) affordable housing overall, (17.7% under the extant consent 14/00422/FUL). It is noted that of the 21 proposed additional units, 9 units (42.8%) are affordable units. The development would provide 10% wheelchair accessible units. All of the 275 residential units would meet or exceed the nationally described space standards.
- 1.6.** The additional floor of commercial floorspace would be contained within Buildings 1 and 2 resulting in four floors of employment workspace compared to the consented three floors. The additional commercial floorspace would not increase the height or massing of the buildings, which would remain at 12 and 8 storeys respectively. These first four floors would form a Business Centre for small and medium enterprises which would be managed by the applicant, a workspace provider (Workspace 14 Ltd). Residential accommodation would continue as before to be provided above the Business Centre.
- 1.7.** In accordance with the Town and Country Planning (Environmental Impact Assessment) Regulations 2017 as amended (the EIA Regulations) the application was submitted with an addendum Environmental Statement. Officers and their environmental and transport consultants conclude that the amendments proposed in the s73 application have no additional environmental impacts when compared to the extant permission (14/00422/FUL).
- 1.8.** The key issues in considering the s73 variation to condition 2 are:

  - Additional Commercial Floorspace
  - Housing Mix, including affordable housing;
  - Design and Residential Quality;
  - Elevational changes; and
  - Overall impact of the proposed changes including any impacts resulting from the increased commercial space (transport matters, environmental matters)
- 1.9.** The principle of redevelopment of the site is supported by development plan policy and is established by virtue of the consented permission (14/00422/FUL).
- 1.10.** The s73 variation to condition 2 (approved drawings) is considered to be acceptable in terms of the proposed amendments to the approved plans, no changes are proposed to the footprint or height of the development. The increase in residential units would make a contribution to overall housing targets. The development would make provision for additional affordable housing on the site and a review mechanism has been agreed to re-assess any additional contribution that could be made over the agreed level at the time of delivery of the development, depending on viability. The increase in office floorspace would help support the provision of additional jobs over the consented scheme.
- 1.11.** Officers are also satisfied that the changes to the design would still ensure a suitably high-quality development and that the alterations to the layout would still ensure a

quality residential development. The proposed amendments would not result in any significant loss of amenity to surrounding residents or prospective residents within the scheme including in terms of outlook/daylight/sunlight or from increased activity. The changes, both individually and cumulatively, are also considered to be acceptable amendments to the extant consent on account of their scale and minimal impact.

- 1.12.** This is a significant regeneration project in the area and would maximise the use of previously developed land and would make a valuable contribution to both local housing needs and the local employment offer.
- 1.13.** The proposal is considered to be acceptable in planning policy terms and the scale and nature results in a development which is not substantially different from the one which has been approved. It is therefore recommended that the s73 application is approved subject to conditions and a deed of variation to the original s106 agreement in order to amend and tie the originally agreed obligations to the new permission. The applicant has been advised to submit a section s96A non-material amendment application, which would change the description of development so that it is consistent with the conditions as altered by the s73.

## **2. RECOMMENDATIONS**

### **2.1 The Committee is asked to:**

- a) **Approve the application for the reasons given in the report and grant planning permission subject to:**
- 1. the satisfactory completion of the deed of variation to the original legal agreement under s.106 of the Town and Country Planning Act 1990 and other enabling powers to secure the planning obligations set out in the recommended heads of terms which are set out in this report; and**
  - 2. the conditions set out in this report.**
- b) **Confirm that their decision has taken into consideration the environmental information addendum submitted in relation to the application, as required by Regulation 26(1) of the EIA Regulations:**
- c) **Agree to delegate authority to the Director of Planning Policy and Decisions to:**
- 1. Finalise the recommended conditions as set out in this report including such refinements, amendments, additions and/or deletions (including to dovetail with and where appropriate, reinforce, the final planning obligations to be contained in the deed of variation to the original section 106 legal agreement) as the Director of Planning Policy and Decisions considers reasonably necessary;**
  - 2. Finalise the recommended deed of variation to the original legal agreement under section 106 of the Town and Country Planning Act 1990 and other enabling powers as set out in this report, including refining, adding to, amending and/or deleting the obligations detailed in the heads of terms set out in this report (including to dovetail with and where appropriate, reinforce the final conditions and informatives to be attached to the planning permission) as the Director of Planning Policy and Decisions considers reasonably necessary; and**

3. Complete the deed of variation to the original section 106 legal agreement referred to above and issue the planning permission.
4. Determine the associated S96A application for non-material amendments referred to above.

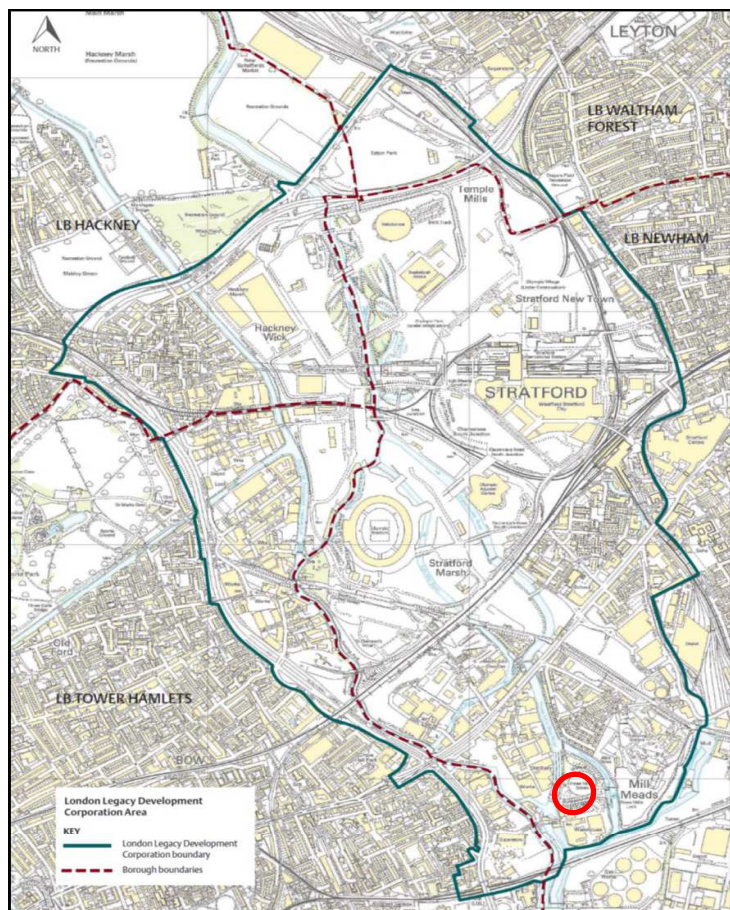
### 3. FINANCIAL IMPLICATIONS

3.1 There are no financial implications.

### 4. LEGAL IMPLICATIONS

4.1. The recommendation is that planning permission is granted, subject to conditions and completion of a deed of variation to the original s106 agreement in order to tie the originally agreed obligations to the new permission to ensure adequate mitigation of the impacts of the development. The contents of the deed of variation and summary of the original Heads of Terms is described in paragraphs 10.94.

#### SITE PLAN



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**Location:** Land at Marshgate Lane, Pudding Mill Stratford, London E15 2NH  
**London Borough:** Newham  
**Proposal:** Application under s73 of the Town and Country Planning Act 1990 (as amended) for the variation of Condition 2 (approved drawings) of

planning permission ref. 14/00422/FUL dated 5 May 2017 (comprehensive mixed use redevelopment comprising: demolition of existing buildings, and the erection of 8 buildings ranging from 3 to 12-storeys in height, comprising a total of 254 residential dwellings including affordable housing, and 4,257m<sup>2</sup> Gross Internal Area (GIA) of B1 (business) floorspace, together with basement, access, servicing, car parking, cycle parking, cycle storage, plant, open space and landscaping).

The variation to Condition 2 (approved drawings) seeks to make changes to the approved scheme to include: (1) increase of employment use from 4,257m<sup>2</sup> to 5,671m<sup>2</sup> (additional 1,414m<sup>2</sup> (GIA) floorspace); (2) decrease of residential floorspace from 28,003m<sup>2</sup> to 27,295m<sup>2</sup> (decrease of 708m<sup>2</sup> (GIA) floorspace); (3) optimisation of residential floorspace, increasing the number of residential units from 254 to 275; and (4) alterations to façades.

**Applicants:** Workspace 14 Limited  
**Agent:** Bilfinger GVA  
**Architects:** Squire and Partners

## **5. SITE & SURROUNDINGS**

- 5.1** The application site has an area of 1.33 ha and is irregular in shape. It contains No. 14 Marshgate Lane and the Marshgate Business Centre. It is bounded by Marshgate Lane to the west, Bow Back River and City Mill River to the south and east and vacant LLDC owned land to the north. The site is currently part open vacant land and part buildings forming the Marshgate Business Centre comprising buildings of up to four storeys in height but predominantly two and three storey which equates to a total floorspace of approximately 8,610m<sup>2</sup> gross internal area (GIA). Since the grant of planning permission for the redevelopment of the site in May 2017 a significant proportion of this floorspace is vacant with the remainder in B8 (storage and distribution), B1 (business) and B2 (general industry) use with 26 people employed in total. The site is located in the Pudding Mill Local Centre.
- 5.2** The buildings on the site are not listed nor is the site in or adjacent to any conservation area. The nearest Conservation Area is Sugar House Lane to the south of Stratford High Street (some 83.75m away to the south east of the application site).
- 5.3** To the north and west of the site much of the land is vacant having been in transport and logistics related uses during the 2012 Games. Much of this land is included in the Legacy Communities Scheme (LCS) outline planning permission (see History section 6). Further to the north is the redeveloped Pudding Mill Lane DLR station.
- 5.4** To the east and south of the site across the Bow Back and City Mill Rivers the area is characterised by large scale existing or projected residential led developments. To the north east on Blaker Road is Otter Close, a four-storey flatted development. To the east is City Mill Lock and Lock Keeper's Cottage where a four-storey building of flats has recently been completed. To the south east and fronting Stratford High Street is a seven and eight storey mainly residential building known as The Lock Building the rear elevation of which faces the application site. Adjoining the Lock Building to the south west is 68-70 Stratford High Street (former Porsche garage) which is currently vacant but with planning permission for residential led redevelopment (see History section 6). Further major schemes are under construction or recently

completed in the vicinity on Stratford High Street including 27 storey development at Stratford Edge, 80-82 Stratford High Street (Weston Homes) and the 31 and 16 storeys development at 2-12 Stratford High Street (Galliard).

- 5.5** Access to the site is from Marshgate Lane which is a vehicular route from Stratford High Street into the Queen Elizabeth Olympic Park (QEOP). At Stratford High Street access is only from and to the eastbound carriageway. The recently enlarged Pudding Mill DLR station is approximately 250m to the north with access from Marshgate Lane. Bromley by Bow station (District Line) is 1.1km to the south west and Stratford Station is 1.6km away to the north east. Stratford High Street is served by five bus routes and the Cycle Superhighway 2 (Stratford to Aldgate).
- 5.6** An existing below ground sewer and easement area is located to the western corner of the site and precludes development in that area. The application site forms part of the wider Pudding Mill Lane development area bounded by Bow Back River and City Mills River to the south and east, River Lea to the west and Great Eastern railway to the north.
- 5.7** The site is in two separate land ownerships: Marshgate Business Centre (W14); and 14 Marshgate Lane (14ML). The applicant owns and operates the majority of the application site (approximately 80%), which is Marshgate Business Centre. 14 Marshgate Lane to the north is owned by another party. A plan showing the different land ownerships is appended to this report.

## **6. RELEVANT PLANNING HISTORY**

- 6.1** A planning application for demolition of the existing buildings, and the erection of 8 buildings ranging from 3 to 12 storeys in height, comprising a total of 254 residential dwellings and 4,257m<sup>2</sup> Gross Internal Area (GIA) of B1 (business) floorspace, together with basement, access, servicing, car parking, cycle parking, cycle storage, plant, open space and landscaping was approved in May 2017
- 6.2** Relevant planning permissions on adjacent sites which have not been implemented are set out below and shown in the appendices.
- 6.3** Legacy Communities Scheme (LCS) (11/90621/OUTODA)
- 6.4** This is an outline permission covering 64 ha of the QEOP for a comprehensive residential led mixed use scheme to be delivered in phases over a period to 2031.
- 6.5** The parcel of land adjacent to the north and west of the current application site is known as Planning Delivery Zone (PDZ) 8 and has outline consent for up to 118,290m<sup>2</sup> of residential (Class C3) floorspace, 5,045m<sup>2</sup> of retail (Class A1-A5) floorspace, 23,791m<sup>2</sup> of office (Class B1a) floorspace, 12,158m<sup>2</sup> light industrial (Class B1b / B1c) floorspace, 169m<sup>2</sup> leisure (Class D1) and 1,482m<sup>2</sup> community (Class D2) floorspace, in buildings of up to 39 metres AOD (approximately 11 / 12 storeys in height) in the parcels closest to the application site and up to 53 metres (AOD) (approximately 17 storeys in height) adjacent to Pudding Mill Station. The approved phasing for PDZ 8 is for construction between 2022 and 2031.
- 6.6** The design principles for the proposed PDZ 8 LCS development are to create a neighbourhood mixing a range of uses (residential, office, light industrial/research and development etc), with a permeable block structure and a flexible framework to accommodate future change.
- 6.7** 68-70 Stratford High Street (former Porsche garage site) (11/90619/FUMODA)



**6.8** This is a full planning permission for the demolition of existing buildings and erection of new development comprising linked buildings of one, five, nine and eighteen storeys to provide 731m<sup>2</sup> of commercial floorspace (for use within classes A1, A2, A3, B1, D1 and D2) at ground floor and 173 residential units, with 36 car-parking spaces, 213 cycle parking spaces, refuse and recycling facilities, access, landscaping and amenity areas.

**6.9** The approved building heights are:

- 5 storey block = 21.8m AOD = approximately 16.8m above ground level
- 9 storey block = 33.8m AOD = approximately 28.8m above ground level
- 18 storey block = 62.3m AOD = approximately 57.3m above ground level

**6.10** The permitted scheme includes a safeguarded area between Stratford High Street and Bow Back River for the approach to a pedestrian / cycle / bus bridge which would link with the proposed safeguarded area in the current Marshgate Lane application site.

#### **6.11 Planning History - Strategic Industrial Land**

**6.12** Pudding Mill Lane has historically been designated Strategic Industrial Land; with the Newham Core Strategy (2012) within its strategic site allocation S09 identifying the location would be de-designated in order to realise its regeneration potential for mixed use comprising employment uses, residential and community uses with a new centre focused around Pudding Mill DLR station. The Legacy Corporation Local Plan (July 2015) Pudding Mill site allocation (SA4.3) has now replaced this policy and while continuing much of the previous policy position in relation to the required mix and type of uses, does not identify Pudding Mill as strategic industrial land.

#### **6.13 ENVIRONMENTAL STATEMENT**

**6.14** An Environmental Statement (ES) accompanied the extant planning permission (14/00422/FUL) to assess the likely significant effects of the proposed development. Following the proposed s73 variation to condition 2 (approved drawings) an Environmental Statement Addendum has been submitted in accordance with the Town and Country Planning (Environmental Impact Assessment) Regulations 2017 (the EIA Regulations).

**6.15** The scope of the ES addendum includes updates to the following topics:

- Socio-economics
- Transport and access
- Noise and vibration
- Air quality
- Water resources and flood risk
- Ground conditions and contamination
- Archaeology and built heritage
- Ecology
- Wind and micro climate
- Daylight, sunlight and overshadowing
- Townscape and visual impact
- Cumulative effects

**6.16** The ES addendum has been reviewed by PPDT's Environmental and Transport Consultants, who along with officers are satisfied that it appropriately describes the environmental effects of the development and identifies appropriate mitigation measures, which would be secured by conditions. PPDT conclude that the amendments proposed in the s73 application have no significant adverse additional environmental impacts when compared to the extant permission 14/00422/FUL.

## **7 DESCRIPTION OF DEVELOPMENT**

**7.1** Application under s73 of the Town and Country Planning Act 1990 (as amended) for a minor material amendment to condition 2 (approved drawings) of planning permission reference 14/00422/FUL granted on the 5<sup>th</sup> May 2017 for: -

**7.2** Demolition of existing buildings, and the erection of 8 buildings ranging from 3 to 12-storeys in height, comprising a total of 275 residential dwellings (27,295m<sup>2</sup> (GIA) floorspace) including affordable housing, and 5,671m<sup>2</sup> Gross Internal Area (GIA) of B1 (business) floorspace, together with basement, access, servicing, car parking, cycle parking, cycle storage, plant, open space and landscaping)).

**7.3** The applicant has been advised to submit a section s96A non-material amendment application, which would change the description of development so that it is consistent with the conditions as altered by the s73.

### **7.4 Summary of Extant Consent (14/00422/FUL)**

**7.5** The extant consent consists of 8 building designed around the proposed safeguarded link to Stratford High Street and a series of linked, landscaped spaces leading to the Bow Back River frontage (see appendices). Two of the buildings at the west of the site would be linked (Buildings 1 and 2) and would be the only mixed used buildings (residential and commercial) within the development. Building 1 would be 12 storeys (40m high) and Building 2 (27.4m high) eight storeys.

**7.6** The other 6 buildings to the north and east would be purely residential blocks and range from 3 to 8 storeys arranged into 5 residential apartment buildings (6 - 8 storeys) and 1 x 3 storey town-house block. The residential accommodation would comprise 233 apartments, 14 maisonettes and 7 townhouses with associated basement car parking, access and landscaping. A business centre (B1a use) is proposed to be provided within the mixed-use buildings 1 & 2.

**7.7** The consented scheme includes: associated open space and landscaping; a taxi drop off / servicing bay on Marshgate Lane close to the main entrance to the business centre (the commercial units could also be serviced from the yard proposed at the south of Buildings 1 and 2); a site management office at the north-west corner of Building 2 which would act as delivery point and allow controlled vehicle access to Bow Back Street; and provision for safeguarding a 15m wide area within the site on the south side of the river for a future bridge link. These elements remain unchanged in the s73 variation.

### **7.8 Proposed s73 Variation Amendments**

**7.9** Buildings 1 and 2 (Employment and Residential Buildings): As consented the three-storey podium to these buildings proposes an employment building providing 4,257m<sup>2</sup> floorspace of B1 (business) with the main entrance on the Marshgate Lane / Pudding Mill Lane corner with reception / café, gallery and collaborative space. On the south

side of the building there would be a working yard accessed from Marshgate Lane and the proposed Marshgate Square for use of the commercial occupiers.

- 7.10** As consented, the central space on top of the podium between the two buildings is a shared amenity space for the residential occupiers. The fourth floor above the podium contains 6 residential flats, and is set back from the lower podium and the upper seven residential floors in Building 1 and upper three residential floors in Building 2.
- 7.11** s73 Variation – Buildings 1 & 2 - Employment Building: The s73 variation proposes an additional 1,414m<sup>2</sup> to the employment building by removing the central space on top of the podium between the two buildings and replacing it with commercial floorspace; increasing the total amount of commercial floorspace to 5,671m<sup>2</sup>.
- 7.12** s73 Variation Residential Buildings 1 and 2: Arising from the reconfiguration of the podium, the 6 residential units and shared amenity space originally proposed on the fourth floor above the podium would be removed to accommodate the proposed additional commercial floorspace. **Five additional units** would be provided in Building 1 and **1 additional unit** in Building 2. These additional units are provided on levels 5 to 8, and this is possible because the units in the extant consent were oversized compared to the national described space standards.
- 7.13** Building 3: As consented Building 3 is eight storeys (27.5m) in height and sited along the east side of the proposed Marshgate Square. It is wholly residential and as consented contains 56 residential units.
- 7.14** s73 Variation – Building 3: Building 3 remains eight storeys (27.5m, however it would contain **60** residential units. **An additional four units** are proposed. This is achieved by reducing the originally consented lift cores from two to one, which would be located centrally within the building. Reconfigured it would still include some maisonettes/duplexes with associated entrances on the lower floors. Nine affordable housing units would be accommodated within 2 floors of the block.
- 7.15** Building 4: As consented building 4 is six storeys (20.6m high), containing 23 units and sited approximately at right angles to Building 3.
- 7.16** s73 Variation – Building 4: Building 4 remains six storeys (20.6m high), however it would contain 29 residential units. As with Building 3 the second core in the consented scheme has been removed with the remaining single core located centrally within the building. As a result, **six additional units** are proposed within this building. Reconfigured there would be five residential units on a typical floor.
- 7.17** Building 5: As consented building 5 (town houses) comprises seven three storey (10.6m high) town houses with private rear gardens.
- 7.18** s73 Variation – Building 5: Building 5 has been altered to take account of the boundary between the two separate land owners more clearly. The number and mix of units remains unchanged albeit there is a change to the interior layout to account for the change in boundary, which involves the minor realignment of two entrances and adjoining rooms.
- 7.19** Building 6: As consented Building 6 is six storeys (20.6m), containing 22 units and lies approximately parallel to Buildings 4 and 8.
- 7.20** s73 – Building 6: As with all the buildings, the footprint and height has been maintained but with optimisation internally to the apartment layouts following the

removal of one of the two cores shown in the consented proposals. An additional unit has been added on each floor, including a duplex on ground floor. As a result, **five additional units** are proposed overall in this building resulting in a total of 27units.

- 7.21 Building 7:** As consented building 7 is eight storeys (27.4m) and is aligned with Building 3 fronting Marshgate Square and Marshgate Lane. The ground floor includes the refuse collection point for the residential part of the development. The main external material is brick with recessed balconies. As consented, it contains 45 affordable residential units (17x1 beds; 14x2 beds; 13x3 beds; 1x4 beds).
- 7.22 s73 – Building 7:** The plan arrangement of Building 7 has altered to meet the alignment of the ownership boundary between the two land ownerships, with a party wall between the two parts of the building introduced along the boundary. Whilst this has altered the arrangement of the apartments the number of units which are all affordable in the building overall remains the same.
- 7.23** At the ground floor, the redesign of refuse arrangements has meant that one apartment has been altered from being a duplex unit to being a residential unit at first floor only, to enable more room for refuse storage on the southern side of the land ownership boundary.
- 7.24** As consented refuse and recycling collection would be from a centralised storage area at Building 7 with refuse collection from a servicing bay on Marshgate Lane. The refuse strategy remains as previously, but with the changes at ground floor, the refuse spaces have now been split between the two land ownerships. This has required that the lift from the basement be moved to be better located for the removal of refuse to the collection point.
- 7.25 s73 – Building 8:** Building 8 remains unchanged as a result of the s73 variation.
- 7.26 s73 - Elevational Changes:** The façade treatment to the buildings in the development remains the same as consented i.e. brick finish to buildings 3 to 8 and GRP panels and glass with vertical pier lines carried down to the ground to buildings 1 and 2. The recessed balcony typology and the distribution of windows is similar, albeit with more windows introduced to ensure daylight to additional rooms/units.

**Table 1 – Summary of Changes**

	<b>Extant Permission 14/00422/FUL</b>	<b>s73 Variation to condition 2 - approved drawings</b>	<b>Difference</b>
<b>Residential units</b>	254	275	Increase of 21 units (8.3%)
<b>Residential Floorspace</b>	23,239m <sup>2</sup> (GIA)	22,531m <sup>2</sup> (GIA)	-708m <sup>2</sup> (GIA) (3%)
		For changes to residential unit mix (see Table 2 below)	
<b>B1 Floorspace</b>	4,257m <sup>2</sup> (GIA)	5,671m <sup>2</sup> (GIA)	Increase of 1,414m <sup>2</sup> (33%)
<b>Car Parking</b>	29 spaces (residential and commercial)	Unchanged	

<b>Cycle Parking</b>	588 spaces (residential and commercial)	595 spaces (residential and commercial)	+7
<b>Amenity Space</b>	2,109m <sup>2</sup>	2,036m <sup>2</sup>	-73m <sup>2</sup> (3.4%)
		Alterations to façades*	

\*No changes are proposed to the height, scale and mass of the development as approved under 14/00422/FUL

**7.27 Affordable Housing:** The scheme proposes a total of 54 affordable units, an increase of 9 units over the originally consented 45 affordable housing units. The tenure and mix of units is set out in table 2 below:

**Table 2 - Overview of Residential Mix and Affordable Housing**

Unit Type	Extant Permission					s73				
	Number of Units			Percentages		Number of Units			Percentages	
	Market	Affordable	Totals	Affordable	All Units	Market	Affordable	Totals	Affordable	All Units
Studio	0	0	0	0	0	10	0	10	0	4
1 Bed	83	17	100	38	39	53	18	71	33	26
2 Bed	62	14	76	31	30	119	22	141	41	51
3 Bed	60	13	73	29	29	37	13	50	24	18
4 Bed	4	1	5	2	2	2	1	3	2	1
<b>Total Number of Beds</b>	<b>209</b>	<b>45</b>	<b>254</b>	<b>100%</b>	<b>100%</b>	<b>221 (+14)</b>	<b>54 (+9)</b>	<b>275</b>	<b>100%</b>	<b>100%</b>

## 8. POLICIES & GUIDANCE

### 8.1 National Planning Policy Framework (NPPF)

The policies in the NPPF are material considerations in the determination of applications. The NPPF recommends that as of April 2013, due weight should be given to relevant local plan policies according to their degree of consistency with the NPPF. The principle of sustainable development permeates the Framework. The Framework makes clear that local authorities should be positive and proactive in encouraging sustainable growth and addressing barriers to investment. The NPPF should be read in conjunction with Planning Practice Guidance, a new web-based resource for all users of the planning system. This describes the importance of good design and how this can be achieved through planning decisions.

The following NPPF policies are relevant to this submission:

1. Building a strong, competitive economy
4. Promoting sustainable transport

- 6. Delivering a wide choice of high quality homes
  - 7. Requiring good design
  - 8. Promoting healthy communities
  - 10. Meeting the challenge of climate change, flooding and coastal change
  - 11. Conserving and enhancing the natural environment
  - 12. Conserving and enhancing the historic environment
- Ensuring viability and deliverability (paragraph 173)

**8.2** For the purposes of S.38(6) of the Planning and Compulsory Purchase Act 2004, the adopted 'Development Plan' for this site comprises the London Plan (March 2015) and the London Legacy Development Corporation's Local Plan 2015-2031 (August 2015).

**8.3** Certain planning guidance documents are also relevant and have been taken into consideration. These are referred to below.

**8.4** **The most relevant policies are listed below:**

**8.5** **London Plan (2016):**

The following London Plan policies are relevant to this submission:

- Policy 3.4 Optimising Housing Potential
- Policy 3.5 Quality and Design of Housing Developments
- Policy 3.9 Mixed and Balanced Communities
- Policy 3.10 Definition of Affordable Housing
- Policy 3.12 Negotiating Affordable Housing on Individual Private Residential and Mixed-Use Schemes
- Policy 3.12 Affordable Housing Thresholds
- Policy 4.1 Developing London's Economy
- Policy 4.12 Improving Opportunities for All
- Policy 5.2 Minimising Carbon Dioxide Emissions
- Policy 5.3 Sustainable Design and Construction
- Policy 5.5 Decentralised Energy Networks
- Policy 5.6 Decentralised Energy in Development Proposals
- Policy 5.7 Renewable Energy
- Policy 5.9 Overheating and Cooling
- Policy 5.12 Flood Risk Management
- Policy 5.13 Sustainable Drainage
- Policy 5.15 Water Use and Supplies
- Policy 5.17 Waste Capacity
- Policy 6.3 Assessing the Effects of Development on Transport Capacity
- Policy 6.9 Cycling
- Policy 6.10 Walking
- Policy 6.13 Parking
- Policy 7.1 Lifetime Neighbourhoods
- Policy 7.2 An inclusive environment
- Policy 7.4 Local character
- Policy 7.5 Public realm
- Policy 7.6 Architecture
- Policy 7.7 Location and Design of Tall and Large Buildings
- Policy 7.8 Heritage Assets and Archaeology
- Policy 7.14 Improving Air Quality
- Policy 7.15 Reducing Noise and Enhancing Soundscapes
- Policy 7.19 Biodiversity and access to nature
- Policy 7.30 London's canals and other rivers and waterspaces

- Policy 8.2 Planning Obligations

#### **8.6 LLDC Local Plan (July 2015):**

- Policy SP.1 Building a strong and diverse economy
- Policy SA4.3 Pudding Mill
- Policy B.1 Location and maintenance of employment uses
- Policy H.1 Providing a mix of housing types
- Policy H.2 Delivering affordable housing
- Policy SP.2 Maximising housing and infrastructure provision
- Policy SP.3 Integrating the built and natural environment
- Policy BN.1 Responding to place
- Policy BN.4 Designing residential schemes
- Policy BN.5 Requiring inclusive design
- Policy BN.8 Maximising opportunities for play
- Policy BN.10 Proposals for tall buildings
- Policy T.2 Transport Improvements
- Policy T.4 Managing development and its transport impacts to promote sustainable transport choices and prioritise pedestrians and cyclists
- Policy T.5 Street Network
- Policy T.6 Facilitating local connectivity
- Policy T.7 Transport assessments and travel plans
- Policy T.8 Parking and parking standards in new development
- Policy T.9 Providing for pedestrians and cyclists
- Policy S.2 Energy in new development
- Policy S.3 Energy infrastructure and heat networks
- Policy S.4 Sustainable design and construction
- Policy S.6 Waste reduction
- Policy S.7 Overheating and urban greening
- Policy S.8 Flood risk

#### **8.7 Other Relevant Material Considerations**

- Mayor of London - Housing SPG (2016)
- Mayor of London – Affordable Housing and Viability SPG (2017)
- LLDC Planning Obligations SPD (2015)
- LLDC Pudding Mill SPD (2017)

#### **8.8 The Draft London Plan (December 2017)**

The Mayor of London published for the purpose of public consultation a draft new London Plan on 29th November 2017. The policies in the draft new London Plan currently have only very limited material weight when making planning decisions. That weight will increase once the new Plan is submitted for its Examination in Public. This report may make reference to policies within the new London Plan where they are directly relevant to the assessment of the application proposal. However, the relevant development plan policies remain those within the current London Plan (March 2016) and the LLDC Local Plan (July 2015).

### **9. CONSULTATION**

- 9.1** The application was advertised in the press, published in the Newham Recorder on 17th January 2018. A total of 4 site notices were displayed in and around the site. 368 neighbouring residential and commercial properties were consulted by post.

- 9.2** Details of the responses received are set out in the paragraphs below.
- 9.3 Responses have been received from the following statutory and non-statutory consultees as set out below:**
- 9.4 Greater London Authority (GLA)**
- The GLA confirmed that given the scale and nature of the proposals that the amendments did not give rise to any new strategic planning issues. However, they asked that the LLDC continue to ensure that the scheme does deliver the maximum reasonable amount of affordable housing it can in accordance with London Plan and draft London policy.
- 9.5 L B Newham Planning (LBN)**
- No comments received
- 9.6 L B Newham (Waste Management)**
- Waste capacity should follow the guidance in BS 5906 for residential waste and should include an extra 25-30% capacity for recycling
- 9.7 L B Tower Hamlets Planning (LBTH)**
- No comments to make
- 9.8 L B Tower Hamlets Environmental Health**
- No adverse comments to make
- 9.9 Transport for London (TfL)**
- TfL has commented that the amended scheme should adopt the amended cycle parking standards which are in the draft London Plan. While the consented provision was slightly above previous London Plan standards the new standards require that 1 bed units provide 1.5 spaces per unit and the commercial element provide 1 space per 75m<sup>2</sup>.
- 9.10 TfL (Infrastructure Team)**
- No comments
- 9.11 Canal and Rivers Trust (CRT)**
- Responded to say ‘...cannot see changes that are likely to have any further impact on the waterway, and therefore have no comments to make...’
- 9.12 HSE**
- No Comments
- 9.13 Historic England**
- Do not consider that it is necessary for this application to be notified to Historic England
- 9.14 Historic England Greater London Archaeological Advisory Service (GLASS)**
- Do not consider it necessary to comment.
- 9.15 London City Airport**



- No safeguarding objection to the development.

**9.16 London Fire and Emergency Planning Authority (LFEPA)**

- Satisfied with the proposals in relation to the fire precautionary arrangements for access and water supplies providing they both comply with Approved Document b Section B5;
- Extra consideration should be given to water supplies from hydrants; and
- Attention is drawn to landscaping and other features so as not to impede fire brigade access to premises

**9.17 Metropolitan Police Service**

- No further comments to make at this stage in relation to the amendments.

**9.18 Natural England:**

- No comments to make

**9.19** No comments have been received at the time of the report going to print from the following:

DCLG; Docklands Light Railway; Environment Agency; EDF Energy; Cofely East London Energy; CGMS Consulting; Crossrail; Secure by Design; London Wildlife Trust; London Cycling Campaign; HS1; National Grid; London Ambulance Service; UK and London Power Networks; Lea Rivers Trust; Health Protection Agency; Thames Water; Network Rail, and BT UK.

**10. ASSESSMENT OF PLANNING ISSUES**

**10.1** Principle of Development:

**10.2** The application is made under section 73 of the Town and Country Planning Act 1990 (as amended). Applications made under s73 can remove conditions attached to an existing planning permission or vary their wording. This application seeks to vary condition 2 (approved drawings) of the existing consent which requires the development to be carried out in accordance with the approved plans. In effect, this allows previously approved drawings to be substituted with new drawings. It should be noted that the s73 does not allow a change to the description of development. The applicant has been advised to submit a s96A non-material amendment application, which would change the description of development so that it is consistent with the conditions as altered by the s73.

**10.3** The changes have been put forward by the applicant as the design detail of the development has progressed following the issuing of planning permission in May 2017 under application reference 14/00422/FUL. This process has resulted in the rationalisation of the floorspace arrangements of the extant scheme as described in section 7.

**10.4** The National Planning Policy Guidance (NPPG) states that amendments secured under a s73 application must be 'minor-material amendments' to the existing consent. There is no statutory definition of a 'minor material amendment' but NPPG states it is likely to include any amendment where its scale and/or nature results in a development which is not substantially different from the one which has been approved.

**10.5** The principle of the loss of the previous uses on site including office and warehouse accommodation and the erection of a residential led mixed-use development,

including flexible commercial floor space, was established under the extant consent (application reference 14/00422/FUL). The proposed alterations relate to the changes to the volume of Buildings 1 to 2 at Level 3 with the provision of an additional complete commercial floor and minor alterations to the external façades of the buildings to accommodate changes to the internal layout of some of the buildings arising from the additional residential accommodation.

**10.6** Given the nature of the s73 proposals, i.e. they do not propose additional height, or alter building footprints, Officers are satisfied that the scale and nature of the proposed s73 amendment would not be substantially different from the extant consent and the proposed minor material amendments are considered both individually and cumulatively to be acceptable.

**10.7** This assessment would therefore not revisit every aspect of the proposed development, only the proposed amendments to the approved plans are assessed in the context of current relevant planning policy/guidance. Officers and their transport and environmental consultants have assessed the proposed amendments proposed in the s73 application as having no significant adverse additional environmental impacts when compared to the consented scheme.

**10.8** The main issues in respect of this application are therefore considered to be as follows:

- Additional Commercial Floorspace
- Housing Mix, including affordable housing;
- Design and Residential Quality;
- Elevational changes; and
- Overall impact of the proposed changes (transport matters, environmental matters)

#### **10.9 Additional Commercial Floorspace**

**10.10** The extant consent (14/00422/FUL) provides a consolidated commercial floorspace offer within the first three floors of Buildings 1 and 2, with a floorspace of 4,257m<sup>2</sup> Gross Internal Area (GIA).

**10.11** The current s73 commercial offer increases this to four floors resulting in a total commercial floorspace in Buildings 1 and 2 of 5,671m<sup>2</sup> Gross Internal Area (GIA). Overall, the applicant is providing significantly more B1 employment space (5,671m<sup>2</sup>) than is currently available on the site (1,444m<sup>2</sup>) and over that provided by the extant scheme. The increase over the consented level of commercial space would be achieved by in-filling the void area at podium level with the additional commercial floorspace.

**10.12** The applicant, Workspace 14 Limited, would manage the commercial workspace. They are a reputable workspace provider and currently provide circa 500,000m<sup>2</sup> of managed business accommodation for new and growing companies across 25 London Boroughs. Their portfolio consists of a range of sizes and types of space including converted factory buildings, co-working hubs and new bespoke purpose built managed business centres.

**10.13** Given not only the track record of Workspace 14 Limited, but the size of the site, its accessible location (within walking distance of Pudding Mill station and the High

Street bus routes), and its Pudding Mill Local Centre location, the additional commercial floorspace is considered to be appropriate.

- 10.14** In the context of the commercial provision approved in the extant scheme, and given the configuration and location of the additional B1 floorspace, Officers are satisfied that any additional activity would not be detrimental to the amenity of prospective occupiers of the development.
- 10.15** Employment: The s73 variation would result in an additional 1,4141m<sup>2</sup> to the office floorspace. Based on the Homes and Communities Agency’s document, ‘Employment Densities Guide’ (2010), such additional B1 floorspace could equate to an additional 117 full time jobs. The extant consent is calculated as providing 337 full time jobs. Officers are satisfied that there is a significant increase in employment density on the site consistent with Strategic Local Plan policy SP.1 and London Plan policies 4.1 and 4.12. The additional floorspace is therefore considered to be acceptable.
- 10.16** Officers are satisfied that the proposals support the objectives of the Local Plan site allocation (SA4.3) in so far as contributing to achieving the overall target of 25% non-residential floorspace within the wider Pudding Mill area, and are in step with national aspirations.
- 10.17** The proposed additional commercial floorspace is considered to be acceptable and is not substantially different in nature from the extant consent and complies with London Plan policies 3.3, 4.1 and 4.12 and Local Plan policies SA4.3, SP.1, SP.2, B1, B2, B6, H1 and H2.
- 10.18 Housing Mix**
- 10.19** Local Plan Policy H.1 requires development proposals to provide at least 50% of units at two bedrooms or more. The s73 variation complies with policy in that it proposes that 71% of the total number of residential units would be 2 bedroom units or over (see table 4 below). This is compared to 61% under the extant permission, where there was a higher proportion of one, and also three bedroom units (see table 3 below).
- 10.20** The original residential development mix within the consented scheme is as follows:

**Table 3**

**Extant Consent 14/00422/FUL - Unit Mix**

<b>Unit Type</b>	<b>Total Units</b>	<b>% Provision</b>
Studio	0	0%
1 bed	100	39%
2 bed	76	30%
3 bed	73	29%
4 bed	5	2%
<b>TOTAL</b>	<b>254</b>	<b>100%</b>

- 10.21** The proposed s73 variation residential development mix is as follows:

**Table 4**

**s73 Variation – Unit Mix**

<b>Unit Type</b>	<b>Total Units</b>	<b>Adjustment</b>	<b>% Provision</b>
Studio	10	+10	4%
1 bed	71	-22	26%
2 bed	141	+65	51%
3 bed	50	-23	18%
4 bed	3	-2	1%
<b>TOTAL</b>	<b>275</b>		<b>100%</b>

- 10.22** As demonstrated in the table above, the proposed scheme assumes a reduction in 1 & 3 bed units underpinning a significant increase in 2 bed units. As per the previous consented scheme residential units would be delivered across all eight blocks. The increase in overall residential unit numbers would be achieved despite a decrease in floorspace of 708m<sup>2</sup> (GIA), bringing the average unit size down from 87m<sup>2</sup> to 78m<sup>2</sup>. Each residential property would still meet the nationally described space standards (DCLG Technical Housing Standards) as referred to in the Mayor’s Housing SPG.
- 10.23** The Housing Requirements Study undertaken as part of the Local Plan review has identified that within the LLDC area there is greatest need for 2-bedroom market homes, followed by 2 and 3 bed intermediate and then 2 and 3 bedroom low cost rent. However, as LLDC are providing for the strategic need for housing across London and strategically there is demand for all sizes and tenures but those in particular demand are 1 bedroom low cost rent units, 2-bedroom market homes and then intermediate tenures across all bedroom sizes.
- 10.24** The s73 proposals provide a significant proportion of 1 and 2 bedroom units (circa 30% and 50%+ respectively). However, it would result in fewer units being 3 bedrooms or above (total 53 units or 19%) which the applicant has advised is on the basis of changes to market demand since the original proposals were approved, and to optimise deliverability. Nevertheless, the 21% increase in the 2-bedroom unit provision over the extant permission is welcomed, and addresses the need identified in the Local Plan Review Housing Requirement Study. Officers consider the proportion of family units (2 or more bedrooms) to be sufficient and it responds to the identified need and accords with Local Plan Policy H.1 in so far as requiring half of the total provision of residential units to be 2-bedroom or more.
- 10.25** Additionally, policies H.1 along with London Plan policy 3.8 require a minimum of 10% of the development to be accessible wheelchair accommodation. The proposals include a 10% provision of wheelchair accessible homes which equates to 27 dwellings, and they are comprised of a mix of unit sizes.
- 10.26** Officers have assessed the site circumstances, including location and viability. Officers are satisfied that in line with policy objectives the s73 variations in terms of unit mix and tenure are acceptable and the proposals overall provide an appropriate balance and mix of units in accordance with local plan policy.

## 10.27 Affordable Housing

- 10.28** The Mayor's Affordable Housing and Viability (AHV) SPG requires schemes that do not meet the 35% affordable housing threshold, to submit to the LPA and where relevant the Mayor, detailed viability information. In this instance, the GLA '...ask that the LLDC continue to ensure that the scheme delivers the maximum reasonable amount of affordable housing it can in accordance with London Plan and draft London Plan policy...'
- 10.29** As the s73 proposals do not deliver 35% affordable housing the applicant has submitted a financial viability appraisal in accordance with the 'viability tested route' set out in the AHV SPG, to demonstrate how much affordable housing the scheme can deliver. The appraisal has been reviewed by PPDT's Viability Consultants.
- 10.30** London Plan policy 3.12 and the AHV SPG requires planning authorities to seek the maximum reasonable amount of affordable housing when negotiating on individual private residential and mixed-use schemes having regard to affordable housing targets; the need to promote mixed and balanced communities; and the size and type of affordable housing needed. Local Plan policy H2 requires affordable housing to be maximised on sites with 10 or more units broken down as 60% affordable rent / social rent and 40% intermediate. The supporting text of policy H2 identifies a minimum target of 35% across the LLDC area.
- 10.31** Extant Consent (14/00422/FUL) Affordable Housing Provision: The extant consent provides for a total of 45 on-site affordable units, which equates to 17.7% by unit. The tenure split, is set out in table 5 and is summarised as providing 29 affordable rent units and 16 intermediate/shared ownership units, which equates to a 64:36 split.

**Table 5**

### **Affordable Housing Unit Mix Provision (s73 variation & extant consent)**

<b>Unit Type</b>	<b>Extant Consent</b>	<b>s73 Variation</b>	<b>% Provision</b>
Studio	0	0	0%
1 bed	17	18	34%
2 bed	14	22	40%
3 bed	13	13	24%
4 bed	1	1	2%
<b>TOTAL</b>	<b>45</b>	<b>54</b>	<b>100%</b>

- 10.32** s73 Proposal: The s.73 variation proposes an increase in the overall affordable housing provision from 45 units (17.7%) in the extant consent to *initially* 53 units (19.2%). This would result in an increase in an affordable housing provision uplift of 1.5% over the consented scheme. With respect to tenure mix, the provision would equate to a 60:40 split (32 affordable rent units / 21 intermediate/shared ownership units).
- 10.33** The affordable housing offer takes into account costs to the development arising from scheme design (including the provision of basement car parking); the likely land remediation costs resulting from its industrial history; and the provision of a significant quantum of commercial floorspace built on a 'workspace model' that aligns with the Mayor's convergence commitment in so far as creating long term

jobs which would contribute to not just the local economy and regeneration of Pudding Mill but to the wider area including the neighbouring Growth Boroughs.

- 10.34** However, following a review of the applicant's financial viability appraisal, PPDT's viability consultant argued that an additional 3 units could be provided and be converted to the affordable housing tenure at a ratio of 34 affordable rent and 22 intermediate/shared ownership units increasing the overall provision to 56 units (20.3%). As with the overall housing mix, the proposed affordable housing mix (table 5) reflects a significant increase in the number of 2 bedroom units.
- 10.35** Subsequent discussions and negotiations with the applicant have led to an increase in the affordable housing offer from their initial position of 19.2% (53 units) to their final offer of 19.6% (54 units)), which is less than the PPDT's viability consultant recommended. However, the applicant has offered the initially proposed affordable rented (AR) tenure units as London affordable rented tenure (LAR).
- 10.36** PPDT's viability consultants have reviewed the revised offer. Whilst it is below their suggested uplift of the originally proposed 53 units to 56 units, they have advised that if the LAR tenure, is to be assumed as a replacement to the AR tenure, it would result in a reduction in the overall value present in the scheme. The analysis of PPDT's viability consultant indicates that the blended affordable value would reduce and in turn lead to a reduction in the site's residential land value. PPDT's viability consultant advises that a shift to the LAR tenure would remove any justification in viability terms for an increase in affordable housing unit numbers, to either their originally suggested 56 units or indeed above the applicant's initially proposed offer of 53 units albeit with a different tenure split (32 AR and 21 intermediate/shared ownership).
- 10.37** Nevertheless, the applicant has confirmed an offer of 54 units (33 LAR / 21 intermediate / shared ownership) equating to a circa 60:40 split. This equates to 19.6% by unit of affordable housing across the site. It is noted that the 9-affordable housing units equates to 42.8% of the 21 additional units proposed in the s73 variation.
- 10.38** PPDT's viability consultant concludes that if the LAR tenure is to be assumed as a replacement to the AR tenure on a 60:40 split across the development, it would equate to the provision of 32 LAR units and 21 intermediate / shared ownership units (53 units) and would be the maximum reasonable amount of affordable housing the scheme can deliver.

**Table 6**

**Affordable Housing Tenure Split**

<b>Options</b>	<b>Tenure Mix</b>	<b>Unit Number</b>	<b>Tenure Split</b>
<b>Extant Consent</b>	29 Affordable Rent / 16 intermediate / shared ownership	45	64:32
<b>s73 - Applicant's Original Offer</b>	32 Affordable Rent / 21 intermediate / shared ownership	53	60:40
<b>PPDT's Viability Consultant Maximum Viable</b>	34 Affordable Rent / 22 intermediate / shared ownership	56	60:40

<b>Applicant's Revised s73 Offer</b>	33 London Affordable Rent / 21 intermediate / shared ownership	54	60:40
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**10.39** It should be noted that the extant consent assumes an Affordable Rent (AR) tenure, which assumes affordable rent below 80% of market rents inclusive of rates. However, as set out in the Mayor's Affordable Homes Programme 2016-21 the Mayor does not consider 80% of market rents to be *genuinely* affordable in most parts of London, and therefore expects most homes let for LAR to be substantially below the AR level. The starting point for London Affordable Rent is the benchmarks for homes which are let in 2017/18 (set out in table 7 below). Updated benchmarks would be published by the GLA on an annual basis.

**Table 7**

**London Affordable Rent benchmarks for 2017-18**

<b>Bedroom size</b>	<b>2017-18 Benchmark</b> (weekly rents, exclusive of service charge)
Bedsit and one bedroom	£144.26
Two bedrooms	£152.73
Three bedrooms	£161.22
Four bedrooms	£169.70
Five bedrooms	£178.18
Six or more bedrooms	£186.66

**10.40** Officers have also considered a 70:30 split as recommended in the Mayor's AHV SPG. In this case the maximum viably reasonable amount of affordable housing the scheme can deliver would equate to 35 LAR units and 14 intermediate/shared ownership units (49 units).

**10.41** However, evidenced from the Local Plan Review Housing Requirements Study which suggests that the 60:40 tenure split is retained for the area, officers are of the view that the applicant's offer of 54 units on a circa 60:40 split, as set in tables 6 and 8, is the most appropriate in this case. Officers and their viability consultants are satisfied that this is the maximum reasonable amount of affordable housing that the scheme can deliver it can in accordance with London Plan and draft London policy. It not only would provide more units overall than could be provided on a 70:30 split ratio, it would provide more units overall than the extant consent with a better percentage of potentially *genuinely* affordable housing in line with the aspirations set out in the Mayor's AHV SPG. The overall housing and tenure mix is set out in table 8 below.

**Table 8**  
**Indicative s73 Tenure Split**

Tenure	Studio	1 bedroom	2 bedroom	3 bedroom	4 bedroom	All Bedroom Types Total
<b>Market</b>	10 (1% of total market)	59 (27% of total market)	119 (55% of total market)	37 (17% of total market)	2 (1% of total market)	221  (80% of total housing)
<b>Shared Ownership</b>	0	5 (24% of total intermediate)	14 (67% of total intermediate)	2 (9% of total intermediate)	0	21  (8% of total housing, 39% of total affordable housing)
<b>London Affordable Rent (LAR)</b>	0	13 (39% of total LAR)	8 (24% of total LAR)	11 (34% of total LAR)	1 (3% of total LAR)	33  (12% of total housing, 61% of total affordable housing)
<b>All Tenures Total</b>	<b>10 (4%)</b>	<b>71 (26%)</b>	<b>141 (51%)</b>	<b>50 (18%)</b>	<b>3 (1%)</b>	<b>275 (100%)</b>

**10.42** Another of the benefits of the s73 scheme is that the affordable housing would be distributed within 2 buildings enabling a degree of ‘pepper potting’. Under the extant consent all of the affordable housing would be contained within one building (Building 7 (45 units)). In the s73 proposals Building 3 would contain 9 units on 2 of its 8 floors, in addition to the 45 affordable housing units contained within Building 7.

**10.43** Some of the affordable units would have direct ground floor access. The intermediate units would be accessible from the north-west core of Building 7. The LAR units would be accessed within the south-east core of Building 7. This would enable independent access to both the intermediate and the LAR units.

**10.44 Design and Residential Quality**



- 10.45** Policy 3.5 of The London Plan and the Mayor's Housing SPG advises that housing developments should be of the highest quality internally, externally and in relation to their context and to the wider environment.
- 10.46** As previously stated there are no alterations proposed to the footprints or heights of the buildings. The additional floorspace generally impacts the layout of the residential units and the elevational treatment of the buildings arising from the arrangement of additional doors and windows on the elevations. There has also been an increase in number of units per core proposed in some of the blocks however this is never above 8, which is in accordance with the Mayor's Housing SPG.
- 10.47** Officers have sought updated plans as it was noted that whilst elevations had been altered to reflect changes in plan, this had not happened consistently leading to uncertainty as to the actual proposed façade appearance. However, subsequent additional information/clarification has been provided by the applicant, which satisfactorily addresses the concerns raised.
- 10.48** Officers are of the view that the internal arrangement of the proposed accommodation is well considered. Whilst there would be a loss of dual aspect units (from 69% dual aspect in the consented scheme to 58% in the proposed scheme), the s73 amendments are considered to be acceptable in this instance as they do not present any directly north facing single aspect units. Indeed, it is noted that a number of the single aspect units have a south-easterly aspect thereby benefiting from some direct sunlight. As with the consented scheme a number of the proposed units across the site (44 in total) have a west-north-west aspect enabling units to receive sunlight at certain points in the day. Furthermore, the layout, including the single aspect units, are in keeping with the floorplans on the upper floors of the consented scheme. Whilst the reduction in the number of cores has led to narrower corridors officers are satisfied that this has not impacted detrimentally on residential quality i.e. there would be no difference in terms of lighting of the corridors from stair cores which would be as per the consented scheme.
- 10.49** The revised plans confirm that the façade amendments are minor, some imperceptible and would not diminish the original architectural design intent.
- 10.50** The volume increase to Buildings 1 and 2 as a result of the additional commercial floor at level 3 would provide a continuation of the structural grid from the upper buildings to the lower building. The s73 amendments still results in the approved vertical patterned re-constituted stone (GRC) cladded finish, punctuated by horizontal bands every two floors. The location and design of the recessed balconies to the east and west elevation remains unchanged. The visual appearance and design of Buildings 1 and 2 remains largely unchanged from the extant consent.
- 10.51** The alterations to the façades of the buildings would allow for the internal changes to the apartment layouts. The balconies would generally remain in the same location and the distribution of windows is similar, albeit more windows are introduced to provide daylight to additional rooms. These amendments would not have a significant visual impact and are acceptable.
- 10.52** Officers analysis concludes that all the alterations are acceptable in design detailing, materials and finish, indeed some of the alterations are imperceptible. The proposed changes to the scheme approved in 2017 are minor and raise no new design or quality issues. Officers are satisfied that the s73 proposed alteration can be viewed as a minor-material amendment.

**Table 8**

Unit Size	Nationally Described Standards (m <sup>2</sup> )	s73 Proposed Sizes(m <sup>2</sup> ) and Unit Numbers
Studio (with shower room / bathroom)	37 / 39	38 – 46 (10 units)
1 bedroom/2 person	50	Between 50 – 67 (71 Units)
2 bedroom/3 person	61	Between 69 – 86 (24 units)
2 bedroom/4 person	70	Between 71 – 116 (117 units)
3 bedroom/5 person	86	Between 90 – 164 (46 units)
3 bedroom/6 person	90	Between 105 – 151 (4 units)
4 bedroom/6 person	99	Between 147 – 156 (3 units)

\*No studio apartments are approved in the extant consent

**10.53** Officers are satisfied that in both the flatted blocks and the terrace row of houses as with the extant consent the quality of the residential accommodation is considered to be good not only providing residential units with varying floor space sizes but the internal floor areas of all units including their external amenity spaces would still meet or exceed the respective minimum standards as per the proposed number of rooms and number of occupants they are intended to serve in line with the DCLG Technical Housing Standards (nationally described space standards) see table 8). Given this it is considered that the proposed development would be in accordance with the London Plan Housing SPG (2016) and the flats and houses would provide an acceptable amount of space for day to day living.

**10.54** Following receipt of revised drawings officers are satisfied that the overall amendments respect the context, quality and design principles associated with the extant consent, and visually make a positive contribution to the development providing a robust, high quality and considered finish and materiality to these buildings. Officers analysis concludes that the alterations both individually and cumulatively are acceptable and would not be substantially different from the extant consent and is a minor material amendment. The proposals accord with policies 7.1, 7.4, 7.6 and 7.7 of the London Plan and Local Plan Sub Area 4 and policies BN.1 and BN.10.

**10.55** Density

**10.56** The site is located within an area that has a PTAL rating of between 3 and 4, and is located approximately 250m from Pudding Mill DLR Station, which is about a 5-minute walk. Within PTAL 4/6 areas London Plan Policy 3.4 states that the maximum density range for development proposals is 200-700hr/ha.

**10.57** The s73 proposals would result in a residential density of 722 habitable rooms per hectares, based on 745 habitable rooms taking into account the commercial element, which is in line with the specified density ranges for a site within a PTAL 4 location. The extant consent would result in 638 habitable rooms per hectare. For context, the LCS residential density range proposed for the adjoin PDZ 8 Development Parcels are between 450-950 and 450-650.

- 10.58** Officers are satisfied that, taking into account the ‘liveability’, design factors, provision of amenity space, and need to make effective use of previously developed land in accessible locations to mitigate climate change and transport impacts, the increased density of the proposed s73 scheme will be broadly consistent with the density character of surrounding developments including the extant consent and the approved LCS scheme. It would fall within the London Plan density ranges for sites within PTAL 4.
- 10.59** Daylight, Sunlight and Overshadowing
- 10.60** Daylight/Sunlight: An updated sunlight/daylight report has been submitted alongside the s73 application. The assessment shows that the internal daylight and sunlight conditions within the development would vary but overall effects would be characteristic of a dense urban environment. The report concludes that there would be minimal changes to the sunlight/daylight (vertical sky component (VSC) and average daylight factor (ADF)) within the proposed scheme and existing nearby residential development as a result of the proposals and that good levels of sunlight/daylight would be maintained in accordance with BRE guidance.
- 10.61** PPDT’s Environmental Consultant’s review of the s73 concludes that there is no change to surrounding receptors. They also state that daylight access is considered to be good. Whilst sunlight access would as a result of the density, height of the blocks, orientation and the provision of balconies remain acceptable as per the extant permission.
- 10.62** Officers and their consultants agree that in terms of the daylight/sunlight assessment the proposals are in accordance with the BRE guidance and would provide satisfactory living conditions for future occupiers of the new development and would not impact on adjoining developments in terms of achieving minimum levels of sunlight to adjoining development.
- 10.63** Officers are also satisfied that there would not be any significant impact upon privacy or outlook in this instance given that the separation distances between the blocks remains unchanged as per the extant consent. As such it is considered that there would be minimal actual and perceived overlooking from the new additional units.
- 10.64** Overshadowing: PPDT’s environmental consultants are satisfied that overshadowing analysis of the surrounding amenity areas shows there would be little or no effect on the existing values as approved under the extant consent. To that end no objections are raised.
- 10.65** In terms of daylight/sunlight and overshadowing the proposals are thus in accordance with Local Plan policies BN.1, BN.4 and SP.1.
- 10.66** Amenity Space and Landscaping and Public Realm
- 10.67** No alterations are proposed to the location and layout of the public realm. As with the extant consent residents would also benefit from the areas of publicly accessible open space within the development.
- 10.68** There is however a reduction of some 73m<sup>2</sup> (3.4%) of amenity space serving the development, which is at the new podium level on the fifth floor, over the originally approved 2,109m<sup>2</sup>. The private amenity space provided in the amended scheme would amount to 2,036m<sup>2</sup>. Private amenity space for the use of residents would be

provided in the form of balconies to all units, communal amenity space within the courtyard around buildings 5, 6, 7 and 8, podium level roof space at Buildings 1 and 2 and private gardens to the town houses in Building 5.

**10.69** Nevertheless, despite the increase in the quantum of development proposed and the decrease in the proposed level of amenity space, officers are satisfied that the amenity space provision is acceptable and would not be substantially different from the extant consent and is a minor material amendment. Given that as with the extant consent, the proposed quantum of public realm exceeds the baseline minimum requirement set out in the Mayor's Housing SPG of 1,717m<sup>2</sup>. Also, the quality and quantity of public open space which residents would have access to would provide a good living environment and comply with Local Plan policy BN.8 and London Plan policies 3.6 and 7.5 which promote quality and good design in new housing developments.

**10.70** As with the extant consent the revised development would open up the currently inaccessible and impermeable site to provide a high quality and extensive public realm, with informal and formal play space, alongside a number of other landscaped character areas, all of which are linked to create a network of navigable green spaces.

#### **10.71 Accessibility and Inclusive Design**

**10.72** The s73 variation includes 10% (27 units) of the total housing to be suitable for wheelchair use of varying mix and tenure in accordance with the London Plan Housing Standards. Officers consider that the quantum and distribution of wheelchair housing is satisfactory for this development and have recommended the imposition of a condition which secures that 10% of the residential units be designed and constructed in accordance with Optional Requirement M4 (3) Category 3 of Part M of the Building Regulations (split proportionately).

**10.73** As reflected within Local Plan Policy BN5, the applicant has committed to ensuring that adaptation may be undertaken at a future date in response to the needs of occupiers. Officers recommend that 90% of the residential units (not covered by the 10% secured above) be designed and constructed in accordance with the M4 (2) Category 2 of Part M of the Building Regulations.

**10.74** A total of 29 accessible parking bays would be available on site accessed via the basement (27 spaces to serve the residential flats including visitor spaces and 2 to serve the commercial units. Officers can confirm that the accessible parking provision complies with the baseline standards in the Housing SPG.

**10.75** The extant consent requires a car park management strategy (secured by s106) to ensure that the details of the way in which spaces are allocated and managed to ensure on-going availability of accessible parking spaces is approved by the Local Planning Authority; and this would carry through to the deed of variation for this S73 application.

**10.76** Officers are satisfied that the proposals meet the development plan objectives in terms of accessibility and inclusive design and are capable of complying with Policy BN.5 of the Local Plan.

## **10.77 Transport Matters**

- 10.78** Cycle Parking: TfL have commented that the amended scheme should adopt the amended cycle parking standards which are in the draft London Plan. While the consented provision was slightly above previous London Plan standards. The consented scheme would provide a total of 590 spaces to serve the scheme. 597 spaces are proposed to serve the revised s73 scheme.
- 10.79** The new standards (draft London Plan) require that 1 bed units have 1.5 spaces per unit and the commercial element have 1 space per 75m<sup>2</sup>. The new residential requirement would therefore be 508 long stay and 7 short stay, and the employment requirement would be 76 long stay and 11 short stay, for an overall total of 584 long stay and 18 short stay spaces. TfL, comment that the long stay element should be increased from 533 by 51 spaces to reach a total of 584 spaces while the short stay element of 18 spaces is still acceptable. The increased commercial area would provide showers and changing facilities to promote cycle use.
- 10.80** The total increase in cycle spaces over the extant consent is 7 spaces. It is noted that the provision in the extant consent is an overprovision against the current London Plan standards. The applicant has confirmed that there would be sufficient space within the basement parking area to incorporate the increase in residential provision as suggested by TfL and have provided an updated plan, which identifies across the site where employment long stay cycle parking would be provided.
- 10.81** Car Parking: The s73 variation proposes no changes to the car parking provision. A parking plan is provided as part of the revised scheme. Car parking is provided in a basement area for a total of 69 vehicles and would be sold on a first come-first serve principle. The commercial element would have 2 accessible spaces. For the residential element 67 spaces are provided including 29 accessible spaces which is at a ratio of 0.25 spaces per unit, which is below London Plan standards and is welcomed by TfL. The applicant has confirmed that there would be a restriction on the number of spaces that can be purchased by an individual dwelling. If a space is unsold it would remain unallocated. Commercial parking would be managed through a Parking Management Strategy, which as part of the extant consent is secured by a s106 obligation; and would also be secured in the Deed of Variation.
- 10.82** Officers are satisfied that the provision of accessible parking bays is in accordance with London Plan standards. London Plan policy 6.13 para 6A.2 states that the non-residential element of a development should provide at least one accessible on or off-street car parking bay designated for accessible parking badge holders.
- 10.83** The London Plan Housing SPG states that each accessible wheelchair dwelling (10% of the development) 'should' have a car parking space. Local Plan Policy T.8 requires amongst other things the provision of on or off street accessible parking to be appropriate to the size, nature and location of the development. The total required provision to serve this development according to the London Plan and Housing SPG (i.e. in terms of accessible wheelchair dwellings and non-residential) this would be 29 spaces (29 accessible spaces are to be provided within the basement car park (27 for the residential units and 2 for the commercial unit)).
- 10.84** It should be noted that the extant consent includes a s106 obligation requiring a Parking Management Strategy which would monitor the provision and management; and this would also apply to the section 73 permission via be in the Deed of Variation.

- 10.85** Officers are satisfied that the parking provision strikes an appropriate balance between meeting expected demand from residents and providing a degree of restraint to deter car ownership. Officers analysis concludes that the proposals meet the aspirations of regional (London Plan policies 6.9, 6.10 and 6.13) and local development plan policy (T.4, T.6, T.8, and T.9 refers), in proposing a development which supports sustainable transport objectives.
- 10.86** Waste Collection: As a result of the s73 proposals there are alterations to the refuse arrangements at the ground floor of Building 7, as revised the building would now have two separate storage areas. The changes have been made to the waste and servicing arrangements due to the land ownership delineation (building 7 straddles two land ownerships). This is to enable refuse collection from both parts of the site and has required that the lift from the basement is moved to be better located for the removal of refuse to the collection point.
- 10.87** PPDT's environmental consultants have commented that the provision of household bulky waste storage within the Building 7 bin store does not meet LBN requirements (i.e. developments of 25+ units should have a household bulky waste store that is separate from storage areas containing normal refuse and recycling bins). Furthermore, the proposed 15m<sup>2</sup> allowance for bulky waste is below the calculated requirement of 55m<sup>2</sup> store that is separate from storage areas containing normal refuse and recycling bins) and does not accord with LB Newham's Waste Management Guidelines for Architects and Property Developers.
- 10.88** The applicant commented that the provision of a 55m<sup>2</sup> space on the ground floor would be excessive and that a 15m<sup>2</sup> space would be sufficient on the grounds that it is highly unlikely that the number of bulky waste items requiring storage at any time will exceed the space provided. For reference, a sofa represents less than 3m<sup>2</sup> and a fridge less than 1m<sup>2</sup>. They go on to state that the reason why a separate space is not provided is that the Building 7 bin store would only be at full capacity on collection days and would largely be available for bulky waste on the other days. LB Newham (Waste department) have commented that the capacity should follow the guidance in BS 5906 for residential waste and should include an extra 25-30% capacity for recycling.
- 10.89** The applicant has shared with PPDT's environmental consultants and LB Newham an updated refuse service plan, and revised drawings illustrating how the development would accord with LB Newham's waste strategy and advised that the bins would be transferred by the management company servicing the development to the collection point in Building 7. The applicant has provided confirmation to officers that the waste strategy has been accepted by LB Newham. Officers and their advisors are satisfied the proposals demonstrate how the development would be serviced in order to avoid vehicle obstructions and awkward manoeuvres and bins being stored to the detriment of the safe and free flow of pedestrians/ cyclists. However due to the journey lengths involved for waste collection/management (8km) PPDT's environmental consultants advise that condition 23 is revised to ensure that transfer operations are conducted safely, including any provision for storing manual handling aids.
- 10.90 Other Environmental Impacts**
- 10.91** Sustainability: Officers and their environmental advisors are satisfied that the original approach and targets for the development have been followed in the amended design. All of the conditions from the extant consent would be retained and separately discharged.

- 10.92** Ecology: The amended design remains within the original footprint and this does not trigger any further ecological impacts or require any further surveys. All of the conditions from the extant consent would be retained and separately discharged.
- 10.93** Air Quality: PPDT's environmental consultants required the applicant to carry out further work on air quality as the ES addendum initially did not address the changes to information on background air quality concentrations, emission factors and assessment of significance. The applicant has made revisions and provided further detail and PPDT's advisors are satisfied, that as with the extant consent, there are no negligible impacts and therefore no objections are raised.
- 10.94** Wind: PPDT's environmental consultants are satisfied that the amendments do not significantly change the massing or the location of entrances proposed on the extant consent. The environmental consultants agree that as with the extant consent the effect of wind is considered to be negligible and no objections are raised.
- 10.95** Noise and Vibration: PPDT's environmental advisors consider that the noise and vibration impacts arising from the development as being acceptable and suggest the imposition of a condition to deal with construction impacts. Condition 7, re-imposed from the extant consent, would mitigate the impact of the works and safeguard amenity and limit noise levels in accordance with London Plan policies 5.3 and 5.8 and Local Plan policy BN.11. Officers are satisfied that the proposed minor material amendments raise now new issues.
- 10.96** Archaeology and Contamination: PPDT's environmental advisors have also commented that the revisions raise no residual concerns with respect to archaeology. It should be noted that to ensure to better alignment with the on-site development process as well as best practice contamination conditions (nos. 13 to 16) from the extant consent have been reworded.
- 10.97 Extant Consent Heads of Terms:** The following are the heads of terms set out in the original s106 agreement (14/00422/FUL) to mitigate the effects of the development.
- 10.98** As highlighted below the proposed deed of variation to the original s106 agreement would tie in the originally agreed s106 obligations to the proposed s73 permission. It would cover the proposed changes to the affordable housing offer, including requiring a review mechanism in accordance with the Mayors Affordable Housing and Viability SPG.
- Final details of the treatment of the podium to Block 1 and 2
  - **To secure the Affordable Housing final offer, and include a review mechanism in accordance with the Mayors Affordable Housing and Viability SPG.**
    - **Affordable housing delivery linked to market housing**
    - **Delayed commencement**
  - **Safeguarding of Land Required for Future Road and Bridge Link**
    - The land that is identified for the alignment of the future road link and bridge from Sugar House Lane to Pudding Mill shall be laid out in accordance with the details contained in the planning application and any subsequent approval of details prior to first occupation of the development and shall be maintained by the Developer in that condition thereafter until such time as the relevant highway authority

- request that it be made available for the construction of the road and bridge link.
- Waterway Survey
- Transportation of Construction and Waste Materials by Water Transport
  - An obligation in the legal agreement to use reasonable endeavours to meet aspirations for the water transport of waste arising from demolition at the site and for new materials to be delivered by waterway.
- Marshgate Lane Design Works Contribution
  - Implement design initiatives / proposals arising from the impact of the development on Marshgate Lane in order to rebalance the street and enhance provision for users including pedestrians and cyclists.
- Relocation Strategy
  - Relocation strategy as part of the legal agreement to ensure that the applicant uses reasonable endeavours to relocate the existing tenants within the Growth Boroughs
- Employment Workspace Model
  - Delivery of workspace as per the model of Workspace14 Limited
  - Workspace to notify the LPA prior to commencement as to whether it would occupy the workspace. If it does not occupy, then it should be offered as low-cost workspace for a period of 2 years
  - Requirement for workspace strategy to ensure employment floorspace is designed and managed to meet the needs of small local companies and businesses
- Local Labour Strategy: To ensure a proportion of the employment during construction and operation (end-use) are from the Host Boroughs and to provide training initiatives
- London Living Wage: The minimum hourly wage as published by the GLA for both construction and end jobs.
- Travel Plan: To implement, monitor and review and provide membership to a car club and include electric car charging points
- Car Park Management Strategy
- Provision of Car Club spaces and Contribution to Membership Fees
- Safeguarded Land for TfL Cycle Docking Station
- Restriction on eligibility of future residents and commercial occupiers for on-street parking permits
- Electric Charging Point Provision
- Waterways Strategy
- Design Monitoring Contribution – In order to maintain design quality in the event that the original architect is not retained.
- Sustainability: Use of reasonable endeavours to secure the extension of the QEOP district heating and cooling network to the development site and thereafter an obligation to secure connection of all buildings; and in the event that this cannot be achieved to submit details of alternative measures by which equivalent carbon savings can be achieved; reasonable endeavours to encourage occupiers to reduce their energy usage
- Management and maintenance of new public realm.
- Public pedestrian and cycle access along the east-west in perpetuity

## **10.99 Community Infrastructure Ley (CIL)**

- 10.100** The site is liable for both Mayoral and LLDC CIL which is based on the current proposals. The s73 application would trigger additional Mayoral CIL which would



only apply to the additional commercial floor space being provided which is 1,414m<sup>2</sup>.

## **11 Human Rights & Equalities Implications**

**11.1** Members should take account of the provisions of the Human Rights Act 1998 as they relate to the application and the conflicting interests of the Applicants and any third party opposing the application in reaching their decisions. The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report. In particular, Article 6 (1), of the European Convention on Human Rights in relation civil rights and a fair hearing; Article 8 of the ECHR in relation to the right to respect for private and family life and Article 1 Protocol 1 of the ECHR in relation to the protection of property have all been taken into account.

**11.2** In addition, the Equality Act 2010 provides protection from discrimination in respect of certain protected characteristics namely: age, disability, gender reassignment, pregnancy and maternity, race, religion, or beliefs and sex and sexual orientation. It places the Local Planning Authority under a legal duty to have due regard to the advancement of equality in the exercise of its powers including planning powers. Officers have taken this into account in the assessment of the application and Members must be mindful of this duty inter alia when determining all planning applications. In particular Members must pay due regard to the need to:

1. Eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Equality Act;
2. Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and;
3. Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

**11.3** Officers are satisfied that the application material and Officers' assessment has taken into account these issues. Particular matters of consideration have included provision of accessible housing and parking bays, as well as the provision of affordable and family housing.

## **12 CONCLUSION**

**12.1** It is considered that the variations proposed as part of the s73 application can be supported, noting that they would be limited in their effect on the originally permitted scheme and would only constitute minor material amendments to the parent permission (14/00422/FUL). The minor material amendments are considered both individually and cumulatively to be acceptable and subject to the necessary alteration to the original s106 agreement by way of the deed of variation and conditions, the officer recommendation is to grant this s73 variation planning permission.

**12.2** The design of the development would remain of a high quality and there would be no detrimental impact upon local or strategic views, nor would harm be caused to local amenity in relation to an unacceptable loss of sunlight/daylight, sense of enclosure or loss of privacy or outlook.

**12.3** The quality of the residential accommodation provided within the development would remain at a high standard. The increased number of residential units in comparison to the extant permission would make a contribution to housing targets

(as well as affordable housing) and the increase in commercial floorspace would improve the quality of the space and help increase job density on the site.

- 12.4** The scheme would remain a sustainable form of development that would continue to deliver a range of public benefits.
- 12.5** The alterations proposed under this s73 application would be in general compliance with national, regional and local plan policies and guidance. Officers are therefore recommending approval of the scheme in accordance with the presumption in favour of sustainable development as per the requirement of the National Planning Policy Framework (NPPF).
- 12.6** As set out in the National Planning Policy Guidance (NPPG) the s106 obligations that were required to mitigate against the parent consent would be tied to the new consent via a deed of variation.
- 12.7** The NPPG also states that where an application under s73 is granted, the effect is the issuance of a new planning permission, sitting alongside the original permission, which remains intact and un-amended. A decision notice describing the new permission should be issued, setting out all of the conditions related to it. To assist with clarity decision notices for the grant of planning permission under section 73 should also repeat the relevant conditions from the original planning permission, unless they have already been discharged. It should be noted that no conditions or obligations under the parent application have been discharged.
- 12.8** As a s73 application cannot be used to vary the time limit for implementation, this condition must remain unchanged from the original permission (see condition 1). The conditions have been reapplied albeit with minor revisions (highlighted below) to include policy reference and modification to the remediation conditions (see conditions 13 to 16) to better align with the on-site development process as well as best practice. The applicant is expected to submit a section s96A non-material amendment application, which would change the description of development to ensure that it is consistent with the conditions as altered by the s73.
- 12.9** No objections have been received from statutory consultees.
- 12.10** The analysis concludes that the proposal is acceptable in planning policy terms and the scale and nature results in a development which is not substantially different from the one which has been approved. In light of the above assessment it is recommended that the application be approved subject to the conditions set out below and the satisfactory completion of a deed of variation to the existing s106 Legal Agreement.

## **13 PLANNING CONDITIONS**

### **1. Time limit**

**S73 - CONSENT LIMITED TO THAT OF ORIGINAL PERMISSION:** The development hereby permitted shall be begun not later than the expiration of [04/05/2020].

**REASON:** To ensure the commencement timescale for the development is not extended beyond that of the original planning permission granted on [05/05/2017] [ref: 14/00422/FUL]. Furthermore, to comply with the provisions of Section 91(1)(a) of the Town and Country Planning Act 1990 as amended by the Planning and Compulsory Purchase Act 2004

**2. Works in accordance with approved details**

Unless minor variations have been agreed by the Local Planning Authority and to the extent that it does not deviate from this permission, the development shall be carried out in accordance with the following details and plan numbers:

INSERT DRAWING NUMBERS

and the description of development contained in the application and any other plans, drawings, documents, details, schemes or strategies which have been approved by the Local Planning Authority pursuant to these conditions.

Reason: To ensure that all works are properly implemented and retained.

**3. Section 106 Obligation**

No development on the 14ML Land shall commence until all of the parties who own a freehold interest in the 14ML Land have entered into the Supplemental Agreement with the Local Planning Authority.

Reason: To ensure that no development of the 14ML Land can take place until that land is bound into the relevant planning obligations contained within the Section 106 Agreement.

**4. Workspace 14 Build Out**

No part of the development permitted on the 14ML Land shall be occupied until at least 84 residential units on the W14 Land have been occupied.

Reason: To ensure the viability review obligations of the Section 106 Agreement capture the achieved sales values of the majority of residential units comprised within the development.

**5. Notice of Commencement**

The development shall not be commenced until written notice of intention to commence the development has been given to the Local Planning Authority. The notice required by this condition shall only be given where there is a genuine prospect of development being commenced within 21 days of the notice and the notice shall confirm and provide evidence that this is the case.

Reason and pre-commencement justification: To enable the LPA to monitor development.

**6. Development in accordance with Environmental Statement**

The Development (including other matters submitted for approval pursuant to this permission) shall be carried out in accordance with the mitigation measures set out in the document WIE10067.3.2.1.Marshgate\_Mitigation Table - Final July 2016.

Reason: To ensure the mitigation measures specified in the Environmental Statement are satisfactorily implemented.

**DEMOLITION & CONSTRUCTION**

**7. Demolition and Construction Management Plan (DCMP)**

The development on either the W14 Land or the 14ML Land shall not be commenced until a demolition and construction management plan (CMP) for the respective area of land, has been submitted to and approved by the Local Planning Authority. The

Demolition and Construction Management Plan and the updates thereto shall be in accordance with all relevant legislation in force and substantially in accordance with all policy adopted and best practice guidance published at the time of submission. The DCMP shall include as a minimum the following information:

- a. The arrangements for liaison with the relevant highway authorities;
- b. The parking of vehicles of site operatives and visitors;
- c. The loading and unloading of plant and materials including a construction logistics plan;
- d. The storage of plant and materials use in constructing the development;
- e. The erection and maintenance of security hoardings;
- f. Measures to control the emission of dust and dirt during construction;
- g. A scheme for recycling and disposing of waste resulting from demolition and construction work;
- h. Dealing with complaints and community liaison;
- i. measures to control the flow of surface water off the slab (temporary drainage details);
- j. Attendance as necessary at the LLDC Construction Transport Management Group (CTMG) and or Construction Coordination Group;
- k. Details of routes and access for construction traffic. Including lorry holding areas; and
- l. Guidance on membership of the Fleet Operator Recognition Scheme and implementation of vehicle safety measures and driver training including cycle awareness and an on-road cycle module.

The development shall be carried out in accordance with the approved details.

Reason and pre-commencement justification: To ensure that the Local Planning Authority can assess whether the development would generate any unacceptable environmental impacts through demolition and construction that would require appropriate mitigation and to be in accordance with London Plan Policy S.3 and Local Plan 2015 Policy T.4.

## **8. Code of Construction Practice**

The development on either the W14 Land or the 14ML Land shall not be commenced until a Code of Construction Practice for the respective area of land has been submitted to and approved by the Local Planning Authority. The Code of Construction Practice shall be in accordance with all relevant legislation in force and substantially in accordance with all policy adopted and best practice guidance published at the time of submission. The Code of Construction Practice shall include proposals for the following:

- Safeguarding of buried services
- Location and height of any proposed stock
- waste generation and materials reuse and recycling
- air quality mitigation measures from demolition and construction activity within the Site
- noise mitigation measures from demolition and construction activity within the Site

The Development shall be carried out in accordance with the approved details.

Reason and pre-commencement justification: To ensure that the Local Planning Authority can assess whether the development would generate any unacceptable environmental impacts through construction that would require appropriate mitigation in accordance with Local Plan Policies BN.11 and S.4.

#### **9. Demolition and Construction Waste Management Plan**

The development on either the W14 Land or the 14ML Land shall not be commenced until a Demolition and Construction Waste Management Plan (DCWMP) for the respective area of land has been submitted to and approved by the Local Planning Authority. The objectives of the DCWMP shall be to ensure all waste arising from the construction works are managed in a sustainable manner, maximising the opportunities to reduce, reuse and recycle waste materials. The DCWMP shall also detail the compliance and assurance requirements to be maintained on the Site during all phases of construction. The DCWMP shall include as a minimum the following information:

- classification of all waste including hazardous waste according to current legislative provisions;
- performance measurement and target setting against estimated waste forecasts;
- reporting of project performance on quantities and options utilised;
- measures to minimise waste generation;
- opportunities for re-use or recycling;
- provision for the segregation of waste streams on the Site that are clearly labelled;
- licensing requirements for disposal sites;
- an appropriate audit trail encompassing waste disposal activities and waste consignment notes;
- measures to avoid fly tipping by others on lands being used for construction. Returns policies for unwanted materials;
- measures to provide adequate training and awareness through toolbox talks; and
- returns policies for unwanted materials.
- The Development shall be carried out in accordance with the approved details.

Reason and pre-commencement justification: Submission required prior to commencement to ensure that the Local Planning Authority to ensure that the impact of demolition and construction is appropriately mitigated and ensure high standards of sustainability are achieved in accordance with Policy 5.18 of the London Plan and Policy S.6 of the Local Plan 2015.

#### **10. Approval of road works necessary**

The development on either the W14 Land or the 14ML Land shall not be commenced until details of the following works to the highway for the respective area of land have been submitted to and approved in writing by the Local Planning Authority in consultation with the Highway Authority. Works to the highway shall only be carried out by the Highway Authority:

- The provision of an on-street loading bay

- The installation of new crossovers and reinstatement of footways where existing crossovers are being blocked up

The building hereby permitted on the respective area of land shall not be occupied until these works have been consented in accordance with the approved details.

Reason and pre-commencement justification: To ensure that occupiers of the site have adequate facilities to support the use and encourage the uptake of sustainable transport methods to and from the site, and to ensure that all road works associated with the proposed development are to a standard approved by the Local Planning Authority and are completed before occupation and in accordance with Local Plan 2015 Policy T.4.

**11. Piling/Foundation Designs**

Piling or any other foundation designs using penetrative methods shall not be permitted on either the 14ML Land or the W14 Land other than with the express prior written consent of the Local Planning Authority, which may be given for those parts of the respective area of land where it has been demonstrated that there is no resultant unacceptable risk to groundwater. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the proposed activities safeguard human health and do not harm controlled waters and groundwater resources in line with paragraph 109 of the NPPF and the Environment Agency's approach to groundwater protection March 2017 and in accordance Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015.

**12. Piling method statement**

No piling, including impact piling shall take place on either the 14ML Land or the W14 Land until a piling method statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling would be carried out, including measures to prevent and minimise the potential for impact on ground water, damage to subsurface water infrastructure, and the programme for the works) for the respective area of land has been submitted to and approved in writing by the Local Planning Authority. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: To ensure that the proposed activities safeguard human health and do not harm controlled waters and groundwater resources in line with paragraph 109 of the NPPF and the Environment Agency's approach to groundwater protection March 2017 and in accordance Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015

**13. Remediation Statement**

No development shall commence on either the 14ML Land or W14 Land until a Remediation Statement has been submitted to and approved by the Local Planning Authority in writing. The Remediation Statement shall outline and justify the assessment approach required to be completed and approved by the Local Planning Authority prior to any commencement of Development. The Remediation Statement shall also define the phases of development for which subsequent submissions are

required to be submitted to discharge the contamination assessment, remediation strategy and methodology.

Reason and pre-commencement justification: Submission required prior to commencement to safeguard human health, controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other off-site receptors in accordance with Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015.

**14. Contamination assessment, remediation strategy and methodology**

No Phase shall be commenced until details of remediation for that Phase as defined in the submitted Remediation Statement have been submitted to and approved in writing by the Local Planning Authority. The contamination assessment, remediation strategy and methodology approach shall be detailed in either of the following documents:

- remediation protection method statement; or
- site specific remediation strategy and remediation method statement

Each submission shall contain as a minimum:

- A review of relevant previous contamination information in conjunction with additional ground investigations undertaken as part of the Development. Any contamination assessment shall use appropriate assessment criteria.
- Details of the proposed development and general work methodology and programme.
- General health and safety and environmental controls including any details of any required authorisations, permits, licences and consents.
- Discovery strategy to deal with unexpected contamination.
- Details of how the remediation works and materials (including site won and imported) are to be validated.
- The remediation method statement shall include details of design, installation and verification of gas and vapour protection measures in accordance with current guidance and British Standards if required by the Local Planning Authority.

The Development shall be carried out in accordance with the approved details.

Reason: To safeguard human health, controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015.

**15. Remediation, validation and protection**

No occupation of any part of the permitted Development (or stage in development as may be agreed in writing with the Local Planning Authority) shall take place until a verification (or validation) report demonstrating completion of works set out in the approved remediation protection method statement, site specific remediation statement and remediation method statement has been submitted to and approved in writing by the Local Planning Authority.

The verification report shall assess and describe the requirements for long-term monitoring and maintenance (including contingency action) to ensure the effectiveness of the remediation measures implemented. The long-term monitoring and maintenance shall be implemented as approved.

Reason: To safeguard human health, controlled waters, property and ecological systems, and to ensure that the development has been carried out safely without unacceptable risks to workers, neighbours and other offsite receptors and in accordance with Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015

**16. Unexpected contamination**

If during development unexpected contamination is encountered then no further development shall be carried out until an addendum to the remediation protection method statement, site specific remediation statement and remediation method statement (referred to in conditions 13 and 14) has been submitted to and approved in writing by the Local Planning Authority (unless otherwise agreed in writing with the Local Planning Authority). The addendum shall be implemented as approved, in writing by the Local Planning Authority.

Reason: To safeguard human health, controlled waters, property and ecological systems, and to ensure that the development is carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with Policies 5.21, 5.14 and 7.19 of the London Plan and Policy BN13 of the Local Plan 2015.

**17. Surface Water Drainage**

Development of either the 14ML Land or W14 Land shall not be commenced except demolition to existing slab level until a detailed surface water drainage scheme for the respective area of land, based on the following agreed documents has been submitted to and approved in writing by the local planning authority.

- Marshgate Business Centre and 14 Marshgate Lane, Flood Risk Assessment, Document Reference: CIV15409 ES 002, October 2014.
- Marshgate Lane, London, E15 2NH, Drainage Report, Job No. 5239, August 2014.

The drainage strategy shall include a restriction in run-off and surface water storage on site as outlined in the FRA. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

Reason and pre-commencement justification: Submission required prior to commencement to the Local Planning Authority to prevent the increased risk of flooding, to improve and protect water quality, and improve habitat and amenity in accordance with Policy 5.13 of the London Plan and Policy BN13 of the Local Plan 2015.

**18. Drainage**

The drainage works referred to in the approved Marshgate Business Centre Drainage strategy prepared by Waterman dated October 2014 shall be implemented in full for each relevant phase of the development, and no discharge of foul or surface water from the site shall be accepted into the public system until the said drainage works



have been completed unless minor variations are agreed in writing in advance with the Local Planning Authority.

Reason: The development may lead to sewage flooding; to ensure that sufficient capacity is made available to cope with the new development; and in order to avoid adverse environmental impact upon the community and in accordance with Policy 5.13 of the London Plan and Policy BN13 of the Local Plan 2015.

#### **19. Archaeological investigation**

- A) For each of the 14ML Land and the W14 Land no development of the respective area of land other than demolition to existing ground level shall take place until (i) a programme of archaeological evaluation for the respective area of land has been submitted to and approved in writing by the Local Planning Authority (ii) the approved archaeological evaluation programme has been implemented and (iii) a report on that evaluation has been submitted to and approved in writing by the Local Planning Authority.
- B) If heritage assets of archaeological interest are identified by the evaluation under Part A, then before development of the respective area of land, other than demolition to existing ground level, commences (i) a Written Scheme of Investigation shall be submitted to and approved in writing by the Local Planning Authority.
- C) No development or demolition of the respective area of land other than demolition to existing ground level shall take place other than in accordance with the Written Scheme of Investigation approved under Part (B) and archaeological works shall be carried out by a suitably qualified investigating body acceptable to the Local Planning Authority.
- D) The development on the respective area of land shall not be occupied until a site investigation and post investigation assessment has been completed in accordance with the programme set out in the Written Scheme of Investigation approved under Part (B), and the provision for analysis, publication and dissemination of the results and archive deposition has been secured.

Reason: To ensure that the archaeological interests are protected and considered appropriately **and in accordance with London Plan policy 7.8 of the Local Plan and policy BN.2 of the Local Plan 2015.**

#### **RESIDENTIAL STANDARDS**

#### **20. Residential standard- internal noise levels**

No residential premises shall be occupied unless it has been designed and constructed in accordance with BS8233:2014 'Sound insulation and noise reduction for buildings- Code of Practice' and attains the following internal noise levels:

- Bedrooms- 30dB LAeq,T\* and 40dB LAfmax
- Living rooms- 30dB LAeq, D\*
- \*T- Night-time 8 hours between 23:00-07:00
- \*D- Daytime 16 hours between 07:00-23:00.

Reason: To ensure that the occupiers and users of the development do not suffer a loss of amenity by reason of excess noise from environmental and transportation sources **and to be in accordance with Policy 7.15 of the London Plan and Policy BN11 of the Local Plan 2015.**

**21. Sound insulation and noise mitigation details – Residential and Non-Residential**

The development on either of the 14ML Land or the W14 Land shall not be occupied until details of the proposed sound insulation scheme to be implemented between the residential accommodation and any non-residential uses of the respective area of land have been submitted to and approved in writing by the Local Planning Authority. The scheme shall include a glazing specification for all windows to ensure a good standard of internal noise can be achieved during day time and night time in accordance with the guideline levels of BS8233 1999: "Sound insulation and noise reduction for buildings – code of practice" or an equivalent standard. Details should include airborne and impact sound insulation. The Development on the respective area of land shall not be occupied until the noise mitigation measures approved as part of the sound insulation scheme have been installed. The approved scheme is to be completed prior to occupation of the Development and thereafter permanently retained.

Reason: To protect the amenity of future occupants and/or neighbours **in accordance with Policy 7.15 of the London Plan and Policy BN11 of the Local Plan 2015.**

**22. Accessible housing**

90% of the residential units hereby permitted shall be designed and constructed in accordance with M4 (2) Category 2 of Part M of the Building Regulations.

10% of the residential units hereby permitted shall be designed and constructed in accordance M4 (3) Category 3 of Part M of the Building Regulations (split proportionately so that 10% of each of the following tenures of residential units shall be so provided: market housing units, intermediate housing units, social rented housing units and affordable rented housing units; and 10% of each of the following sizes of residential units shall be so provided: one, two and three bedroom units).

The actual number of units to be provided in accordance with this condition would be the number of whole units that is as near as arithmetically possible to the specified percentage and 0.5 or above shall be rounded up to the nearest whole.

Reason: To ensure adequate accessible housing is provided and to be in accordance with Local Plan 2015 Policy BN.5: Requiring inclusive design.

**23. Refuse storage**

~~Before the first occupation of the development hereby permitted on either of the 14ML Land or the W14 Land, the refuse and recycling storage arrangements for the respective area of land shown on the approved drawing/s shall be provided and made available for use by the occupiers of the Development and the facilities provided shall thereafter be retained for the life of the Development of the respective area of land and neither they nor the space they occupy shall be used for any other purpose unless it can be demonstrated that these facilities are no longer required or can be reduced in size without affecting recycling rates, to be agreed in writing by the Local Planning Authority.~~

~~Reason: To ensure there are refuse storage arrangements in place in advance of the use commencing to avoid highway and safety issues and ensure that that the refuse would be appropriately stored within the site in the interest of protecting the amenity of the site and the area in general from litter, odour and potential vermin/pest nuisance.~~

Before the first occupation of the development hereby permitted on either of the 14ML Land or the W14 Land, the waste and recycling storage, transfer and collection arrangements for the respective area of land shown on the approved drawing/s shall be submitted in a Waste Servicing Strategy and approved in writing by the local planning authority, and made available for use by the occupiers of the Development. The Waste Servicing Strategy shall include the following key pieces of information:

- Details and plans showing where residents and commercial tenants in each building should deposit their waste;
- Description of the waste containers and equipment to be housed in each waste storage area;
- Explanation of how and when waste containers will be transferred between waste rooms, the route the container will take during transfer, and where the necessary equipment for facilitating the operations will be stored; and
- Details of arrangements for collection contractors for municipal and commercial waste, including where the waste will be collected from, where the waste collection vehicles will park, and the route for transferring bins between waste stores and vehicles.

The facilities and management processes provided shall thereafter be retained and maintained for the life of the Development of the respective area of land and neither they nor the space they occupy shall be used for any other purpose unless it can be demonstrated that these facilities are no longer required or can be reduced in size without affecting recycling rates, to be agreed in writing by the Local Planning Authority.

~~Reason: To ensure there are refuse storage and management arrangements in place in advance of the use commencing to avoid highway and safety issues and ensure that that the refuse would be appropriately stored within the site in the interest of protecting the amenity of the site and the area in general from litter, odour and potential vermin/pest nuisance in accordance with Policy S.6 of the Local Plan.~~

## **PARKING, SERVICING, CYCLES**

### **24. Deliveries and servicing management plan**

The development on either of the 14ML Land or the W14 Land shall not be occupied until a delivery and servicing management plan (DSMP) detailing how all elements of the respective area of land are to be serviced has been submitted to and approved in writing by the Local Planning Authority. The DSMP shall be prepared in accordance with TfL's online guidance on delivery and servicing plans found at <http://www.tfl.gov.uk/info-for/freight/planning/delivery-and-servicing-plans#on-this-page-1> or such replacement best practice guidance as shall apply at the date of submission of the DSMP. The approved DSMP for the respective area of land shall be implemented from first occupation of that respective area of land and thereafter for the life of the development on the respective area of land.

Reason: In the interests of highway and pedestrian safety and residential amenity making adequate provision for deliveries and servicing, and encouraging sustainable

delivery methods in accordance with Policy 6.11 of the London Plan and Policy T4 of the Local Plan 2015.

**25. Cycle Storage and facilities- details to be submitted**

The development on either of the 14ML Land or the W14 Land shall not be occupied until details (1:50 scale drawings) of the facilities to be provided for the secure storage of cycles (for both residential and commercial elements) and on site changing facilities and showers (for the commercial element) for the respective area of land have been submitted to and approved in writing by the Local Planning Authority. The development of the respective area of land shall be carried out in accordance with the approved details and the facilities provided shall be retained for the life of the development on the respective area of land and the space used for no other purpose.

**Reason:** In order to ensure that satisfactory secure cycle parking and facilities for cyclists are provided and retained and in accordance with Policy 6.3 of the London Plan and Policy T4 of the Local Plan 2015.

**SUSTAINABILITY**

**26. Renewable energy**

The development on the W14 Land and on the 14ML Land shall be constructed and operated in accordance with the submitted Price/Myers Energy Assessment dated 1st April 2015 and achieve reductions in regulated CO2 emissions through the use of on-site renewable energy generation sources approved as part of this development.

**Reason:** To ensure a high standard of sustainable design and construction and to ensure sufficient information is available to monitor the effects of the development in accordance with Policy 5.2 of The London Plan and Policies S2 and S5 of the Local Plan 2015.

**27. BREEAM**

Before any fit out works to the commercial premises hereby authorised begins, an independently verified BREEAM report (detailing performance in each category, overall score, BREEAM Design Stage rating and a BREEAM certificate of building performance) which demonstrates that a minimum 'Excellent' rating has been achieved shall be submitted to and approved in writing by the Local Planning Authority and the development of the respective area of land shall not be carried out otherwise than in accordance with any such approval given. Within three months of occupation of any commercial premises hereby permitted, a certified Post Construction Review (or other verification process agreed with the Local Planning Authority) shall be submitted to and approved in writing by the Local Planning Authority, confirming that the agreed standards above have been met.

**Reason:** To ensure that high standards of sustainability are achieved in in accordance with Policies 5.2, 5.3 and 5.6 of The London Plan and Policies S2 and S4 of the Local Plan 2015.

**28. Material samples**

Prior to the construction of the development on either of the 14ML Land or the W14 Land, a schedule detailing brick bonding(s) where appropriate and samples of materials and finishes to be used on the external surfaces of the development of the respective area of land shall be submitted to and approved in writing by the Local Planning Authority. The approved materials shall be used in the implementation of the development. A sample panel(s) of brickwork of not less than 1m x 1m showing

mortar mix, bonding and pointing type shall be constructed for the Local Planning Authority to inspect and approve and shall be retained on site until completion of the works, and the brickwork shall be constructed in accordance with the approved panel(s).

Reason and pre-commencement justification: To safeguard the appearance of the buildings and the character of the area generally and to enable the Local Planning Authority to properly consider and control the development **in the interest of visual amenity and to accord with Policies BN.1 and BN.4 of the Local Plan 2015.**

- 29.** Prior to the construction of buildings 1 and 2 details of the architectural design, including façade treatment, as well as materials and colour of the external surfaces of the podium level (levels 1, 2 and 3) of buildings 1 and 2 (at a scale to be agreed with the Local Planning Authority) shall be submitted to and approved in writing by the Local Planning Authority and shall be constructed in accordance with the approved details.

Reason and pre-commencement justification: **To safeguard the appearance of the buildings and the character of the area generally and to enable the Local Planning Authority to properly consider and control the development and to be in accordance with Strategic Policy SP.3 and Policies BN.1 and BN.10 of the Local Plan 2015.**

**30. Detailed drawings**

Prior to the construction of the development on either of the 14ML Land or the W14 Land detailed drawings including sections (at a scale to be agreed in writing with the Local Planning Authority) of the respective area of land shall be submitted to and approved in writing by the Local Planning Authority as follows:

- Commercial façade details (ground level entrances and shopfront): elevations and sections through shopfronts, including details of doors, windows and signage and junctions with new pedestrian space as well as canopies, security shutters and areas for signage;
- Residential façade details (including elevations facing the internal courtyard): elevations and sections annotated with materials and finishes of all windows (including reveals and sills), entrances, external bin stores, balconies, and balustrades, pipework and parapets; and all openings adjacent to the highways.

The development of the respective area of land shall not be carried out otherwise than in accordance with the approved drawings.

Reason and pre-commencement justification: To enable the Local Planning Authority to properly consider and control the development and to be in accordance and in order to ensure a high quality of design and detailing is achieved **and to be in accordance with Strategic Policy SP.3 and Policies BN.1, BN.4 and BN.10 of the Local Plan 2015.**

**31. Green/brown roof**

Prior to the construction of the development on either of the 14ML Land or the W14 Land, details of the biodiverse roofs (including a specification and maintenance plan) to buildings 2, 3, 4, 5, 6, 7 and 8 as relevant to the respective area of land shall be submitted to and approved in writing by the Local Planning Authority and the

development of the respective area of land shall not be carried out otherwise than in accordance with any such approval given.

Reason and pre-commencement justification: To ensure the proposed development would enhance the visual **amenities of the locality and is designed for the maximum benefit of local biodiversity and to be in accordance with Policy S.4 of the Local Plan 2015.**

**32. Landscaping Plan (including roof terrace)**

Prior to the construction of the development on either of the 14ML Land or W14 Land as shown on the appended phasing plan detailed drawings, the following information regarding the respective area of land shall be submitted to and approved in writing by the Local Planning Authority:

- Detailed drawings 1:50 and 1:10 of a hard and soft landscaping scheme showing the treatment of all parts of the site not covered by buildings and roof terrace areas (**including wind mitigation measures**, boundary treatments, surfacing materials of any parking, access, or pathways layouts, materials and edge details and material samples of hard landscaping);
- Full details of the treatment of site boundaries (including bollards)
- Details of all existing trees on and adjacent to the land, and details of any to be retained, together with measures for their protection, during the course of development;
- Waterside landscaping;
- Planting schedules;
- Sections through street tree pits;
- Species mix;
- Details of biodiversity enhancements (bird and bat nesting boxes etc);
- Details of the increased marginal habitat highlighted in Appendix 14.1 of the 2014 Environmental Statement
- Details including plans, elevations and specifications of any play equipment to be provided and
- Details of parapet/balustrade and planting buffer around the roof terrace

The planting, seeding and/or turfing shall be carried out as approved in the first planting season following completion of building works comprised in the development of the respective area of land and any tree or shrub that is found to be dead, dying, severely damaged or diseased within two years of the completion of the building works OR two years of the carrying out of the landscaping scheme (whichever is later), shall be replaced in the next planting season by specimens of similar size and species in the first suitable planting season. Planting shall comply to BS:4428 Code of practice for general landscaping operations, BS:3996 Nursery stock specification, BS:5837 Trees in relation to construction and BS:7370 Recommendations for

establishing and managing grounds maintenance organisations and for design considerations related to maintenance. All other works including hard surface materials and play equipment shall be carried out and completed prior to the occupation of any part of the development.

Reason and pre-commencement justification: To ensure the proposed development would enhance the visual amenities of the locality and in order that the Local Planning Authority may ensure that the design and details are of high quality **and to be in accordance with Policies BN.1 and BN.3 of the Local Plan 2015.**

**33. Overheating**

Before the construction of the façades for the development on either of the W14 Land or the 14ML Land, an assessment of the internal temperature in summer of the development of the respective area of land shall be submitted to the Local Planning Authority, so as to demonstrate compliance with the requirements of the Building Regulations 2010 (Schedule 1, Part L) such assessment must use the method of calculation set out in the SAP 2012 (Appendix P) (or any subsequent edition of the SAP as may amend or replace the 2012 edition, as published by BRE). The assessment shall include details of any mitigation measures that are proposed to be used to reduce overheating, which shall include without limitation and where appropriate design of the facades; provision of ventilation; and internal layout. The mitigation measures shall be approved in writing by the Local Planning Authority. Following approval of the mitigation measures the building on the respective area of land shall be constructed in accordance with the approved details and maintained in this condition thereafter, unless other minor variations are agreed in writing by the Local Planning Authority.

**Reason: To ensure a comfortable level of amenity for residents of the development and in the interests of visual amenity and to be in accordance with Policy S.4 of the Local Plan 2015.**

**34. Photovoltaics**

Prior to the commencement of the development on either of the W14 Land or the 14ML Land full details of photovoltaic (PV) panels and a strategy for their installation on the respective area of land shall be submitted to and approved in writing by the Local Planning Authority. The development of the respective area of land shall only be carried out in accordance with the approved details. The approved details shall be implemented prior to the first use of the building on the respective area of land and shall thereafter be permanently maintained to the satisfaction of the Local Planning Authority.

**Reason and pre-commencement justification: To ensure that the development incorporates renewable technologies and meets a high standard of sustainable design and construction and is in accordance with Policy 5.2 of the London Plan and Policies S2 and S5 of the Local Plan 2015.**

**PERMITTED DEVELOPMENT**

**35. Land Use**

Notwithstanding the provisions of Part 3 of Schedule 2 of the Town and Country Planning (General Permitted Development) Order 2015 (or any Order revoking and/or re-enacting that Order) the commercial premises hereby approved shall only be used for the purposes specified in the application (being use class B1 as defined in the

Schedule to the Town and Country Planning (Use Classes) Order 1987 (as amended at the date of this permission).

Reason: To provide control over the loss of employment generating use in accordance with the regeneration objectives for the Legacy Corporation area as set out in its purposes and within the Corporation's Local Plan.

In granting this permission the Local Planning Authority has had regard to the special circumstances of this case and wishes to have the opportunity of exercising control over any subsequent alternative use.

### **CAFÉ / BREAKOUT AREA**

**36. Café / breakout hours of use**

The café / breakout area shall not operate outside the hours of 0800 to 2300 hours on any day.

Reason: In the interests of residential amenity and to be in accordance with Policy BN.1 of the Local Plan 2015.

**37. Café / breakout external alterations**

Prior to the use of the café/breakout area, details of the external appearance and specification of the flue at roof top level to Building 1 shall be submitted and approved in writing by the Local Planning Authority and this shall be implemented before first use of the café/breakout area, and also retained in accordance with the approved details. No other external flue, ventilation equipment or any other external alteration shall be installed in relation to the café / breakout use without the prior written approval of the Local Planning Authority.

Reason: In the interest of visual and resident amenity **and to be in accordance with Policy BN.1 of the Local Plan 2015.**

**38. Lighting**

No architectural lighting, security lighting or other external means of illumination of the site shall be provided, installed or operated in the development, except in accordance with a detailed scheme which shall provide for lighting that is low level, hooded and directional, and has been submitted to and approved in writing by the Local Planning Authority. The scheme shall be implemented in accordance with the approved details and retained thereafter.

Reason: In the interest of residential amenity and to **protect ecological systems in accordance with Local Plan policy BN.3.**

### **INFORMATIVES:**

1. The Scheme is liable for Mayoral and LLDC CIL
2. The applicant is reminded that this planning permission is subject to the accompanying S106 Legal Agreement.
3. Thames Water Informatives:



- The applicant is reminded of their responsibility to make proper provision for drainage to ground, water courses or a suitable sewer.
- The applicant is reminded of the need for a groundwater discharge permit
- The developer should take account of minimum water pressure in the design of the proposed development.

#### 4. English Heritage Greater London Archaeology informatives

- Written schemes of investigation would need to be prepared and implemented by a suitably qualified archaeological practice in accordance guidelines.
- In order to protect groundwater quality from further deterioration. No infiltration based sustainable drainage systems should be constructed on land affected by contamination as contaminants can remobilise and cause groundwater pollution.
- Piling or any other foundation designs using penetrative methods should not cause preferential pathways for contaminants to migrate to groundwater and cause pollution.
- All investigations of land potentially affected by contamination should be carried out by or under the direction of a suitably qualified competent person.

### **Appendices**

Appendix 1 Location Plan

Appendix 2 Drawings

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Subject: Land at Marshgate Lane, Pudding Mill, Stratford, London E15 2NH  
(Reference 17/00669/VAR)

Date: 22 May 2018

Report to: Planning Decisions Committee

Report of: Anne Ogundiya, Principal Planning Development Manager

FOR DECISION

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**This report will be considered in public**

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## 1. Introduction

1.1 This report updates members on the following:

- Residential GIA amendments
- Amends Tables 1, 2, 4 and, 8 of the PDC report
- Amends the triggers to conditions 13 and 14
- Additional informative included
- Density error
- Typographical error
- Section 96a non-material amendment application

## 2. The applicant has updated the residential GIA floorspace as follows:

2.1 Proposal Description page 391 - The variation to Condition 2 (approved drawings) seeks to make changes to the approved scheme to include: (1) increase of employment use from 4,257m<sup>2</sup> to 5,671m<sup>2</sup> (additional 1,414m<sup>2</sup> (GIA) floorspace); (2) decrease of residential floorspace from 28,003m<sup>2</sup> to ~~27,295m<sup>2</sup>~~ **27,210m<sup>2</sup>** (decrease of ~~708~~**793m<sup>2</sup>** (GIA) floorspace); (3) optimisation of residential floorspace, increasing the number of residential units from 254 to 275; and (4) alterations to façades.

2.2 Paragraph 10.22: The residential GIA reference in the third sentence should read **793m<sup>2</sup>** and not 708m<sup>2</sup>. It should be noted that the overall residential floorspace decrease still equates to 3%.

## 3. Table Amendments

**Table 1 – Summary of Changes**

	<b>Extant Permission 14/00422/FUL</b>	<b>s73 Variation to condition 2 - approved drawings</b>	<b>Difference</b>

Residential units	254	275	Increase of 21 units (8.3%)
Residential Floorspace	23,239m <sup>2</sup> 28,003m <sup>2</sup> (GIA)	22,531m <sup>2</sup> 27,210m <sup>2</sup> (GIA)	-708m <sup>2</sup> -793m <sup>2</sup> (GIA) (3%)
		For changes to residential unit mix (see Table 2 below)	
B1 Floorspace	4,257m <sup>2</sup> (GIA)	5,671m <sup>2</sup> (GIA)	Increase of 1,414m <sup>2</sup> (33%)
Car Parking	40 spaces	40 spaces	unchanged
Plus Accessible Car Parking	29 spaces (residential and commercial)	29 spaces (residential and commercial)	Unchanged
Cycle Parking	588 spaces (residential and commercial) <b>590</b>	<b>597</b> 595 spaces (residential and commercial)	+7
Amenity Space	2,109m <sup>2</sup>	2,036m <sup>2</sup>	-73m <sup>2</sup> (3.4%)
		Alterations to façades*	

Table 2 - Overview of Residential Mix and Affordable Housing

Unit Type	Extant Permission					s73				
	Number of Units			Percentages		Number of Units			Percentages	
	Market	Affordable	Totals	Affordable	All Units	Market	Affordable	Totals	Affordable	All Units
Studio	0	0	0	0	0	10	0	10	0	4
1 Bed	83	17	100	38	39	53	18	71	33	26
2 Bed	62	14	76	31	30	119	22	141	41	51
3 Bed	60	13	73	29	29	37	13	50	24	18
4 Bed	4	1	5	2	2	2	1	3	2	1
<b>Total Number of Beds</b>	<b>209</b>	<b>45</b>	<b>254</b>	<b>100%</b>	<b>100%</b>	<b>221 (+12) (+14)</b>	<b>54 (+9)</b>	<b>275</b>	<b>100%</b>	<b>100%</b>

**Table 4 - s73 Variation – Unit Mix**

Unit Type	Total Units	Adjustment	% Provision
Studio	10	+10	4%
1 bed	71	-22-29	26%
2 bed	141	+65	51%
3 bed	50	-23	18%
4 bed	3	-2	1%
<b>TOTAL</b>	<b>275</b>		<b>100%</b>

**Table 8**

**Indicative s73 Tenure Split**

Tenure	Studio	1 bedroom	2 bedroom	3 bedroom	4 bedroom	All Bedroom Types Total
<b>Market</b>	10 (1% of total market)	<del>59</del> 53 (27% of total market)	119 (55% of total market)	37 (17% of total market)	2 (1% of total market)	221 (80% of total housing)
<b>Shared Ownership</b>	0	5 (24% of total intermediate)	14 (67% of total intermediate)	2 (9% of total intermediate)	0	21 (8% of total housing, 39% of total affordable housing)
<b>London Affordable Rent (LAR)</b>	0	13 (39% of total LAR)	8 (24% of total LAR)	11 (34% of total LAR)	1 (3% of total LAR)	33 (12% of total housing, 61% of total affordable housing)
<b>All Tenures Total</b>	<b>10 (4%)</b>	<b>71 (26%)</b>	<b>141 (51%)</b>	<b>50 (18%)</b>	<b>3 (1%)</b>	<b>275 (100%)</b>

#### **4. Amendments to the triggers to the remediation conditions 13 and 14**

- 4.1 In order to be consistent with the consented scheme officers have included the phrase ‘...**except demolition to existing slab level...**’ after the words ‘...No development ...’ to condition 13 and after the words ‘...No phase shall be commenced...’ to condition 14. This would enable demolition works to be carried out prior to the submission of remediation details to PPDT.

#### **5. Informative (Fire Safety Regulations)**

- 5.1 Please note that this planning application has been assessed against current planning legislation only. The applicant (or any subsequent owner or developer) is therefore reminded that the onus of responsibility to ensure the development meets current fire safety regulations lies fully with them and that they are legally obliged to obtain the relevant Building Regulations consent.
- 5.2 In accordance with LFEPA guidance the applicant is strongly recommended to ensure the installation of sprinklers in the new development. The development shall conform with Part B5 of Approved Document B of the Building Regulations.

#### **6. Density Error**

- 6.1 The s73 proposals would result in a residential density of ‘...~~722~~ **675** habitable rooms per hectares...’

#### **7. Typographical Error**

- 7.1 Paragraph 10.15. The figure in the first sentence should read as **1,414m<sup>2</sup>** and not 1,4141m<sup>2</sup>.
- 7.2 Refer to the subject heading on page 387 – the application reference suffix is recorded as AOD. It should read **17/00669/VAR**.

#### **8. Section 96a Non-material amendment submission**

- 8.1 The applicant has recently submitted a section 96a non-material amendment application (NMA) which seeks to change the description of development to match the proposed variations to the approved drawings permitted under 14/00422/FUL. Should the variations (17/00669/VAR) be approved by members, officers would seek to approve the submitted NMA, with a decision issued at the same time as the variation 17/00669/VAR.

Appendix 1 Location Plan



Aerial photo of Site



Aerial view of Consented Scheme

## Ground Floor Plan



## Appendix 2 Drawings

### Building 1 and 2



Building 1 and 2: Southern elevation as consented





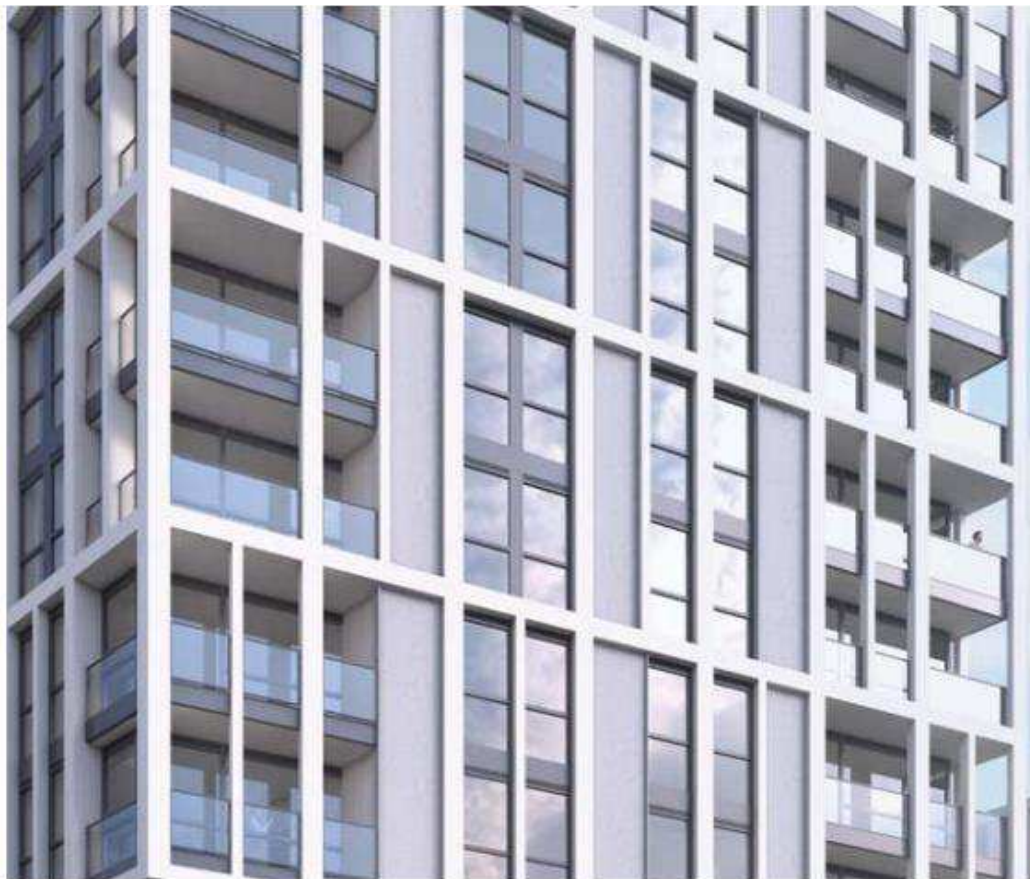
Building 1 and 2: Southern elevation as proposed



Building 1 and 2: Level 3 as consented



Building 1 and 2: Employment use typical level 3 as proposed



Bay study image of Buildings 1 and 2



Building 4: Consented East Elevation



Building 4: Proposed East Elevation



Bay Study image of Buildings 4 and 6



Building 7: Consented South Elevation



Building 7: Proposed South Elevation



Bay Study image of Buildings 3 and 7

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**Subject:** 1-2 Hepscoth Road, London and 33-35 Monier Road, London, E3 2PR - (18/00302/DOV) and (18/00303/DOV)

**Meeting date:** 25 September 2018

**Report to:** Planning Decisions Committee

**Report of:** Anne Ogundiya, Principal Planning Development Manager

**FOR DECISION**

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**This report would be considered in public**

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## **1. EXECUTIVE SUMMARY**

- 1.1. This report considers two inter linked deed of variation submissions relating to two s.106 legal agreements for the following two sites:
  - 1.1.1 1-2 Hepscoth Road (15/00446/FUL) mixed use redevelopment comprising 6-storey building to provide 922 sqm (GIA) of commercial space (use class B1c) with yard area, 30 residential dwellings (7 affordable housing), 14 x 1 bed, 12 x 2 bed and 4 x 3 bed, amenity areas, disabled parking, cycle parking and refuse/recycling stores.
  - 1.1.2 33-35 Monier Road (15/00212/FUL) mixed use redevelopment comprising 6-storey building comprising 667m<sup>2</sup> of commercial workspace on the ground floor and 45 residential flats (10 affordable housing) on the upper floors; 15 x one bedroom, 17 x 2 bedroom and 13 x 3 bedroom. The proposal includes landscaped public realm and roof top level amenity space as well as on site cycle parking and refuse facilities.
- 1.2. When planning permission was granted for these two sites they were each subject to a legal agreement controlling, amongst other things, the tenure of the residential flats. The owners of the building have now undertaken a review of the accommodation and following this review they have approached officers with a view to renegotiate the Heads of Terms of the s.106 legal agreements, specifically as far as they relate to affordable housing.
- 1.3. The Hepscoth Road (15/00446/FUL) proposed deed of variation seeks to remove the requirement to provide the approved 7 units of affordable housing on the site. The extant legal agreement requires the provision of 7 affordable units delivered as part of the 30-unit scheme.

- 1.4. The applicant has advised that all of the approved residential units (30 units) at Hepscott Road including the 7 affordable units would be delivered as private rented units (PRS). This would be secured via the deed of variation. The PRS would be delivered through the applicant's PRS brand, Vive Living.
- 1.5. The Monier Road (15/00212/FUL) deed of variation seeks to increase the provision of affordable housing by 7 units from the approved 10 affordable units on the site. Resulting in a total of 17 of the approved 45 housing units being affordable units. The additional 7 affordable units arise from the loss of the Hepscott Road affordable units and the tenure is not altered from that which was originally proposed.
- 1.6. The key issue in considering the submitted variation details relates to the following:
  - Housing Mix / Need
  - Housing Location
- 1.7. The applicant has demonstrated that both sites would still deliver a range of housing tenure types, with a good mix of tenures. Officers are confident that the quality of accommodation would be high regardless of tenure type or location. The proposal successfully optimises the capabilities of the site and would deliver much needed housing units of a suitable standard, meeting the needs of future occupants and the housing market more generally.
- 1.8. Officers are satisfied that the issues raised have been robustly addressed. Given the above, Officers have concluded that rather than insist on the retention of the 7 affordable units within the 1-2 Hepscott Road development it is reasonable to agree to the proposed variations as the developments would continue to make a significant contribution to meeting housing need as well as affordable homes in the local area.
- 1.9. The proposals are considered to be acceptable in planning policy terms (Local Plan policies H1 and H2 refer). As such, it is recommended that both deeds of variation are agreed as set out in the recommendation below.

## **2. RECOMMENDATIONS**

### **2.1 The Committee is invited to:**

- a) **Application reference 18/00302/DOV APPROVE the Deed of Variation pursuant to s.106 of the Town and Country Planning Act 1990 (as amended) as detailed in Section 7 and the appended draft Heads of Terms.**
- b) **AGREE TO DELEGATE AUTHORITY to the Director of Planning Policy and Decisions to:**
  1. **Finalise the Deed of Variation referred to above as the Director of Planning Policy and Decisions considers reasonably necessary;**
  2. **Complete and issue the Deed of Variation referred to above.**
- a) **Application reference 18/00303/DOV APPROVE the Deed of Variation pursuant to s.106 of the Town and Country Planning Act 1990 (as**

amended) as detailed in Section 7 and the appended draft Heads of Terms.

**b) AGREE TO DELEGATE AUTHORITY to the Director of Planning Policy and Decisions to:**

**1. Finalise the Deed of Variation referred to above as the Director of Planning Policy and Decisions considers reasonably necessary;**

**2. Complete and issue the Deed of Variation referred to above.**

**3. FINANCIAL IMPLICATIONS**

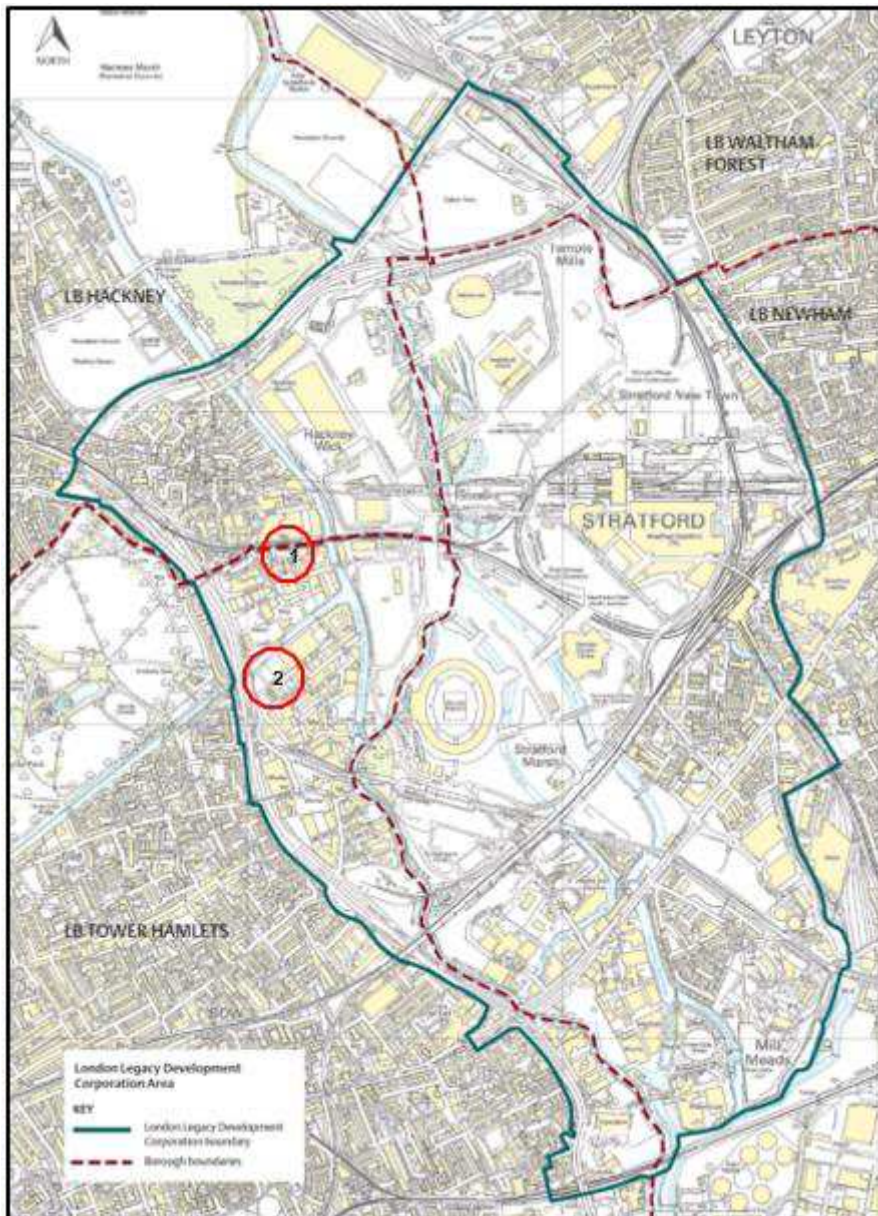
3.1 There are no financial implications

**4. LEGAL IMPLICATIONS**

4.1. The recommendation is that the proposed deeds of variation (DoV) to the s.106 legal agreements dated 20 June 2016 and 24 March 2016 relating to planning permission references 1-2 Hepscott Road (15/00446/FUL) and 33-35 Monier Road (15/00212/FUL) respectively are granted.

4.2. The proposed variations and changes can only be legally affected by the deeds of variation to the s.106 agreements. The draft Heads of Terms appendices 3 and 4 to this report set out the proposed changes to the s.106 agreements. It is intended that LLDC PPDT would enter into negotiations with the applicant to agree the precise wording of the DoVs to the s.106 agreements. All reasonable legal costs incurred by the LLDC would be covered by the applicant.

## SITE PLAN



**Location:** 1-2 Hepscoth Road ❶, Hackney Wick and 33-35 Monier Road ❷, Fish Island, London

**London Borough:** Tower Hamlets

**Proposal:** Two Deed of Variations (DoV) to the s.106 agreements at 1-2 Hepscoth Road (15/00446/FUL) and 33-35 Monier Road (15/00212/FUL) to amend the distribution of the affordable homes.

**Applicants:** Aitch Group

**Agent:** CMA Planning

## 5. SITE & SURROUNDINGS

- 5.1. 1-2 Hepscott Road: The site is a broadly rectangular shaped plot located on the eastern side of Hepscott Road. It is bordered by White Post Lane to the north, Hepscott Road to the west and Rothbury Road to the south. The site is approximately 0.1185 hectares in size and in current use as a stone cutters yard (use class B8). The River Lea Navigation is located approximately 0.1 miles to the east and a similar distance to the south. Hackney Wick station is located approximately 54 metres to the north of the site.
- 5.2. The site is within the Hackney Wick Neighbourhood Centre boundary and site allocation SA1.1 (Hackney Wick Station Area) as detailed within the LLDC Local Plan. The site is within an area of flood risk (Flood Zone 3) and is adjacent to the boundary of the Fish Island and White Post Lane Conservation Area.
- 5.3. The surrounding area is characterised by predominantly warehouse and light industrial buildings. Immediately adjacent to the site is a small scrapyards and a car mechanics.
- 5.4. 33-35 Monier Road: The site is roughly square shaped and bounded by Monier Road to the south east, is bounded by other commercial sites (part of the Neptune Wharf development site ref: 12/00210/OUT). The site is approximately 0.157 ha in area and is currently occupied by a part single, part two storey commercial post-war building with an open yard on the northern part of the site.
- 5.5. The immediate surrounding area is mainly commercial in use and characterised by buildings of post war 20<sup>th</sup> century date and open yards. These existing uses include wholesalers, gas equipment suppliers and building material suppliers. Some of these uses and structures, such as the gas equipment suppliers, have been removed/demolished as part of the approved Neptune Wharf redevelopment.

## 6. RELEVANT PLANNING HISTORY

- 6.1. Neptune Wharf 12/00210/OUT was approved 27th March 2014 and surrounds the application site to the west and north. The Neptune Wharf development is a part outline, part detailed application for planning permission.
  - The scheme is being developed in four phases, phases 1 and 2 of which would be opposite the north of the application site. The Neptune Wharf scheme includes public access via a northwards continuation from Smeed Road towards the Hertford Union canal. A six-storey block (Block N) was approved in detail within Phase 2 of the approved Neptune Wharf scheme and directly adjoins the north-west boundary of the 33-35 Monier Road site.
  - Neptune Wharf, Phase 3 includes the erection of three buildings (Blocks O, P and Q) with commercial uses at ground floor and residential units above.

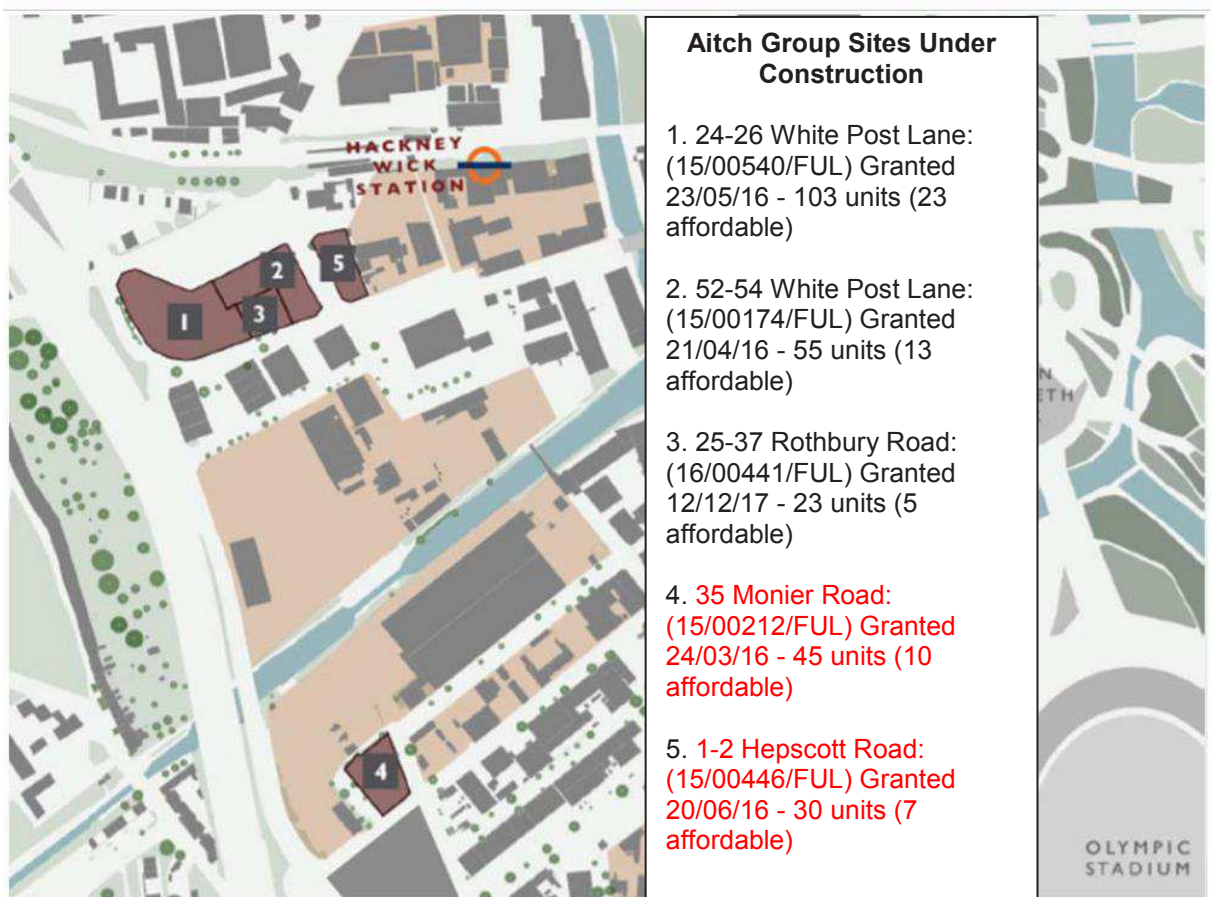
- Phase 4 comprises a 3 Form Entry Primary School. The Primary School site is approved to the south west of Remus Road with vehicular access from the school site to Wyke Road approved directly to the north of the application site.
- 6.2. 14/00374/FUL (planning permission granted September 2015): This scheme known as 'Monier Road West' is sited to the east of Smeed Road and has planning permission for mixed use redevelopment. The approved plans show six storey development fronting Smeed Road with commercial ground floor use (2,2250m<sup>2</sup>) and residential flats (120 flats) on the upper floors. This site is located to the immediate east of 33-35 Monier Road.
  - 6.3. 13/00204/FUM (full planning permission granted March 2014): This scheme known as 'Monier Road East' is sited at the junction of Monier Road and Roach Road and has planning permission for mixed use redevelopment. The permission comprises 483 m<sup>2</sup> of commercial floorspace at ground floor and 71 residential units on the upper floors which range from four to seven storeys in height. This site is located to the east of 33-35 Monier Road.
  - 6.4. Aitch Group Planning Permissions within Hackney Wick and Fish Island:
  - 6.5. 52-54 White Post Lane (reference: 15/00416/FUL) - Application for the redevelopment of the Mr Bagel warehouse for 2367sqm (GIA) of commercial space (use class B1c) with commercial yard and 55 residential dwellings. Planning permission approved on the 21<sup>st</sup> April 2016 - The redevelopment of the site is underway.
  - 6.6. 24-26 White Post Lane (reference: 15/00540/FUL) - Application seeking the demolition of the existing building and the erection of 4no. linked buildings up to 6no. storeys in height to include 103 residential units and 2,916 m<sup>2</sup> of commercial floorspace (Use Class B1(c)). Planning permission approved on the 23<sup>rd</sup> May 2016- The redevelopment of the site is underway.
  - 6.7. 33-35 Monier Road (reference: 15/00212/FUL) - Application for mixed use 6 storey redevelopment incorporating 667 m<sup>2</sup> of commercial workspace (B1 use class) at ground level and ancillary café/breakout space; along with 45 residential units at levels one to five; new public realm, landscaped amenity space and associated works. Planning permission approved on the 24<sup>th</sup> March 2016 - The redevelopment of the site is underway.
  - 6.8. 1-2 Hepscott Road (reference 15/00446/FUL) - Application for demolition of existing buildings and construction of a new 6 storey building to provide 922 sqm (GIA) of commercial space (use class B1c) with yard area, 30 residential dwellings (14 x 1 bed, 12 x 2 bed and 4 x 3 bed), amenity areas, disabled parking, cycle parking and refuse/recycling stores. Planning permission approved on the 20<sup>th</sup> June 2016 - The redevelopment of the site is underway.
  - 6.9. 1-7 Dace Road (reference 16/00462/FUL) - Application for the demolition of all buildings to enable a mixed use redevelopment of four new blocks ranging in height

from five (5) to six (6) storeys (with some single storey elements) to provide B1a, B1c, B2 and B8), with a maximum floorspace of B1a up to 2,500 sq m, 110 residential units including affordable housing (Use Class C3) and associated open space, amenity areas, vehicular access, accessible parking, cycle parking and refuse/recycling stores. Planning permission approved on the 25<sup>th</sup> January 2018 - The redevelopment of the site has not yet commenced.

## 7. DESCRIPTION OF DEVELOPMENT

7.1 This report considers two Deed of Variations (DoV) to the s.106 agreements at 1-2 Hepscott Road (15/00446/FUL) dated 20 June 2016 and 33-35 Monier Road (15/00212/FUL) 24 March 2016 to amend the distribution of the affordable homes.

7.2 The location plan shows the sites which are numbered 4 and 5.



Location Plan 1

7.3 **18/00302/DOV: 1-2 Hepscott Road - Deed of Variation (15/00446/FUL – Extant Consent)**

7.4 The original agreement for 1-2 Hepscott Road dated 20 June 2016 requires the provision of the following affordable housing units:

**Table 1**

<b>Units</b>	<b>1 person</b>	<b>2 bed/2 person</b>	<b>3 bed/4 person</b>	<b>3 bed/5 person</b>	<b>Total Units</b>
<b>Intermediate</b>	<b>2</b>	<b>2</b>	<b>0</b>		<b>4</b>
<b>Affordable Rented</b>	<b>2</b>	<b>0</b>	<b>1</b>		<b>3</b>

- 7.5 50% of the affordable housing units are required to be provided prior to the occupation of 50% of the market housing units and 100% of the affordable housing units are required to be provide prior to the occupation of 70% of the market housing units.
- 7.6 The original agreement requires a viability review to be undertaken in the event that development has not commenced within 18 months of the date of the planning permission. Officers do not consider it necessary to reimpose a viability review obligation, the Hepscott Road development is under construction, and the applicant has advised that completion is currently anticipated on the 7<sup>th</sup> December 2018 (marketing of the units has commenced). They have added that the date is dependent on the CHP system which is part of the approved 24-26 White Post Lane development, being energised and fully tested and commissioned to provide heat and power to Hepscott Road.
- 7.7 Proposed Revised Position
- 7.8 The proposed revised position is that no affordable housing units would be provided on site at Hepscott Road. Instead all seven units would be provided on the Monier Road Development in the same tenure mix set out in the Table 1 above. Instead Hepscott Road would deliver private sector rentals (PRS) only through Aitch Group's PRS brand, Vive Living.
- 7.9 The provision of all the affordable housing units on the Monier Road Development would be required to be provided prior to the occupation of any of the dwellings on Hepscott Road.



7.10 **18/00303/DOV: 33-35 Monier Road - Deed of Variation (15/00212/FUL – Extant Consent)**

7.11 The original agreement for 33-35 Monier Road dated 24 March 2016 requires the provision of the following affordable housing units:

**Table 2**

Units	1 bed/2 person	2 bed/4 person	3 bed/5 person	Units
Intermediate	2	1	1	4
Affordable Rented	2	2	2	6

7.12 50% of the affordable housing units are required to be provided prior to the occupation of 50% of the market housing units and 100% of the affordable housing units are required to be provide prior to the occupation of 70% of the market housing units.

7.13 The original agreement requires a viability review to be undertaken in the event that development has not commenced within 15 months of the dated of the planning permission. If required by the viability review an additional 6 Affordable Housing Units (representing 35% provision) or an Affordable Housing Payment towards off-site provision (up to a cap of 50% of the residential units) shall be provided.

7.14 Proposed Revised Position

7.15 The proposed revised position is that an additional seven affordable housing units, being the affordable housing units relocated from Hepscott Road, would be provided on site. Therefore, the revised affordable housing provision would increase from 10 to 17 affordable housing units in the following mix:

**Table 3**

Units	1 bed/2 person	2 bed/4 person	3 bed/5 person	Total Units
Intermediate	4	3	1	8
Affordable Rented	4	2	3	9

7.16 50% of the affordable housing units are required to be provided prior to the occupation of 50% of the market housing units and 100% of the affordable housing units are required to be provide prior to the occupation of 70% of the market housing units. It is noted that the provision of 17 affordable housing units amounts to 38% provision.

7.17 As with Hepscott Road the consented agreement requires a viability review to be undertaken in the event that development has not commenced within 18 months of the date of the planning permission. The Monier Road scheme is under construction, and the applicant has advised that completion is anticipated at the end of September 2018. The marketing of the development has commenced. To that end, as with the Hepscott Road scheme officers do not consider it necessary to reimpose a viability review obligation.

## **8. POLICIES & GUIDANCE**

### **8.1 National Planning Policy Framework (NPPF – July 2018)**

The policies in the NPPF are material considerations in the determination of applications.

The following NPPF policies are relevant to this submission:

- 5. Delivering a sufficient supply of homes
- 8. Promoting healthy and safe communities
- 12. Achieving well-designed places

8.2 For the purposes of S.38(6) of the Planning and Compulsory Purchase Act 2004, the adopted 'Development Plan' for this site current is the London Legacy Development Corporation's Local Plan 2015-2031 (August 2015). The contents of the Government's National Planning Policy Framework are also relevant and have been taken into consideration.

8.3 **The most relevant policies are listed below:**

### **8.4 London Plan (Consolidated Alterations - March 2015):**

The following London Plan policies are relevant to this submission:

Objective 2 - Providing Housing and Neighbourhoods

- 3.4 Optimising Housing Potential
- 3.5 Quality and Design of Housing Developments
- 3.8 Housing Choice
- 3.9 Mixed and Balanced Communities
- 3.10 Definition of Affordable Housing
- 3.12 Negotiating Affordable Housing on Individual Private Residential and Mixed-Use Schemes
- 3.12 Affordable Housing Thresholds
- 4.1 Developing London's Economy

8.5 **London Plan (August 2015) - Housing Standards, and Parking Standards (Draft)**

These two documents are in draft format to be considered by an inspector in a public examination commencing on 21 October 2018. Weight can still be attached to the documents particularly as they have been consulted on.

8.7 **LLDC Local Plan (July 2015) Policies which are relevant to the consideration of this submission are:**

SP.1 – Building a Strong and Diverse Economy

H.1 - Providing a Mix of Housing Types

H.2 - Delivering Affordable Housing

BN.1 - Responding to Place

8.8. **Other Relevant Material Considerations**

Mayor of London - Affordable Housing and Viability SPG (2017)

8.9 **The Draft London Plan (December 2017 with minor suggested changes July 2018)**

The Mayor of London published for the purpose of public consultation a draft new London Plan on 29th November 2017, with minor suggested changes in July 2018. The policies in the draft new London Plan currently have only very limited material weight when making planning decisions. That weight would increase once the new Plan is submitted for its Examination in Public. This report may make reference to policies within the new London Plan where they are directly relevant to the assessment of the application proposal. However, the relevant development plan policies remain those within the current London Plan (March 2016) and the LLDC Local Plan (July 2015).

The following draft London Plan policies are relevant to this submission:

GG3 – Creating a Healthy City

GG4 - Delivering the Homes London Needs

H5 – Delivering Affordable Housing

H6 – Threshold Approach to Applications

H7 - Affordable Housing Tenure

**9. CONSULTATION**

9.1 Similar responses for both proposals: 18/00302/DOV – 1-2 Hepscott Road and 18/00303/DOV – 33-35 Monier Road

- LB Hackney – No objections
- LB Tower Hamlets:
  - The proposal would result in an offsite affordable housing provision and this Local Planning Authority would only consider an off-site provision under special circumstances.

- The reason provided is not considered sufficient to warrant off site provision, furthermore, a minimum of 50% affordable housing should be secured. The proposal would be contrary to the Council's policy DM3 of the Managing Development Documents and therefore it cannot be supported.

## **10. ASSESSMENT OF PLANNING ISSUES**

- 10.1 The main issues in respect of these two-linked deed of variation applications relate to how the applications meet the Corporation's relevant policies on housing tenure mix and affordable housing. Assessment of the development proposals against the London Plan (adopted and the new draft London Plan), the Housing SPG and Local Plan policies H1, H2 and BN.1 in terms of the tenure mix, need and location detail are discussed below.
- 10.2 The applicant (Aitch Group) has secured planning permission for six sites in the Hackney Wick and Fish Island area (1-2 Hepscoot Road, 24-26 and 52-54 White Post Lane, 25-37 Rothbury Avenue, 1-7 Dace Road and 33-35 Monier Road (see appendices)). Four of the sites are located in and around Hepscoot Road in Hackney Wick and form what is known as 'Bagel Island' (Hepscoot Road, Rothbury Road White Post Lane x 2). The other two (Dace Road and Monier Road) are located in Fish Island.
- 10.3 On five of the sites (Hepscoot Road, White Post Lane x 2, Rothbury Road and Monier Road) the permissions have been implemented with construction works being pursued on the sites. The Dace Road planning permission has not been implemented.
- 10.4 The 3 sites adjacent to 1-2 Hepscoot Road (2 x White Post Lane and Rothbury Road) would be delivered as a mix of market and affordable tenure units as would those at 33-35 Monier (28 market and 17 affordable - as proposed by the deed of variation). The five implemented schemes would provide a total of 256 homes and would deliver a total of 58 affordable housing units. Officers note that of the 211 residential units to be delivered by the applicant in Hackney Wick 41 of the units would be affordable housing.
- 10.5 LB Tower Hamlets have commented that the proposals would result in an off-site affordable housing provision for 1-2 Hepscoot Road and that 50% affordable housing should be secured.
- 10.6 The 7 affordable housing units approved as part of the 1-2 Hepscoot Road development would be relocated to the 33-35 Monier Road development. Officers agree that the proposals for Hepscoot amounts to an off-site affordable housing provision.
- 10.7 There is no in principle objection to off-site affordable housing. The draft London Plan policy H5 states that affordable housing must only be provided off-site in exceptional circumstances where it can be demonstrated that an off-site contribution would better deliver mixed and inclusive communities than an on-site contribution. Local Plan policy H2 also allows for an off-site contribution subject to meeting the following criteria, which are addressed in the following assessment:
- Provides equivalent number and type of affordable units across all sites related to the proposal;

- Does not prejudice the delivery of affordable housing;
- Is delivered at no financial advantage to developer;
- Is linked to the completion of the market housing elements of the scheme
- Is located where able to provide for local housing needs; and
- Would be beneficial to achieve and maintain mixed and balanced communities.

10.8 The affordable housing approved at both schemes equated to 23% and 22% respectively; and, as proposed, Hepscoth would have no affordable housing and Monier would have 38% affordable housing. The overall number of affordable housing as a result of the proposed deed of variation changes remains the same. The Local Plan (policy H.1) has subject to viability, set 35% as a minimum target of affordable homes across the whole of the Legacy Corporation area.

### **Housing Mix / Housing Need**

10.9 London Plan policy 3.3 seeks to increase the housing capacity in London and sets out targets for planning authorities which would *inter alia* improve housing choice. The draft London Plan states that to meet the growing housing need, London must seek to deliver new homes through a wide range of development options. Building more housing as part of the development of town centres would also be important, providing homes in well-connected places that would help to sustain local communities.

10.10 1-2 Hepscoth Road: The applicant has advised that the proposals would result in all of the open market sale accommodation at the Hepscoth Road site (30 units including 7 affordable housing units) being delivered as private rented sector housing (PRS), which would in effect offer a fourth tenure of housing across the applicant's various sites (i.e. in addition to the market sale, affordable intermediate and affordable rent tenures). PRS has a key role to play in helping meeting housing needs, for the growing population for whom renting may be favourable, as home ownership may be unachievable and access to social/affordable housing is less unlikely.

10.11 The DoV PRS proposition would enable the applicant to deliver a greater range of choice to future residents in the Hackney Wick Fish Island area in accordance with the London Plan policies 3.8 and 3.9 and the draft London Plan policies GG3 and GG4 which requires new developments to offer a range of housing choices, in terms of the mix of housing sizes and types, taking account of the housing requirements of different groups. Local Plan policy H.1 supports London Plan policy 3.8 and recognises that providing for a range of housing sizes and types is essential to creating sustainable new neighbourhoods and avoids problems that may arise from the over concentration of certain sizes and types of accommodation.

10.12 The Hepscoth Road site is within the Hackney Neighbourhood town centre. The draft London Pan states that '...the particular suitability of town centres to accommodate a

diverse range of housing should be considered and encouraged, including for smaller households, Build to Rent etc'. Although the development has less than 50 units and does not qualify as a Build to Rent scheme as defined by the London Plan Affordable Housing SPG, the applicant has agreed to its general provisions with a covenant within the DoV to ensure the new private rented homes are secured for the rental market for a minimum of 15 years. During this period, the private rented homes would be retained in single ownership. The applicant has also agreed that all the units are self-contained and let separately and that there is a unified ownership and unified management of the development. This would be through the applicant's PRS brand, Vive Living.

- 10.13 Longer tenancies (three years or more) would be made available to all tenants; there would be break clauses for renters, which would enable the tenant to end the tenancy with a month's notice any time after the first six months. In addition, as set out in the SPG the applicant has advised that they would ensure certainty for the period of the tenancy in terms of the rent and service charge, the basis of which would be made clear to the tenant before a tenancy agreement is signed, including any annual increases.
- 10.14 As set out in Table 3, the applicant is not seeking a variation to the tenure mix required by the extant s.106 agreements. The proposal would result in the same level of affordable rent accommodation across the two sites, i.e. 9 homes / 29 habitable rooms, but the level of shared ownership accommodation would increase across the two sites, from 8 homes / 18 habitable rooms to 8 homes / 21 habitable rooms.
- 10.15 Whilst this is a relatively small increase in affordable accommodation, given the need for affordable homes this is considered a benefit and it is reasonable to accept that the proposed variations and the development would continue to make a significant contribution to meeting the need for affordable homes in the local area.
- 10.16 Officers analysis concludes that the proposed DoV to 1-2 Hepscoth Road is acceptable. In coming to this conclusion Officers note the small increase in the affordable accommodation to be provided on Monier Road and the benefits that the introduction of a fourth housing tenure (PRS) would bring to the area in so far as it relates to increased housing choice alongside the proposed mechanism in place to ensure the integrity of the development is maintained, with a unified ownership and management of all the units.

### **Housing Location**

- 10.17 33-35 Monier Road: Officers do not consider that the Hepscoth Road proposals would prejudice the delivery of affordable housing. As required by Local Plan policy H.2 an equivalent number and type of units is being proposed at Monier Road. The proposals would have the effect of moving the 7 approved affordable homes from Hepscoth Road, Hackney Wick, to Monier Road, Fish Island. Whilst the mix of homes of the Monier Road scheme would not change, the tenure would, with the increased affordable homes on the site. The resultant child yield would also increase from the approved child yield of 12 children to 15 children, which would have an impact on play space provision. However, the Monier Road development was approved with a provision of

communal amenity space in excess of the Mayor's Housing SPG targets. 85m<sup>2</sup> of communal amenity space was required to serve the scheme and 193m<sup>2</sup> was approved. As revised in accordance with the SPG targets the community amenity space provision is increased from 85m<sup>2</sup> to 135m<sup>2</sup>, which is still well within the approved 193m<sup>2</sup> communal amenity space provision (an excess of 58m<sup>2</sup>). Officers are satisfied that the approved communal amenity space can accommodate the change to child yield that this change in tenure would generate.

- 10.18 Given the potential increase in child yield and families that might result it could be argued that relocating the affordable housing to the Monier Road site on Fish Island from Hepscott Road is a better location for more affordable homes than Hepscott Road. The Hepscott Road site falls within the Hackney Wick neighbourhood centre boundary, which, in due course, is likely to be busier / noisier than the mixed residential and employment neighbourhood of Fish Island north. This would support the move of the affordable homes which are more likely to contain families to what is likely be a quieter area. Similarly, it is considered that the Hepscott Road development which is nearer the Hackney Wick station Overground Station (within 3 minutes' walk) than the Monier Road site is better located for a Private Rented Sector (PRS) scheme. The most successful PRS schemes are in urban locations, with local transport within walkable distance.
- 10.19 The Monier Road site is located within an area that has a current PTAL rating of 2, with a PTAL forecast of 3. The site located approximately a 12-minute walk from the station. However, whilst further from the station than Hepscott Road, it is located next to the proposed new primary school at the Neptune Wharf development. Indeed, the public space within the northern section of the Monier Road scheme has been designed to provide a pedestrian and cycle route to the approved Neptune Wharf primary school site (appendix 1 - Location Plan 2) and would read as a continuation of Wyke Road.
- 10.20 In general terms, the amount of development currently under construction in and around the Monier Road site (Fish Island north) (i.e. Neptune Wharf, Monier Road West and East, Bream Street, Dace Road etc.), is such that the pace of change is currently greater than the Hepscott Road site. This supports the locational advantages of this site in so far relocating the affordable housing to Monier Road, ensuring the family accommodation is located within a community that is emerging earlier than Hepscott Road. This ties in with objective 2 of the Local Plan in terms of establishing and maintaining locally distinctive neighbourhoods which meet housing needs, while providing easily accessible social infrastructure.
- 10.21 There is currently a constrained highway access via Monier Road but this would develop as the approved bridge over the Lea Navigation is built as part of the Sweetwater development (construction currently on site (H14 and H16) expected delivery circa end of 2019). In terms of walking / cycling there is good existing connectivity to east via the Lock and the bridges also north over Hertford Union at Omega Works. It is also noted that the recently approved replacement Roach Point Bridge (17/00307/FUL), would provide a walking and cycling route across the Hertford Union canal would also improve connectivity and help facilitate the new north/south

route linking Hackney Wick Overground station to Fish Island, and would reduce walking times to the station.

- 10.22 The Monier Road site is closer to the canal, as well as the public realm (that is being created at Neptune Wharf) and The Greenway than the Hepscott Road site, which is considered to be of benefit to the families likely to occupy the affordable housing units.
- 10.23 Therefore, despite having a relatively low PTAL, officers are satisfied that the site and the affordable housing in particular is reasonably accessible by walking or cycling both currently and with emerging developments, and is served reasonably well by public transport and other amenities.
- 10.24 Officers consider that the preceding paragraphs robustly demonstrate in line with development plan policy the exceptional circumstances supporting the proposed off-site contribution in so far as the proposals would enable the delivery overall of a better mixed, balanced and inclusive community than previously approved, by locating the housing tenures in locations that better suit the intended occupants, for the reasons discussed.
- 10.25 Financial Advantage: As set out above, one of the criterion of Local Plan policy H.2 is to ensure that the developer is not financially advantaged by providing affordable housing off-site. The applicant has provided a detailed breakdown of the costs associated with selling the private units, for both sites, on the open market. They have also advised that the housing market has slowed down and with respect to sales is currently challenging, which has led to the decision to propose to turn Hepscott into a rental product scheme. Doing so, they say, would reduce the number of units on the local open market and would go some way to safeguard the local housing market from a glut of new property that might prove difficult to sell.
- 10.26 Based on the submitted financial detail, the proposal results in a combined profit reduction of circa 1%, which suggests that the applicant is not economically advantaged by the proposals. The applicant has considered the long-term approach to recouping profit i.e. through PRS, which have a lower gross development value than a private sale scheme as they are cash flowed over a longer period i.e. 25 years rather than circa 2/3 years. Officers are satisfied that the proposals in accordance with criterion 7 of Local Plan policy H.2 would be delivered at no financial advantage to developer.
- 10.27 Pepper-potting: Members should note that there is no Local Plan policy in support of pepper-potting rather Local Plan Policy H.1 states that providing for a range of different dwelling sizes and types is essential to create sustainable new neighbourhoods and to avoid problems which may arise from over-concentration of certain size and types of accommodation. The principal aim in line with national policy is to achieve mixed and balanced communities. Officers are satisfied that the proposed DoV is not at variance with the policy position.



- 10.28 The applicant is proposing to provide the affordable housing centrally rather than to distribute it across the two sites. They have advised that in this particular case, owing to the relatively small number of units involved, the registered social landlord is in support of the proposals (see appendix 2), which would assist with their management operations, and the proposals would also support the applicant's intention to deliver the Hepscott Road site as a PRS only site. There are a total of 45 units in the Monier Road scheme, and as proposed 17 of the units would be affordable and 28 units would be market housing.
- 10.29 Officers are of the view that in this instance clustering the 17 affordable units from both developments together, would be beneficial in so far as they would be dispersed within the Monier Road block and indistinguishable from the market housing. To that end there would still be a degree of pepper-potting albeit on one site. This is considered to be in line with national and local development plan policy and designed to help ensure integrated, balanced and sustainable communities on residential development sites.
- 10.30 Officers analysis concludes that the tenure mix proposed at 33-35 Monier Road is acceptable, and the proposals represents an appropriate response to planning policy.
- 10.31 Conclusion: In considering the detailed mix of the unit sizes across the two sites officers have assessed the site circumstances, including location and viability. Officers are satisfied that in line with policy objectives the proposals as revised would still provide an appropriate balance and mix of units and that the developments would complement the residential accommodation provided in the adjacent Neptune Wharf and Monier Road West, the Bagel Island / Wick Road developments and would generally respond well to the emerging character of the area. The proposals would still result in a mixed and balanced development.
- 10.32 Officers conclude that the proposed deeds of variation can be approved.

## **11. Human Rights & Equalities Implications**

- 11.1 Members should take account of the provisions of the Human Rights Act 1998 as they relate to the application and the conflicting interests of the Applicants and any third party opposing the application in reaching their decisions. The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report. In particular, Article 6 (1), of the European Convention on Human Rights in relation civil rights and a fair hearing; Article 8 of the ECHR in relation to the right to respect for private and family life and Article 1 Protocol 1 of the ECHR in relation to the protection of property have all been taken into account
- 11.2 In addition, the Equality Act 2010 provides protection from discrimination in respect of certain protected characteristics namely: age, disability, gender reassignment, pregnancy and maternity, race, religion, or beliefs and sex and sexual orientation. It places the Local Planning Authority under a legal duty to have due regard to the advancement of equality in the exercise of its powers including planning powers. Officers have taken this into account in the assessment of the application and Members must be mindful of this duty inter alia when determining all planning applications. In particular Members must pay due regard to the need to:

1. Eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Equality Act;
  2. Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and
  3. Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.
- 11.3 Officers are satisfied that the application material and Officers' assessment has taken into account these issues. Particular matters of consideration have included the provision of affordable and family housing.

## **12 CONCLUSION**

- 12.1 The proposal maintains the headline figures for contributions and obligations, whilst incorporating mechanisms to ensure that the obligations maintain an equivalent cost to the developer. The variations proposed would continue to ensure that the impact of the development would be mitigated and would serve the same purpose that was intended by the original s.106 legal agreements.
- 12.2 For these reasons, and the reasons set out above, it is recommended that subject to their completion the proposed Deeds of Variation be agreed.

### **Appendices**

Appendix 1 Location Plan

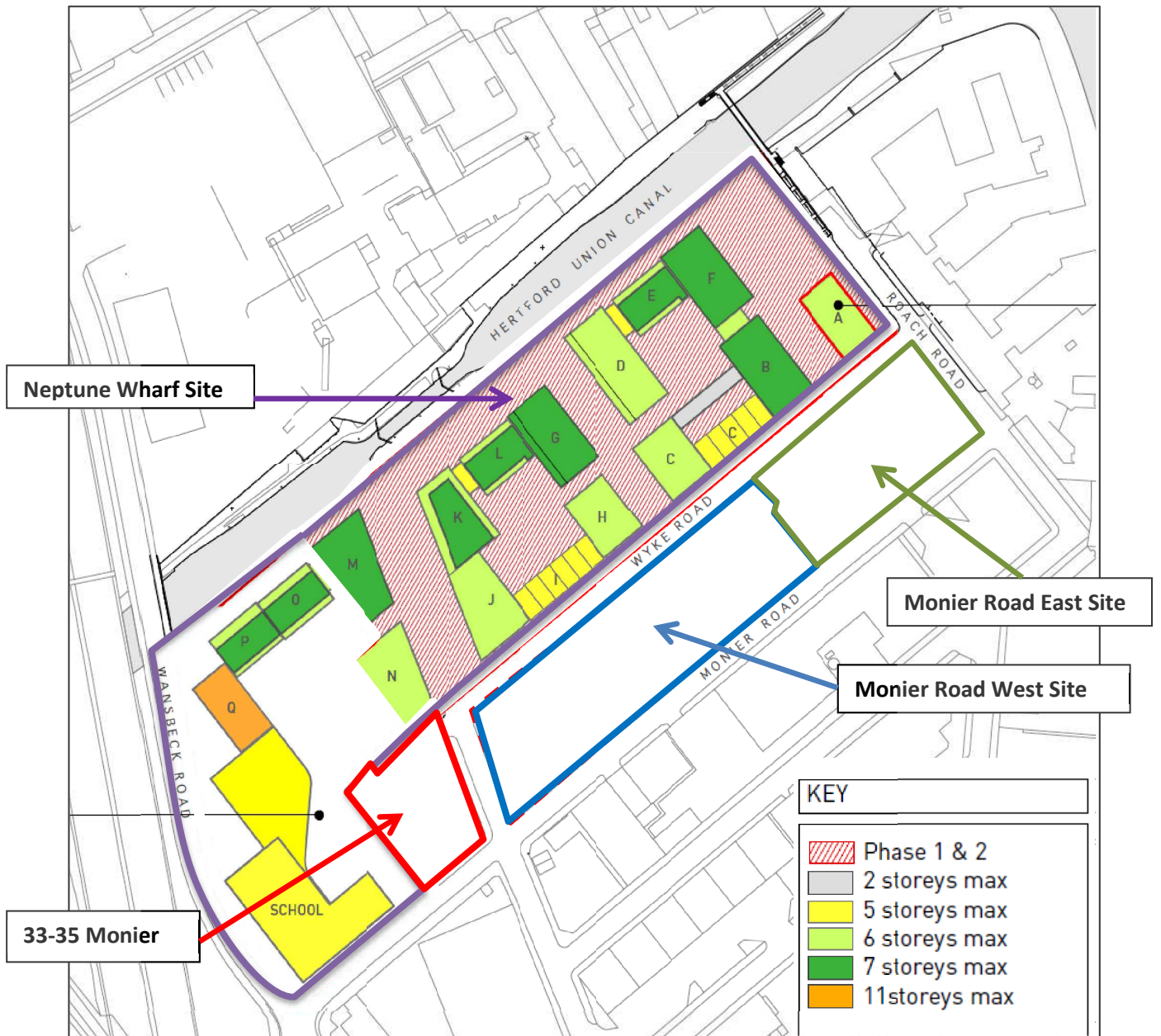
Appendix 2 Registered Social Landlord Letter

Appendix 3 Draft Heads of Terms 1- 2 Hepscott Road

Appendix 4 Draft Heads of Terms 33- 35 Monier Road

# Appendix 1

## Site Location Plan (Emerging Consented Context)



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Home Group  
2nd Floor, Bravington House  
2 Bravingtons Walk  
Kings Cross  
N1 9AF

10<sup>th</sup> September 2018

Dear Sirs,

**Re: 33-35 Monier Road**

As you are aware, Home Group is in legal with Aitch Group to acquire all the affordable homes within their HWFI developments at Bagel Island and 33-35 Monier Road. We note that the original planning permission for 1-2 Hepscott Road details a requirement for 7 affordable homes. We would support the proposal to relocate these to the 33-35 Monier Road scheme.

Home Group seeks to offer high quality, affordable homes and the ability to secure a lower service charge and or dedicated access to affordable apartments is preferable in terms of maximising affordability and ensuring optimal management efficiencies. The ability to co-locate the additional 7 affordable homes with the existing 10 affordable apartments within 33-35 Monier Road would secure further management efficiencies in that all 17 affordable homes can be located on two floor virtually dedicated to affordable housing within a single development.

Home Group is eager to facilitate the delivery of these affordable homes in the HWFI area and look forward to helping to establish the new community that is being created.

Given the comments detailed above, we would strongly recommend the relocation of the 7 affordable homes from 1-2 Hepscott Road to 33-35 Monier Road and would welcome your support in achieving this outcome.

Kind regards

A handwritten signature in blue ink, appearing to read "James Gray", is written over a light blue circular background.

James Gray

Senior Development Manager  
[james.gray@homegroup.org.uk](mailto:james.gray@homegroup.org.uk)  
Mob: +044 755 788 1693

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### Appendix 3

#### 1-2 HEPSCOTT ROAD – HEADS OF TERMS – highlighting the obligations that have been amended by the DOVs.

1. **Affordable Housing** - 7 of the residential units originally to be provided as Affordable Housing at the Development shall be provided as Affordable Housing in accordance with the Monier Road Agreement forming part of the Monier Road Development. Required to be provided prior to any occupation of the residential units on site. There shall be no viability review.
2. **PRS** – The residential units shall be provided as PRS housing in accordance with a management scheme for a minimum period of 15 years. The management scheme shall require: that there must be on-site management with a prompt issue resolution system; establishment of a complaints procedure for residents; membership of a designated professional body; advertise the availability of units on a recognised internet lettings listing or portal; be offered in accordance with the current Vive Living PRS Tenancy Agreement which shall include as a minimum: tenancy periods of three years or more; a break clause for tenants and landlords with a months notice any time after the first six months; and certainty before a tenancy is signed of the rent and service charge (including any annual increases) for the period of the tenancy. An application may be made to release any of the units from the PRS restriction and whether this shall be granted shall be at the discretion of LLDC.
3. **Footpath Improvements** - A section 278 agreement is required to be entered into to secure the necessary footpath improvements to the surrounding streets.
4. **Restriction on On-Street Parking Permits** - Owners and occupiers are restricted from applying or obtaining an on-street parking permit.
5. **Travel Plan** - A travel plan is required for approval by the LPA which appoints a travel plan monitoring officer who shall implement the travel plan during the lifetime of the development.
6. **Workspace** - A workspace strategy to provide how the workspace will be marketed to local businesses.
7. **Local Labour and Local Business** – Reasonable endeavours shall be used to secure local labour and local business.
8. **District Heating Network** - Reasonable endeavours shall be used to: connect all buildings to the District Energy Network; or where this is not possible the Local CHP Plant; or where both are not possible make a financial contribution towards offset solutions.
9. **Reduction of Energy Demand** - Reasonable endeavours shall be used to encourage occupiers to reduce their energy usage.
10. **Design Monitoring** – Design monitoring costs of £50,000 shall be paid in the event that the architect is not retained.
11. **Construction** - The development shall be constructed in accordance with the National Considerate Constructors Scheme.
12. **Heritage Contribution** - The developer shall pay the heritage contribution (£142,000) which shall be used to improve and/or enhance non-designated heritage assets within the Hackney Wick Central Masterplan Area.

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Appendix 4

**33-35 MONIER ROAD – HEADS OF TERMS - highlighting the obligations that have been amended by the DOVs.**

**1. Affordable Housing – The following affordable housing units shall be provided**

	<b>1 bed/2 person units</b>	<b>2 bed/4 person units</b>	<b>3 bed/5 person units</b>	<b>Total number of units</b>
<b>Intermediate Units</b>	4	3	1	8
<b>Affordable Rented Housing Units</b>	4	2	3	9

**There shall be no viability review.**

2. **Car Club** – One car club space shall be provided in a location to be agreed with LLDC. The car club space shall be operated for the lifetime of the development and the occupation of each residential or commercial unit shall be offered free membership.
3. **Restriction on On-Street Parking Permits** - Owners and occupiers are restricted from applying or obtaining an on-street parking permit.
4. **Electric Charging Points** - Not less than 20% of the residential parking spaces shall have electric charging points and not less than 20% of the residential parking spaces shall have passive provision.
5. **Highway Works** – Highway works shall be carried out for the improvements and alterations to the public highway and public footway at the Site including: 1. Monier Road, 2. Smeed Road and 3. Remus Road.
6. **Blue Badges** – Two blue badge spaces shall be provided on site or the adjacent highway.
7. **Parking Review** – Reasonable endeavours shall be used to secure off site street servicing and wheelchair parking.
8. **Travel Plan** - A travel plan is required for approval by the LPA which appoints a travel plan monitoring officer who shall implement the travel plan during the lifetime of the development.
9. **Workspace** - A workspace strategy to provide how the workspace will be marketed to local businesses. Unit B shall be offered as affordable workspace and offered at an average of £11.25 per square foot (excluding utility bills), which equates to a 25% discounted market rate, for a period of five years
10. **Local Labour and Local Business** – Reasonable endeavours shall be used to secure local labour and local business.
11. **District Heating Network** - Reasonable endeavours shall be used to: connect all buildings to the District Energy Network; or where this is not possible the Local CHP Plant; or where both are not possible make a financial contribution towards offset solutions.

12. **Reduction of Energy Demand** - Reasonable endeavours shall be used to encourage occupiers to reduce their energy usage.
13. **Design Monitoring** – Design monitoring costs of £25,000 shall be paid in the event that the architect is not retained.
14. **Public Open Space** – The public open space and play areas shall be delivered and managed in accordance with the delivery scheme approved by LLDC.



**Subject:** Land at Sugar House (formerly known as Strand East), comprising Plots R7 and R8 (refs: 17/00468/NMA, 18/00366/NMA, 17/00369/REM and 15/00384/REM)

**Meeting date:** 25 September 2018

**Report to:** Planning Decisions Committee

**Report of:** Sara Dawes, Principal Planning Development Manager

FOR DECISION

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**This report will be considered in public**

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**1. EXECUTIVE SUMMARY**

- 1.1. This report considers a number of linked applications for Non-Material Amendments and Reserved Matters/Approval of Details respectively, submitted by Vastint UK BV for Plot R7 and R8 of the proposed redevelopment of a 10 hectare peninsular south of Stratford High Street.
- 1.2. The overall site benefits from part outline and part full planning permission granted by the London Thames Gateway Development Corporation on September 2012 ref: 12/00336/LTGDC/LBNM (“the 2012 permission”). Plots R7 and R8 falls within the part of the site with outline planning permission with the reserved matters of layout, scale, appearance and landscaping required to demonstrate compliance with the approved parameter plans, Design Code and relevant planning conditions that form part of the 2012 permission.
- 1.3. The first application (ref:17/00468/NMA) relates to Non-Material Amendments (NMA) to vary the ‘Building Line Requirement’, ‘Maximum Storey Heights’ and ‘Underground Parking Strategy’ parameter plans of the 2012 permission.
- 1.4. The second application (ref:18/00366/NMA) relates to Non-Material Amendments to vary the Site Wide Housing Strategy specified in condition A15 of the 2012 Planning Permission.
- 1.5. This report also considers Reserved Matters Applications for Plot R7 (ref:17/00369/REM) and Plot R8 (ref:15/00384/REM) for residential schemes of 82 homes and 116 homes respectively, with a shared underground car park with 47 spaces, and an element of commercial retail and/or office floorspace in Plot R7, as per the descriptions of development set out above. The applications also seek to discharge conditions C8 (Housing Quality), C11 (daylight, sunlight and overshadowing), C15 (parking management plan) and C36 (waste management plan) for R7 and R8, which are required to be discharged on a plot by plot basis.
- 1.6. No objections have been received from statutory consultees in response to the schemes for Plot R7 and R8.
- 1.7. The building use, and number and mix of homes is compliant with the requirements of the 2012 permission (as amended) as well as the Local Plan Site Allocation. The buildings are designed to achieve a high standard of living accommodation in terms of their access, aspect, size, access to amenity space,

daylight, sunlight and overshadowing, overheating, car and cycle parking, refuse and recycling storage and collection.

- 1.8. The proposed layout and scale complies with the maximum storey heights, underground parking strategy, and characterisation of open spaces parameter plans as approved by the 2012 permission (as amended). An increase to eight levels in response to design development, adjusted site levels and more generous floor to ceiling dimensions above that assumed in the 2012 permission is not considered to give rise to any new or different significant townscape, heritage or environmental effects.
- 1.9. The Quality Review Panel supports the Reserved Matters applications for Plot R7 and Plot R8, which are subject to Local Plan Policy BN.10.

## 2. RECOMMENDATIONS

- 2.1 The Committee is invited to:

Non-Material Amendments – Plot R7 and R8 (ref:17/00468/NMA)

- (a) **APPROVE** the Application for Non-Material Amendments to Parameter Plans PP-1-101 rev B 'Building Line Requirement', PP-1-103 rev M 'Maximum Storey Heights', and PP-108 rev C 'Underground Car Parking Strategy' attached to planning permission ref: 12/00336/LTGOUT/LBNM dated 27<sup>th</sup> September 2012 (as amended).

Non-Material Amendments – Site Wide Housing Strategy (ref: 18/00369/NMA)

- (b) **APPROVE** the Application for Non-Material Amendments to vary Condition A15 (Site Wide Dwelling Mix) of planning permission ref: 12/00336/LTGOUT/LBNM dated 27<sup>th</sup> September 2012 (as amended).

Plot R7, 17/00369/REM

- (c) **APPROVE** the Application for the Approval of Reserved Matters for the construction of two blocks comprising 82 residential units and approximately 330sqm of non-residential floorspace (falling within Use Classes A1 (retail), A3 (restaurants and cafes), A4 (drinking establishments) and/or B1 (office)), an underground vehicular car park (including a connection to development Plot R8 at basement level), and hard and soft landscaping, pursuant to conditions A3 (Time Limits) and C1 (Reserved Matters – Layout, Scale, Appearance and Landscaping), and the partial submission of details pursuant to Conditions C8 (Housing Standards), C11 (Daylight, Sunlight and Overshadowing), C15 (Parking Management Plan) and C36 (Waste Management Strategy) of planning permission 12/00336/LTGOUT/LBNM dated 27<sup>th</sup> September 2012 (as amended).

Plot R8, 15/00384/REM

(d) APPROVE the Application for the Approval of Reserved Matters for the construction of two blocks comprising 116 residential units, an underground vehicular car park (including a connection to development Plot R7 at basement level), and hard and soft landscaping, pursuant to conditions A3 (Time Limits) and C1 (Reserved Matters – Layout, Scale, Appearance and Landscaping), and the partial submission of details pursuant to Conditions C8 (Housing Standards), C11 (Daylight, Sunlight and Overshadowing), C15 (Parking Management Plan) and C36 (Waste Management Strategy) of planning permission 12/00336/LTGOUT/LBNM dated 27th September 2012 (as amended).

2.2 AGREE TO DELEGATE AUTHORITY to the Director of Planning Policy and Decisions to finalise the recommended conditions as set out in this report including such refinements, amendments, additions and/or deletions as the Director of Planning Policy and Decisions considers reasonably necessary.

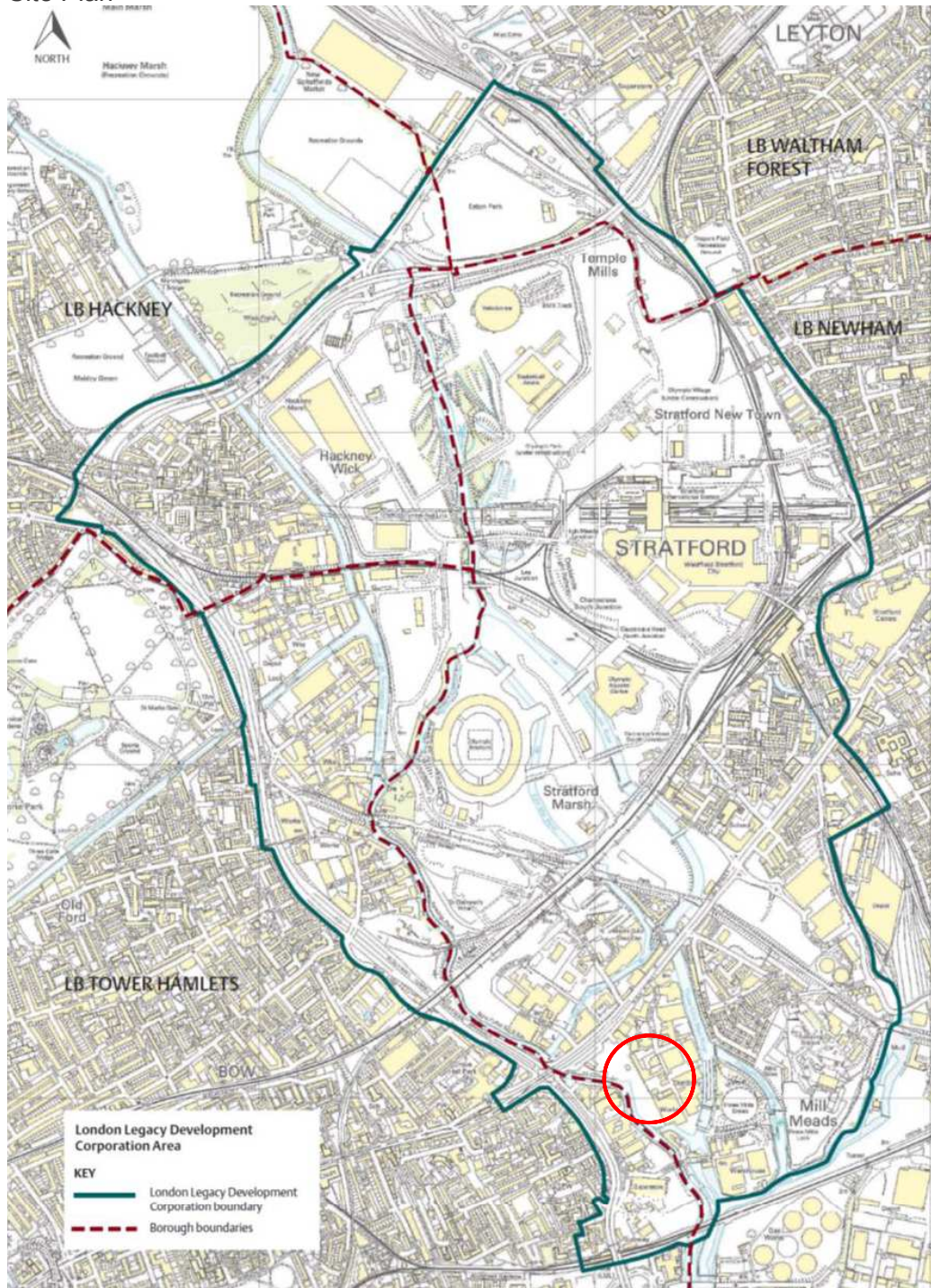
**3. FINANCIAL IMPLICATIONS**

3.1. None

**4. LEGAL IMPLICATIONS**

4.1. None

# Site Plan



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**Location:** Land to the south of High Street Stratford, east of Hunts Lane and east of River Lea Navigation, Stratford, E15

**London Borough:** Newham

**Proposal:**17/00468/NMA

Application for Non-Material Amendments to Parameter Plans PP-1-101 rev B 'Building Line Requirement', PP-1-103 rev M 'Maximum Storey Heights', and PP-108 rev C 'Underground Car Parking Strategy' attached to planning permission ref: 12/00336/LTGOUT/LBNM dated 27<sup>th</sup> September 2012 (as amended).

18/00366/NMA

Application for Non-Material Amendments to vary Condition A15 (Site wide Dwelling Mix) of planning permission ref: 12/00336/LTGOUT/LBNM dated 27<sup>th</sup> September 2012 (as amended).

17/00369/REM – Plot R7

Application for the Approval of Reserved Matters for the construction of two blocks comprising 82 residential units and approximately 330sqm of non-residential floorspace (falling within Use Classes A1 (retail), A3 (restaurants and cafes), A4 (drinking establishments) and/or B1 (office)), an underground vehicular car park (including a connection to development Plot R8 at basement level), and hard and soft landscaping, pursuant to conditions A3 (Time Limits) and C1 (Reserved Matters – Layout, Scale, Appearance and Landscaping), and the partial submission of details pursuant to Conditions C8 (Housing Standards), C11 (Daylight, Sunlight and Overshadowing), C15 (Parking Management Plan) and C36 (Waste Management Strategy) of planning permission 12/00336/LTGOUT/LBNM dated 27<sup>th</sup> September 2012 (as amended).

15/00384/REM – Plot R8

Application for the Approval of Reserved Matters for the construction of two blocks comprising 116 residential units, an underground vehicular car park (including a connection to development Plot R7 at basement level), and hard and soft landscaping, pursuant to conditions A3 (Time Limits) and C1 (Reserved Matters – Layout, Scale, Appearance and Landscaping), and the partial submission of details pursuant to Conditions C8 (Housing Standards), C11 (Daylight, Sunlight and Overshadowing), C15 (Parking Management Plan) and C36 (Waste Management Strategy) of planning permission 12/00336/LTGOUT/LBNM dated 27<sup>th</sup> September 2012 (as amended).

**Applicants:**

Vastint UK BV

**Agent:**

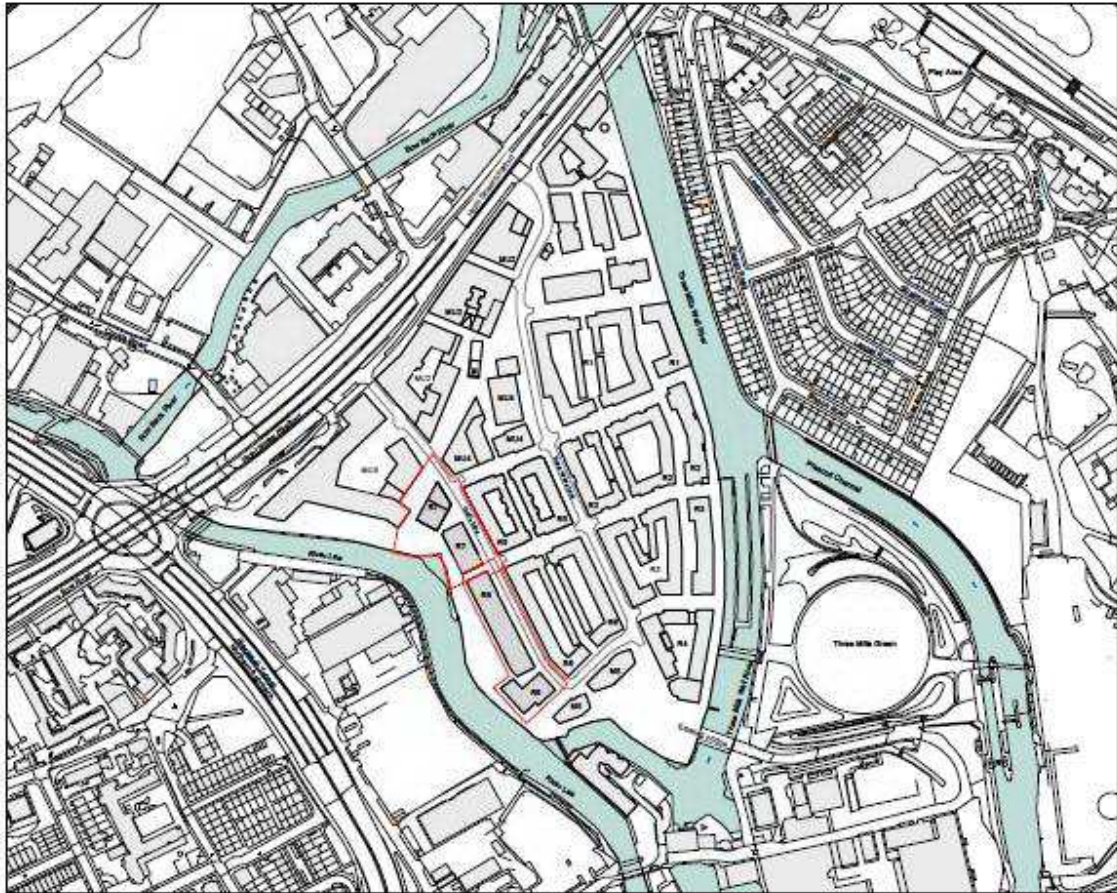
GL Hearn

**Architect:**

ARC-ML (Plot R7)

MaccreeonorLavington (Plot R8)

Sugar House Island (Strand East) Masterplan Plot R7 and R8 in red



**5. SITE & SURROUNDINGS**

- 5.1. The application site forms part of a 10 hectare peninsular of land bounded by Stratford High Street, Three Mills Wall River and the River Lea Navigation. The majority of the peninsular, including the application site, has been cleared, with remediation and earthworks being undertaken in preparation for redevelopment.
- 5.2. The application sites are located on the western edge of the wider redevelopment site known as Sugar House Island (formerly Strand East) and is bounded by Hunts Lane and residential development Plot R5 and R8 to the east, the riverside park and River Lea Navigation to the west, mixed use Plot MU4 to the north, and mixed use Plot MU5 and the future bus bridge to the south.
- 5.3. The site boundary for Plots R7 and R8 deliberately includes the adjacent sections of Hunts Lane to the east, the riverside park to the west, and shared streets to the north and south to demonstrate its integration with the design of the public realm.
- 5.4. The wider Sugar House Island/Strand East site falls within the Local Plan - Sugar House Lane Site Allocation SA4.2 and is partially located within the Three Mills Conservation Area (TMCA) along the eastern boundary and the Sugar House Lane Conservation Area (SHLCA); and Flood Zone 3. To the south of the site is the Three Mills complex, comprising the Grade I listed 'Tidal Mill' known at the House Mill; the Grade II listed Clock Mill and the former Custom House.



- 5.5. The northern tip of the Plot R7 tower is within the Sugar House Lane Conservation Area (SHLCA). Plot R8 is not within a conservation area.

## **6. APPLICATION PROPOSALS**

### Background

- 6.1. The application site comprises one of thirteen development plots and associated public realm infrastructure that forms part of a masterplan which benefits from the 2012 permission. Each plot within the outline part of the 2012 permission requires the submission of a reserved matters application (RMA) to be prepared in accordance with the associated parameter plans, Design Code, planning conditions and S106 Agreement.
- 6.2. The parameter plans, when considered alongside planning conditions and a Design Code, establish part of the brief for the detailed design of each building. The parameter plans establish the following:
- Building line requirements;
  - Maximum storey heights;
  - Ground, first and upper floor uses;
  - Characterisation of open spaces;
  - Levels strategy;
  - Underground car parking strategy.
- 6.3. The Design Code identifies the different block typologies (commercial, hotel, residential, mews/perimeter, riverside, mixed-use, towers and public buildings) within the masterplan and recommends design principles (access, levels, parking, massing, materials, fenestration, signage and plant/rooftop service) to be adopted in the design of each building.

### Plot R7

- 6.4. Plot R7 is identified in the 2012 permission as a linear block and separate accent tower and plinth, serviced by a basement car park and adjacent public realm.
- 6.5. The 2012 permission (as amended by other NMA applications including 17/00009/NMA) permits buildings ranging from a maximum of 4 to 14 storeys for Plot R7. The linear block along the Riverside Park is approved to be an 8 storey element mirroring a similar block to the south at Plot R8, and a 4 storey element with a set-back 5<sup>th</sup> floor. The accent tower has been approved at 14 storeys with a 4 storey plinth.

### Plot R8

- 6.6. The 2012 permission (as amended) permits buildings ranging from a maximum of 4 to 16 storeys for Plot R8. The linear block along the Riverside Park is approved to be an 8 storey element, and a 4 storey element with a set-back 5<sup>th</sup> floor. The accent tower is permitted to be 16 storeys with a lower 4 storey block at the base fronting the park.

### Environmental Impact Assessment

- 6.7. In support of the Reserved Matters applications for Plot R7 and Plot R8, the applicant submitted requests for Screening Opinions as to whether the proposed development (including the proposed amendments to the parameter plans), would generate new or different significant environmental effects that would require an update to the 2012 Environmental Statement.
- 6.8. The information submitted in support of the requests concluded that there would be no new or different significant effects on traffic and access, socio-economics, noise, air quality, ecology, townscape and visual, daylight and sunlight, wind,

hydrology, flooding and drainage, ground conditions, heritage assets, waste, or sustainability compared to the 2012 Environmental Statement. To inform this conclusion, further assessments were undertaken in relation to townscape and visual, daylight and sunlight, wind and heritage assets as well as any cumulative effects, which were submitted with the EIA Screening request.

Non-Material Amendments to Plot R7 and R8 (ref: 17/00468/NMA)

- 6.9. The application proposes the following non-material amendments to the approved parameter plans:

Building Line Requirement

- a) Amendment to the Maximum Building Line to permit an increase in the building footprint of the linear blocks of both Plots R7 and R8 by approximately 1.8-2.1m, to accommodate the minimum basement width of two rows of parking with a central carriageway. This relates to the basement storey only, and the actual building line from ground floor upwards accords with the currently approved building line requirement.
- b) Amendment to the Maximum Building Line to permit an increase in the building footprint of the R7 accent tower by approximately 0.8m to enable a logical internal layout of the building and to accommodate the required car park ramp.

Maximum Storey Heights

- c) Amendment to the position of the set-back storey on top of the fourth floor element of R7 linear block (B) so that there is a small setback on the east and western sides.
- d) Amendment to the set-back storey of the linear block at R8, so that it exceeds the 1:2 ratio of the approved parameter plan and falls partially outside the setback zone.

Underground Parking Strategy

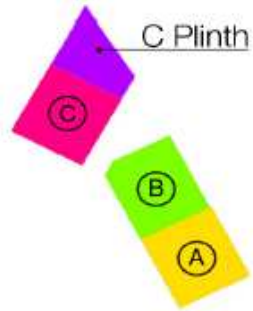
- e) Amendment to the 'Possible Underground Parking Zone' for a single car park to extend under both plots.
- f) Amendment to the location of the access to the underground car park to between the linear block and accent tower of Plot R7.

- 6.10. The 2012 permission assumed a consistent 5.3m AOD ground level across the entire site. The approved site wide Public Realm and Infrastructure RMA (ref: 15/00239/REM established revised ground levels across the whole site.)

- 6.11. The ground levels for Plot R7 and R8 have consequently changed to approx. 5.6m-6.8m AOD. There has also been an increase in the overall building height due to an increase in floor to floor heights from 3m to 3.15m to increase the floor to ceiling heights.

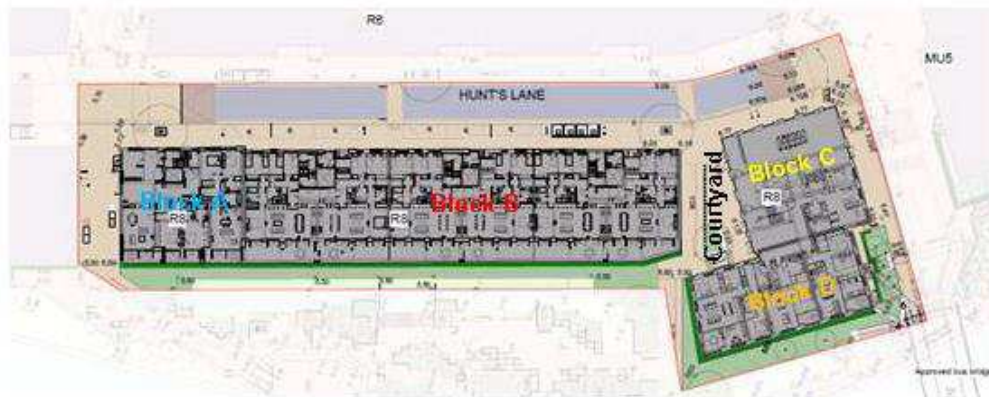
- 6.12. While the proposed changes to the 'Maximum Heights' Parameter Plans refers only to storey heights, the proposed changes to the true height of the development – when compared to that assumed in the 2012 permission - have been amended as follows:

Plot R7 – true building heights



	<b>2012 Outline Assumed height</b>	<b>17/00369/REM Proposed heights (Detailed design)</b>
<b>Ground Floor FFL AOD (m)</b>	All blocks: 5.300	Block A: 6.495 Block B: 6.495 Block C (north): 5.600 Block C (north, raised): 6.800 Block C (east): 5.600 Block C (south): 5.600 Block C (south, raised): 6.800 Block C (west): 5.600 Block C (west, raised): 6.800
<b>Top of Building AOD (m)</b>	Block A: 30.300 Block B: 21.500 Block C (plinth): 18.300 Block C: 48.300	Block A: 33.270 Block B: 23.670 Block C (plinth): 21.540 Block C: 53.140
<b>Building Height</b>	Block A: 25.000 Block B: 16.000 Block C (plinth): 13.000 Block C: 43.000	Block A: 27.770 Block B: 18.170 Block C (plinth): 16.040 Block C: 47.540

Plot R8 – true building heights



	<b>2012 Outline Assumed height</b>	<b>15/00384/REM Proposed heights (Detailed design)</b>
<b>Ground Floor FFL AOD (m)</b>	<b>All blocks: 5.300</b>	<b>Block A: 5.750 Block B: 5.908 – 6.223 Block C: 6.173 Block D: 6.173</b>
<b>Top of Building AOD (m)</b>	<b>Block A: 27.300 Block B: 21.300 Block C: 54.300 Block D: 18.300</b>	<b>Block A: 33.552 Block B: 24.103 Block C: 59.175 Block D: 21.375</b>
<b>Building Height</b>	<b>Block A: 25.000 Block B: 16.000 Block C: 49.000 Block D: 13.000</b>	<b>Block A: 27.802 Block B: 17.880 – 18.195 Block C: 53.002 Block D: 15.202</b>

Non-Material Amendment to the Site Wide Dwelling Mix (18/00366/NMA)

- 6.13. The application proposes a non-material amendment to vary Condition A15 (Site-wide Dwelling Mix) of the 2012 Planning Permission. For the avoidance of doubt, there is no change in the overall number of dwellings (1,200 dwellings).

Original 2012 Permission (12/00336/LTGOUT/LBNM)		Currently approved dwelling mix (16/00081/NMA)		Proposed dwelling mix – (18/00366/NMA)		Difference (Feb 16 – Jun 18)
Unit Type	No. of units	Unit Type	No. of units	Unit Type	No. of units	
Studio	<b>8 (1%)</b>	Studio	<b>65 (5%)</b>	Studio	<b>52 (4%)</b>	<b>-13</b>
1-bed	<b>468 (39%)</b>	1-bed	<b>435 (36%)</b>	1-bed	<b>423 (35%)</b>	<b>-12</b>
2-bed	<b>244 (20%)</b>	2-bed	<b>217 (18%)</b>	2-bed	<b>250 (21%)</b>	<b>+33</b>
3-bed	<b>430 (36%)</b>	3-bed	<b>364 (30%)</b>	3-bed	<b>390 (33%)</b>	<b>+26</b>
4-bed	<b>34 (3%)</b>	4-bed	<b>118 (10%)</b>	4-bed	<b>85 (7%)</b>	<b>-33</b>
5-bed	<b>16 (1%)</b>	5-bed	<b>1 (0%)</b>	5-bed	<b>0 (0%)</b>	<b>-1</b>
<b>Total</b>	<b>1,200</b>	<b>Total</b>	<b>1,200</b>	<b>Total</b>	<b>1,200</b>	
<b>Provision of Family Housing</b>						
2-bed +	<b>724 (60%)</b>	2-bed +	<b>700 (58%)</b>	2-bed +	<b>725 (60%)</b>	<b>+25</b>
3-bed +	<b>480 (40%)</b>	3-bed +	<b>483 (40%)</b>	3-bed +	<b>475 (40%)</b>	<b>-8</b>

Reserved Matters Plot R7 (reference 17/00369/REM)

- 6.14. The reserved matters application seeks approval for the layout, scale, appearance and landscaping for Plot R7, which are submitted pursuant to conditions A3 (Time Limits) and C1 (Reserved Matters – Layout, Scale, Appearance and Landscaping) of the 2012 permission.
- 6.15. The proposals for Plot R7 comprise the following elements:
- A linear block along the Riverside Park with an 8 storey (Block A) element mirroring a similar block to the south at Plot R8, and a 4 storey element with a set-back fifth floor (Block B);
  - A 14-storey accent tower with a 4 storey plinth element (Block C);
  - 82 residential units (44 x 1beds; 13 x 2 beds; 25 x 3 beds) of which 11 (13.4%) would be wheelchair adaptable units.
  - 330sqm of commercial floorspace falling within Use Classes A1, A3, A4 and/or B1.
  - A shared underground car park with Plot R8 which is accessed from Plot R7;
  - All associated hard and soft landscaping within and surrounding the plot.

Reserved Matters Plot R8 (reference 15/00384/REM)

- 6.16. The reserved matters application seeks approval for the layout, scale appearance and landscaping for Plot R8, which are submitted pursuant to conditions A3 (Time Limits) and C1 (Reserved Matters – Layout, Scale, Appearance and Landscaping) of the 2012 permission.
- 6.17. The proposals for R8 comprise the following elements:
- A linear block along the Riverside Park with an 8 storey (Building A) element, and a 4storey element with a set-back fifth floor (Building B).
  - A 16 storey accent tower to the south of the plot (Building C), with a 4 storey plinth element to the west (Building D)
  - 116 residential units (52 x 1 beds; 20 x 2 beds; 23 x 3 beds; 21 x 4 beds, of which 12 (10.3%) (4 x 1 beds; 4 x 2 beds; 4 x 3 beds) would be wheelchair adaptable units
  - A shared underground car park with Plot R7, also accessed from Plot R7.
  - All associated hard and soft landscaping within and surrounding the plot.
- 6.18. The application has been amended since original submission in 2015. It is important to note that since the scheme was originally submitted, an application under Section 96A (ref: 17/00009/NMA) was approved by Planning Decisions Committee for an in-principle agreement for an increase in height of the northern block (Block A) of Plot R8 by one storey (from 7 to 8 storeys), as part of a strategic decision to redistribute height from the eastern part of the wider site (Plots R2 and R4) to plots on the western side of the peninsula (Plots R7 and R8), in order to minimise the development's impact on the heritage assets at Three Mills. The application was accompanied by an EIA Screening Opinion Request (ref: 17/00008/SCRES).
- 6.19. The revisions to the Plot R8 RMA application made since original submission in 2015 are as summarised below:
- Additional storey to the northern block (Block A) to increase the height from 7 to 8 storeys. The block now mirrors the height/massing of the proposed adjoining Block A of Plot R7 (17/00369/RMA)
  - 3 additional residential units, increasing the total number for plot R8 from 113 to 116 units.
  - Removal of the indicative ground floor/basement layouts for Plot R7, as full details for that plot have since been submitted for approval (ref: 17/00369/REM), and no longer form part of this RMA.
  - Red line boundary for the application has been amended and now relates to Plot R8 and its immediate surroundings only, now that it is no longer necessary for the Plot R8 RMA to provide indicative layouts for Plot R7 shared underground car park.
  - Amendments to the public realm and landscaping details so that it takes into account the approved Public Realm and Infrastructure RMA (ref:15/00239/RMA) which was submitted and approved since original submission of the Plot R8 RMA, and makes a number of minor amendments:
    - Inclusion of a retaining wall to the bus bridge abutment now that the structure of the bus bridge is more resolved
    - Inclusion of a cable stay landing area to the east of the Blocks C/D
    - Provision of a hedge and gated maintenance path to the green eastern edge of Blocks C/D
    - Adjusted positioning of the tree contained in the passageway in between plots R7/R8 to outside Plot R8

- Rationalisation of the passageway between Hunts Lane and the Riverside Park in response to the needs/detailed design of both Plots R7 and R8. (reduction from approx.10m to 6m)

### Approval of Details

- 6.20. The reserved matters applications also seek the partial approval of plot-specific details submitted pursuant to the following conditions of the 2012 permission.
- Condition C8 (Housing Standards)
  - Condition C11 (Daylight/ Sunlight/ Overshadowing)
  - Condition C15 (Parking Management)
  - Condition C36 (Waste Management)
- 6.21. The assessment of these aspects of the submission are detailed in the relevant sections below.

## **7. RELEVANT PLANNING HISTORY**

### Sugar House Lane (Strand East)

- 7.1. On 27th September 2012 the London Thames Gateway Development Corporation granted planning permission (12/00336/LTGOUT) for a hybrid planning application for comprehensive mixed use development comprising:

#### Detailed application

- demolition of existing buildings where stated;
- 8 residential units (C3);
- 300sqm financial and professional services (A2); 500sqm public house/bar (A4); 2,620sqm office and workshops/non-residential institution (B1/D1); 8,170sqm offices (B1);
- public square;
- access including limited emergency services access along Three Mills Wall River and east-west along Sugar House Lane;
- 28 parking spaces;
- hard and soft landscaping.

#### Outline application (all matters reserved except access)

- demolition of buildings where stated;
- 1192 residential units (C3) of which 10% of properties wheelchair accessible;
- 12,593sqm flexible uses including retail (A1), financial and professional services (A2), restaurants, cafes and bars (A3/4), offices and workshops (B1), non-residential institution (D1) and assembly and leisure (D2);
- 33,950sqm offices and workshops (B1);
- 22,500sqm (350 bedroom) hotel (C1);
- pedestrian bridge across Three Mills Wall River;
- a riverside park;
- car, motorcycle and bicycle parking;
- servicing and ancillary highway works.

- 7.2. The 2012 permission includes a planning condition that requires compliance with the Design Code submitted in support of the 2012 planning application.
- 7.3. There is a S106 Agreement attached to the 2012 planning permission which requires the applicant to, in summary:
1. Contribute a Discounted Standard Charge of £8,543 per residential unit in accordance with the LTGDC's Planning Obligations Community Benefits

- Strategy comprising the Works in Kind at (3. below) and ring fenced contributions at (4. below);
2. Provide 8% (without grant) (96 units) and 11% (with grant) (132 units) affordable housing split 50:50 split between affordable rent and intermediate tenures subject to a review mechanism following completion of the 400th, 800th and 1,200th units.
  3. The cost of the following Works in Kind to be offset against the Discounted Standard Charge as reflected above and in accordance with the Planning Obligations Community Benefit Strategy:
    - a. 75% (£999,000) of the cost of Bridge 1 (a two way single lane bus, cycle and pedestrian bridge between the site and Bromley by Bow North Phase 1);
    - b. 90% (£279,720) of the cost of Bridge 2 (a pedestrian and cycle bridge between the site and Bromley by Bow Northern Phase);
    - c. 60% (£293,040) of the cost of Bridge 3 (a pedestrian and cycle bridge between the site and Three Mills Green);
    - d. 75% (£83,250) of the cost of works to Bridge 4 (the vehicular, pedestrian and cycle bridge between the site and Three Mills island and 3 Mills Studios)
    - e. 50% (£1,387,500) of the All Movements Junction Works between Stratford High Street and Sugar House Lane;
    - f. 50% (£616,050) of the cost of the Hub & Open Space at Riverside Park;
    - g. 80% (£133,200) of the Water Bus Stop;
    - h. The cost (£2,600,000) to the scheme of Providing “Low Cost” Accommodation (in relation to Community Use floorspace)
  4. The following financial contributions to be ring fenced within the Discounted Standard Charge:
    - a. £2,390,000 contribution towards education improvements in the locality (ring fenced for London Borough of Newham)
    - b. £1,100,000 contribution towards diverting and extending a bus service through the site when Bridge 1 and the necessary road network within Bromley by Bow North is implemented and pedestrian improvements at Bow roundabout and improvements to Bromley by Bow station (ring fenced for TfL);
    - c. £70,000 towards bus infrastructure
    - d. £150,000 towards Skills and Training to be agreed (ring fenced for London Borough of Newham)
  5. Local Labour, Skills and Training Initiatives:
    - a. Enter into S278 and S38 Agreements with the Local Highway Authority to undertake the Highway Works;
    - b. Review the provision of a Controlled Parking Zone and indemnify the reasonable costs of the Council to implement a CPZ within the site if required.
    - c. Implement a Community Facilities Strategy to be submitted prior to the anticipated completion date of each Plot containing community uses and include details of advertising and letting terms.

- d. To implement a Creative Industries Strategy including advertising and heads of terms for first letting (see DCMS definition of creative industry).
- e. To prepare and implement a site-wide Energy Strategy
- f. Contribute £150,000 towards the River Lea Tidal Mill Trust House Mill restoration project.

#### Strand East Plots and Public Realm Infrastructure Reserved Matters

- 7.4. Reserved Matters Approval has been granted for the following plots:
- Plot MU2 (15/00250/REM) : 27,000sqm commercial floorspace (Class B1, A1, A2, A3, A4) and community (D1) floorspace - December 2015 Committee
  - Plot R6 (15/00435/REM) : 103 residential units – November 2014 Committee
  - Plot MU1 (15/00484/REM) : Primary School (Class D1) – March 2016 Committee
  - Plot R1 (16/00223/REM) : 161 residential units and 628sqm commercial floorspace – July 2016 Committee
  - Plot R3 (16/00412/REM) : 156 residential units – November 2016 Committee
  - Plot MU5 (15/00359/REM) : 42 residential units and 391sqm commercial floorspace -November 2016 Committee
  - Bridge 3 (16/00423/REM) : pedestrian bridge – December 2016 Committee
  - Plot R2 (16/00440/REM) : 212 residential units and 70sqm commercial floorspace – May 2017 Committee
  - Plot R4 (15/00327/REM) : 89 residential units and 661sqm commercial floorspace – May 2017 Committee
  - Plot R5 (17/00348/REM) : 86 residential units and approximately 80sqm of commercial floorspace (Use Classes, A1, A2, A3, A4 and B1), - November 2017 Committee
  - Site Wide Public Realm Infrastructure (15/00239/REM) : Members resolved to delegate the decision to the Director of Planning Policy and Decisions in April 2016 with a view to resolving the issue of the location of the residents' garden for the moorings community. The applicant 'carved out' the section of riverfront that forms part of Plot R3 from the application boundary of the Public Realm Infrastructure RMA, and reserved matters approval was granted on 6th June 2016.
- 7.5. The following plots are subject to reserved matters applications which have been submitted and are currently under consideration:
- Plot R7 (17/00369/REM) : subject of this report
  - Plot R8 (15/00384/REM) : subject of this report
  - Plot MU3 (15/00481/REM) : 349 bed hotel, 21 residential units, 4,160sqm commercial floorspace.

#### Non-Material Amendments to parameter plans

- 7.6. A number of Non-Material Amendment applications have been approved that make changes to the parameter plans associated with the OPP for various plots.
- 7.7. Importantly, and relevant to these current applications, an application for non-material amendments to the 'Maximum Storey Heights' parameter plan attached



to planning permission 12/00336/LTGOUT/LBNM to accommodate amendments to the number of storeys permitted at Plot R7 and R8, was approved by Planning Decisions Committee on 28<sup>th</sup> March 2017 (ref:17/00009/NMA). The approved amendments comprised:

- a) Increase the height of the accent tower of Plot R7 from 13 to 14 storeys
- b) Increase the height of the southern building element of Plot R7 from 7 to 8 storeys.
- c) Increase the height of the northern building element of Plot R8 from 7 to 8 storeys.

Housing Mix: Non Material Amendment (reference 16/00081/NMA)

- 7.8. On 10 June 2016 the Legacy Corporation granted non-material amendments to the site-wide dwelling mix. The approved dwelling mix is set out in the table in para. 6.13, that also shows the changes proposed in application ref: 18/00336/NMA which is considered in this report.

Bus Bridge (13/00586/FUL)

- 7.9. Planning permission was granted on 10<sup>th</sup> August 2014 for the construction of a single lane, two directional bridge over the River Lea Navigation to accommodate buses, cycles and pedestrians and a two directional two lane roadway and accompanying footpath that links the bridge to Hancock Road to the west and Sugar House Lane to the east.
- 7.10. The bus bridge would land to the south of Plot R8.

## **8. POLICIES & GUIDANCE**

The Planning (Listed Buildings and Conservation Areas) Act 1990

- 8.1. The Planning (Listed Buildings and Conservation Areas) Act 1990 ("LBCAA 1990") is the primary legislation under which the impact of a development on a heritage asset is to be assessed. The legislation is at the top of the hierarchy, followed by national policy and guidance (NPPF and PPG), then followed by local policies and guidance (LLDC Local Plan).
- 8.2. Section 72 (s72) of the LBCAA 1990 applies in the consideration of the impact of development on conservation areas. S72 requires the Local Planning Authority to pay special attention to the desirability of preserving or enhancing the character or appearance of a conservation area in relation to buildings or land within a conservation area.
- 8.3. Section 72(1) of the LBCAA 1990 states as follows:  
*"In the exercise, with respect to any buildings or other land in a conservation area, of any functions under or by virtue of any of the provisions mentioned in [amongst others, the Town and Country Planning Act 1990], special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area."*

National Planning Policy Framework (July 2018) (NPPF)

- 8.4. The National Planning Policy Framework sets out national planning policy and is a material consideration in planning decisions. It sets out a presumption in favour of sustainable development which Plans and decisions should apply. This requires that in order to achieve this, development proposals that accord with an up to date development plan should be approved without delay. Where there are no relevant development plan policies or the policies that are most relevant are out of date, that permission is granted unless the application of policies within the

NPPF that protect areas or assets of particular importance provides a clear reason for refusing development, or any adverse impacts of granting permission would significantly or demonstrably outweigh the benefits, when assessed against the policies in the NPPF as a whole. The NPPF should be read in conjunction with the Planning Practice Guidance, a web-based resource for all users of the planning system. This set out detailed guidance in support of the policy areas in the NPPF, including the importance of good design and how this can be achieved through planning decisions.

#### 8.5. **Regional Planning Policy**

##### London Plan (March 2016)

Policy 2.4	The 2012 Games and their legacy
Policy 2.9	Inner London
Policy 2.13	Opportunity Areas and Intensification Areas
Policy 3.3	Increasing housing supply
Policy 3.4	Optimising housing potential
Policy 3.5	Quality and design of housing developments
Policy 3.6	Children and young people's play and informal recreation facilities
Policy 3.7	Large residential developments
Policy 3.8	Housing choice
Policy 3.10	Definition of affordable housing
Policy 3.11	Affordable housing targets
Policy 5.3	Sustainable design and construction
Policy 5.11	Green roofs and development sit environs
Policy 6.7	Better streets and surface transport
Policy 6.9	Cycling
Policy 6.10	Walking
Policy 6.13	Parking
Policy 7.2	An inclusive environment
Policy 7.3	Designing out crime
Policy 7.4	Local character
Policy 7.5	Public realm
Policy 7.6	Architecture
Policy 7.7	Location and design of tall buildings
Policy 7.8	Heritage assets and archaeology
Policy 7.9	Heritage-led regeneration
Policy 7.15	Reducing and managing noise, improving and enhancing the acoustic environment
Policy 7.19	Biodiversity and access to nature
Policy 7.21	Trees and woodlands

#### 8.6. **Local Planning Policy**

##### London Legacy Development Corporation Local Plan (July 2015)

Policy SP.2	Maximising housing and infrastructure provision within new neighbourhoods
Policy H.1	Providing a mix of housing types
Policy H.2	Delivering affordable housing
Policy BN.1	Responding to place
Policy BN.3	Maximising biodiversity
Policy BN.4	Designing residential schemes
Policy BN.5	Requiring inclusive design
Policy BN.8	Maximising opportunities for play
Policy BN.10	Proposals for tall buildings
Policy BN.16	Preserving or enhancing heritage assets

Policy IN.2	Planning for waste
Policy T.4	Managing development and its transport impacts
Policy T.8	Parking and parking standards in new development
Policy T.9	Providing for pedestrians and cyclists
Policy S.2	Energy in new development
Policy S.3	Energy infrastructure and heat networks
Policy S.4	Sustainable design and construction
Policy 4.4	Protecting and enhancing heritage assets at Three Mills Island and Sugar House Lane
Policy SA3.1	Stratford High Street Policy Area

#### Local Plan Site Allocation SA4.2 - Sugar House Lane

8.7. The site allocation proposes “a new medium-density, mixed-use area of business (including cultural and creative) and local retail space focussed in the northern and southern part of the site; new homes with a significant number of family homes; Local Open Space, play space and public realm. A new all movements junction to enable access to the area and new and enhanced bridges to link the area to surrounding communities will be required alongside development. Proposals for development above 15 metres above ground level will only be acceptable subject to the provisions of Policy BN.10. The relevant supporting development principles refer to:

- The area adopting a genuinely mixed-use character retaining a strong employment focus that includes a base for creative industries and introduces a new residential community served by a range of local amenities and high quality public transport, pedestrian and cycle connections.
- The area will be defined by its unique natural environmental and historic industrial legacy that includes extensive canal and river frontage, robust yet adaptable buildings and intricate yards and passages;
- The historic character of the area should be celebrated by weaving high-quality new development into the historic fabric;
- High quality public, communal and private amenity spaces that create a sense of place and meet the needs of residents, workers and visitors;
- Preserve or enhance the Sugar House Lane Conservation Area.

8.8. The site allocation makes specific reference to the scheme granted planning permission under the 2012 permission meeting the requirement of the Site Allocation

#### 8.9. **Other relevant guidance and material considerations**

- National Planning Practice Guidance (on-line planning resource)
- Olympic Legacy Supplementary Planning Guidance (July 2012)
- Mayor of London Housing Supplementary Planning Guidance (March 2016)
- Mayor of London Homes for Londoners Affordable Housing and Viability Supplementary Planning Guidance (August 2017)
- Mayor of London Play and Informal Recreation Supplementary Planning Guidance (September 2012)
- London Legacy Development Corporation Draft Hackney Wick and Fish Island Supplementary Planning Document (July 2016)
- London Legacy Development Corporation Planning Obligations Supplementary Planning Document (2016)
- Sugar House Lane Conservation Area Appraisal and Development Management Guidelines (2010)
- The London Plan, draft for public consultation, December 2017

- 8.10. The Mayor of London published for the purpose of public consultation a draft new London Plan on 29<sup>th</sup> November 2017. The policies in the draft new London Plan currently have only very limited material weight when making planning decisions. That weight will increase once the new Plan is submitted for its Examination in Public. This report may make reference to policies within the new London Plan where they are directly relevant to the assessment of the application proposal, however, the relevant development plan policies remain those within the current London Plan (March 2015) and the LLDC Local Plan (July 2015).

## 8. CONSULTATIONS

### Plot R7 and R8 Non-Material Amendment Application (Ref: 17/00468/NMA)

- 9.1. A local planning authority has discretion as to what consultation to undertake in relation to applications submitted under s96A of the Town and Country Planning Act (as amended). No public consultation was considered to be necessary, or undertaken on this application due to the scale and non-material nature of the amendments proposed, along with the fact that public consultation was carried out for the RMA's for Plot R7 and R8, and as such the public and statutory consultees would have seen the RMA proposals in the context of the NMA applications.
- 9.2. PPDT's environmental consultant confirmed that the changes proposed to Plot R7 and R8 raise no new or different significant impacts to those assessed in the original 2012 permission and Environmental Statement.

### Non-Material Amendment Application for the Site Wide Dwelling Mix (Ref: 18/00366/NMA)

- 9.3. As per 9.1 above, no public consultation was considered to be necessary or undertaken on this application due to the non-material scale and nature of the amendments proposed.

### Plot R7 Reserved Matters (Ref: 17/00369/REM)

- 9.4. The application was advertised by site and press notices and individual letters of notification were sent to surrounding occupiers.

Consultee	Response
LB Tower Hamlets	No response
TfL	<p>The provision of blue badge spaces is one space short of the required one space per accessible unit, however the DAS justifies this by the accessibility for the site and to proximity to public transport.</p> <p>The applicant is providing cycle parking for Plot R7 to meet current London Plan standards, which is above the amount required in the consented scheme, which is welcomed.</p> <p>Clarification on cycle storage racks and aisle width. It will be for the applicant to demonstrate and install cycle parking where the upper tier of two-tier parking can be easily accessed.</p> <p>Subject to clarifying the provision for cycle parking, TfL would not object to this application being granted.</p> <p><i>Officers comments: The applicant has identified the bike storage product as 'BDS two tier rack'. The minimum aisle</i></p>

	<p>as per their specifications for racks on both sides is 1500mm. As shown on ARC-ML's drawings, an aisle width of 1800mm is proposed which is considered acceptable. Full details will be submitted and discharged as part of Condition C16 of the 2012 permission in due course.</p>
Historic England	<p>Do not wish to offer any comments on this occasion. This application should be determined in accordance with national and local policy guidance, and on the basis of your specialist conservation advice.</p>
Cadent / National Grid	<p>There is no record of apparatus in the immediate vicinity of your enquiry. Cadent and National Grid therefore have no objection to these proposed activities.</p>
Metropolitan Police	<p>No objection to the scheme proceeding at this stage. The achievement of Secured by Design on site will be subject to a final inspection of all works carried out in the completion of the project.</p> <p><u>Officer comment: Condition C7 of the 2012 Planning permission requires compliance with Secured by Design.</u></p>
LB Newham Planning	<p>No response.</p>
LB Newham Environmental Control	<p>No comments to make.</p>
Canal & River Trust	<p>The height of block C is very tall, and it is unfortunate that the tallest element is closest to the navigation. We note that the original 2012 application included the parameter plan drawing number PP-1-103, which required this block to be maximum 11 storeys, but the current application refers to a plan PP-1-103 rev K, which accepts up to 14 storeys. We would be pleased to see this plan and details of the application that amended this.</p> <p>Our preference would be for the height to be moved northwards away from the waterspace, with the lower podium block facing onto the navigation. However, given the distance to the water and the location of the site on the northern side of the river, we expect that overshadowing will not be as much of an issue as if it were on the southern or western bank.</p> <p>In terms of architectural expression we have no concerns about the simple form and detailing of blocks A and B, which also have some depth to the façade of each, but the detailing of Block C, with increased areas of reconstituted stone further up the building, seems to increase the verticality of the building and accentuate the overall height. In comparison with blocks A and B, despite the difference in materials, the elevations seem a bit 'flat' relying on the recessed corners to provide relief, whereas the side elevations have some additional interest provided by cantilevered balconies. We consider that this would be improved by having a</p>

	<p>stepped form, reduced material palette and some cantilevered balconies to break up the silhouette of what is a tall and fairly stark principal elevation facing the waterspace.</p> <p><i>Officers comment: The height of the accent tower was originally approved at 13 storeys (not 11) as part of the 2012 permission. In May 2017 the height of the accent tower has been amended to 14 storeys (PP-1-109K), as the height of the accent tower in block R2 has been reduced from 16 to 14 storeys (ref:17/00009/NMA). The principle of the height increase has therefore been accepted, and reviewed as part of the EIA Screening Request accompanying the 17/0009/NMA application).</i></p> <p><i>The detailed design of the tower element is required to comply with Local Plan Policy BN.10 (Tall Buildings) and is supported by QRP. See the Design section below for detailed design assessment.</i></p> <p>Landscaping: Comments are provided on the Riverside Park; cyclists who fail to dismount in the Riverside Park, and privacy for the ground floor occupiers of the southern block. Request a condition for details of the proposed landscaping.</p> <p><i>Officers comment: The detailed design of the Riverside Park has already been approved as part of the Public Realm and Infrastructure RMA (ref: 15/00239/REM) in June 2016. This RMA only results in a few minor amendments to correspond with the detailed design of the built environment.</i></p> <p><i>The Riverside Park is not considered to be a cycle route – the cycle routes would be Sugar House Lane and Hunts Lane, which provide more direct routes through the site to Stratford High Street.</i></p> <p><i>Privacy is considered to be sufficient, as there is a significant difference in level as well as defensible planting to protect the amenity of future residents.</i></p> <p><i>Condition C5 of the outline planning permission requires final planting and lighting details, so it is not necessary for a similar condition to be attached to the Plot R7 RMA.</i></p>
PPDT's transport consultant	<p>The submitted documents and drawings are satisfactory.</p> <p>Best practice would be provision of 20% active and 80% passive electric charging bays, in line with Draft London Plan standards. It is recognised however that the provision is in line with the condition A17 of the 2012 outline permission, and the applicant has stated that provision of additional EVCP will be reviewed once the development is operational.</p>

PPDT's environmental consultant	PPDT's Environmental Consultants assessed the Heritage Statement, Townscape and Visual Impact Assessment, Code for Sustainable Homes, BREEAM, Waste, Daylight, Sunlight and Overshadowing, Wind, and Overheating elements of the proposal. After additional information/clarification was submitted on some areas, details were found to be acceptable.
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Plot R8 Reserved Matters (Ref: 15/00384/REM)

- 9.5. The application was advertised by site and press notices and individual letters of notification were sent to surrounding occupiers in 2015 when the application was originally submitted, and again in June 2018 when the application was amended. The consultation responses to both rounds of consultation are summarised below.

Consultee	Response
LB Tower Hamlets	2015 consultation: No objection 2018 consultation: No objection
TfL	2015 consultation: No objections. It is welcomed that cycle parking provision is in excess of the requirement in condition A19, now providing 2 spaces for all units with more than one bedroom. Condition A17 did not include reference to passive provision for EVCP. It would be welcomed if the car parking provision was constructed in such a way to allow further EV charging points.  2018 consultation: Given the location away from Strategic Road Network and nature of proposals, TfL has limited comments to make on this application. I had previously made comments on application 15/00239/REM about the site wide infrastructure arrangements, including identifying any infrastructure to support the bus bridge and potential bus stop.  <i>Officers comment: Site wide Infrastructure comments have been addressed as part of the Public Realm RMA (15/00239/REM) which was approved in June 2016.</i>
Historic England	2015 consultation: Do not wish to offer any comments on this occasion  2018 consultation: Do not wish to offer any comments on this occasion. This application should be determined in accordance with national and local policy guidance, and on the basis of your specialist conservation advice.
GLAAS	2015 consultation: No response.  2018 consultation: We do not consider that it is necessary for this application to be notified under the GLAAS charter.
Cadent / National Grid	2018 consultation: Searches have identified that there is apparatus in the vicinity of your enquiry which may be affected by the activities specified, comprising low or

	<p>medium pressure (below 2 bar) gas pipes and associated equipment.</p> <p>Please inform Plant Protection, as soon as possible, the decision your authority is likely to make regarding this application.</p> <p>If minded to approve attach an informative.</p> <p><i><u>Officers comment:</u> suggested informative is attached.</i></p>
Metropolitan Police	<p>2015 consultation: No objection to the scheme proceeding however request conditions that it shall incorporate measures to minimise the risk of crime and follow secured by design throughout the development.</p> <p>2018 consultation: No objection to the scheme proceeding at this stage. The achievement of Secured by Design on site will be subject to a final inspection of all works carried out at the completion of the project.</p>
LB Newham Planning	<p>2015 consultation: <u>Support the application</u> and request following comments are taken into account:</p> <p>a) Bedrooms facing the street on Hunts Lane which may compromise the privacy of occupants. Can the internal floor levels be raised to solve this issue?</p> <p>b) The position and proportion of the cores look good. Newham is supportive of the generous flat sizes and amenity spaces.</p> <p>c) If possible more space should be provided in lobbies for cycle storage so that residents do not have to go down to the basement to park bikes. However, it is noted that the generous flat sizes may be affected by such a change.</p> <p>d) Conventional balconies are not provided, but the folding glazing allows the corner of the living space to be opened up during good weather. When closed the home benefits from additional usable space internally, and the generous unit sizes more than compensate for the lack of a dedicated balcony.</p> <p>e) Development provides a generous, high quality residential environment. The architecture, materiality and detailing is of a high quality.</p> <p><i><u>Officers comment:</u> a) internal levels are raised above the street level to address potential privacy issues. c) This would affect flat sizes, so cycle parking is provided in generous lockers in the basement, as per other blocks in the development.</i></p> <p>2018 consultation: No response.</p>
LB Newham Transport	<p>2015 consultation: No response.</p> <p>2018 consultation: The parking management strategy is considered to be comprehensive, but there are some anomalies which require further information:</p>



	<p>a) Parking allocation strategy – LBN prefer to give priority to Blue Badge, followed by family units and car sharers.</p> <p>b) Maintenance of car park barriers.</p> <p><i>Officers comment: a) car parking allocation would be secured by condition as with previous blocks. b) The applicant has confirmed that the shutter system is capable of being manually controlled in the event of failure. This would be the responsibility of the on-site estates management team.</i></p>
Canal & River Trust	<p>2015 consultation: No objection</p> <p>2018 consultation: Based upon the information available we have no comment to make.</p>
Environment Agency	<p>2015 consultation: No specific comments to make on the application. We do recommend that you satisfy yourselves that the details associated with this application do not prevent the applicant from implementing the approved surface water drainage scheme 15/00239/REM.</p> <p>2018 consultation: We object to the proposed development as submitted because no assessment of the risks to legally protected species has been provided.</p> <p><i>Officers comment: Information has been sent to the EA regarding the ecological studies that were carried out as part of the Environmental Statement which demonstrates the only element of interest relates to Japanese Knotweed and the proximity to the River Lea Navigation and London Canals Site of Metropolitan Importance for Nature Conservation (SMINC). The Japanese Knotweed has been treated. Impact on the watercourse as a result of the construction and operational activities were considered as part of the ES.</i></p> <p>EA Letter dated 17.08.18: We have reviewed the document 'Phase R8 – Sugar House Lane Ecology letter ref EA Objection_v1.pdf dated 26<sup>th</sup> July 2018. Based on the information provided, we are able to remove our objection and have no further comments to make on this application.</p>
London Fire and Emergency Planning Authority	<p>2015 consultation: The brigade is satisfied with the proposals.</p> <p>2018 consultation: No response</p>
Lee Valley Regional Park Authority	<p>2015 consultation: The authority welcomes the redevelopment of this site.</p> <p>2018 consultation: No response</p>
Thames Water	<p>2015 consultation: The reserved matters application does not affect Thames Water and as such we have no observations to make.</p> <p>2018 consultation: If the developer follows the sequential approach to the disposal of surface water we would have</p>

	<p>no objection. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required.</p> <p>Thames Water would advise that with regard to Foul Water sewage network infrastructure capacity, we would not have any objection.</p>
PPDT's transport consultant	<p>2018 consultation: The submitted documents and drawings are satisfactory.</p> <p>Best practice would be provision of 20% active and 80% passive electric charging bays, in line with Draft London Plan standards. It is recognised however that the provision is in line with the condition A17 of the 2012 outline permission, and the applicant has stated that provision of additional EVCP will be reviewed once the development is operational.</p>
PPDT's environmental consultant	<p>PPDT's Environmental Consultants assessed the Heritage Statement, Townscape and Visual Impact Assessment, Code for Sustainable Homes, BREEAM, Waste, Daylight, Sunlight and Overshadowing, Wind, and Overheating elements of the proposal. After additional information/clarification was submitted on some areas, details were found to be acceptable.</p>

Quality Review panel – Review of Plot R7, 6 April 2017

- 9.6. The Quality Review Panel recommends that the design team continue to develop the proposal – and in particular the design of the tower block / podium – in order to arrive at the optimum response to the site context, and also to achieve exceptional architectural quality. This should include interrogation of the relationship between the proposed tower and podium, and also reconsideration of the building's silhouette. The panel supports the condensed space between Plots R7 and R8, as well as the arrangement of the access ramp to the basement car park.

Quality Review Panel – 2<sup>nd</sup> review of Plot R7, 4<sup>th</sup> May 2017

- 9.7. The Quality Review Panel commends the design team on an effective response to its earlier comments on the proposal for Strand East Plot R7. Revisions to the design of the entrance to the basement car park, and the treatment at ground floor level of the sharply angled corner of the podium, enhance the public realm. The architectural expression of the tower block / podium is well considered, with the two elements now relating more successfully to each other. The scheme promises high quality residential accommodation. The panel recommends further exploration of the best solution for access to cycle storage at basement level.
- 9.8. The QRP is confident that the design team will continue to evolve the proposal for Strand East Plot R7 successfully, in consultation with planning officers.

Quality Review Panel – Review of Plot R8, 30 April 2015

- 9.9. The panel finds much to admire in the proposals for Strand East Plot R8, which are based on rigorous thinking about residential typologies, and the design of homes from the inside out. The architectural expression of both the linear block is developing in a positive direction, although the panel offers some comments

on potential refinements. Limited information is available at this stage on the landscape design. This will be critical to making the most of the riverside park, and ensuring this is a welcoming, accessible and high quality space. This may require some adjustment to the layout of buildings, to create a generous and legible route from Hunts Lane to the park. The panel would welcome further information on the landscape design and a future review.

- 9.10. The panel admires the simplicity and elegance of the architecture proposed for the 16 storey tower.
- 9.11. The residential layout of the linear block promises accommodation of the highest quality.
- 9.12. The panel offers its warm support to the proposals for Strand East Plot R8 and would welcome a further opportunity to comment on the landscape design and access to linear park.
- 9.13. *Officers comments: Since the QRP review of Plot R8, the Site wide Public realm and Infrastructure RMA was approved in June 2016 (ref: 15/00239/REM), which provides full details of the landscape design and riverside park. QRP supported the Public Realm and Infrastructure RMA proposals.*

## **10. ASSESSMENT OF PLANNING ISSUES**

### **10.1. Non-Material Amendment Applications (ref: 17/00468/NMA & 18/00366/NMA)**

- 10.2. The first application for Non-Material Amendments proposes amendments to Parameter Plans PP-1-101 Rev B (Building Line Requirement), PP-1-103 Rev K (Maximum Storey Heights), PP-1-108 Rev B (Underground Car Parking Strategy) attached to the 2012 Planning Permission, to accommodate amendments to the extent of the yellow building line, set-back storeys and access to the underground car parking zone permitted at development plots R7 and R8.
- 10.3. The application proposes the following non-material amendments:

#### Building Line Requirement

- g) Amendment to the Maximum Building Line to permit an increase in the building footprint of the linear blocks of both Plots R7 and R8 by approximately 1.8-2.1m, to accommodate the minimum basement width of two rows of parking with a central carriageway. This relates to basement storey only, and the actual building line from Ground Floor upwards accords with the currently approved building line requirement.
- h) Amendment to the Maximum Building Line to permit an increase in the building footprint of the R7 accent tower by approximately 0.8m to enable a logical internal layout of the building and to accommodate the required car park ramp.

#### Maximum Storey Heights

- i) Amendment to the position of the set-back storey on top of the fourth floor element of R7 linear block (B) so that there is a small setback on the east and western sides.
- j) Amendment to the set-back storey of the linear block at R8, so that exceeds the 1:2 ratio of the approved parameter plan and falls partially outside the setback zone.

#### Underground Parking Strategy

- k) Amendment to the 'Possible Underground Parking Zone' of the Underground Parking Strategy parameter plan for a single shared car park to extend under both plots.

- l) Amendment to the location of the access to the underground car park to between the linear block and accent tower of Plot R7.
- 10.4. The proposed amendments to the Maximum Building Line parameter plan are required to provide a feasible underground car park and predominantly relate to the basement level and ground floor amenity provision. The proposed amendments have been tested in the technical assessments submitted in support of the RMA and the associated EIA Screening Opinion request, and no significantly new or different environmental effects other than those already identified in the Environmental Statement accompanying the 2012 permission have been identified.
- 10.5. The increase in building footprint of the linear blocks relates to the basement level only, and the actual building line from the ground floor upwards accords with the currently approved building line requirement. Whilst it appears there would be a loss of open space resulting from the change in maximum building line, the Plot R7 and R8 building footprints currently proposed formed part of the approved site-wide Public Realm and Infrastructure RMA (15/00239/REM) and therefore do not represent a real loss of publicly accessible space from that previously assessed. There would be an increase in shared/public open space between the tower element and linear block at Plot R8, which was previously assumed to come forward as a continuous building line.
- 10.6. The proposed amendments to the Maximum Storey Heights have been tested in the technical assessments submitted in support of the RMA and the associated EIA Screening Opinion request, and no significantly new or different environmental effects other than those already identified in the Environmental Statement accompanying the 2012 permission have been identified.
- 10.7. The proposed amendments to the Underground Parking Strategy involving a revised entry ramp location do not raise any concerns from a transport perspective. It is welcomed that this will allow a holding area at the approach to the ramp, to avoid vehicles waiting on Hunts Lane. The amendment would be non-material in nature and would not result in any new or different environmental effects.
- 10.8. The proposed amendments to the Building Line Requirements, Maximum Storey Heights, and Underground Parking Strategy are considered to be non-material in nature, and therefore recommended for approval.

Site Wide Dwelling Mix

- 10.9. The second Non-material amendment application proposes to amend condition A15 (Site-Wide Dwelling Mix) (18/00366/NMA). Condition A15 of the 2012 Planning Permission defines the number of each unit size type. Condition A15 has previously been amended by NMA reference 16/00081/NMA, granted in June 2016.
- 10.10. The table below sets out the approved and proposed dwelling mix:

Original 2012 Permission (12/00336/LTGOUT/LBNM)		Currently approved dwelling mix (16/00081/NMA)		Proposed dwelling mix – June 2018		
Unit Type	No. of units	Unit Type	No. of units	Unit Type	No. of units	Difference (Feb 16 – Jun 18)
Studio	8 (1%)	Studio	65 (5%)	Studio	52 (4%)	-13
1-bed	468 (39%)	1-bed	435 (36%)	1-bed	423 (35%)	-12
2-bed	244 (20%)	2-bed	217 (18%)	2-bed	250 (21%)	+33

3-bed	<b>430 (36%)</b>	3-bed	<b>364 (30%)</b>	3-bed	<b>390 (33%)</b>	<b>+26</b>
4-bed	<b>34 (3%)</b>	4-bed	<b>118 (10%)</b>	4-bed	<b>85 (7%)</b>	<b>-33</b>
5-bed	<b>16 (1%)</b>	5-bed	<b>1 (0%)</b>	5-bed	<b>0 (0%)</b>	<b>-1</b>
Total	<b>1,200</b>	Total	<b>1,200</b>	Total	<b>1,200</b>	
<b>Provision of Family Housing</b>						
2-bed +	<b>724 (60%)</b>	2-bed +	<b>700 (58%)</b>	2-bed +	<b>725 (60%)</b>	<b>+25</b>
3-bed +	<b>480 (40%)</b>	3-bed +	<b>483 (40%)</b>	3-bed +	<b>475 (40%)</b>	<b>-8</b>

- 10.11. As can be seen from the table, a shift from studios and 1-bed units as well as 4-bed and 5-bed units to 2-bed and 3-bed units is proposed. The applicant has explained that the number of 4 and 5 bed units has decreased because the remaining plots are unlikely to accommodate the remaining number of 4 bed units. However, it is noted that the number of currently proposed 4 bed units across the site exceeds the initial 2012 target. As all plots with mews/town houses have now been determined, the larger units have been downsized to 2 and 3 bed units.
- 10.12. When compared to the originally permitted dwelling mix and the currently approved dwelling mix, the proposed amendment to the site-wide dwelling mix is not considered to have an adverse effect on the provision of family dwellings. LLDC Local plan policy H1 requires 50% of dwellings to have 2 or more bedrooms. The proposal would result in 60% of dwellings across the entire development with 2 or more bedrooms (725 no.), which exceeds the Local Plan Policy H.1 and is a slight increase from that permitted. 39.6% of units would have 3 or more bedrooms (475 no. which is a reduction of 8 units from the currently approved position).
- 10.13. The proposed changes to the dwelling mix would not affect the provision of affordable housing or review mechanism contained in the accompanying S106 agreement. It is considered that the proposed dwelling mix would not have a material impact on transport and/or other technical/environmental matters.
- 10.14. It is considered that the proposed amendments to Condition A15 which do not result in a change to the overall total number of units, and only relate to a small percentage of the overall number of units, do not constitute a material alteration to the 2012 Planning Permission. It is therefore recommended that the Non-material amendment to the dwelling mix be approved.

#### **Reserved Matters Applications (17/00369/REM & 15/00384/REM)**

- 10.15. The following paragraphs consider the Reserved Matters (layout, scale, appearance and landscaping), and the supporting technical information required by the 2012 permission specification for Plots R7 and R8.
- 10.16. The applications for Reserved Matters for Plot R7 are submitted on the basis that the applications for Non-Material Amendments to the 2012 'Maximum Storey Heights', 'Building Line Requirement' and 'Underground Parking Strategy' parameter plans; and the amendments to the Site Wide Dwelling Mix considered at paragraphs 10.1. to 10.14. above, and recommended for approval at paragraph 2.1 of this report, have been approved. The following paragraphs consider the Reserved Matters - layout, scale, appearance and landscaping – of Plots R7 and R8 and related planning issues.

## Land Use

- 10.17. The 2012 permission approved parameter plans determine the permissible ground, first and upper floor building uses across the site. Plot R7 and R8 have outline planning permission to be redeveloped for residential use with an element of commercial floorspace at ground floor level in Plot R7. The reserved matters applications both comply with the land use parameter plans.
- 10.18. The total number of units within the outline part of the 2012 permission is capped by planning condition (A8) at 1,192 units. The Plot R7 application would contribute 82 units towards this total, and the Plot R8 application would contribute 116 units towards the total. The following table shows how the number of residential units proposed contributes to the total number of permitted units:

Plot	Reserved Matters Application Status	Maximum number of residential units (as restricted by planning condition A9)	Number of residential units proposed
Plot R6	RMA approved	1,192 units	103
Plot R1	RMA approved		161
Plot R3	RMA approved		156
Plot MU5	RMA approved		42
Plot R2	RMA approved		211
Plot R4	RMA approved		89
Plot R8	RMA submitted		116
Plot MU3	RMA submitted		21
Plot R5	RMA approved		86
Plot R7	RMA submitted		82
Plot MU4	RMA to be submitted		-
<b>Total</b>		<b>1192</b>	<b>1067</b>

- 10.19. The site is located within the Local Plan Site Allocation SA4.2: Sugar House Lane. It makes explicit reference to the 2012 permission and its compliance with the objectives of the site allocation to create a new medium-density mixed use area including new residential accommodation and a significant number of family homes.
- 10.20. The proposals also include an element of flexible commercial floorspace located in Block C (tower and plinth) of Plot R7 comprising 330sqm of Class A1, A3, A4 and/or B1 floorspace. There is no commercial floorspace proposed in Plot R8. See table below for the permitted uses across the scheme.

Use	Permitted Floorspace (sqm) (GIA) Outline PP	Proposed in R7	Proposed in R8	Total proposed to date (all RMAs including those approved)	Total approved to date (all RMAs)
<b>Residential (C3)</b>	1192 units	82 units	116 units	1,068 (R4; MU5; R6; R8; MU3; R1; R2; R3; R4; R5; R7) – excluding 8 units in the NEQ	848 units (R1; R2; R3; R4; R5; R6; MU5)

<b>Flexible Uses Floorspace (A1, A2, A3/4, B1, D1, D2)</b>	12,593m <sup>2</sup>	330m <sup>2</sup>	-	<b>9,274 sqm (R4; MU5; MU2; MU3; MU1; R1; R2; R5; R7)</b>	<b>7,176 sqm (MU1; MU2; MU5; R1; R2; R4; R5)</b>
<b>Offices and workshops (B1)</b>	33,950m <sup>2</sup>		-	28,273 sqm (MU2; MU3)	25,153 sqm (MU2)
<b>Hotel (C1)</b>	350 beds or 22,500m <sup>2</sup>		-	349 (MU3)	-

10.21. The applications are considered to comply with the 2012 permission and site Allocation SA4.2: Sugar House Lane of the Local Plan.

Design and Compliance with Reserved Matters of – Scale, Layout, Appearance and Landscaping

Design overview

10.22. Plot R7 is located at the west end of the Strand East site and would comprise two residential blocks which are set against Chimney Walk (and the existing chimney of note in this location), the Lee River Navigation open space and Hunts Lane.

10.23. The riverside linear block (A+B) has a proposed height of 5 storeys with an 8 storey bookend on the southern end which mirrors the massing of Plot R8; all storeys are proposed as residential. The tower block (C) to the north of the plot, which fronts Hunts Lane and Chimney Walk, would be predominantly 14 storeys with a lower 4 storey plinth (included within the 14 storeys) and comprises 1 commercial storey at ground floor with residential above. There would be a shared basement for plots R7 and R8 with access provided via a ramp accessed on the southern side of Block C.

10.24. The QRP panel praised the scheme's potential to provide high quality residential accommodation and the well-considered architectural expression of the tower block and podium. Given the proposed height of the scheme being above 15m it will be required to meet policy BN.10 criteria.

10.25. Plot R8 is located to the south-west of the Strand East site and is proposed to comprise entirely residential blocks which front the Lee River Navigation open space, Hunts Lane and the approved bus bridge. The riverside linear block (A+B) has a proposed height of 5 storeys with an 8 storey bookend on the northern end which mirrors the massing of Plot R7. The accent tower block (C) is 16 storeys with a lower 4 storey block (D) at the base fronting the park. There is a shared basement for Plots R7 and R8 with vehicle access provided via a ramp accessed from Plot R7.

10.26. The QRP panel praised the scheme for being based on, 'rigorous thinking about residential typologies, and the design of homes from the inside out,' finding 'much to admire.' The applicant successfully responded to feedback from QRP on possible improvements to the scheme including to the refuse strategy and the building's relationship to the bus bridge and introduced birch trees and a planting bed to provide a spatial barrier between the public realm and the building.

Heritage and Conservation

10.27. The north tip of Plot R7 is within the Sugar House Lane Conservation Area. It is considered appropriate to consider the impact of the proposal on the conservation area and the setting of heritage assets in accordance with paragraph 200 of the NPPF in addition to the requirements under part 10 of Local Plan Policy BN.10 and part 8 of policy BN.16 for such assessment.

- 10.28. An existing chimney of merit sits to the north of Plot R7 and forms the end of the proposed Chimney Walk, a pedestrian street connecting Sugar House Lane with the Riverside Park. The proposed reserved matters scheme for R7 works within the parameters of the outline masterplan permission to respect and compliment the setting of the chimney, conservation area and waterway. Furthermore, the sensitive choice of high quality materials, which make reference to a palette of materials found in the area, and their robust detailing is complementary to the character of the Conservation Area.
- 10.29. Although close to the 3 Mills Conservation Area, Plot R8 doesn't sit within a Conservation Area or close to buildings or structures of merit such as the chimney close to Plot R7. The proposed reserved matters scheme does however work within the parameters of the outline masterplan permission to respect and compliment the setting of the waterway. The sensitive choice of high quality materials, which reference a palette of materials found in the area, and their robust detailing is complementary to the character of the nearby Sugar House Lane Conservation Area.
- 10.30. Officers have considered the impact of the proposals on the Sugar House Lane Conservation Area in accordance with paragraph 200 of the NPPF and have concluded that the design, massing, height and scale of the proposals are acceptable and sympathetic to the surrounding historical context, and as such would preserve the character and appearance of the conservation area.
- 10.31. The proposals have also been assessed against the provisions of the Planning (Listed Buildings and Conservation Areas) Act 1990, with reference to Section 66 and 72. Officers support the proposals and consider that they would comply with the NPPF in terms of making a positive contribution to local character and distinctiveness. Due to the high quality design which is in accordance with the 2012 permission, officers are satisfied that the proposals would not adversely affect the character of the Conservation Area.

#### Form, Scale, Height and Massing

- 10.32. The form and varying heights of the proposed blocks for Plots R7 and R8 have been designed to comply with the OPP (as amended) and the composition works successfully in its relationships with the differing conditions of Hunts Lane, Chimney Walk and the Riverside Park.
- 10.33. For Plot R7, the height of the tower element would act as a marker within the wider site, signifying the junction between Chimney Walk and the Riverside Park, whilst the lower 4 and 5 storey elements respect the predominant low-rise residential scale of the Strand East masterplan area. The 8 storey bookend element of the linear riverside block would provide variation to a relatively long block and signifies the end of the east/west link across the site.
- 10.34. Although the gap between the R7 and R8 bookend elements appears quite narrow, it has been demonstrated through precedent studies and analysis by the design team that this would not be detrimental to the streetscape environment, with the QRP panel suggesting this arrangement could contribute to a 'sense of drama with views glimpsed to the riverside park'.
- 10.35. For Plot R8, the 4 storey blocks (B (with set-back fifth storey, D) would respect the predominant low-rise residential scale of the Strand East masterplan area, whilst the height of the tower element (C) acts as a marker within the wider site, signifying the end of the Riverside Park and crossing point of the bus bridge. The 8 storey bookend element (A) of the linear riverside block would provide variation to the relatively long block and, complemented by the 8 storey bookend of R7, signifies the end of the east/west link across the site.
- 10.36. The 2012 permission approved 'Maximum Storey Heights' parameter plan and Design Code establish the maximum storey heights for individual buildings



across the entire site. The maximum heights are defined as storey heights rather than heights above ordnance datum or true heights above ground level. The 2012 permission (as amended) supports maximum building heights ranging between 4 and 8 storeys for Plot R7, and 4 and 16 storeys for Plot R8.

- 10.37. The applications are supported by a Townscape and Visual Impact Assessment (TVIA) for each Plot which describes the increase in ground levels and building heights above those assumed in the 2012 Environmental Statement. The increase in ground levels was approved by the Public Realm and Infrastructure RMA (15/00239/REM).
- 10.38. For Plot R7 the approved ground levels have changed from a consistent 5.3m AOD across the plot to approximately 5.5m - 5.6m AOD around the site. For Plot R8 the approved ground levels have changed from the consistent 5.3m AOD across the plot to approximately 5.5m – 6.0m AOD around the site. Paragraphs 6.10 to 6.12 set out the changes with regard to approved ground levels and true building heights.
- 10.39. The increase in true building height takes into account the new ground levels; a general increase in floor to floor heights of the residential units from 3m to 3.15m to achieve more generous floor to ceiling heights within homes; an increase in the floor to floor height of the commercial units, from 4m to between 4m and 4.95m; the increased building footprints; and increased footprint of the set-back storeys in order for efficient unit plans which comply with the minimum space standards set out in the Mayor’s Housing SPG.
- 10.40. The change in site levels and increase in building heights, both individually and cumulatively, is not considered to result in any new or different significant townscape or heritage impacts, particularly as they are ‘contained’ within the site and maintain the human-scale attributes of the masterplan. The environmental impact of taller buildings, particularly in relation to daylight and sunlight, has been subject to detailed assessment and this is considered below. It is noted that, notwithstanding the net increase in overall heights, the application remains compliant with the ‘Maximum Storey Heights’ parameter plan (as amended).
- 10.41. The Local Plan Site Allocation requires the ‘prevailing and generally expected heights’ at Sugar House Lane to be 15m (5 residential storeys) above existing ground level with development proposed above that height to be subject to the tests of Policy BN.10. Where the proposed true heights exceed the prevailing heights identified in the Local Plan, they are compliant with the 2012 permission, and are considered to be successful in terms of their urban design and place making response. The scale and heights proposed are considered to be acceptable in townscape terms, and the quality of the residential accommodation and architectural expression is considered to be high.
- 10.42. Despite complying with the storey heights permitted under the 2012 permission (as amended), the detailed design is required to meet Policy BN.10. The high quality of residential accommodation, calm and robust architectural expression and considered response the scheme makes to embed itself in the already permitted public realm design is considered to satisfy the requirements set out by BN.10 as is set out in the table below

#### Tall Buildings Assessment

- 10.43. Given the high quality of residential accommodation, calm and robust architectural expression and considered response the proposals make to embed themselves in the already permitted public realm design, they are considered to satisfy the requirements set out by BN.10 as is set out below:

BN.10 Criteria Summary for Plot R7 and R8

<i>Exhibit outstanding architecture and incorporate high-quality materials, finishes and details</i>	The choice of material palette has been carefully considered and the schemes provide a high-quality composition of materials that are robust and complementary to the emerging palette of Strand East.
<i>Respect the scale and grain of their context</i>	Conforming to the parameters of the outline permission, the schemes for Plot R7 and R8 are considered to successfully respond to the grain and scale of the surrounding and emerging area.
<i>Relate well to street widths and make a positive contribution to the streetscape</i>	<p>The Plot R7 and R8 proposals would contribute to the emerging character of Hunts Lane and Chimney Walk and would accommodate public routes from the interior of Strand East to the riverside park, with both the route between Plot 8 blocks B and C, and by respecting the masterplan principle of creating an east/west route through the site that terminates between the bookends of R7 and R8. The residential entrance to Plot R8 located on the alley between R7 and R8 would help to advocate this space.</p> <p>The chamfer introduced to the ground floor of the Plot R7 tower block at the intersection of Hunts Lane/Chimney Walk is a welcome feature which enhances the public realm at this corner.</p>
<i>Generate an active street frontage</i>	<p>Plot R7 Block C would provide active frontage on all approaches with a commercial unit wrapping around much of the ground floor and with the residential lobby creating activity on Hunts Lane. Although purely a residential building the residential lobby of Block A/B would provide activation of the corner of Hunts Lane between R7 and R8. Soft landscaping would create a pleasant backdrop to the riverside park whilst providing defensible space to the ground floor residential units of Block A/B.</p> <p>The Plot R8 scheme seeks to introduce active frontage where possible and most beneficial with a community space at the ground floor of the tower block which activates the corner of Hunts Lane and residential lobbies activating the routes through from Hunts Lane to the Riverside Park. By locating parking in a basement there is still partially active frontage across the ground floor of all blocks through residential windows which overlook the street or park. Soft landscaping creates a welcome backdrop to the riverside park whilst providing defensible space to the ground floor residential balconies of Block A/B.</p>
<i>Provide accessible public space within their curtilage</i>	<p>Plot R7 would respect and reinforce the accessible public space set out in the outline permission, especially the adjoining Riverside Park and public routes across the scheme to the park.</p> <p>Plot R8 would also respect the accessible public space set out in the outline permission including the creation of the tower courtyard, a semi open courtyard that provides a quiet stopping space between Hunts Lane and the Riverside Park.</p>

<p><i>Incorporate sufficient communal space</i></p>	<p>No communal amenity space would be provided as part of Plot R7, but all units would have well located private amenity spaces which are more often than not located to have a view of the river and a sunny aspect. The riverside park is adjacent to the plot and would provide an excellent area of public open space for residents to enjoy.</p> <p>The Plot R8 tower block would provide a community space at ground floor fronting onto Hunts Lane which is envisaged as a space for residents to use for reading/meetings/events/ clubs and activities. There would be a terrace on the roof of Block D for residents of Block C/D with a range of planters and seating that takes in views of the river. The riverside park is adjacent to the plot and would provide an excellent area of public open space for residents to enjoy.</p>
<p><i>Contribute to defining public routes and spaces</i></p>	<p>The Plot R7 and R8 proposals work successfully with the strategies set out by the outline masterplan permission and public realm permission. The bookend elements of the plots would signify the termination of the east/west route through the Strand East site, and the Plot R7 tower block would act as a marker at the junction between Chimney Walk and the riverside park. The Plot R8 tower block would act as a marker at the end of the Riverside Park and junction of the new bus bridge connection.</p>
<p><i>Promote legibility</i></p>	<p>The scheme would promote active healthy streets and contribute to the improved legibility of the area with taller marker elements enhancing wayfinding through the Strand East site. The locations for entrances to residential lobbies are well considered, which includes positioning them on corners to activate minor routes whilst being clearly legible from Hunts Lane.</p>
<p><i>Create new or enhance existing views, vistas and sightlines</i></p>	<p>The proposals work successfully with the strategy proposed by the outline masterplan permission and public realm permission, including accommodating the east/west view through the site. Plot R7 would visually signify the junction of Chimney Walk with the riverside park with the tower element, and Plot R8 would visually signify the end of the Riverside Park and the new bus bridge connection to the site.</p>
<p><i>Preserve or enhance heritage assets and the view to / from these, and contribute positively to the setting of heritage assets, including conservation areas</i></p>	<p>Block C of Plot R7 would respect the setting of the existing chimney of note, stepping down to 4 storeys in this location and the sensitive choice of high quality materials, which make reference to a palette of materials found in the area and their robust detailing is complementary to the Sugar House Lane Conservation Area that the northern tip of R7 encroaches into.</p> <p>Plot R8 does not fall within a conservation area but is near the Three Mills Conservation Area. The choice of high quality materials references a palette of materials found in the area and robust and simple detailing is harmonious with the rest of the site, which is designed to complement the character of the Sugar House Lane conservation area.</p>

<i>Micro-climatic conditions</i>	The scheme's layouts are carefully considered to ensure units would receive good levels of natural light, especially in living areas, and with all apartments with northern elevations being dual aspect.
<i>Amenity: impacts to the surrounding area</i>	The proposals would not negatively impact the proposed amenity space in the surrounding area, and would help to enclose and frame the Riverside Pak.
<i>Existing views of landmarks, parkland, heritage assets, waterways, and views along street corridors.</i>	The schemes would create a complementary backdrop to the riverside park and the gaps between the buildings create glimpses of the waterway from the interior of the site.  Plot R7 would work successfully to respect the chimney of note on Chimney Walk, stepping down to 4 storeys in this location.
<i>Tall buildings should be located within the Centre boundaries outlined within the Local Plan</i>	The taller elements of the scheme are as the outline permission for the site and would create a high quality and well considered varied townscape, whilst also marking important junctions and routes which cross the site.

#### Layout, Uses and Quality

- 10.44. Policies BN.1, BN.5 and BN.10 of the Local Plan require new development to consider how uses integrate with, and relate to, public and private space; provide an accessible and inclusive environment; contribute positively to the streetscape; generate an active frontage; provide accessible public space; define routes and spaces; and promote legibility.
- 10.45. The 2012 permission approved 'Building Line Requirement' parameter plan establishes 'obligatory' (i.e. the building line must meet the parameter plan), 'partially obligatory' (i.e. facades must adhere to at least 75% to the building line; for example this would allow for the inclusion of projecting balconies), 'maximum' (i.e. no projection beyond) and 'indicative' (i.e. to be determined through detailed planning application) lines for buildings permitted across the entire site.
- 10.46. Plot R7 is subject to partially obligatory building lines along Hunts Lane and the northern most edge of the block, and indicative building line to the southernmost edge, along with maximum building line to the western edge. The proposals comply with these requirements as amended by the accompanying NMA.
- 10.47. Plot R8 is also subject to partially obligatory building lines along Hunts Lane and the southern most edge of the block, and indicative building line to the northern edge, along with maximum building line to the western edge. The proposals comply with these requirement as amended by the accompanying NMA.
- 10.48. The 2012 Design Code identifies Plots R7 and R8 as Riverside Blocks; and requires the design to incorporate the following:
- Take inspiration from converted mills and warehouses in other post-industrial areas of disused docks and wharfs
  - Make maximum use of views up and down river

- West side blocks intended to have more differentiated perimeter and should open up to park with terraces, balconies and pocket courtyards.
- Vertical circulation and cores shall be predominantly located internally and away from principal elevations.
- There shall be no extensive shared horizontal circulation (corridors) along main street elevations.
- The ground level shall be set sufficiently above street level to provide adequate privacy
- There shall be only one entrance to the underground car park.
- Apartments shall be predominantly dual-aspect. Only studio or 1 bed flats shall be permissible as single aspect.

10.49. Plots R7 and R8 have accent towers which are identified in the 2012 Design Code, and required to incorporating the following:

- Slender, transparent towers at defined locations with internal balconies (loggias) and high levels of glazing.
- Each tower shall have only one residential entrance.
- Individual commercial premises shall be accessed directly from the street.
- Vertical circulation and cores shall be located internally and away from principal elevations.
- Ground floor residential units shall be set sufficiently above street/pedestrian level to provide adequate privacy.
- The towers shall have a square or rectangular plan.
- Each tower shall use predominantly only one other (solid) material besides glass.
- Habitable rooms to perimeter facades of towers
- Balcony or loggia to be at least 1.5m deep
- Generous windows with distinct proportions and clear architectural pattern

10.50. The proposals for Plot R7 & R8 meet the design code requirements.

Plot R7

10.51. The quality of the residential units is demonstrated through an appropriate mix of 1 to 3 bed units, all of which meet or exceed the GLA Housing SPG minimum standards. Efficiently designed layouts ensure no core serves more than 4 units and wherever possible circulation spaces are located in the centre of the plan to minimise circulation and wasted space.

10.52. Proposed uses are as set out by the outline permission and where ground floor residential units are proposed the potential privacy issue created is mitigated by raising the height of residential areas by 0.8-1.1m from Hunts Lane, with step-free access provided via a through lift provided at the entrance lobby which is at street level, whilst terraces fronting the riverside park are also elevated above the public footpath with dense hedging to provide a further degree of privacy.

10.53. The applicant has worked hard to minimise the impact of the basement parking ramp on the public realm between Block A/B and Block C, providing planting and seating outside the residential entrance to Block C and a continuous surface of clay bricks that visually promotes pedestrian priority. QRP suggested exploration of the best solution for access to the basement cycle store; the applicant has revised the arrangements to introduce an entrance on the side of Block C that will allow cyclists more practical and direct access to a lift down to the basement cycle store.

Plot R8

- 10.54. The proposed use is as set out by the outline permission with both blocks being residential use only and providing 116 units overall. Where ground floor residential units are proposed the potential privacy issue created is mitigated by raising the height of residential areas by 0.63-1.77m from street level with step-free access provided via a through lift provided at the entrance lobby which is at street level. Terraces fronting the riverside park are elevated above park level with 3m deep defensible planting to provide a further degree of privacy for residents.
- 10.55. The quality of the residential units is demonstrated through an appropriate mix of 1 to 3 bed units, all of which meet or exceed the GLA Housing SPG minimum standards. Efficiently designed layouts ensure most cores serve 2- 4 units, where a core serves 6 units (block C/D levels 1-3) the applicant has looked to improve the experience of the corridor by introducing natural light, and wherever possible circulation spaces are located in the centre of the plan to minimise circulation and wasted space. 90% of units have 2 or more aspects, the remainder, not north facing, are 1 bedroom units. In response to constraints of the parameter plans and wind conditions, private amenity space in blocks C and D is provided in the form of winter gardens, for 2/3 bed units, and as additional floorspace in the living areas of the 1 bed units. QRP commented that 'the indoor/outdoor quality of the apartments is particularly attractive, with glazed corners designed to open up in smaller flats, and winter gardens in larger ones.' Residents of these units also benefit from a 270m<sup>2</sup> roof terrace on the roof of block D and use of the communal space at entrance level.
- 10.56. More detail is provided on housing quality and compliance with London Plan policies in the Housing Quality section below. The proposed layout of the block and the internal floorplans are considered to comply with the 2012 'Building Line Requirement' parameter plan, the 2012 Design Code and Policies B.1, BN.5 and B.10 of the Local Plan.

#### Appearance and Materials

- 10.57. Policy B.1 and BN.10 of the Local Plan require careful consideration to be given to architectural style, materials, fenestration, colour, building orientation and overall appearance, and the 2012 Design Code identifies a number of design principles. The Design Code gave design guidance for the scheme as a whole. Particular emphasis was put on materiality, fenestration and the creation of a brick plinth.
- 10.58. The appearance of the Plot R7 and R8 buildings has been developed in response to designing apartments from the inside out; achieving generous ceiling height; maximising dual aspect units; and incorporating private amenity spaces. The external design has been developed to respond to the emerging context of the Strand East/ Sugar House Island Masterplan.
- 10.59. The architecture is informed by a common structural approach and variation in material and proportion to enable the blocks to mediate between different contextual relationships.
- 10.60. The material choices for Plots R7 and R8 comply with the Design Code which states that the Riverside Buildings 'shall appear as more transparent forms with larger proportion of void than solid', and that the Accent Towers 'should contain one predominant material (solid) other than glass, with additional materials used sparingly for secondary purposes or as accents'.
- 10.61. For Plot R7, the material approach is robust and of high quality with the predominant material being a warm buff brick, a natural and 'earthy' material chosen for its link to the industrial brick heritage of the site. The material palette has been kept intentionally limited, to brick, reconstituted stone and metal window

frames and balustrades, so as not to create an overly busy palette when viewed as part of the overall composition of the area. The reconstituted stone in a warm buff tone complements the brickwork and provides calm articulation to the tower element and expression to its 'crown.'

- 10.62. The Plot R8 scheme proposes use of high quality materials with the predominant material being brick with a variation in colour between the blocks. Thoughtful details such as decorative brickwork to the setback element of Block B and the concrete plinth and decorative overlay screens above entrance doors on Block A help to elevate these blocks and provide a layer of richness to the facades whilst remaining simple and unfussy. Work has gone into the architectural expression of the tower (block C) to ensure there's relief and depth in the façade with windows set back one and a half brick stretchers from the outer brick face. Open corners without a column member help to emphasise the verticality of the tower resulting in a visually more elegant proportion.
- 10.63. The proposed development for Plots R7 and R8 is considered to adopt an appearance that incorporates, subject to agreeing samples and specifications, high-quality materials, finishes and details that combine to achieve an architectural quality that is supported by officers and the Quality Review Panel. The application is considered to be in accordance with Policies BN.1 and BN.10 of the Local Plan.

#### Landscaping

- 10.64. Policy 7.5 of The London Plan requires the public realm to be secure, accessible, inclusive, connected, easy to understand and maintain, relate to local context, and incorporate the highest quality design, landscaping, planting, street furniture and surfaces. Local Plan Policies BN.1 and BN.3 require development to relate well to an area's natural and man-made landscape features and contribute to tree planting. Policy 7.2 of The London Plan and Policy BN.5 of the Local Plan make specific reference to the need for new development to achieve the highest standards of accessible and inclusive design.
- 10.65. The 2012 approved 'Characterisation of Open Spaces' parameter plan identifies the character of spaces to be included within Plots R7 and R8. The Plots are edged by Hunts Lane to the east, the Riverside Park to the west, Chimney Walk to the north, and the bus bridge to the south. A pedestrian entrance to the Riverside park is proposed between the mirroring block elements of R7 and R8.
- 10.66. The design of the surrounding public realm (Riverside Park, Hunts Lane, Sugar House Lane, shared surfaces) as well as the landscaping strategy has been prepared by Planit-IE and is submitted for approval as part of this RMA.
- 10.67. As the Riverside Park would be located between the River Lea Navigation and Plots R7 and R8, particular regard has been had to the provision of access to the park between both plots. The design of the public realm in this part of the site seeks to ensure that this space is perceived as a logical and active through-route to the public open space to the west of the site.
- 10.68. For Plot R7, the primary step free pedestrian routes between the Riverside Park and Hunts Lane, are Chimney walk to the north, between the tower and linear block adjacent to the car park entrance, and between R7 and R8 bookends.
- 10.69. A semi-enclosed courtyard between R8 buildings B and C/D forms the primary pedestrian route between the Riverside Park and Hunts Lane/The Hub, whilst also forming a public space for residents and visitors (of the wider Sugar House Island site) and the same time. The courtyard can be sub-divided into two spaces, with an easy-to-navigate path leading between Hunts Lane and the Riverside Park, and a small plaza area with seating elements and ornamental planting.

10.70. Since original submission of the Plot R8 RMA, the site wide public realm and infrastructure RMA surrounding the plot was approved (ref: 15/00384/REM) which included full landscaping details for all parts of the public realm falling within this development plot. The revised landscaping plan for R8 seeks approval for a number of minor amendments to the public realm, as set out in para. 6.19. These are considered to be acceptable.

Housing Mix and Tenure

10.71. **As discussed earlier in this report, Condition A15 (as amended by NMA) of the 2012 Planning Permission** defines the site wide dwelling mix. The table below provides a breakdown of the units provided under this submission of reserved matters in comparison to the overall approved mix and other RMAs submitted/approved to date.

Unit Type	18/00366/NMA (June 2018)	NEQ	Plot R1	Plot R2	Plot R3	Plot R4	Plot R5	Plot R6	Plot R7	Plot R8	Plot MU3	Plot MU4	Plot MU5	Total to date (all plots proposed/approved)
Studio	52 (4%)	4	10	23	15	0	0	0	0	0	0	0	0	52 (5%)
1-bed	423 (35%)	3	28	94	55	25	2	24	44	52	21	0	11	359 (33%)
2-bed	250 (21%)	1	38	18	12	32	36	30	13	20	0	0	27	227 (22%)
3-bed	390 (33%)	0	74	66	60	24	42	34	25	23	0	0	4	352 (33%)
4-bed	85 (7%)	0	11	10	14	6	10	15	0	21	0	0	0	85 (8%)
5-bed	0 (0%)	0	0	0	0	0	0	0	0	0	0	0	0	0 (0%)
<b>Total</b>	<b>1,200 (100%)</b>	<b>8</b>	<b>161</b>	<b>211</b>	<b>156</b>	<b>89</b>	<b>86</b>	<b>103</b>	<b>82</b>	<b>116</b>	<b>21</b>	<b>0</b>	<b>42</b>	<b>1,075 (90%)</b>

10.72. Within Plots R7 and R8 the proposed dwelling mix is detailed in the table below:

Unit size	Plot R7 (w/c units)	Percentage	Plot R8 (w/c units)	Percentage
1 bed	44	54%	52 (4)	45%
2 bed	13 (1)	16%	20 (4)	17%
3 bed	25 (10)	30%	23 (4)	20%
4 bed	0	0%	21	18%
<b>Total</b>	<b>82</b>	<b>100%</b>	<b>116</b>	<b>100%</b>
2+ beds	38	46%	64	55%
3+ beds	25	30%	44	38%

10.73. The outline planning permission requirement of 40% 3+ bedrooms is not met on an individual plot basis for R7 and R8 (though this is not a requirement), it is met on a site-wide basis. The lower proportions of 3+ bed units in these plots reflects the flatted block typology, with more larger family units located in the mews and perimeter blocks elsewhere within the wider scheme which have private and communal courtyards and roof terraces.

10.74. The approved site-wide mix requires 8% (without grant) or 11% (with grant) affordable housing to be provide on a 50:50 split between affordable rented and discounted market sale. Affordable rented accommodation is defined as 80% of market rents for 1 and 2 bedroom homes and 60% of market rent for 3 and 4



bedroom homes. Discounted market sale is defined in the S106 as 70% of open market value (with no rent payable on the unpurchased option) with eligibility based on household incomes of no more than £71,000 (gross) to purchase a 1 or 2 bedroom home and no more than £85,000 to purchase a 3 or 4 bedroom home. When the owner of a DMS home wishes to sell, the S106 Agreement requires them to offer the property to Newham Council at 70% of the market value, or, if sold on the open market, pay 30% of the value achieved to Newham Council for its investment in additional affordable housing.

- 10.75. The s106 Agreement also requires a financial review of the development on the completion of 400, 800 and 1,200 units to determine whether additional affordable housing financial contributions are made. Units have been designed to be tenure-blind and as such affordable rented, Discount Market Sale and/or private dwellings cannot be differentiated.
- 10.76. No affordable housing units are proposed in Plots R7 and R8. The applicant has confirmed the reason for this is because there is a desire (and requirement in S106) for early delivery of the affordable housing units. Plots R7 and R8 would be delivered as later phases.
- 10.77. With regard to the Discount Market Sale units, the applicant has confirmed that these will be delivered across the site, in either DMS, private or mixed affordable cores. The final details of location of the discount market sale units and affordable rented units for the whole scheme will be submitted for approval to demonstrate compliance with the Affordable Housing S106 clauses. The table below provides the cumulative affordable housing totals:

	Discount Market Sale Homes			Affordable Rent Homes			TOTAL		
	included to date*	S106 (no grant)	future	included to date*	S106 (no grant)	future	included to date*	S106 (no grant)	future
Studio	0	0	0	0	0	0	0	0	0
1 bed	3	19	16	7	19	12	10	38	28
2 bed	2	10	8	4	10	6	6	20	14
3 bed	4	17	13	14	19	5	18	36	18
4 bed	1	2	1	0	0	0	1	2	1
5 bed	0	0	0	0	0	0	0	0	0
<b>Total</b>	<b>10</b>	<b>48</b>	<b>38</b>	<b>25</b>	<b>48</b>	<b>23</b>	<b>35</b>	<b>96</b>	<b>61</b>

- 10.78. The application is considered to be in accordance with the 2012 Planning Permission in this regard.

#### Housing Quality

- 10.79. Policies 3.5 and 3.6 of The London Plan and Policy BN.4 of the Local Plan require housing developments to be of the highest quality internally and externally, referring to the requirement to meet the minimum space standards adopted in the National Described Space Standards – Technical Requirements and the Mayor of London’s Housing SPG and ensure children have safe access to good quality, secure, and stimulating play and informal recreation.
- 10.80. Condition C9 requires that Lifetime Homes Standards (or any standard amending or replacing it) should be adopted ‘where physically and financially practical’. Lifetime Homes Standards no longer exist, so Building Regulations Part M4 is used as a substitute in accordance with London Plan Policy 3.8. As set out in Policy 3.8 of The London Plan and Policy BN5 of the Local Plan 90% of new building homes should meet Part M4(2) of the Building Regulations, with the remaining 10% meeting Part M4(3).
- 10.81. The Housing SPG states that a minimum of 5m<sup>2</sup> of private outdoor space should be provided for 1-2 person dwellings and an extra 1m<sup>2</sup> should be provided for

each additional occupant. The SPG also advises that where site constraints make it impossible to provide private open space for all dwellings, a proportion of dwellings may instead be provided with additional internal living space equivalent to the area of the private open space requirement.

#### Plot R7

- 10.82. All homes within Plot R7 would meet or exceed the minimum internal space standards adopted in the Mayors Housing SPG.
- 10.83. 13% (11no. out of 82no.) units in Plot R7 would be wheelchair user dwellings in accordance with Building Regulations Part M4(3), comprising 1no. x 2b/3p dwellings and 10no. x 3b/4p dwellings, which overall would provide an acceptable mix site wide.
- 10.84. The remaining 87% (70no.) dwellings in Plot R7 would meet Building Regulations Part M4(2) 'accessible and adaptable dwellings'.
- 10.85. 93% (76no.) of homes in Plot R7 would be dual aspect. No single aspect north facing units are being proposed. The 6no. single aspect units are 1 bed units, and would face west on to the Riverside Park. Each core would only serve a maximum of two to four units.
- 10.86. All proposed dwellings would also exceed the minimum floor to ceiling height (2.5m), achieving a minimum of 2.63m.
- 10.87. All residential units within Plot R7 would benefit from their own internal and/or external private amenity space in the form of terraces, balconies or additional internal space, in accordance with the Mayor's Housing SPG. As Plot R7 does not benefit from communal courtyards or roof terraces unlike other plots of the development, the Applicant has sought to maximise the provision of private amenity space.
- 10.88. It is noted that 6no. units, located on Floors 1-3 of Block C, are 3-bed 4 person dwellings which require a minimum of 7 sqm of private amenity space. To ensure a consistent architecture and elevation of this building element, the inset balconies on these floors remain at 5 sqm as per the all the floors above. In order to meet the minimum standards, the remaining 2 sqm of private amenity space is provided in the form of additional internalised amenity. All rooms within these 6 apartments exceed the Nationally Described Space Standards. It is also noted that Riverside Park is adjacent to the Plot, providing significant public open space.

#### Plot R8

- 10.89. All homes within Plot R8 would meet or exceed the minimum internal space standards adopted in the Mayor's Housing SPG from between 8% and 35%. It is noted however, that the single bedroom in 24 dwellings have a width of between 2.0 – 2.1m, which is slightly less than required by Para. 10(c) of the Nationally Described Space Standards, which requires single bedrooms to be at least 7.5sqm in size with a width of 2.15m. All of these single bedrooms are 8.1sqm in size (0.6sqm above the required standard). The specific units also comfortably exceed the overall space standard by between 13.4 to 18.6sqm. and as such on balance, this is considered to be acceptable.
- 10.90. 10% (12no. out of 116no.) units in Plot R8 would be wheelchair user dwellings in accordance with Building Regulations Part M4(3), comprising 4no. x 1b/2p dwellings, 4no. x 2b/3p dwellings, and 4no. 3b/4p dwellings, which overall would provide an acceptable mix site wide.
- 10.91. The remaining 90% (105no.) dwellings in Plot R8 would meet Building Regulations Part M4(2) 'accessible and adaptable dwellings'.

10.92. The table below shows an overview of the wheelchair adaptable units across the outline element of the site (proposed and approved), and demonstrates that of the proposed and approved plots, 10% of units across the site would be achieved.

Unit Type	NEQ	Plot R1	Plot R2	Plot R3	Plot R4	Plot R5	Plot R6	Plot R7	Plot R8	Plot MU3	Plot MU4	Plot MU5	Total
1-bed	0	3	13	7	0	0	6	0	4	3	-	4	40
2-bed	0	3	0	1	4	0	1	1	4	0	-	0	14
3-bed	0	10	8	6	0	10	3	10	4	0	-	0	51
4-bed	0	0	0	2	0	0	0	0	0	0	-	0	2
<b>Total</b>	<b>0</b>	<b>16</b>	<b>21</b>	<b>16</b>	<b>4</b>	<b>10</b>	<b>10</b>	<b>11</b>	<b>12</b>	<b>3</b>	<b>-</b>	<b>4</b>	<b>107 (out of 1,072 units) = 10%</b>

10.93. 87% of homes in Plot R8 (104/116 units) would be dual aspect, which is supported. No single aspect north-facing units are proposed. The single aspect units are 1 bed units and would face west on to the Riverside Park. Each core would serve a maximum of 2 to 6 dwellings per floor.

10.94. All proposed dwellings would also exceed the minimum floor to ceiling height (2.5m) and would achieve 2.73m.

10.95. All residential units would benefit from access to their own private external amenity space provided in the form of ground and rooftop terraces, loggias and balconies designed to meet or exceed the amenity space size standard. A roof terrace is also proposed for the residents of Buildings C and D which is located on the roof of Building D and accessed via a corridor from the fourth floor of the accent tower.

10.96. The extent to which all homes would meet or exceed the minimum space standards; the incorporation of private amenity space to all homes in Plots R7 and R8 would result in a high standard of living accommodation. The proposal is considered to meet the requirements of condition C8 (Housing Quality).

#### Children's Play space

10.97. Children's play is expected to take place formally within the Riverside Park and The Hub, and informally within Chimney Walk and the shared spaces within the wider site. The landscape architects Planet-IE have advocated this approach on the basis of implemented precedents.

10.98. Unlike the perimeter and mews block typologies of some of the other plots, the linear blocks of R7 and R8 do not have residential courtyards. However, given the proximity to the large Riverside Park, Chimney Walk and The Hub, this is considered to be acceptable.

10.99. The main area of concentration of play in the Riverside Park is adjacent to Blocks A/B of Plot R8 and links well to the activity area of The Hub. Block play forms, areas of unusual seating, informal natural play, sensory journeys and a series of play nets/hammocks, etc. are embedded into approved design of the Public Realm RMA. The Riverside Park provides approximately 2,470sqm of play for all ages 0-11 and 12+.

10.100. The Park would contain integrated play opportunities with non-prescriptive features such a sensory spaces and planting that are more open to interpretation, this is a play type that is lacking within the 0-11 years 400m walking distance. Throughout the park a trim trail is proposed that will have combined exercise and play items that are open to use by all age ranges.

- 10.101. A set of amphitheatre seats and an open event space adjacent to the river and close to the main play area would also provide an open hard space with the opportunity to hold activities tailored towards children against a backdrop of the river. These steps and low walls elsewhere in the park would provide integrated play opportunities. As the park contains a wide range of sensory and native planting there is also an opportunity for wayfinding, signage and an educational trail that highlights some of the area's history, flora and fauna.
- 10.102. The Hub, as a large open space, would provide opportunities for play and amenity with an emphasis on creative interpretation rather than traditional fenced-off play equipment. There would be approx. 470 sqm of play for all ages 0-11 and 12+, and would be circa 2-3 mins walk from Plot R8; 3-4 mins walk from Plot R7)
- 10.103. A shallow water feature, possibly with playful jets, would be in use during warm days that supervised children of all ages will be able to use and interact with. The paving material is textured and slip resistant to provide a safe environment. The large open flexible space in the centre of the hub that is overlooked by the active edges of the surrounding buildings can be used for a multitude of traditional play activities and where children are free to take ownership of the space with play.
- 10.104. Play and seating blocks as seen throughout the character areas are placed in areas where children can play and adults can supervise. A soft flexible lawn space which combines the needs of a play safety surface is located in the southern tip of the Hub where younger child can run around and older children play games such as informal ball games.
- Chimney Walk
- 10.105. The Chimney Walk is approx. 260sqm and would contains designed and integrated play opportunities within the hard and soft landscaping of the area. Play and seating blocks are integrated in the approved design and seen throughout Chimney Walk in areas where children can play and adults can supervise.
- 10.106. The quantum of open spaces and play space for the whole scheme is set out in Condition A13 of the 2012 permission and the public realm and infrastructure RMA. This plot contributes the appropriate amount towards open space and play space provision in accordance with the outline planning permission.
- 10.107. The applications for Plot R7 and R8 are considered to be in accordance with Policies 3.5 and 3.6 of The London Plan and the Housing SPG and Policy BN.4 of the Local Plan, and it is recommended that condition C8 of the 2012 permission is partially discharged (insofar as it relates to Plot R7 and R8).

#### Daylight and Sunlight

- 10.108. Policies 7.6 and 7.7 of The London Plan and Policies BN.1, BN.4 and BN.10 of the Local Plan require new development to demonstrate that they will not create unacceptable daylight, sunlight and overshadowing impacts.
- 10.109. The 2012 permission includes a planning condition (C11) which requires the submission and approval of a Daylight, Sunlight and Overshadowing Assessment for each development plot. The assessment is required to ensure the impact of each residential development plot on the living conditions (internal rooms and external amenity space) of future residents is properly considered and addressed.

10.110. The applicant has submitted a Daylight and Sunlight Assessment for each plot which considers the extent to which the daylight and sunlight levels experienced within habitable rooms and the residential courtyards complies with the BRE guidance 'Site Layout Planning for Daylight and Sunlight – A Guide to Good Practice'. The scope of the assessment takes into account the height and massing of development either consented or proposed on adjacent plots and includes:

- a) Daylight levels within habitable rooms using Average Daylight Factor (ADF) criterion which quantifies the level of daylight received in a room taking in account colours (reflectance) of walls, floors and ceilings;
- b) Distribution of natural light within habitable rooms using No Sky Line (NSL) criterion which estimates the percentage of the working plane that receives direct sunlight;
- c) Access to direct sunlight of each living area on each façade using the Annual Probable Sunlight Hours (APSH) criterion which quantifies light that falls directly from the sun on a façade.
- d) Access to sunlight to the external courtyards and roof terraces, assessed by calculating the amount of time where the spaces are overshadowed on 21<sup>st</sup> March.

10.111. The Daylight and Sunlight Assessment takes into account the changes in ground levels and building heights when compared to that tested by the 2012 permission.

Plot R7

Daylight

10.112. The daylight assessment for Plot R7 concludes that overall 85% of habitable rooms would meet or exceed both the recommended ADF levels and NSL criterion. The daylight assessment demonstrates that 77% of living areas/kitchens and 91% of bedrooms would meet or exceed both the recommended ADF and NSL levels. Of those rooms that do not meet the minimum targets, at least 60% of them reflect marginal incompliances (0.1-0.2%).

10.113. All residential units in Plot R7 would have at least one (and in the majority of cases, more) habitable rooms that meet the recommended minimum ADF and NSL levels. It is concluded that the proposed scheme would provide adequate levels of daylight and sunlight.

Sunlight

10.114. The sunlight assessment for Plot R7 concludes that 80% of living rooms facing within 90 degrees of due south would receive or exceed the amount of Annual Probable Sunlight Hours and winter sunlight target recommended by the guidance. The development is therefore expected to perform well with regards to sunlight availability.

10.115. The south facing living rooms that would not achieve the recommended APSH are in most cases obstructed by balconies, which at the same time would provide shading to reduce the risk of overheating, and would also provide valuable private amenity space for residents.

10.116. The BRE guidance recommends that a minimum of 50% of amenity space receives a minimum of 2 hours of sunlight on 21 March. The assessment demonstrates that 100% of each of the four private amenity areas of Plot R7 (at ground floor level facing the Riverside Park) meet that target.

10.117. PPDT's environmental consultants have verified the methodology and results of the Daylight and Sunlight Assessment.

10.118. Officers consider that given the fixed parameters, the proposals have sought to achieve the optimum solution in terms of the detailed design quality, which overall results in a high standard of living accommodation. The proposal is therefore considered to be acceptable within the context of Policies 7.6 and 7.7 of The London Plan and Policies BN.1, BN.4 and BN.10 of the Local Plan and it is recommended that condition C11 of the 2012 permission is partially (insofar as it relates to Plot R7) discharged.

Plot R8

Daylight

10.119. The daylight assessment for Plot R8 concludes that overall 76.4% of habitable rooms would meet or exceed both the recommended ADF levels and NSL criterion. The daylight assessment demonstrates that 67% of living areas/kitchens and 83% of bedrooms would meet or exceed both the recommended ADF and NSL levels.

10.120. Officers have explored the reasons for 33% of the living rooms that would not meet the minimum ADF and NSL criteria, and all of them have balconies and are deep living/kitchen/dining rooms typologies.

10.121. The Average Daylight Factor is the ratio of light measured inside a space to the light measured externally. Daylight Factor is tested on the working plane of a grid of points through the space (i.e. a 0.5m by 0.5m grid within each room). The average of all these points is then calculated and reported as ADF, and as such, the results are dependent on a unit's layout and the width and depth of respective rooms. The daylight factor is higher near the light source (i.e. a window), thus exceeding the recommended targets, and areas in deep rooms furthest away from the light source can fall to zero, thereby meaning that overall the room might fail to meet what is suggested by guidance.

10.122. There would be 14 units (12% of units in the scheme) that would have no habitable rooms that meet the recommended minimum ADF levels. Of the 14 units, 11 units would be only marginally below the ADF targets (i.e. with a room or rooms 0.1-0.2 below the ADF targets). 12 are 1 bed flats, which do not meet the targets, due to the balconies and deep living room, and have been designed to enable the bedroom to also face the Riverside park. There is also a 1 x 2 bed unit at ground floor level of block A; and a 1 x 3 bed unit at first floor level of Block D that would not meet the recommended targets. The ground floor 2 bed unit in Block A is a dual aspect unit with aspects over the Riverside Park and Hunts Lane. Due to the depth of living room and inset balcony, there is not enough daylight penetration to the back of the room to meet the ADF targets. The 3 bed unit in Block D has a large dual aspect living room with east and north-west aspects but due to the inset balcony, and location of the neighbouring block, there is not enough daylight penetration to the back of the room to meet the ADF targets.

10.123. The applicant has sought to maximise the daylight to the units, and where possible, locating the majority of the living rooms with the open aspect of the Riverside Park. However, given the balance between providing good daylighting, generous balconies, and preventing overheating, the performance of some of the units is not ideal.

Sunlight

10.124. The favourable south west orientation of the blocks have been used to their best potential, with the location of the majority of living rooms in the south west facing elevations with views over the Riverside Park (all living areas except for 2 per floor on the taller part of Block D). Good sunlighting has been achieved while balancing out the risk of overheating by utilising the balconies as shading elements. The sunlight assessment for Plot R8 concludes that 93% of living

rooms would receive or exceed the amount of Annual Probable Sunlight Hours and winter sunlight target recommended by the guidance. The development is therefore expected to perform particularly well with regards to sunlight availability.

- 10.125. South facing living rooms that would not achieve the recommended APSH are in most cases obstructed by balconies, which at the same time provide shading to reduce the risk of overheating, and also provide private amenity space for the residents.
- 10.126. The BRE guidance recommends that a minimum of 50% of amenity space receives a minimum of 2 hours of sunlight on 21 March. The assessment demonstrates that 100% of the communal spaces located to the West of Blocks B and D would receive the recommended minimum of 2 hours of sunlight on 21 March. 98% of the Block D roof terrace would also receive sufficient sunlight according to the BRE recommendations.
- 10.127. PPDT's environmental consultants have verified the methodology and results of the Daylight and Sunlight Assessment. Officers consider that given the fixed parameters, the proposals have sought to achieve the optimum solution in terms of the detailed design quality, which results in a high standard of living accommodation.
- 10.128. The proposal is therefore considered to be acceptable within the context of Policies 7.6 and 7.7 of The London Plan and Policies BN.1, BN.4 and BN.10 of the Local Plan and it is recommended that condition C11 of the 2012 permission is partially (insofar as it relates to Plot R2) discharged.

Daylight/sunlight impact on surrounding residential amenity

- 10.129. The closest existing residential properties to Plots R7 and R8 are Bow River Village and Island House, which are more than 70m away. It is considered that there would not be an unacceptable impact on these properties from the development. The assessments for these and all other Strand East plots have accounted for the presence of the surrounding proposed buildings within the Strand East Masterplan. The minor amendments to the massing of Plot R7 and Plot R8 are not anticipated to have any impact on any other plot within the Strand East development, or any existing residential properties.

Transport

- 10.130. The 2012 permission includes planning conditions (A16, A17, A18 and A19) which set the maximum or minimum number of car, motorcycle and cycle parking spaces to be allocated to each land use type, including associated provision of Blue Badge and electric vehicle parking spaces and a planning condition (C15) which requires the submission of a Parking Management Plan development plot. In support of the reserved matters application and to discharge condition C15 the applicant has submitted a Parking Management Plan.
- 10.131. The applications for Plot R7 and R8 propose a shared underground car park with 47 car parking spaces (25 standard spaces and 22 blue badge holder spaces), 11 of which would have electric charging points, and motorcycle spaces. 316 cycle parking spaces are also located within the basement across the two plots.

Parking Spaces – Condition Description	Plot R7 Conditioned provision	<b>Plot R7 Provided</b>	Plot R8 Conditioned provision	<b>Plot R8 Provided</b>	Total R7/R8 provided
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Car park spaces (max 0.85 spaces per dwelling)	70 spaces (0.85 per dwelling max)	<b>11 (0.13 spaces per dwelling)</b>	98 spaces (0.85 spaces per dwelling)	<b>36 (0.37 spaces per dwelling)</b>	47 (0.24 spaces per dwelling)
Blue badge (min 10% of spaces)	1	<b>10</b>	4	<b>12</b>	22
EVCP (min 20% of spaces)	2	<b>2</b>	8	<b>9</b>	11
Motorcycle (min 1 space per 10 units)	9	<b>15</b>	11	<b>11</b>	16
Bicycle (min 1 space per dwelling)	82	<b>136</b>	116	<b>180</b>	216

10.132. The table demonstrates that if considered separately, Plot R7 and R8 would provide sufficient parking per plot. The table also demonstrates that car parking has been minimised compared to the maximum permitted by the outline permission. This is in line with London Plan and draft London Plan policies aimed at reducing car parking (0.24 spaces per unit are proposed compared with the 0.85 maximum in the permission). Blue badge spaces would be provided on a nearly 1 space per part M4(3) wheelchair unit (0.96 space per unit), rather than 10% of car parking spaces, which results in 47% of the car parking in Plot R7/R8 being allocated to disabled occupiers.

10.133. The following table demonstrates that the proposed number of car parking spaces allocated to Plots R7 and R8, when considered independently and in aggregate with other approved plots, complies with the relevant planning condition A16:

Use	Max No. of Spaces	Public Realm	MU2	R4	MU5	R6	MU3	R1	R2	R3	R5	R7/R8	Remaining
Residential (C3)	0.85 spaces per dwelling or up to 1013 spaces in total of which 10% will be for blue badge holders	0	23 (2)	0	0	80 (13)	18 (2)	120 (16)	92 (21)	94 (20)	47 (28)	47 (22)	492 with need to account for 0.85 max.
Employment (B1)	1 space per 1,000sqm, or up to 39 spaces in total, of which 10% will be for blue badge holders	0	27 (3)	0	0	0	0	0	0	0	0	0	12 with need to accord with 1 space per 1,000sqm max
Retail (A1/A2/A3/A4)	Up to 27 at-grade spaces in total 10% of which will be for blue badge holders	27 (3)	0	0	0	0	0	0	0	0	0	0	0
Hotel (C1)	Up to 160 spaces 10% of which will be for blue badge holders	0	0	0	0	0	98 (11)	0	0	0	0	0	62
Community Use (B1/D2)	Up to 42 spaces of which 14 will be for blue badge holders	27 (14)	0	0	0	0	0	0	0	0	0	0	15
<b>Total</b>	<b>Up to 1,281 spaces</b>	<b>54 (15)</b>	<b>50 (5)</b>	<b>0 (0)</b>	<b>0 (0)</b>	<b>80 (13)</b>	<b>116 (13)</b>	<b>120 (16)</b>	<b>92 (21)</b>	<b>94 (20)</b>	<b>47 (28)</b>	<b>47 (22)</b>	<b>581</b>

10.134. Bicycle parking spaces for 316 bicycles for residents of Plot R7 and R8 would be provided in dedicated storage rooms located throughout the car park with would be accessed via the lifts. This exceeds the minimum of 1 space per unit required by Condition A19, and The London Plan and draft London Plan cycle parking standards.



Use	No. of Spaces	Public Realm	MU2	R4	MU5	R6	MU3	R1	R2	R3	R5	R7/R8	Required
C3 - Residential	A minimum of 1 space per unit, or 1192 spaces	188	0	95	84	219	22	284	358	312	188	316	0
B1 - Employment	A minimum of 1 space per 125 sqm, or 157 spaces	30	300	0	0	0	62	0	0	0	0	0	0
A-Class and D-Class combined	A minimum of 50 spaces	50	0	0	0	0	52	0	0	0	0	0	0
Total	1,399	268	300	95	84	206	136	284	358	312	188	316	0

10.135. The commercial use within Plot R7 would utilise the cycle parking that would be provided within the public realm. 54 cycle parking spaces would be provided in the form of Sheffield style cycle stands located within the public realm in the immediate vicinity of the commercial uses.

10.136. 26 motorcycle spaces are proposed within the basement car park. This equates to 0.13 spaces per dwelling when accounting for the 198 dwellings in Plot R7 and R8 combined. This provision is considered appropriate given the approximate 1 space per 10 dwellings conditioned to serve the development.

Use	Min No. of Spaces	Public Realm	MU2	R4	MU5	R6	MU3	R1	R2	R3	R5	R7/R8	Required
C3 - Residential	119	0	10	0	0	15	3	23	21	17	28	26	0
B1 - Employment	39	10	29	0	0	0	0	0	0	0	0	0	0
A1 / A2 - Retail	27	27	0	0	0	0	0	0	0	0	0	0	0
Other uses combined	19	9	0	0	0	0	10	0	0	0	0	0	0
Total	204	46	39	0	0	15	13	23	21	17	28	26	0

### Vehicular Access

10.137. Access to the R7/R8 car park is proposed via a one-way ramp from Hunts Lane, the entry into which would be controlled with an automatic 'speedgate' at the building edge. To minimise waiting of vehicles on Hunts Lane the gate would be controlled by a remote control key fob or automatic sensor that detects the approach of a resident's vehicle and opens the gate. The ramp to the basement would be controlled via a separate traffic/signal barrier with entering vehicles given entry down to the car park by default to limit queuing in the public realm. Sufficient space within the entrance to the plot is provided to enable two vehicles to wait off Hunts Lane should a vehicle be exiting the car park.

10.138. With regard to cycle parking access, due to the proposed gradient, length and height clearance of the ramp, there will be restricted access, so cyclists would access the cycle storage by means of the lift at ground floor. Residents of Plot R8 would access the basement from their lobbies, while Plot R7 residents would access the basement from a double entry lift with direct cycle access from the public realm adjacent to the ramp.

### Deliveries and Servicing

10.139. The Plot R7 commercial units would be located at ground floor of Block C. There would be formal loading and servicing bays located in the vicinity of the commercial unit, informal loading opportunities within the private streets and kerbside loading along Sugar House Lane, to ensure that the commercial unit(s) could be adequately serviced.

10.140. The on-street loading bays would operate 7 days a week, subject to a maximum of 30 minutes use and could be used by commercial occupiers or residents of Strand East. Informal loading could take place within the privately-owned streets and mews for a period of 20 minutes, with special permits available to facilitate longer loading/servicing durations if necessary.

10.141. The car parking management plan is considered to be acceptable, subject to a condition securing the final allocation of car parking spaces. Condition C15 is recommended to be partially discharged (in so far as it relates to Plots R7 and R8).

#### Waste Management

10.142. Policy S.6 of the Local Plan requires new development to demonstrate that adequate provision has been made for domestic and commercial waste storage and collection.

10.143. The 2012 permission includes a planning condition (C36) which requires the submission and approval of a Waste Management Strategy for each development plot. In support of the reserved matters application and to discharge condition C15 the applicant has submitted an Operational Waste Management Plan.

10.144. The application proposes the use of a site wide Underground Refuse System (URS), which has been approved for all the plots with Reserved Matters Approval, and agreed with the London Borough of Newham's Waste Disposal and Reduction Manager. The use of an underground system has several benefits; it improves the ground floor design of buildings as refuse stores do not need to be provided; it can be sensitively located as street furniture within the public realm; and its location means large refuse vehicles can collect efficiently.

10.145. Underground Refuse System (URS) bins are proposed close to the residential entrances of Plots R7 and R8, along with a bulky waste store in each of the plots. A commercial bin store is proposed in the base of the Plot R7 tower next to the commercial unit in Plot R7. The application is considered to comply with Policy S.6 of the Local Plan and it is recommended that condition C36 of the 2012 permission is partially (insofar as it relates to Plots R7 and R8) be discharged.

#### Sustainability

10.146. Policy 5.2 of The London Plan and Policy S.2 of the Local Plan require development to minimise carbon dioxide emissions by reducing energy; supplying energy efficiently; and meeting remaining energy requirements through renewable energy sources where viable in order to achieve a 40% improvement on the 2010 Building Regulations Target Emission Rate between 2015-2016 and zero carbon from 2016. Policy 5.6 of The London Plan and Policy S3 of the Local Plan require major development to maximise the opportunities to connect to existing or proposed decentralised energy networks.

10.147. The S106 Agreement relating to the 2012 permission requires:

- a) Approval of the Energy Study before any reserved matters are approved;
- b) The Energy Study to consider connection to a district heating network, with details provided of the connection timing and reasonable endeavours to achieve and maintain the connection;
- c) If the Energy Study concludes that connection to a district heating network is not preferred, then an alternative strategy shall be developed, implemented and maintained which shall achieve a reduction of at least 25% in carbon dioxide emissions.

- 10.148. To discharge the S106 obligation the applicant has submitted an Energy Study which identifies the following options as having the potential to achieve a reduction of at least 25% in carbon dioxide emissions measured against Part L of the Building Regulations 2010:
- a) Connection to the Cofely East London Energy (CELE) district heating network;
  - b) A site-wide energy network through the provision of an on-site energy centre; and
  - c) A community heat pump approach.
- 10.149. The submission describes the Energy Study as being prepared to take into account and balance the environmental benefits; economic benefits for end-users; cost of installation; and impact on development viability. The Energy Study shows that connection to a district energy network would achieve the 25% reduction in carbon dioxide emissions, with the de-carbonisation opportunities of a network connection offering “carbon resilience” if the use of biomass as a fuel increases. The Energy Study also shows that a district energy approach can offer an economic benefit to home owners if the services provided and customer charging strategy are carefully designed.
- 10.150. The Energy Study concludes that connection to ENGIE district heating network is the preferred approach to supply base energy to the development, noting that the viability and overall suitability of this is subject to the terms and conditions of the final contract proposal. Compliance with the CO2 reduction target is also dependent on Cofely achieving a sufficiently low CO2 emissions factor.
- 10.151. In the event that the connection to the ENGIE district heating network is not feasible (it is noted that it does not currently serve the site), the Energy Study confirms that a site-wide energy network would be implemented, including the provision of an on-site energy centre, the location and design of which would be determined at a later date. The applicant has confirmed that the detailed design of each plot (residential and commercial) assume connection to the ENGIE district heating network.
- 10.152. Temporary planning permission was granted earlier this year for a boiler house at Plot MU4 (ref: 16/00644/DEM) to provide heat for the first phase of the Strand East development, until a site wide connection is made to a district heating network.
- 10.153. The 2012 Planning Permission includes a planning condition (C38) which requires all residential units to achieve a Code for Sustainable Homes Level 4 (or the equivalent at the time of submission). The applicant has submitted a Code for Sustainable Homes Pre-Assessment for Plots R7 and R8 which confirms that the scheme would achieve this performance level. The Code for Sustainable Homes (CfSH) was abolished in March 2015. The new national technical standards replace CfSH and are set at the equivalent of code level 4. The approved documents for energy, water and sustainability of the former CfSH are now regulated under Building Regulations Part L.
- 10.154. The 2012 permission includes a planning condition (B27) which requires all residential units to meet BREEAM standard ‘very good’. The applicant has submitted a BREEAM pre-assessment tracker which confirms that the scheme would achieve this performance level.
- 10.155. The application is thereby considered to comply with the 2012 permission and Policy 5.6 of The London Plan and Policy S.3 of the Local Plan.

#### Overheating

- 10.156. As required by condition C11 of the OPP, Plots R7 and R8 have been assessed regarding their overheating performance. Mitigation measures are proposed in addition to the provision of glazing with a G-value of 0.7 and openable window openings, of solar glass and opaque internal blinds.

- 10.157. PPDT's Environmental consultant has confirmed the proposals are acceptable in this regard, but recommend a condition relating to minimising heat gain. The proposals are in accordance with London Plan Policy 5.9 which requires major development to reduce overheating.

#### Wind Mitigation

- 10.158. The wind assessments for Plot R7 and R8 demonstrate that, with designed-in mitigation measures such as balconies with solid screens, wind conditions within the site and surrounding area would be suitable, in terms of both pedestrian comfort and safety, for their intended usage throughout the year.

## **11. HUMAN RIGHTS & EQUALITIES IMPLICATIONS**

- 11.1 Members should take account of the provisions of the Human Rights Act 1998 as they relate to the application and the conflicting interests of the Applicants and any third party opposing the application in reaching their decisions. The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report. In particular, Article 6 (1), of the European Convention on Human Rights in relation civil rights and a fair hearing; Article 8 of the ECHR in relation to the right to respect for private and family life and Article 1 Protocol 1 of the ECHR in relation to the protection of property have all been taken into account.
- 11.2 In addition the Equality Act 2010 provides protection from discrimination in respect of certain protected characteristics namely: age, disability, gender reassignment, pregnancy and maternity, race, religion, or beliefs and sex and sexual orientation. It places the Local Planning Authority under a legal duty to have due regard to the advancement of equality in the exercise of its powers including planning powers. Officers have taken this into account in the assessment of the application and Members must be mindful of this duty inter alia when determining all planning applications. In particular Members must pay due regard to the need to:
- a) Eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Equality Act;
  - b) Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and;
  - c) Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.
- 11.3 Officers are satisfied that the application material and Officers' assessment has taken into account these issues, with particular regard to the creation of a high quality city and providing homes for all.

## **12 CONCLUSION**

- 12.1 The proposed non-material amendments submitted under application references 17/00468/NMA and 18/00366/NMA are non-material in nature and not considered to materially alter the 2012 permission or create any new or different significant environmental impacts.
- 12.2 The proposed reserved matters for Plots R7 and R8 – layout, scale, appearance and landscaping – under application references 17/00369/REM and 15/00384/REM are considered to result in a scheme which is compliant with the parameter plans (as amended by the NMA), Design Code and planning conditions that form part of the 2012 outline planning permission, and the relevant London Plan and Local Plan policies. The comments received have been

considered in detail and addressed in this report and the scheme would not have any significant adverse impacts and would not result in any new or different significant environmental effects from those set out in the 2012 Environmental Statement.

- 12.3 It is recommended that the applications for non-material amendments, reserved matters and approval of details are GRANTED in accordance with section 2.0 of the report and subject to the following conditions.

### **13 PLANNING CONDITIONS - PLOT R7**

1. The development shall be carried out in accordance with the following details and plan numbers:

(to be confirmed and inserted prior to the decision notice being issued)

and the description of development contained in the application and any other plans, drawings, documents, details, schemes or strategies which have been approved by the Local Planning Authority pursuant to these conditions.

Reason: To ensure that all works are properly implemented and retained, in accordance with Policies SA4.2, BN.4, BN.5 and BN.10 of the Local Plan 2015.

2. The development shall be constructed and occupied only in accordance with the following reports:
- a. Parking Management Plan prepared by Peter Brett Associates dated August 2017
  - b. Waste Management Strategy Final prepared by Peter Brett Associates dated August 2017
  - c. Overheating Study by Mott MacDonald dated August 2017
  - d. BREEAM Pre-assessment prepared by Mott MacDonald dated August 2017
  - e. Code for Sustainable Homes Pre-assessment prepared by Mott MacDonald dated August 2017

Reason: In the interests of highway management and safety and promoting sustainable travel patterns, energy efficiency and waste management, in accordance with Policies T.7 and S.6 of the Local Plan 2015.

3. Prior to commencement of above ground works, a Site Waste Management Plan (SWMP) relating to the requirements of Condition C36(a) of planning permission 12/00336/LTGOUT/LBNM shall be submitted to and approved in writing by the Local Planning Authority. The SWMP shall be compliant with the revoked SWMP regulations 2008. The development shall be constructed in accordance with the approved SWMP.

Reason: In the interests of minimising the production of waste, in accordance with Policy S.6 of the Local Plan 2015.

4. The basement level hereby permitted shall not be used until details of the car parking allocation have been submitted to and approved in writing by the Local Planning Authority. The car parking allocation shall be implemented prior to occupation, and maintained thereafter only in accordance with the approved details and to the satisfaction of the Local Planning Authority.

Reason: In the interests of estate management and promoting sustainable travel behaviour.

5. Prior to first occupation of the development hereby permitted, details of how the mitigation measures required by the overheating study have been incorporated into the development, shall have first been submitted to and approved in writing

by the Local Planning Authority. The development shall be implemented and retained thereafter in accordance with the approved details.

Reason: To prevent overheating of the residential units, in accordance with Policy S.7 of the Local Plan 2015.

## **INFORMATIVES**

### **1. Positive and Proactive Statement**

In accordance with the National Planning Policy Framework and with Article 35 of the Town and Country Planning (Development Management Procedure) (England) Order 2015 (as amended), the following statement explains how the LLDC as Local Planning Authority has worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with this planning application:

Following submission of the planning application to LLDC, the local planning authority continued to work with the applicant in a positive and proactive manner. The planning application complies with planning policy as stated above and was determined in a timely manner.

The applicant has been kept informed of the progress of the application and has been given the opportunity to respond to and address any problems arising.

## **14 PLANNING CONDITIONS - PLOT R8**

1. The development shall be carried out in accordance with the following details and plan numbers:

(to be confirmed and inserted prior to the decision notice being issued)

and the description of development contained in the application and any other plans, drawings, documents, details, schemes or strategies which have been approved by the Local Planning Authority pursuant to these conditions.

Reason: To ensure that all works are properly implemented and retained, in accordance with Policies SA4.2, BN.4, BN.5 and BN.10 of the Local Plan 2015.

2. The development shall be constructed and occupied only in accordance with the following reports:
  - a. Parking Management Plan prepared by Peter Brett Associates dated June 2018
  - b. Waste Management Strategy Final prepared by Peter Brett Associates dated March 2018
  - c. Strand East R8 Thermal Comfort Version P01 prepared by Bryden Wood dated 13 June 2018
  - d. BREEAM Pre-assessment prepared by Mott MacDonald dated March 2018
  - e. Strand East R8 Code for Sustainable Homes Pre-assessment Review prepared by Bryden Wood dated 14<sup>th</sup> May 2018

Reason: In the interests of highway management and safety and promoting sustainable travel patterns, energy efficiency and waste management, in accordance with Policies T.7 and S.6 of the Local Plan 2015.

3. Prior to commencement of above ground works, a Site Waste Management Plan (SWMP) relating to the requirements of Condition C36(a) shall be submitted to and approved in writing by the Local Planning Authority. The SWMP shall be

compliant with the revoked SWMP regulations 2008. The development shall be constructed in accordance with the approved SWMP.

Reason: In the interests of minimising the production of waste, in accordance with Policy S.6 of the Local Plan 2015.

4. The basement level hereby permitted shall not be used until details of the car parking allocation have been submitted to and approved in writing by the Local Planning Authority. The car parking allocation shall be implemented prior to occupation, and maintained thereafter only in accordance with the approved details and to the satisfaction of the Local Planning Authority.

Reason: In the interests of estate management and promoting sustainable travel behaviour.

5. Prior to first occupation of the development hereby permitted, details of how the mitigation measures required by the overheating study have been incorporated into the development, shall have first been submitted to and approved in writing by the Local Planning Authority. The development shall be implemented and retained thereafter in accordance with the approved details.

Reason: To prevent overheating of the residential units, in accordance with Policy S.7 of the Local Plan 2015.

## **INFORMATIVES**

### **1. Positive and Proactive Statement**

In accordance with the National Planning Policy Framework and with Article 35 of the Town and Country Planning (Development Management Procedure) (England) Order 2015 (as amended), the following statement explains how the LLDC as Local Planning Authority has worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with this planning application:

Following submission of the planning application to LLDC, the local planning authority continued to work with the applicant in a positive and proactive manner. The planning application complies with planning policy as stated above and was determined in a timely manner.

The applicant has been kept informed of the progress of the application and has been given the opportunity to respond to and address any problems arising.

### **2. Cadent Gas Informative**

Considerations in relation to gas pipeline/s identified on site:  
Cadent have identified operational gas apparatus within the application site boundary. This may include a legal interest (easements or wayleaves) in the land which restricts activity in proximity to Cadent assets in private land. The Applicant must ensure that proposed works do not infringe on Cadent's legal rights and any details of such restrictions should be obtained from the landowner in the first instance.

If buildings or structures are proposed directly above the gas apparatus then development should only take place following a diversion of this apparatus. The Applicant should contact Cadent's Plant Protection Team at the earliest opportunity to discuss proposed diversions of apparatus to avoid any unnecessary delays.

If any construction traffic is likely to cross a Cadent pipeline then the Applicant must contact Cadent's Plant Protection Team to see if any protection measures

are required. All developers are required to contact Cadent's Plant Protection Team for approval before carrying out any works on site and ensuring requirements are adhered to.

Email: [plantprotection@cadentgas.com](mailto:plantprotection@cadentgas.com) Tel: 0800 688 588

## 15 APPENDICES

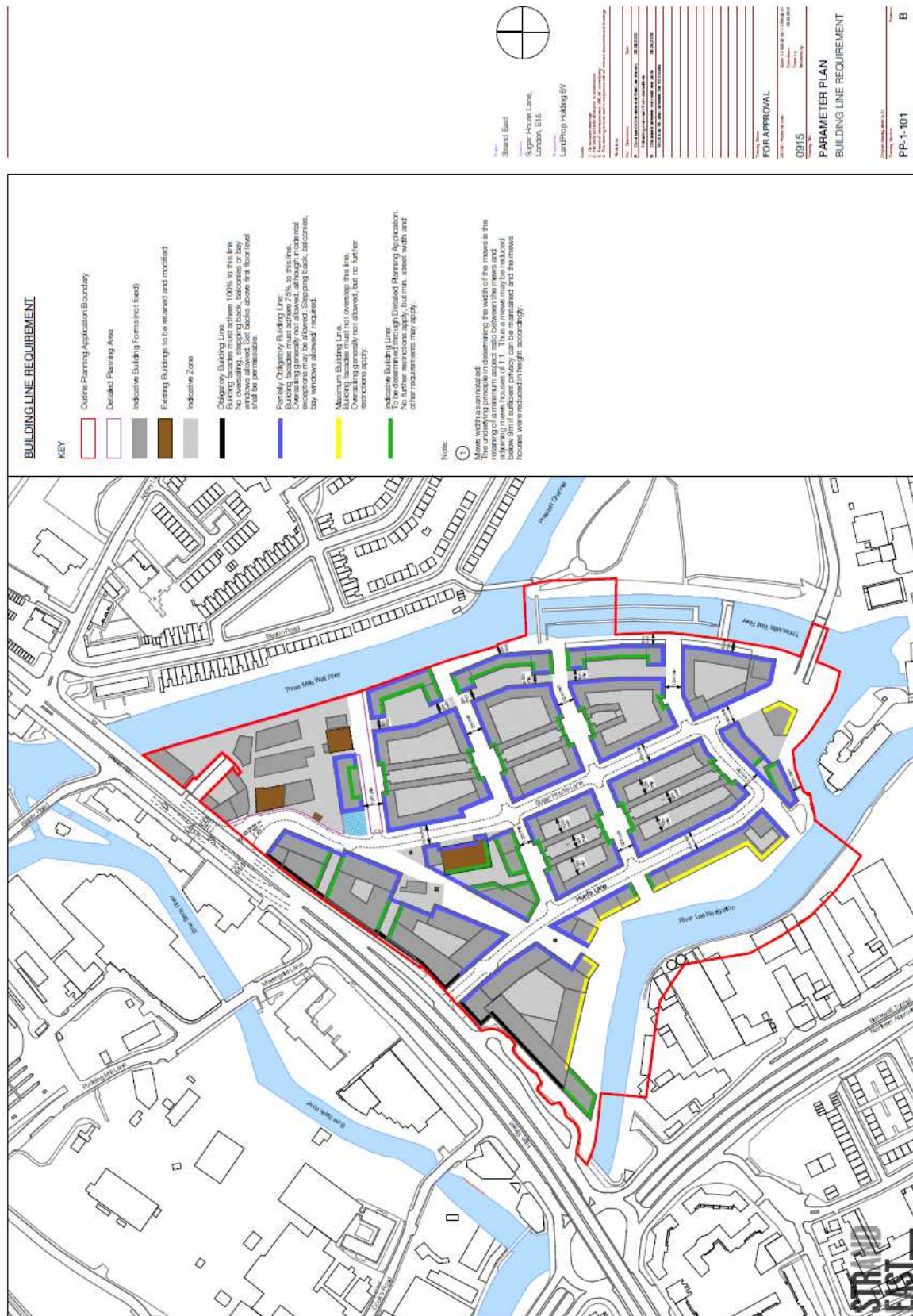
Appendices attached to this report are as follows

Appendix 1	Approved 'Building Line Requirement', 'Maximum Storey Heights', 'Uses at Ground Floor', and 'Underground Car Parking Strategy' Parameter Plans
Appendix 2	Proposed 'Building Line Requirement', 'Maximum Storey Heights', 'Uses at Ground Floor', and 'Underground Car Parking Strategy' Parameter Plans
Appendix 3	Plot R7 Proposed Floorplans
Appendix 4	Plot R7 Proposed Elevations / CGIs
Appendix 5	Plot R8 Proposed Floorplans
Appendix 6	Plot R8 Proposed Elevations
Appendix 7	Plot R7 QRP Comments (04/05/17)
Appendix 8	Plot R8 QRP Comments (30/04/15))

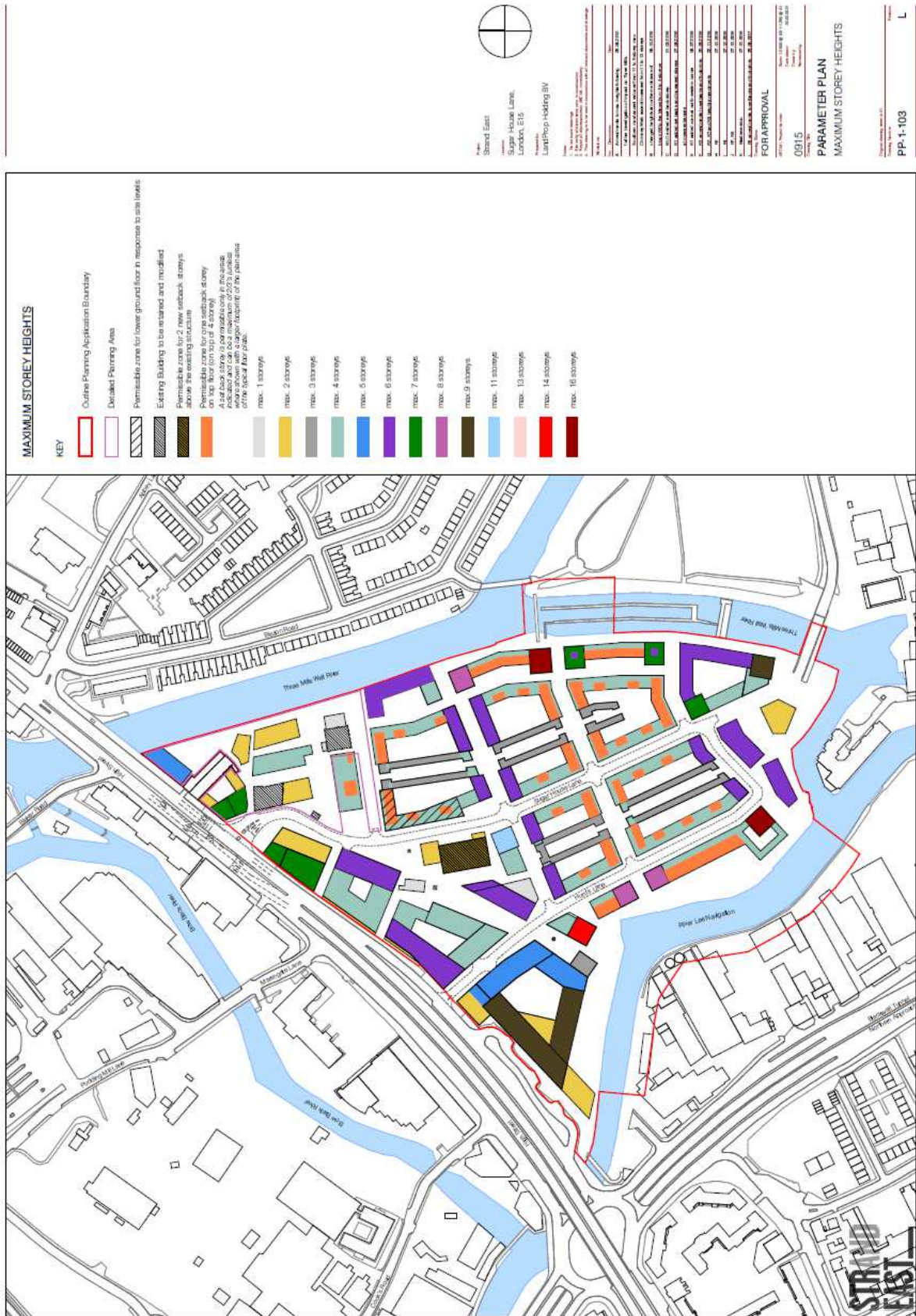


# Plot R7 and R8 Appendix 1 - Approved Parameter Plans

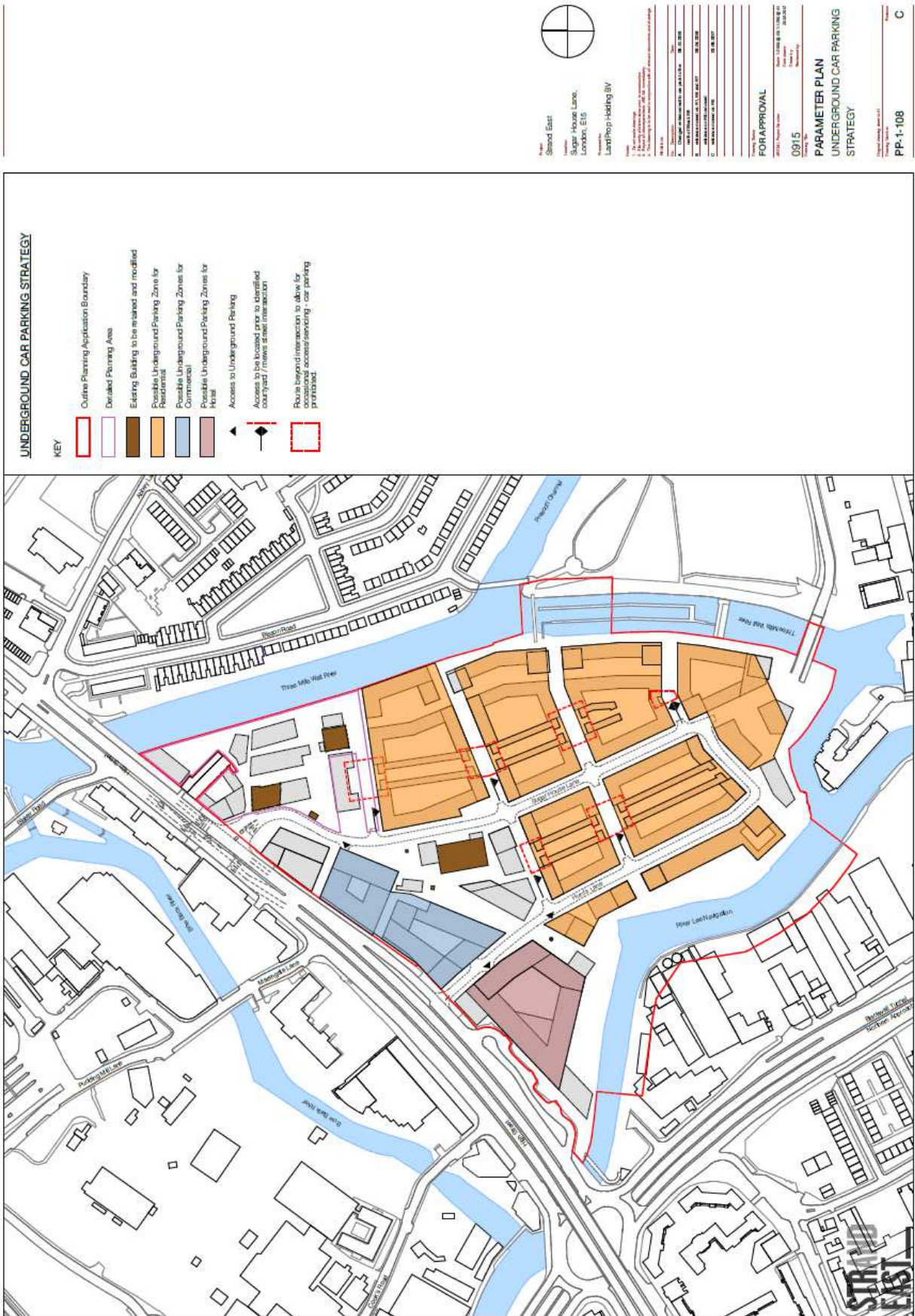
## Building Line Requirement



# Maximum Storey Heights



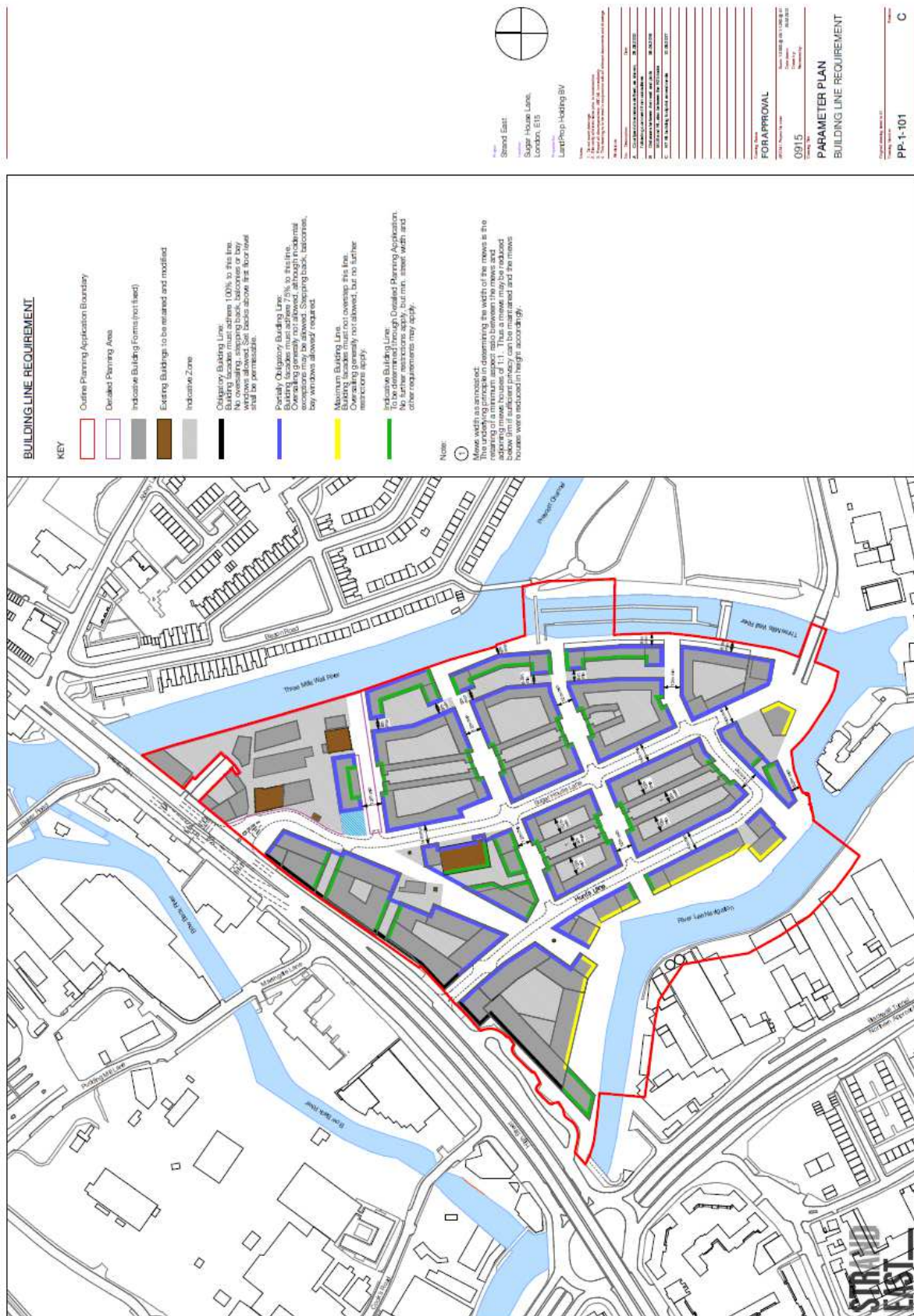
# Underground Parking Strategy



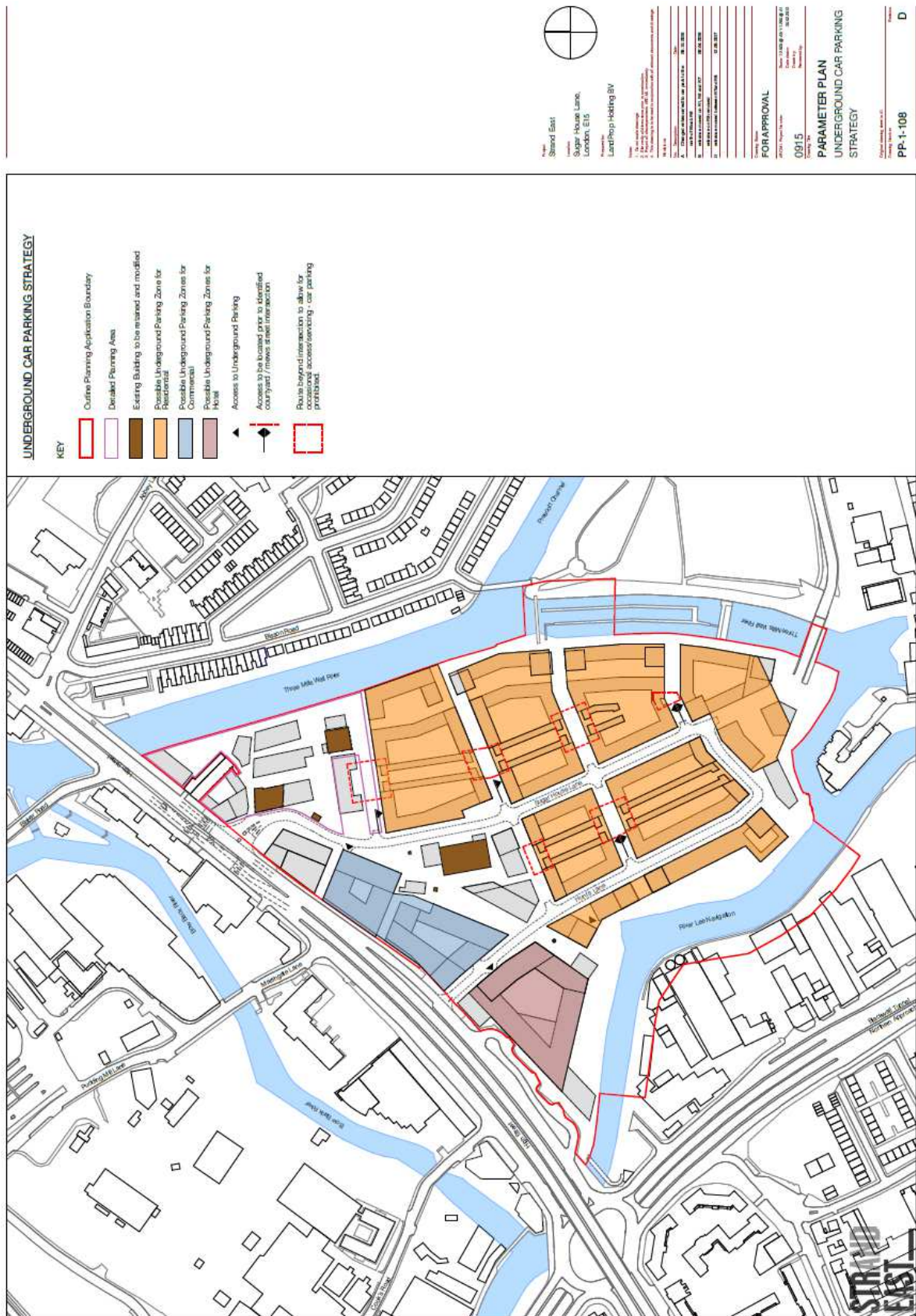
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# Plot R7 and R8 Appendix 2 - Proposed Parameter Plans

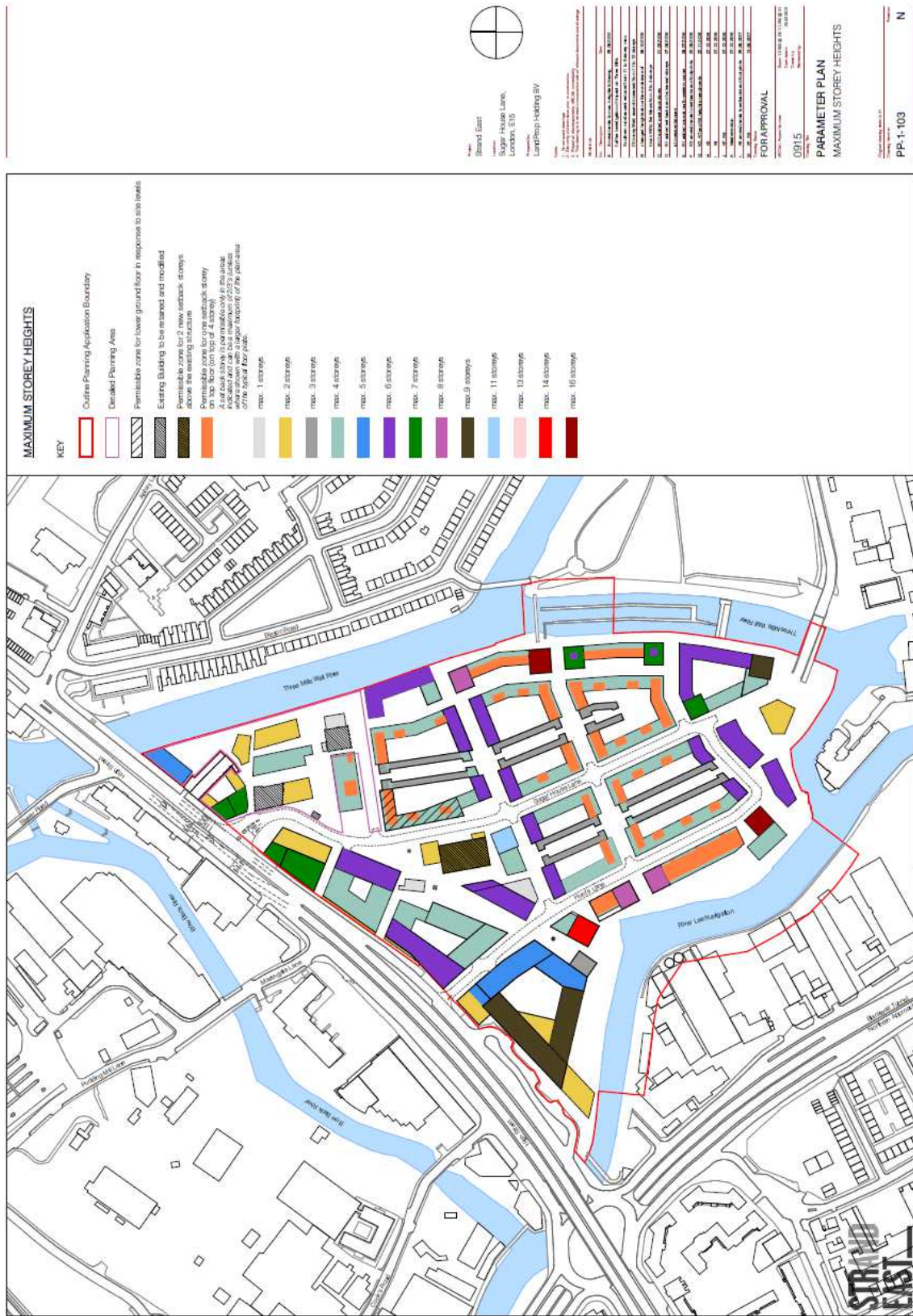
## Building Line Requirement



# Underground Parking Strategy



# Maximum Storey Heights



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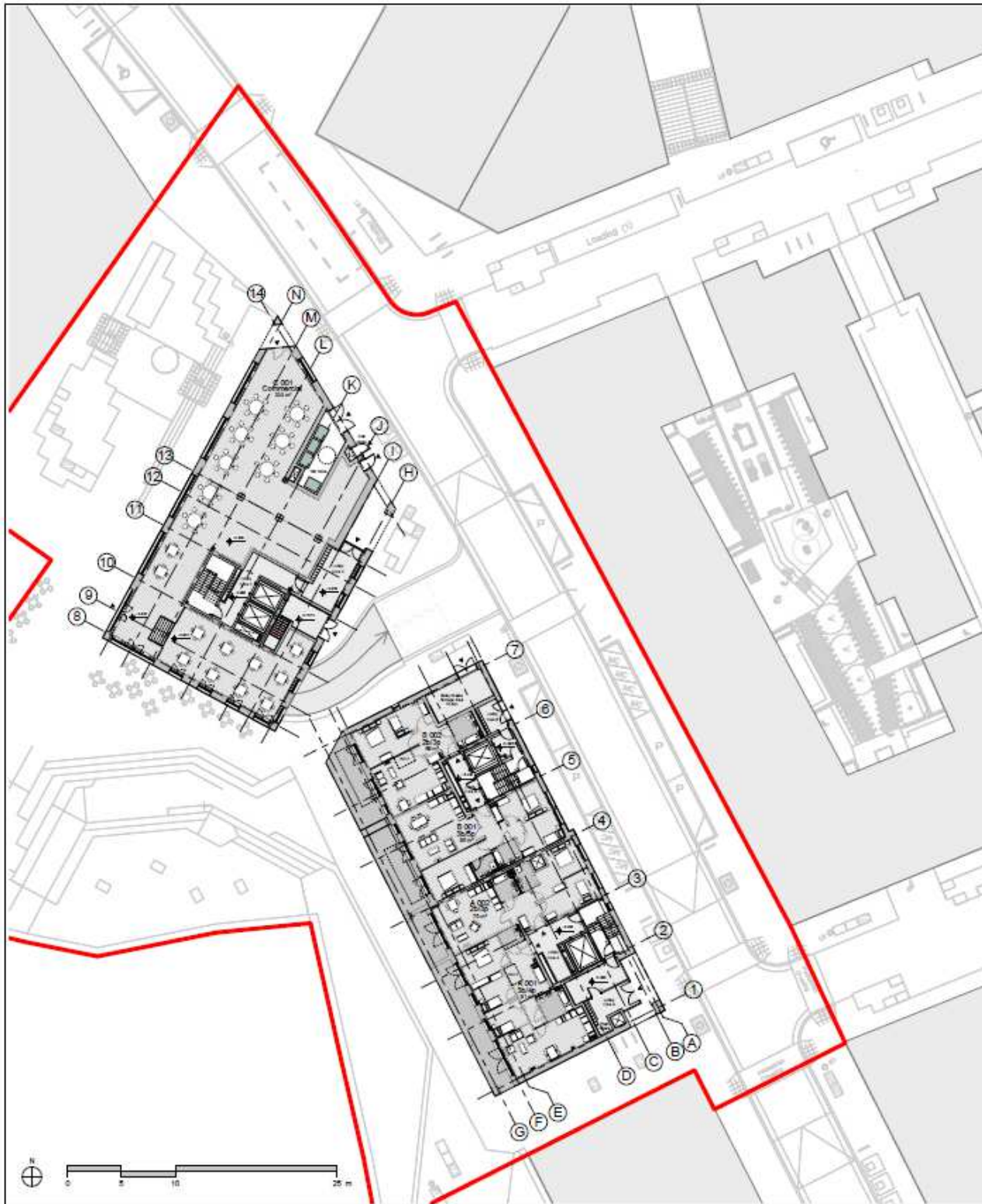


Plot R7 Appendix 5 - Proposed floorplans

Basement



Ground floor



Typical upper floor (1<sup>st</sup> floor)



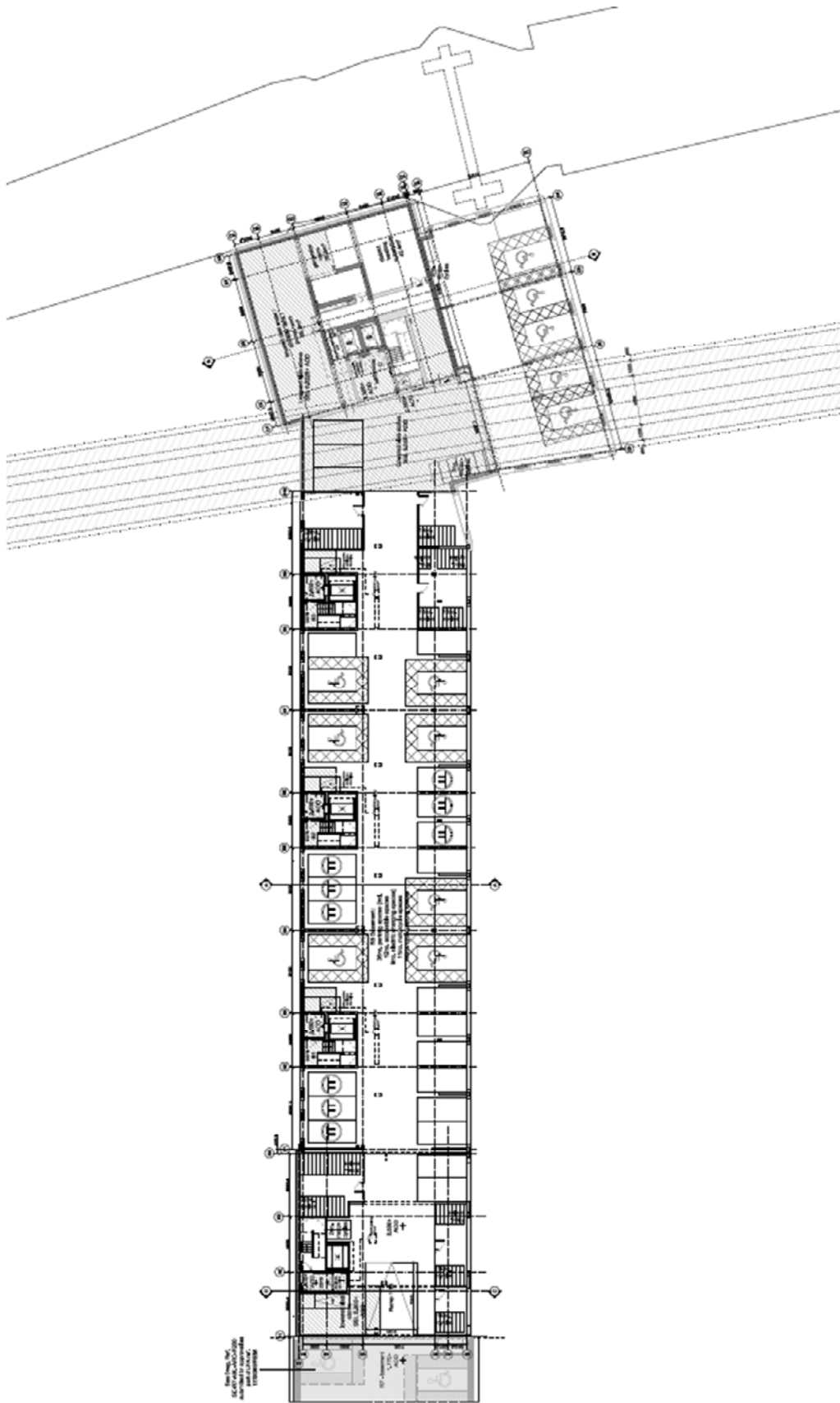
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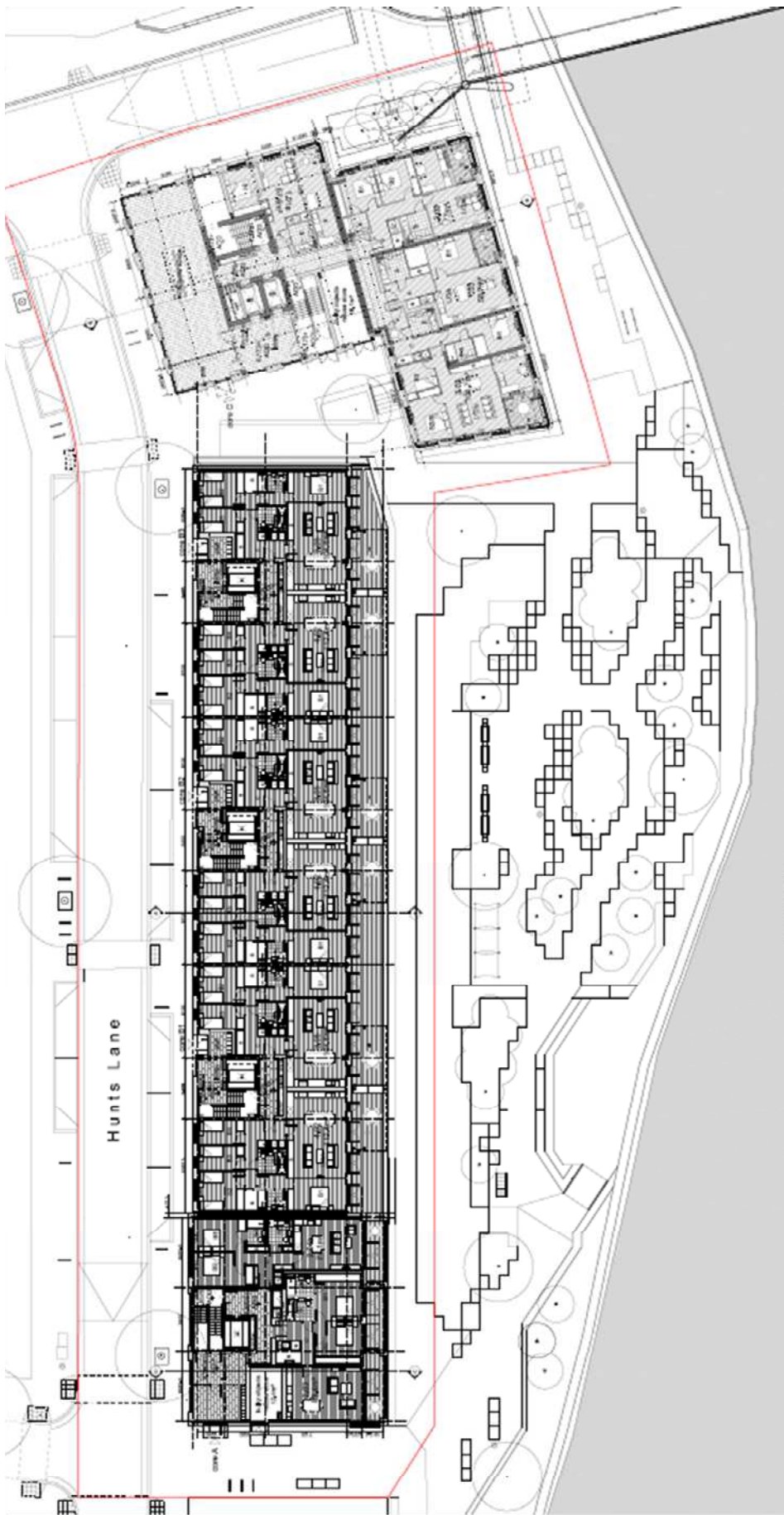
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Plot R8 Appendix 5 - Proposed floorplans

Basement

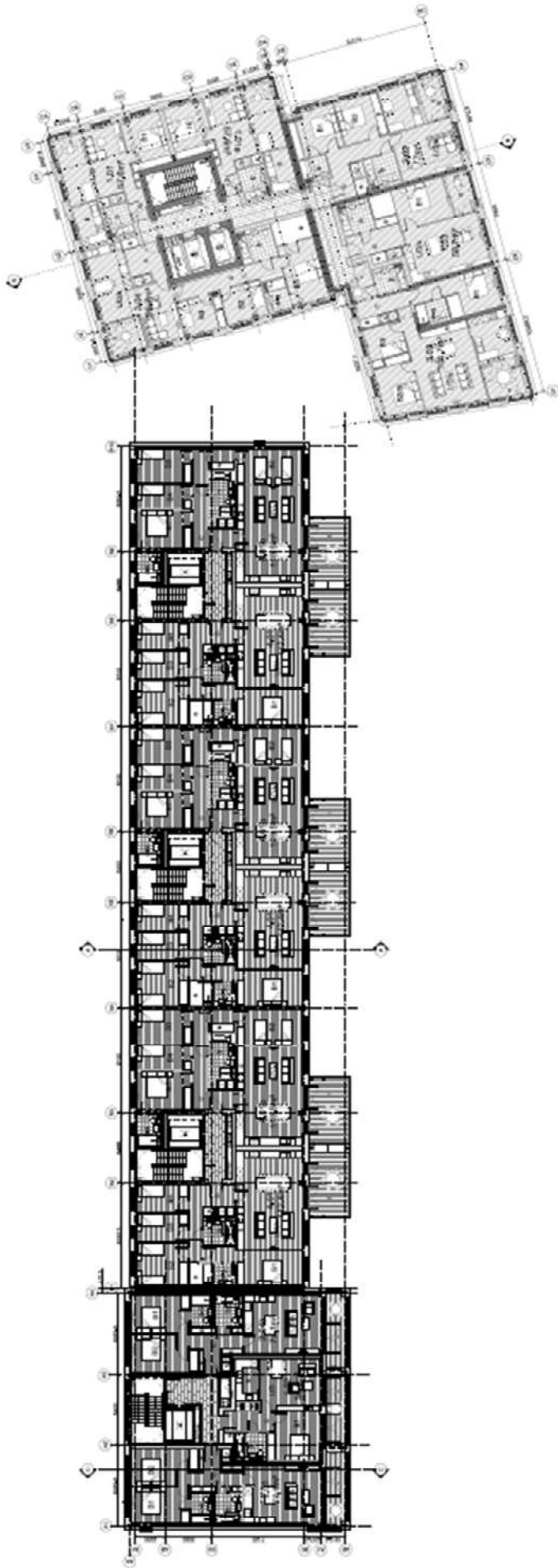


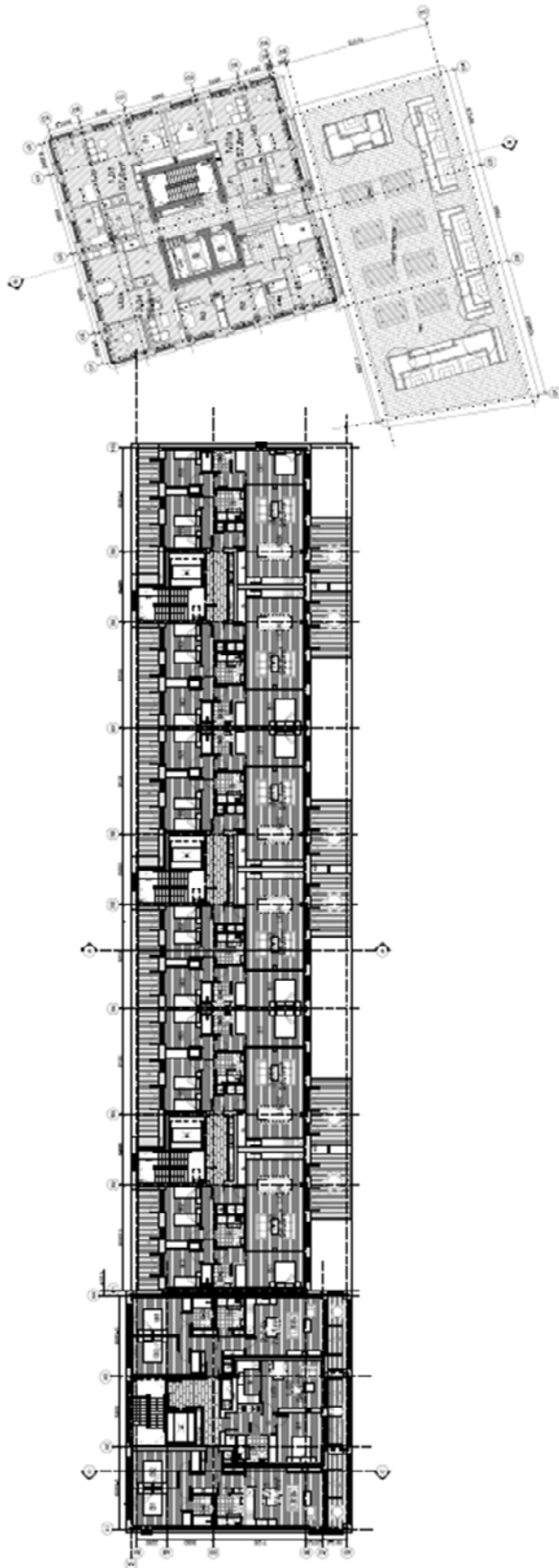
Ground floor





Typical upper floor (Level 1-3)







West elevation (park front)



East elevation (Hunts Lane)



View of Building D from the bus bridge



FRAME PROJECTS

## **London Legacy Development Corporation Quality Review Panel**

### **Report of Formal Review Meeting: Strand East Plot R7**

Thursday 4 May 2017

Level 10, 1 Stratford Place, Montfichet Road, London E20 1EJ

#### **Panel**

Peter Studdert (chair)  
Johnny Winter

#### **Attendees**

Sara Dawes	LLDC Planning Policy and Decisions Team
Sophie Backhouse	London Legacy Development Corporation
Tessa Kordeczka	Frame Projects

#### **Apologies / report copied to**

Anthony Hollingsworth	LLDC Planning Policy and Decisions Team
Ben Hull	London Borough of Newham

## **1. Project name and site address**

Strand East / Sugar House Lane redevelopment – reserved matters application for Plot R7 at: land to the south of Stratford High Street, east of the River Lea Navigation and west and north of the Three Mills Wall River.

## **2. Presenting team**

Michael Westlake	ARC-ML
Fiona Young	ARC-ML
Hilary Boyle	Vastint UK B.V.
Antony Nelson	Planit-IE
Christopher Schiele	GL Hearn

## **3. Planning authority's views**

The design of the tower block / podium has evolved since the previous review of Strand East Plot R7 by the Quality Review Panel and the panel's comments on the revised design, in particular the treatment of the riverside elevation, are sought. The public realm at the entrance to the ramp to the basement car park, between the tower block / podium and the linear riverside block, has also been refined. Reconfiguration of the internal layouts of the tower block has resulted in considerable improvements.

## **4. Quality Review Panel's views**

### *Summary*

The Quality Review Panel commends the design team on an effective response to its earlier comments on the proposal for Strand East Plot R7. Revisions to the design of the entrance to the basement car park, and the treatment at ground floor level of the sharply angled corner of the podium, enhance the public realm. The architectural expression of the tower block / podium is well considered, with the two elements now relating more successfully to each other. The scheme promises high quality residential accommodation. The panel recommends further exploration of the best solution for access to cycle storage at basement level. These comments are expanded below, and those made at the previous review that remain relevant are repeated for clarity.



### *Response to site*

- The panel repeats its view that the access ramp from Hunts Lane to the basement car park – which serves both Plot R7 and Plot R8 – will work well. It welcomes revisions to the public realm at the entrance to the car park which enhance the environment for pedestrians.
- The panel supports the revised design of the sharply angled corner of the podium block at Hunts Lane. Chamfering this corner at the ground floor adds to the public realm by improving legibility and navigability, including towards the riverside park.  
The panel agrees that the chamfer should be restricted to the ground floor.

### *Architectural expression*

- The tower block / podium is a significant building that has to both successfully signal a connection from Chimney Walk towards the riverside park and also create a landmark building in a prominent riverside location. The panel commends the design team on its effective response to its earlier comments.
- The panel finds the architectural quality of the tower block / podium to be much improved: the two elements now relate well to each other; the symmetry of the tower block's silhouette is considerably strengthened; and the number of single aspect units – already minimal – is reduced.
- At its previous review, the panel had suggested revisiting the architectural expression of the eight storey element of the linear riverside block – where the elevation shifts from three structural bays to two at the upper storeys, reflecting the internal layout.
- The panel thinks that this is broadly acceptable, and that the visual coherence of the elevation could be strengthened by skilful detailing.

### *Residential accommodation*

- The panel repeats its support for the highly efficient plan and layout of residential accommodation – now further improved. The scheme promises high quality homes, and the panel particularly welcomes the fact that all residential accommodation for Plot R7 is to be tenure blind.

### *Cycle storage*

- As currently planned, access to cycle storage in the basement is gained through the residential entrance lobby and lifts of the tower block. (It will not be possible for cyclists to use the vehicle ramp to the car park from Hunts Lane.)
- The panel supports the decision to locate cycle storage in the basement: this maximises the potential for active frontages at ground floor level.



- In order to encourage cycle use, however, storage should be as convenient as possible. The panel thinks that the rather circuitous route to reach secure cycle storage may be a deterrent – or result in cycles being taken up to apartments.
- Also, if cycles are taken through the residential entrance lobby, there is a risk that this would detract from the smartness of the lobby and lifts. Their design would have to be suitably robust.
- The panel therefore recommends that the design team explore an alternative plan that would allow a more direct and practical route from the street to the basement cycle storage.

*Next steps*

- The Quality Review Panel is confident that the design team will continue to evolve the proposal for Strand East Plot R7 successfully, in consultation with planning officers.





**London Legacy Development Corporation Quality Review Panel**

**Report of Formal Review Meeting: Strand East Plot R8**

Thursday 30 April 2015

Level 10, 1 Stratford Place, Montfichet Road, London E20 1EJ

**Panel**

Peter Studdert (chair)  
Catherine Burd  
Liam Bond  
Andrew Harland

**Attendees**

Will Steadman	LLDC Planning Policy and Decisions
Pippa Gueterbock	London Legacy Development Corporation
Deborah Denner	Fortismere Associates

**Apologies / report copied to**

Anthony Hollingsworth	LLDC Planning Policy and Decisions
Allison De Marco	LLDC Planning Policy and Decisions
Esther Everett	London Legacy Development Corporation
Steve Tomlinson	London Legacy Development Corporation
James Bolt	London Borough of Newham
Ben Hull	London Borough of Newham

## 1. Project name and site address

Strand East/Sugar House Lane – Plot R8

## 2. Presenting team

Michiel van Soest	Vastint UK B.V.
Hilary Boyle	Vastint UK B.V.
Richard Lavington	Maccreeanor Lavington
Ken Thompson	Maccreeanor Lavington
Jennie Bean	GL Hearn

## 3. Planning authority's views

LLDC planning officers have attended one pre-application meeting to discuss the reserved matters proposals for Strand East Plot R8. This plot includes one of the two tallest buildings in the approved masterplan for the site – a 16 storey tower. Planning officers will be interested in the architectural relationship between this and the linear block occupying the remainder of the site. The scheme will also need to respond successfully to different contexts on each side of the site, including Hunts Road and the proposed bridge, and the riverside park.

## 4. Quality Review Panel's views

### *Summary*

The panel finds much to admire in the proposals for Strand East Plot R8, which are based on rigorous thinking about residential typologies, and the design of homes from the inside out. The architectural expression of both the tower and linear block is developing in a positive direction, although the panel offers some comments on potential refinements. Limited information is available at this stage on the landscape design. This will be critical to making the most of the riverside park, and ensuring this is a welcoming, accessible and high quality space. This may require some adjustment to the layout of buildings, to create a generous and legible route from Hunts Lane to the park. The panel would welcome further information on the landscape design at a future review. More detailed comments are provided below.

### *Bridge on Hunts Lane*

- The parameters of the outline planning approval for Strand East create a tight relationship between the buildings and the proposed bridge over the Lea Navigation.
- This has already informed the design, but the panel think this could be taken further – to provide a positive setting for the bridge, and/or enhance access to the riverside park.

- One approach would be to create a densely planted bank between the buildings and bridge, to provide a green setting for the elegant bridge structure.
- Alternatively, stepped access to the riverside park could be provided in this location.
- The panel recognises that equality of access would be an issue if stepped access to the park is provided, but thinks this could be addressed by nearby ramped access, between the tower and linear block, and on neighbouring sites.
- Discussing this issue with the LLDC's Built Environment and Access Panel could help reach a solution.

### *Tower*

- The panel admires the simplicity and elegance of the architecture proposed for the 16 storey tower.
- This also promises to provide high quality living accommodation, with two or four dual aspect flats per floor, planned with skill and care.
- The indoor / outdoor quality of the apartments is particularly attractive, with glazed corners designed to open up in smaller flats, and winter gardens in larger ones.
- At ground level, a spacious lobby / reading room is provided for residents. The panel think this will be a fantastic facility, and would encourage Vastint to allow use of this by the wider community.
- A small podium block is proposed at the base of the tower. The panel thinks that the relationship between the podium and tower would benefit from further exploration in plan and elevation.
- At a detailed level, a strategy for window cleaning will be needed to ensure ease of maintenance – either from inside or outside.

### *Linear block*

- As with the tower, the residential layout of the linear block promises accommodation of the highest quality.
- The architecture seems slightly less well developed, but the panel is confident that this will evolve to match the quality of the tower before a reserved matters submission.

### *Affordable housing*

- No information on the distribution of affordable housing across the Strand East site has been provided – and the panel notes this should be clarified to ensure good integration, and timely delivery.

### *Landscape design*

- Limited information on landscape design was provided at this review, and the panel would welcome further information on the design of the riverside park, and spaces around the site.
- Analysis of wind conditions, sunlight and daylight would be valuable to inform the landscape design.
- The panel think that access from Hunts Lane to the riverside park should be improved.
- Adjusting the layout of the linear block and tower, could help achieve views between these buildings towards the park and river, from Hunts Lane – drawing people through.
- One possible means of achieving views and access towards the riverside park would be to carve out a generous colonnaded undercroft.
- Illustrating pedestrian views from Hunts Lane towards the park would be valuable in support of the reserved matters submission.

### *Car parking*

- The design of car parking to slightly raise the ground floor residential accommodation above street level is welcomed.
- This both creates an opportunity to bring light and ventilation into the car park, and improves privacy for ground floor accommodation.
- The panel also supports the decision to extend car parking onto the neighbouring Plot R7 – but thinks some plot testing will be required to ensure this does not place unforeseen limitations on future design work for this site.

### *Bin stores*

- The proposed below ground bin stores at Strand East promise to significantly enhance the quality of the public realm.
- As currently shown, residents of Plot R8 would have to cross Hunts Lane to deposit their waste in the nearest bin stores.

- The panel would encourage the provision of a refuse collection point on, or closer to this site.

*Next steps*

- The panel offers its warm support to the proposals for Strand East Plot R8, and would welcome a further opportunity to comment on the landscape design and access to the riverside park.

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**Subject: The International Quarter London, Building S9 - 18/00255/REM**

**Meeting date: 25 September 2018**

**Report to: Planning Decisions Committee**

**Report of: Richard McFerran – Principal Planning Development Manager**

FOR DECISION

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**This report will be considered in public**

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**1. EXECUTIVE SUMMARY**

- 1.1. This application is pursuant to a revised reserved matters application for Building S9 in Zone 2 of the Stratford City development, known as The International Quarter London South (IQL South) under the Stratford City Outline Planning Permission (10/90641/EXTODA). Building S9 benefits from two previous reserved matters consents; application ref: 16/00671/REM which relates to the substructure and application ref: 16/00672/REM which relates to the superstructure. Construction works associated with S9 are ongoing and the substructure and superstructure are being built out in accordance with the approved details. The building is due to open in Summer 2019 and will provide office space for the British Council and Cancer Research.
- 1.2. A further reserved matters application (ref: 16/00683/REM) was previously submitted and approved for the area of public realm between Buildings S4 and S9 which was previously known as Carpenters Square. However, since this previous approval the tenant within the ground floor of Building S9 has been confirmed as the British Council. Resultantly the ground floor façade facing onto the square has been designed as an exhibition and gallery space which has necessitated changes to the design of the public realm. In addition, the applicant has been in discussion with LLDC with respect to refining the IQL South masterplan for the remaining unbuilt plots which includes revisions to the wider landscaping strategy.
- 1.3. This application for reserved matters pursuant to Building S9 seeks to capture these proposed changes within the public realm and landscape design surrounding the building. For the purposes of clarification, this application seeks to amend public realm and landscaping details only. The application does not include any changes to the siting, design or access of Building S9 as originally approved.
- 1.4. It should also be noted that as part of a revised naming strategy across the wider site, Carpenters Square has been changed to Redman Place after a local war hero and employee at the Stratford Railway Works. This application provides details for the majority of Redman Place with the remaining smaller portion consisting of part of the shared road and S4 retail spill-out areas included as part of application ref: 18/00354/REM for Building S4. The two applications should therefore be read in conjunction with one another in order to get a full understanding of Redman Place.

- 1.5. It is considered that the main issues in relation to this application are:
- Overall conformity with the Stratford City Outline Planning Permission and approved Zonal Masterplan including the amount, location and character of open space;
  - Landscaping and appearance;
  - Access and inclusive design; and
  - Environmental impacts.
- 1.6. Officers have assessed that the proposed development and consider that it would be in conformity with the Stratford City Outline Planning Permission and would also be largely consistent with the approved Zonal Masterplan and Site Wide Strategies for Zone 2. There are minor deviations from the Zonal Masterplan with respect to vehicular access and pedestrian routes through the site (see para. 10.26) however these are considered to be minor in nature and justified in terms of improving pedestrian circulation through the site by way of reducing conflict between pedestrians and service vehicles.
- 1.7. With respect to the amount and location of public open space, this reserved matters application proposes 4,765 sqm which would be in excess of the approved ZMP requirements. This comprises 1,183 sqm of planted areas and 3,582 sqm of hardscaping across the remainder of the site. The location of open spaces is in keeping with those shown on the approved parameter plans and the character would also be consistent with the principles of the ZMP including provision of formal public gardens, pedestrian connections, park furniture, an area for seasonal displays and performances and provision of public art.
- 1.8. The quality and appearance of the public realm around Building S9 would be of a high standard and is supported by the Quality Review Panel. The planting strategy would respond to the character, use and nature of each character area within the site including Gallery Gardens (which would front onto the British Council exhibition), Bridge Plaza (which would act as a threshold onto the proposed Carpenters Land Bridge to be submitted as part of the East Bank application in Autumn 2018) and Railway Walk (located to the south-west/rear of the building). The proposed areas of planting would provide provision for bird and insect habitats. The proposed materials within the public realm are also considered to be acceptable and would be in keeping with the existing public realm within the wider IQL site.
- 1.9. With regards to access and inclusive design, the submission is considered to demonstrate adequate circulation arrangements through the public realm that would accord with the principles of the Stratford City Outline Planning Permission and Zonal Masterplan. Redman Place would accommodate the shared secondary rout known as S2 (within the landscaping proposals for Building S4). The public realm includes a safeguarded bridge landing zone for the proposed Carpenter's Land Bridge connection and is suitably designed to provide an appropriate threshold into the site. The public realm would also provide for an appropriate number of cycle parking spaces. The proposed deviation to the Zonal Masterplan with respect to vehicular access is acceptable noting that it prevents conflict with vehicles and pedestrians using the Carpenter's Land Bridge. The public realm has been designed to be inclusive for all and have been found to be acceptable by LLDC's Built Environment Review Panel.
- 1.10. In terms of environmental impacts, the proposed public realm surrounding Building S9 is considered to achieve suitable levels of sunlight and daylight in accordance BRE guidance, whilst resultant wind conditions are also considered to be acceptable, subject to appropriate mitigation.



1.11. The revised landscaping and public realm around Building S9 is therefore considered to be acceptable and is recommended for approval subject to the conditions laid out in Section 13 of this report.

## **2. RECOMMENDATIONS**

### **2.1 The Committee is invited to:**

**2.1.1 APPROVE application 18/00255/REM, with the FULL DISCHARGE of conditions B1, B8, B9, B10, Q1 and Q4 of the Stratford City Outline Planning Permission (10/90641/EXTODA) in relation to reserved matters for landscaping works for Plot S9 subject to the conditions set out within this report.**

**2.1.2 AGREE TO DELEGATE AUTHORITY to the Director of Planning Policy and Decisions to finalise the recommended conditions as set out in this report including such refinements, amendments, additions and/or deletions as the Director of Planning Policy and Decisions considers reasonably necessary.**

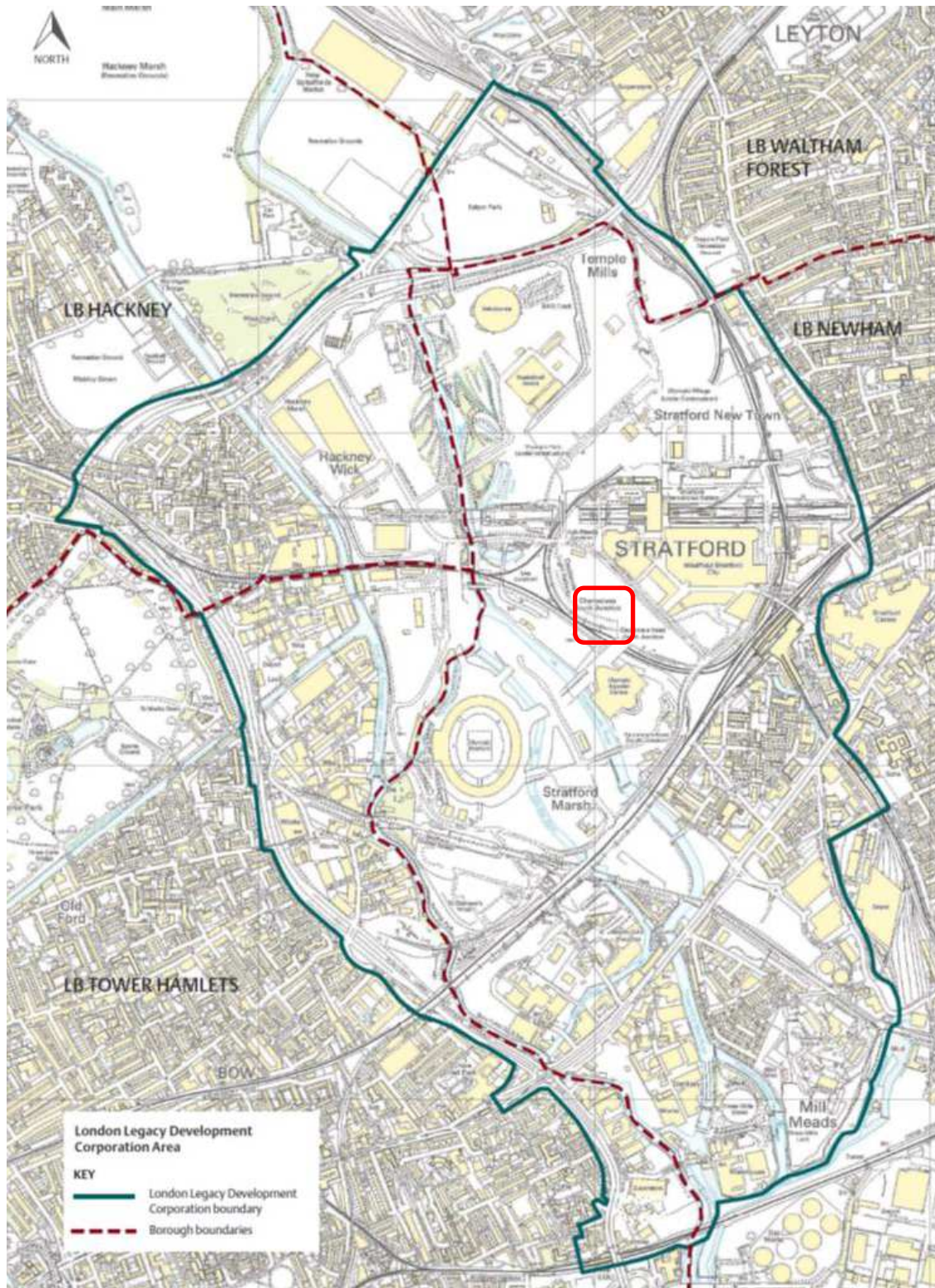
## **3. FINANCIAL IMPLICATIONS**

3.1. None

## **4. LEGAL IMPLICATIONS**

4.1. None

## Site Plan



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**Location:** Plot S9, Stratford City Zone 2, The International Quarter London (IQL) South, Land adjacent to Westfield Avenue, Stratford City.

**London Borough:** Newham

**Proposal:** Application for the approval of Reserved Matters pursuant to conditions B1, B8, B9, B10, Q1 and Q4 of the Stratford City Outline Planning Permission (Ref: 10/90641/EXTODA) comprising details of layout, scale, appearance, access and landscaping of Redman Place (that forms part of the area formerly known as Carpenters Square), part of Secondary Road S2 (both temporary and permanent works) and landscape around Building S9, comprising public open space including hard and soft landscaping and associated works. The application also seeks approval in writing to allow a deviation to the location of vehicular Plot access within Plot P04 as shown within the Zonal Masterplan for Zone 2 pursuant to Condition A4 of the Stratford City Outline Planning Permission (ref: 10/90641/EXTODA).

**Applicants:** Stratford City Business District Limited (SCBD Ltd)

**Agent:** Quod Ltd

**Landscape Architect:** Gustafson, Porter and Bowman.

## 5. SITE & SURROUNDINGS

- 5.1. The application site comprises an area of 0.49 ha, including Building S9 and the surrounding public realm, within the area known as International Quarter London (IQL) South. IQL South forms Zone 2 of the Stratford City Outline Planning Permission (ref: 10/90641/EXTODA) and is bounded by Westfield Avenue to the north east, Montfichet Road to the south east and railway lines to the south west. Westfield Shopping Centre is located on the opposite side of Westfield Avenue from the site whilst the proposed East Bank site, comprising of new cultural and educational buildings, is located 80m from the site on the opposite side of the railway land. The Queen Elizabeth Olympic Park is located further to the east approximately 200m from the application site.
- 5.2. IQL South is a multi-phased development and a number of buildings within the wider masterplan have been built out and are occupied. This includes Glasshouse Gardens which are two residential buildings and Buildings S5 and S6 which are office buildings housing Transport for London and the Financial Conduct Authority. The areas of public realm which surround these buildings have also been delivered including the Entrance Plaza (otherwise known as The Stitch), Turing Street and Endeavour Square. Construction activities associated with Building S9 are partially completed, and this application relates to surrounding public realm only. It is anticipated that Building S9 will be occupied in Summer 2019.
- 5.3. The remaining areas of IQL South remain undeveloped albeit they benefit from outline planning permission under the parent consent. The applicant has been in discussions with LLDC PPDT in relation to revisions to the masterplan for the undeveloped plots including the introduction of residential buildings at the northern end of the site. Details of the masterplan draft changes have previously presented to Members during Member Briefing sessions. The issues raised as part of these briefings will be addressed in the consideration of the revised masterplan applications as they come forward.
- 5.4. The application site is not located within a conservation area nor are there any designated or non-designated heritage assets within the site or surrounding area. The site is in close proximity to local bus routes and the various connections available at Westfield Stratford City and Stratford Station and this is reflected in the PTAL rating of 6b.

## 6. RELEVANT PLANNING HISTORY

6.1. The original Outline Planning permission which covers the site was granted in February 2005 (P/03/0603) by the London Borough of Newham. A number of variations were subsequently permitted: 07/90023/VARODA, 10/9061/VARODA and 10/90641/EXTODA. The description of the main development covered by these consents is:

*“Comprehensive mixed use development of rail lands site comprising B1 offices, residential, retail development in the full range of Class A1, A2 and A3 uses, commercial leisure uses, hotels and conference facilities, community, health and education facilities, open space, landscaping, water features, parking, transport interchanges, associated infrastructure and a town centre link.”*

6.2. The Zonal Masterplan (ZMP) for Zone 2 was originally approved on the 28<sup>th</sup> March 2012 (ref: 11/90463/AODODA) and superseded by a revised ZMP approved 22<sup>nd</sup> September 2015 (ref: 15/00005/AOD). The Zonal Masterplan sets out the parameters that describe the principles to be followed in the detailed design of the development.

6.3. Reserved Matters approvals have been granted for a number of plots/public realm areas or on site-wide matters within Zone 2 as follows:

- **Plots S7-S8** (residential 333 units max 30 storeys) application ref: 13/00409/REM was approved in January 2014 and the two buildings are now fully occupied.
- **Plot S6** (commercial uses) applications 14/00483/REM and 15/00003/REM were approved in February and October 2015 for a building up to a maximum of 12 storeys (+70m AOD approx.) providing 33,688.8sqm of total floorspace (32,796.6sqm of B1 office floorspace and 591.5sqm of A1-A3 retail and a 300.7sqm crèche (Use Class D1). Construction works are complete, and the building is occupied by Transport for London.
- **Plot S5** – (commercial uses) applications 14/00482/REM and 15/00002/REM were approved February and October 2015 for a building up to a maximum of 20 storeys (+107m AOD approx.) providing 62,120sqm total floorspace (61,233sqm of B1 office and 887sqm A1-A5 retail. Construction works are complete, and the building is occupied by the Financial Conduct Authority.
- **Plot S4** – (commercial uses) applications 16/00339/REM and 16/00342/REM for a building up to a maximum of 21 storeys (+109.9m AOD) providing 74,848sqm of B1 and 604sqm A1-A5 retail floorspace, which Planning Decisions Committee (PDC) was approved in October 2016 – however construction works were not begun and the site is now subject of a fresh application (see para 6.4).
- **Plot S9** – Reserved matters approval was granted for a commercial building (16/00672/REM and 16/00671/REM). This building is currently under construction; and is due to be occupied by the British Council and Cancer Research.
- **Carpenter’s Square** – (public realm) application 16/00683/REM for landscaping around future buildings, comprising public open space including hard and soft landscaping and associated works. The application also consented deviations to a primary pedestrian route and the locations of vehicular plot access and shared surface from the approved ZMP. The consent was not implemented and revised landscaping proposals for S9 are subject of this application and revised landscaping proposals for S4 are contained within a new application for this plot (see para. 6.4).

- **The Pavilion** – (commercial uses) application ref: 18/00252/REM for details of layout, scale, appearance, access and landscaping of the IQL Pavilion, comprising a part two, part three storey building for complimentary retail use. Application pending consideration with committee recommendation for approval (July 2018).

6.4. In addition to the above, revised reserved matters applications for Building S4 – 18/00354/REM (superstructure and landscaping) and 18/00355/REM (substructure) were submitted in July 2018 and are reported separately on this agenda. These proposals seek permission for a new 21 storey building with a basement to provide 44,916 sqm of office floorspace, 825 sqm of complimentary retail, 1,170 sqm of leisure floorspace and the surrounding public realm. The principles of the revised S4 proposal incorporate a number of the urban design principles for the emerging masterplan for the remainder of IQL South, whilst the public realm has been designed to integrate with the scheme proposed under this application.

6.5. It should also be noted that the revised landscaping proposals under this application were accompanied by a request for an EIA Screening Opinion (re: 18/00256/SCRES). The Screening Opinion concluded that there were no new or different significant environmental effects that would arise as a result of the proposed reserved matters application for Redman Place. Accordingly, it was concluded that there were no changes which would trigger the requirement for a further environmental impact assessment.

## 7. APPLICATION PROPOSALS

7.1. This application is for the approval of Reserved Matters pursuant to conditions B1, B8, B9, B10, Q1 and Q4 of the Stratford City Outline Planning Permission (Ref: 10/90641/EXTODA) comprising details of layout, scale, appearance, access and landscaping of Redman Place (that forms part of the area formerly known as Carpenters Square), part of Secondary Road S2 (both temporary and permanent works) and landscape around Building S9, comprising public open space including hard and soft landscaping and associated works. The application also seeks approval in writing to allow a deviation to the location of vehicular Plot access within Plot P04 as shown within the Zonal Masterplan for Zone 2 pursuant to Condition A4 of the Stratford City Outline Planning Permission (ref: 10/90641/EXTODA).

7.2. The conditions of the Stratford City Outline Planning Permission that are relevant to this reserved matters application are summarised below:

<b>Condition B1</b>	Details of design, appearance, access and landscaping.
<b>Condition B8</b>	Requirements for reserved matters applications including the submission of a ZMP conformity statement.
<b>Condition B9</b>	Details of reprofiling and earthworks.
<b>Condition B10</b>	Details of external surfaces including hard landscaping.
<b>Condition Q1</b>	Requirement for landscaping in each zone to take account of landscaping in adjacent zones.
<b>Condition Q4</b>	Details of open space and design and landscaping.
<b>Condition A4</b>	All reserved matters to be in accordance with the ZMP unless minor variations are agreed in writing with the LPA.

7.3. A detailed description of the proposed S9 landscaping works within this application is as follows:

Public Realm Character Areas

7.4. The S9 public realm is divided into four character areas, which have been specifically designed to respond to their location and use. The character areas are as follows:

- 1) Redman Place;
- 2) Orchard Walk and Bridge Plaza;
- 3) S9 Service Road; and
- 4) Interface between Endeavour Square and Redman Place.



*Figure 1: Breakdown of character areas within S9 public realm*

7.5. The first character area is Redman Place (previously known as Carpenter’s Square) which covers the majority of the area between S4 and S9. The eastern portion of Redman Place is covered under application ref: 18/00354/REM for Building S4 and would include a shared north/south pedestrian/cyclist/vehicle route which would run through the site.

7.6. The majority of Redman Place consists of a linear soft landscaped area known as Gallery Gardens. This has been designed to respond to the British Council exhibition area within S9 and would incorporate a covered stepped ‘amphitheatre’ which would create a covered area for viewing projections/displays on the gallery façade. It would also act as place for outdoor working, gathering and sitting. At the southern end of Gallery Gardens, it is proposed that an interactive piece of play/art would be installed to attract people into the garden, act as a wayfinding tool and add visual interest. Gallery Gardens would also include two outdoor working shelters to help assist with the applicant’s vision for flexible outdoor working.

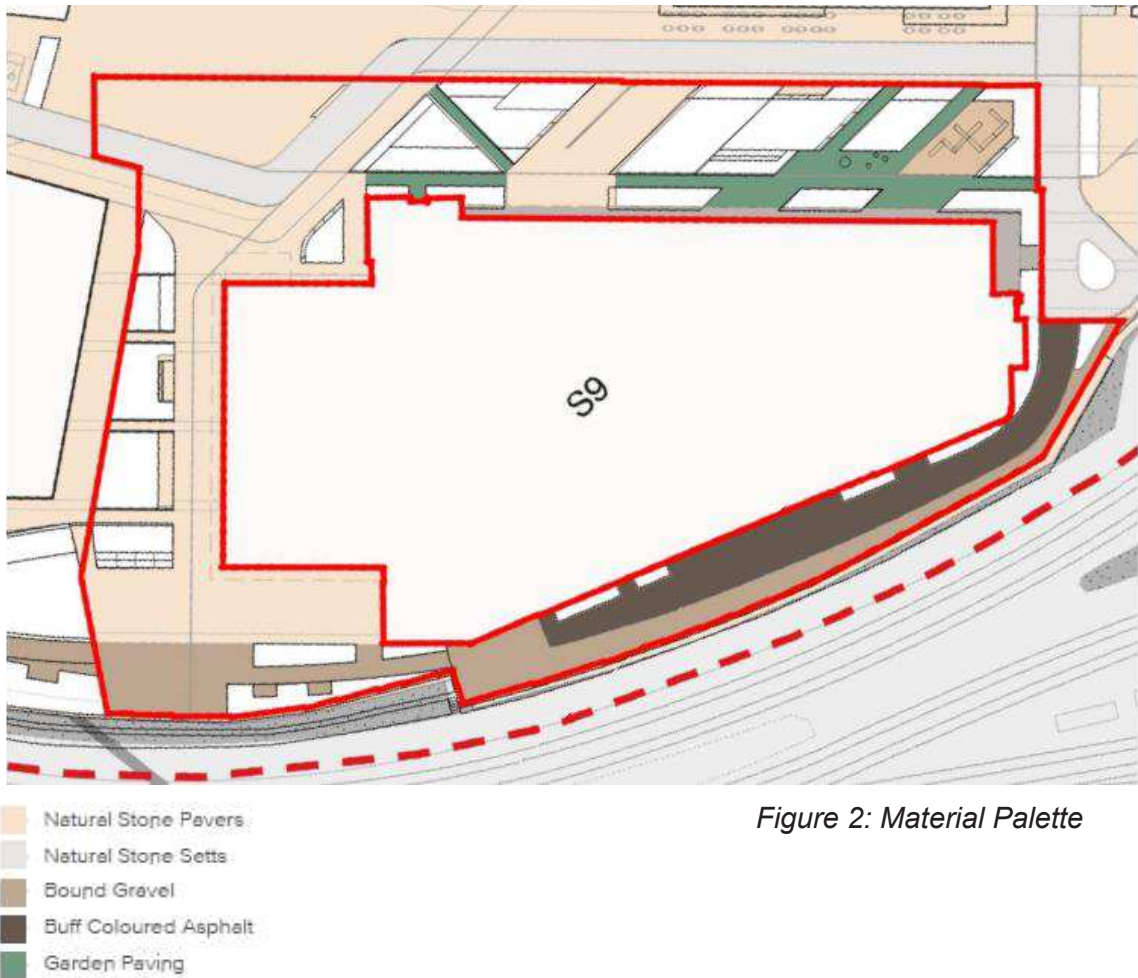
7.7. Gallery Gardens would be divided into four distinct planting zones. The first would be the ‘Rain Gardens’ located on the outer portion of the landscape adjacent to the shared road which would collect rainwater runoff. The planting would be lush and evergreen with a variety of ferns and woodland grasses. The second would be the ‘Woodland Core’ which is the largest area of the garden and the planting in this zone

would feature flowering shrubs. The third zone would be the 'Flowering Woodland Perennials' which would be located centrally in the gardens and provide structure to the space. The final planting zone would be the 'Eastern Embrace' located at the sunniest part of the garden the southern end. This zone would help transition from Endeavour Square and feature a variety of plants with rich eye-catching colour blocks.

- 7.8. Orchard Walk and Bridge Plaza is the character area located to the north of S9 between it and S10. The space would act as a threshold from the proposed Carpenters Land Bridge and the emerging East Bank site to the west. It therefore forms an important pedestrian route through the site and has been designed to reflect this. Bridge Plaza would be a hard-landscaped space adjacent to the bridge deck that would be bordered by a lawn, planting (including perennials, flowering plants and grasses) and trees. The area has been designed to host informal events including buskers, dance and music with the lawn used for informal seating. Orchard Walk would link Bridge Plaza and Redman Place. It would be a main through route and therefore have a generous hard landscaped linear path between the buildings. This would be framed by fruit-bearing trees including apple, plum and sweet cherry.
- 7.9. The S9 service road would be located to the southwest of Building S9. The service road would include a separate pedestrian path along the edge of the retaining wall. The path would be on average 1.8m wide and 1.2m at its narrowest. The path would be protected from the service road by a raised kerb and would be partly planted by a crescent of trees to frame the view of the IQL development from East Bank.
- 7.10. The Endeavour Square interface would be located to the southeast of Building S9. The connection between Endeavour Square and Redman Place would be seamless. The roadway would be flush with the pavement and demarked by contrasting granite kerbs. Bollards would be located within the Redman Place access road between the garden and the façade of the S4 building. Car entry to Redman Place would be controlled by the Estate Management Office and restricted to emergency vehicles and blue badge holders. Taxis will be encouraged to drop off at either end of the development in order to maintain a primarily pedestrian environment within Redman Place. An area of planting to visually separate Redman Place and Endeavour Square would be located in this area. The planting would emphasize the connection between spaces.

#### Materials

- 7.11. The coloured bands of sandstone paving used in Endeavour Square would be continued into Redman Place. Similar to Endeavour Square, the paving would be turned on a diagonal using a rhomboid shape that is approximately 420x600mm. The paving will be robust enough to take vehicular loads and within the shared pedestrian and vehicular route, paving stones will be smaller setts 140x450mm using the same specification and colour tone as Endeavour Square.



7.12. Grooves would be recessed in the stone to take the stainless steel 'rails' that can be seen in the existing public realm and evoke the railway history of the area and assist with wayfinding. These lines would reflect the flow of people bending to lead visitors toward Redman Square and towards East Bank. The steel lines would be approximately 70mm wide and would be flush with the paving. The route that links Westfield Shopping Centre with the proposed Carpenter's Land Bridge will be paved with the same sandstone to match the colour banding of brown and beige sandstone with grey bands between.





Figure 3: Metal 'railway lines' plan

- 7.13. The paving within the Gallery Garden would be in buff coloured sandstone but cut to rhomboid shapes that are approximately 192x590mm. The paving modules in this location would be interspersed with low level shade tolerant planting in order to soften the character of the hard paving for the garden area.
- 7.14. Timber would be used for the furniture within the garden rooms and within the pavilion buildings in the landscape, including the flooring where undercover. A continuous surface of bound gravel is proposed for the hard landscaping within Bridge Plaza to form an appropriate threshold between IQL South and East Bank. Bound Gravel is also proposed for the pedestrian pavement known as Railway Walk which would run adjacent to the service road along the rear of the building.

#### Furniture and Lighting

- 7.15. Furniture would be integrated with planting. Precast concrete benches would line the planting beds which would contain the planters and trees within Garden Gallery. Solid timber benches would also sit atop precast planters in various locations.
- 7.16. The precast concrete benches would be approximately 500x1800mm modules and would be approximately 400mm high with a profiles finish to allow foot space below the bench. There would be straight and curved modules which would form the diamond shape of the planter which would be in keeping with the examples already installed within Endeavour Square. These would be complemented with a series of 'smart benches' (benches which are equipped with mobile device charging and WiFi connectivity) which are proposed for Gallery Gardens and Orchard Walk.
- 7.17. All street furniture including lighting poles would have a bead blasted steel finish or paint finish which would match in tone and gloss level. All bollards would have a stainless-steel finish with a contrasting 150mm band at the top. The proposed furniture, including waste bins and cycle stands, would be in keeping with the design and finishes of the examples already installed within Endeavour Square.
- 7.18. A range of lighting elements are proposed throughout the public realm and would be in keeping with those installed within the existing public realm. These would include a catenary lighting system suspended above along Orchard Walk. Low level planting would be integrated between planting and inground luminaries would provide up

lighting to trees. High column spotlights, lighting integrated within architectural elements and feature lighting to public art and sculptures is also proposed.

## 8. POLICIES & GUIDANCE

### National Planning Policy Framework

- 8.1. The revised National Planning Policy Framework was published in July 2018. This document sets out the Government's planning policies for England including the presumption in favour of sustainable development. It is a material consideration in the determination of all applications. The policies in the NPPF are therefore material considerations in the determination of applications.
- 8.2. The following NPPF sections are relevant to this planning application:
  4. Decision making
  6. Building a strong, competitive economy
  8. Promoting healthy and safe communities
  9. Promoting sustainable transport
  10. Supporting high quality communications
  11. Making effective use of land
  12. Achieving well-designed places
  14. Meeting the challenge of climate change, flooding and coastal change
- 8.3. For the purposes of S.38(6) of the Planning and Compulsory Purchase Act 2004, the adopted 'Development Plan' for this site current is The London Legacy Development Corporation's Local Plan 2015-2031 (July 2015) and the London Plan (2016).

- 8.4. The most relevant policies are listed below:

### The Draft London Plan (December 2017)

- 8.5. The Mayor of London published, for the purpose of public consultation, a draft new London Plan on 29th November 2017. The policies in the draft new London Plan currently have only very limited material weight when making planning decisions. That weight will increase once the new Plan is submitted for its Examination in Public. This report may make reference to policies within the new London Plan where they are directly relevant to the assessment of the application proposal. However, the relevant development plan policies remain those within the current London Plan (March 2016) and the LLDC Local Plan (July 2015).

### The London Plan (March 2016)

Policy 2.13	Opportunity areas
Policy 2.14	Areas for regeneration
Policy 4.1	Developing London's Economy
Policy 4.12	Improving Opportunities for All
Policy 5.2	Minimising Carbon Dioxide Emissions
Policy 5.3	Sustainable Design and Construction
Policy 5.9	Overheating and Cooling
Policy 5.11	Green Roof and Development Site Environs
Policy 5.12	Flood Risk Management
Policy 5.13	Sustainable Drainage
Policy 5.15	Water Use and Supplies
Policy 6.3	Assessing the Effects of Development on Transport Capacity
Policy 6.9	Cycling
Policy 6.10	Walking
Policy 6.13	Parking

Policy 7.1	Building London's Communities and Neighbourhoods
Policy 7.2	An inclusive environment
Policy 7.4	Local character
Policy 7.5	Public realm
Policy 7.6	Architecture
Policy 7.14	Improving Air Quality
Policy 7.15	Reducing Noise and Enhancing Soundscapes
Policy 7.19	Biodiversity and access to nature

London Legacy Development Corporation Local Plan (July 2015)

Policy B.2	Thriving town, neighbourhood and local centres
Policy CI.1	Providing new and retaining existing community infrastructure
Policy SP.3	Integrating the built and natural environment
Policy BN.1	Responding to place
Policy BN.3	Maximising biodiversity
Policy BN.5	Requiring inclusive design
Policy BN.8	Maximising opportunities for play
Policy BN.11	Reducing noise and improving air quality
Policy T.2	Transport Improvements
Policy T.4	Managing development and its transport impacts to promote sustainable transport choices and prioritise pedestrians and cyclists
Policy T.5	Street Network
Policy T.6	Facilitating local connectivity
Policy T.9	Providing for pedestrians and cyclists
Policy SP.5	A sustainable and healthy place to live and work
Policy S.1	Health and wellbeing
Policy S.4	Sustainable design and construction
Policy S.5	Water supply and waste water disposal
Policy S.8	Flood risk
Policy SA3.1	Stratford Town Centre West

Other relevant material considerations:

- Mayor of London – Accessible London (2014)
- Mayor of London –Olympic Legacy SPG (2012)

## 9. CONSULTATIONS

- 9.1 The applications were advertised in the Newham Recorder on 6<sup>th</sup> June 2018 and three site notices were placed in proximity to the site on 14<sup>th</sup> June 2018; letters were sent to statutory and non-statutory consultees notifying of the applications. Details of the consultation responses received are set out in the table and paragraphs below:

Consultee	Response
LB Newham – Planning	No comment to make.
LB Newham – Environmental Health	No objections to the application.
London Fire and Emergency Planning	No objections to the application.
Metropolitan Police	No objection subject to a condition requiring compliance with Secured by Design (or alternatively the Metropolitan Police's written agreement).

	<u>Officer comment</u> : condition included as per recommendation.
Historic England (Archaeology)	No objections to the application.
Historic England (Heritage)	No objections to the application.
Natural England	No comments to make on the application.
Transport for London	No objections to the application.
<p>In addition, the following were consulted and no responses have been received:</p> <p>DLR, EDF Energy, East London Waste Authority, GLA, British Gas, Health Protection Agency, LB Newham (Highways, Transport, Waste, Public Space &amp; Landscape) LLDC - Park Operation &amp; Venues, LLDC – Security and Park Safety, London and Continental Railways Ltd, London Power Networks, London Underground Infrastructure Protection, Sus Trans, Network Rail, National Grid, London Power Networks, London Ambulance Service, Lee Valley Regional Park Authority, Thames Water Authority, Transport for London.</p>	

PPDT’s Environmental Consultant (Arup)

- 9.2 PPDT’s Environmental Consultant (Arup) were consulted on the application and originally sought clarification on landscape and ecology issues. Further information was provided by the applicant on these issues and considered acceptable. It is also noted that Arup originally raised concerns with respect to wind issues including the methodology used in the assessment and the impact of wind within the public realm. Additional information has been subsequently provided and considered acceptable subject to conditions requiring an additional wind study to building entrances and details of any wind mitigation measures to be installed in the public realm. These conditions are included as per the recommendation.

PPDT’s Transport Consultant (Jacobs)

- 9.3 PPDT’s Transport Consultant (Jacobs) were consulted on the application and raised no objections following clarification over the proposed shared surfacing and cycle parking.

LLDC Landscape Design

- 9.4 LLDC’s Landscape Design Officer originally sought clarification on a number of issues including surface materials, planting and street furniture. Additional information was provided and considered acceptable. No objections are raised subject to conditions securing detailing of planting and tree pits. These conditions are included as per the recommendation.

Quality Review Panel

- 9.5 The Quality Review Panel reviewed the landscaping strategy for the wider revised IQL South Masterplan on 14<sup>th</sup> June 2018 – including the details for the revised public realm around Building S9. The panel were supportive of the proposed landscaping scheme and consider that it has evolved well from previous proposals. The panel were supportive of the strategic decisions taken in developing the landscape and public realm strategy and were encouraged by its richness and diversity. The panel repeated their support for the railway line feature and recommended that it be followed throughout IQL South.

## 10. ASSESSMENT OF PLANNING ISSUES

### Principle of Development

- 10.1. The principle of development within IQL South is considered to be established on the basis that the site benefits from outline planning permission by way of the Stratford City OPP (ref: 10/9064/EXTODA). Development of the site is required to be within the approved parameters for Zone 2, including those contained within the approved Zonal Masterplan (ZMP) and accompanying Site Wide Strategies. This includes details of key open spaces such as Carpenter's Square, together with key access arrangements such as circulation and access, pedestrian movements and the road network. This reserved matters application seeks to demonstrate compliance with this parent consent. The parent planning permission includes requirements for the provision of landscaping and public realm throughout the IQL South site and accordingly the principle of the development contained within this application is acceptable.
- 10.2. The proposed development will contribute towards the achievement of strategic objectives contained within the London Plan and the localised regeneration objectives of the LLDC Local Plan. This includes compliance with Policy SA.3.1 (Stratford Town Centre West) which requires key connections to be enhanced across the area, that connectivity routes within private ownership should maintain the format and appearance of public space, and that points where key connections meet shall be gateways for enhancement.
- 10.3. It is acknowledged that an application for reserved matters for the public realm within the area known as Carpenter's Square was previously approved in July 2017 under application ref: 16/00683/REM. However, this approval was not implemented and the landscaping details have been revised to address the proposed use of the ground floor of Building S9 as well as respond to the emerging revised masterplan for the rest of the IQL South site. Whilst one single application was previously submitted in relation to the public realm around Buildings S4 and S9, the revised landscaping details are split over two applications – this application and application ref: 18/00354/REM for Building S4. From a procedural point of view, officers consider that this approach is acceptable and remains within the spirit of the parent planning permission. The details for the landscaping surrounding Building S4 have been submitted and the two applications can be read in conjunction to demonstrate a fully integrated and seamless piece of public realm – now known as Redman Place.
- 10.4. This split approach across two separate applications is also deemed to be acceptable on the basis that S9 is running to a different construction programme to S4. The construction works for Building S9 are now significantly advanced with the building likely to be occupied in summer 2019. As such there is a need for certainty around procurement and to be able to deliver the works so that the building can operate with a significant area of public realm in place when it opens next year.

### Overall Conformity with Parameter Plans, Stratford City OPP and Zonal Masterplan (Including Provision of Open Space)

- 10.5. Condition A4 of the Stratford City OPP requires that all reserved matters applications be submitted in accordance with the approved ZMP. Condition B8(a), requires the applicant to demonstrate that each reserved masterplan accords with the Zonal Masterplan for that Zone, which the applicant has done in the form of a Zonal Masterplan Conformity Statement
- 10.6. In terms of the quantum of public open space, the application is considered to be consistent with the area requirements for Carpenters Square as set out in the Stratford City OPP s.106 Agreement and the approved ZMP. Indeed the overall amount of open space within this reserved matters application is considered to

significantly exceed the minimum requirements of the s.106 and approved ZMP as demonstrated in the following table:

s.106 Requirement		Approved Zonal Masterplan		Reserved Matters Proposals
s.106 Location	Area	Location	Area	Area
Carpenter's Square	0.286ha	POS1	0.286ha	4,765 sqm

*Table 1: Table of proposed area of open space against minimum requirements*

- 10.7. The 4,765 sqm would consist of 1,183 sqm of planted areas in Gallery Gardens and Orchard Walk, together with 3,582 sqm of hardscaping across the remainder of the site. It should also be noted that the above figures relate solely to this reserved matters application. Carpenter's Square, now known as Redman Place, also includes public realm which is included as part of the Building S4 reserved matters (superstructure and landscaping) application. The total public realm provided around Buildings S4 and S9 would be 5,388 sqm, significantly in excess of the minimum requirements envisaged for the area known as Carpenter's Square within the original permission.
- 10.8. It is also acknowledged that this application follows on from previous reserved matters application for Building S9 (ref: 16/00671/REM and 16/00672/REM). Neither the building footprint or the red line boundary for the application has changed and as such it is considered that the same amount of open space is provided under this reserved matters application albeit the design and layout of soft and hard landscaping has changed.
- 10.9. The location and character of the public realm is also considered to be consistent with the principles of the approved ZMP, Open Space Strategy and s106. Gallery Gardens and Orchard Walk would provide a variety of plants and trees whilst the layout of the public realm would also provide key pedestrian connections, including those to the proposed Carpenter's Land Bridge.
- 10.10. The proposals would include a departure from the approved ZMP with respect to the allocation of vehicular plot access. The detail of this is discussed in para. 10.26 however the scale of the departure is minor and is considered to be acceptable in principle noting that Condition A4 of the Stratford City OPP permits minor deviations from the approved ZMP.

Landscaping and external appearance

- 10.11. Conditions Q4 and B10 of the Stratford City OPP requires reserved matters applications to specify:
- The location, type, size and species of planting;
  - Details of hard landscaping proposals, including a material palette which includes colours and transitions associated with paved areas and pedestrian routes;
  - The location and strategy for street furniture, lighting and signage; and
  - A strategy for features of artwork.

- 10.12. The proposed planting strategy for the public realm is considered to respond to the character use and nature of each character area within the red line boundary. Gallery Gardens would be the main landscaped space within Redman Place. It would be a linear area of soft landscape with linear planting zones which would respond to the ZMP, the shape of the space between S4 and S9 and also the ground floor gallery/exhibition use of S9. The planting would provide interest throughout seasons through varied bloom and foliage texture. The planting surrounding Bridge Plaza, including the lawn areas, would respond to the sunny character of this part of the site and the proposed use for events. The trees along Orchard Walk would be mature so to suitably respond to the scale of the buildings whilst the fruit bearing specimens would ensure seasonal interest and important habitats for birds and insects. The line of trees to the rear of the building would characterise this part of Railway Walk and provide visual interest when viewed from East Bank and the F10 bridge. As per the comments from LLDC's Landscape Design Officer a condition is attached to secure detail of all planting including trees/lawns/planting plans (including densities). Final details of all tree pits will also be reserved via condition noting concern over the lack of organic material within the indicative tree pits.
- 10.13. With respect to materials, a palette of hard landscape materials is proposed, comprising a variety of sandstone, bound gravel, timber paving and permeable paving. The predominant material within the reserved matters application would be sandstone paving throughout Redman Place, Orchard Walk and parts of Bridge Plaza. The colour of the sandstone pavers would be in keeping with those already installed elsewhere in the site including Endeavour Square and Turing Street. The size of the paving would change to respond to different character areas with the smallest sandstone sets being used within Gallery Gardens and interspersed with low level planting to soften the character of these more intimate spaces. The sets used within the shared vehicle/pedestrian areas would be robust to ensure they can take heavy vehicular loads.
- 10.14. Bound gravel is proposed in Bridge Place and Railway Walk. The use of this material in Bridge Place is intended to provide a suitable threshold into the site and is envisaged to provide a visual connection with East Bank on the opposite side of Carpenter's Land Bridge. Full details of all hard landscaping within the site, including samples, are required to be submitted and approved by the Local Planning Authority under Condition B10 of the parent consent. An informative is attached to advise that the details of the bound gravel as part of this future submission will be expected to match that used within the East Bank site.
- 10.15. The use of the metal 'rail' lines throughout the site is welcomed as per the comments from QRP. These features would help to provide consistency with the public realm already delivered within IQL South and would also assist with wayfinding. The metal rails also evoke memories of the Stratford Railway Works and would help provide a distinct sense of place.
- 10.16. With regards to street furniture, the submitted details are considered to be acceptable with respect to their location, design and functionality. Street furniture would be integrated within the landscaping as much as possible thereby helping to reduce visual clutter within the public realm. This includes precast concrete benches which line planting beds and solid timber benches which sit on top of precast planters to form seats. Lighting fixtures, cycle stands and waste bins would all be in keeping with the furniture already installed within the site.
- 10.17. The submission also includes indicative details of features including smart benches and outdoor workspace shelters. The principle and location of these structures are considered to be acceptable and will assist in activating the public realm, however, full details of their design will be reserved by condition to ensure an acceptable appearance.

- 10.18. In terms of the lighting strategy, illumination would be achieved from a variety of sources including catenary lighting, pole mounted lighting, lighting fixtures to buildings, integrated lighting within architectural features, uplighting and 'Gobo' projections. The proposed lighting is considered to respond successfully to the individual character areas and provide an appropriate level of illumination depending on use. The lighting proposals would provide general illumination and assist with wayfinding and the creation of a sense of place. It has also been designed to maintain a safe public realm and all times of the night. The lighting proposals are considered to be sensitively designed to minimise light spill and ensure there would be no detrimental impact on habitat zones.
- 10.19. With respect to play, the southern end of Gallery Garden has been identified as an area which is suitable for play purposes. The submission provides an indicative play/art piece which would assist with wayfinding, provide activity in this important node within the site and attract people into Gallery Gardens. The principle of this is considered to be acceptable and a condition is recommended to ensure full details are secured by condition.
- 10.20. A condition is also recommended with respect to ongoing management arrangements for the public realm. This will be secured through an Estate Management Plan as required under Condition 7.

#### Inclusive Design and Access

- 10.21. The details for access and circulation are considered to be acceptable and would comply with the requirements of the relevant parameter plans from the Stratford City Outline Planning Permission and the key principles of the approved Zonal Masterplan. The public realm has been designed to accommodate key connections through the site – including a link between Westfield Avenue and the proposed Carpenter's Land Bridge through to the East Bank site. The landscaping and buildings have been designed to assist with wayfinding along these key routes, including the use of the metal 'railway' lines which would be 70mm in width and flush with the ground. The lines, which have already been delivered within the existing IQL public realm and are considered to emphasise key pedestrian routes through the site.
- 10.22. It is also noted that Redman Place would accommodate the required secondary road through the site which would be shared between vehicles, pedestrians and cyclists. Vehicle access to this road would be controlled by drop down bollards and would be limited to essential vehicles such as emergency vehicles or blue badge vehicles to ensure that the space would remain predominantly pedestrian. Vehicular access into the estate would be undertaken by estate management via their onsite security control rooms.
- 10.23. The scheme does not propose any vehicular parking albeit two temporary blue badge spaces will be provided during the phased period as described in para 10.28. These will be relocated once the construction works for Building S4 are completed and the permanent landscaping installed. The principle of no vehicular parking is acceptable noting the high PTAL rating (6b) of the site and the negative effect that parking would have within the public realm. No motorcycle parking is provided within this reserved matters application; however, 8 motorcycle parking spaces would be provided within Building S9 as per the previous approval.
- 10.24. With respect to cycle parking, this application makes provision for 70 cycle parking spaces within the public realm. These would be largely within Orchard Walk and Bridge Plaza and would be provided in the form of Sheffield type stands which would be in keeping with the existing stands within the completed areas of IQL public realm. The provision of 70 spaces is considered to be acceptable noting that the parent planning permission does not have any minimum requirements for cycle parking for individual buildings within IQL.



- 10.25. As per the previous applications for Building S9 (ref: 16/00671/REM) and Carpenter's Square (ref: 16/00683/REM), the building would be serviced from the rear of the site via the S9 Service Road. This in turn would be accessed from Westfield Avenue via Turing Street. Whilst this is a route for servicing vehicles it is considered that the space has been designed to make suitable provisions for pedestrians noting that an approximately 1.8m wide pedestrian path, which would be separated from the service road by a raised kerb and a row of trees, would allow adequate circulation along this part of Railway Walk.
- 10.26. In comparison to the ZMP the primary pedestrian connection from Westfield Avenue to the bridge would be moved from between Buildings S4 and S5 to between Buildings S3 and S4 where there would be no conflict with service vehicles and is a positive change from the previous consent. As per the previous reserved matters approval for Building S9, there would be a further variation from the approved ZMP in that the vehicular route along the entirety of the retaining wall to the rear of Building S9 is not being sought. This is in order to reduce any potential conflict between a vehicular route and the bridge safeguarding area and is therefore a welcome amendment from the parent planning permission.
- 10.27. The proposed landscape design is also considered to take account of inclusive design issues noting that the proposals were presented to LLDC's Built Environment Access Panel (BEAP) and considered to be acceptable subject to minor comments which have been addressed within the submission. The public realm would be appropriately graded to ensure gentle gradients throughout the site with all pedestrian routes considered to be inclusive and designed to be accessible for everyone. Following discussion at BEAP, shared surfaces would have a width of 4.8m to include physical demarcation and contrasting features to delineate wheelchair and pedestrian routes within a 'comfort space' – a safe zone which would be exclusively used by pedestrians. The BEAP session also resulted in an accessible taxi drop off being relocated to the south end of Turning Street. The new location would allow for a raised drop off area within the shared space in order to allow easy ramp deployment by taxis.
- 10.28. The landscaping would be phased in order to provide access to buildings and a suitable amount of public realm during ongoing construction with associated sites. The first stage of this would be constructed by June 2019 with a temporary access route to Building S9. This route would be provided for within the area designated for rain garden planting within Gallery Gardens. Taxis and emergency vehicles would be permitted to enter the site when required with a temporary turning area provided in front of the Building S9 entrance. Two temporary blue badge parking spaces would be provided at the end of the temporary road. Once the works for Building S4 are completed, the landscape surrounding the building, including the shared surface road, would be installed. The rain garden planting can then be completed allowing for the permanent landscape condition of Redman Place in 2021.

#### Environmental Impacts

- 10.29. The Stratford City OPP establishes the height of buildings in the Stratford City area and conditions were attached to ensure that the daylight levels within blocks would be considered further at Reserved Matters Stage. Conditions V1 and V2 of the Stratford City OPP require the development to be designed according to the BRE 'Site Layout Planning for Sunlight and Daylight: A Guide to Good Practice' and the guidance in BS8206: Part II and the Applications Manual: Day Lighting and Window Design – Lighting Guide LG10 (1999).
- 10.30. An assessment has been made in relation to the overshadowing impact on the public realm surrounding Building S9. It is considered to demonstrate that in excess of 50% of the entire of public realm surrounding Building S9 would receive at least 2 hours of direct sunlight exposure on 21<sup>st</sup> March and would therefore be compliant

with BRE standards. The areas within Orchard Walk, Bridge Plaza and Railway Walk would receive the most direct sunlight given they would be unobstructed by buildings. These areas have been designed to take advantage of this direct sunlight with lawns, area for events and external spill-out space from the commercial space within Building S9.

- 10.31. It is acknowledged that Redman Place would be overshadowed during significant parts of the year. However, the height and footprint of the building has not changed since the reserved matters approval for this building and the extent of overshadowing remains consistent with the previous approval. It is acknowledged that the layout and design of Redman Place has changed under these proposals; however, these are considered to be improvements noting that the eastern side of Redman Place, adjacent to Building S4 has been designed to accommodate spill out areas for ground floor cafes and restaurants to take advantage of the good levels of sunlight received in these locations during summer months and to help activate the public realm.
- 10.32. Gallery Gardens, would be located to the western side of Redman Place, would be more heavily overshadowed throughout the year by Building S9. However, the landscaping has been designed to deal with this condition through the use of shade tolerant planting. The art/play structure at the southern end of Gallery Gardens would be located in this position given that it would be the sunniest part of Gallery Gardens with good levels of sunlight in the morning and early afternoon.
- 10.33. With respect to wind, a wind assessment has been undertaken in accordance with Conditions V3 and V4 of the Stratford City OPP. It has examined the wind safety and comfort conditions across a number of points of interest within the public realm including locations which are likely to be used for circulation and locations which are likely to be used for leisure/sitting. PPDT's environmental consultants have reviewed the information and consider that it demonstrates acceptable wind conditions within the areas that have been tested. They have suggested a condition requiring further assessment of wind conditions around building entrances and a condition requiring detail of wind mitigation measures within the public realm.

## **11. HUMAN RIGHTS & EQUALITIES IMPLICATIONS**

- 11.1. Members should take account of the provisions of the Human Rights Act 1998 as they relate to the application and the conflicting interests of the Applicants and any third party opposing the application in reaching their decisions. The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report. In particular, Article 6 (1), of the European Convention on Human Rights in relation civil rights and a fair hearing; Article 8 of the ECHR in relation to the right to respect for private and family life and Article 1 Protocol 1 of the ECHR in relation to the protection of property have all been taken into account.
- 11.2 In addition the Equality Act 2010 provides protection from discrimination in respect of certain protected characteristics namely: age, disability, gender reassignment, pregnancy and maternity, race, religion, or beliefs and sex and sexual orientation. It places the Local Planning Authority under a legal duty to have due regard to the advancement of equality in the exercise of its powers including planning powers. Officers have taken this into account in the assessment of the application and Members must be mindful of this duty inter alia when determining all planning applications. In particular Members must pay due regard to the need to:
  - Eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Equality Act;

- Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and
  - Foster good relations between persons who share a relevant protected characteristic and persons who do not share it
- 11.3 Officers are satisfied that the application material and Officers' assessment has taken into account these issues. Officers consider that the effects of the proposal would not be so adverse as to cause harm and justify a refusal of consent or permission.

## 12. CONCLUSION

- 12.1. The applications are in conformity with the Stratford City OPP and Zonal Masterplan for Zone 2, subject to only minor deviations, and do not generate any new or different likely significant environmental effects.
- 12.2. The proposed design and appearance of the landscaping surrounding S9, subject to appropriate conditions, is considered to be of a high quality. The public realm has been designed to take account of inclusive design and address transportation issues and it would also benefit from good levels of sunlight and an acceptable wind performance subject to securing any recommended mitigation measures by condition.

## 13. PLANNING CONDITIONS

### Approved Plans

- 1) The development hereby permitted shall be installed and displayed in accordance with the following details:

(Plan and documents reference numbers to be added)

together with the description of the proposal contained in the application and any other plans, drawings, documents, details, schemes or strategies which have been approved in writing by the Local Planning Authority pursuant to these conditions.

Reason: To ensure that all works are properly implemented and retained.

### Detailed Drawings

- 2) Prior to commencement of the relevant part of the public realm works hereby permitted, detailed drawings (at scales of 1:5, 1:10 or 1:20 where appropriate) of the following features shall be submitted to and approved in writing by the Local Planning Authority. The development hereby permitted shall be built in accordance with the approved details and retained thereafter. The following details are required:

- a) Amphitheatre: Layout, seating and structures;
- b) Outdoor working space: Layout, seating and structures;
- c) Precast benches;
- d) Play/Art structure and playable space;
- e) Timber bench tops;
- f) Smart benches;
- g) Bollards;
- h) CCTV/WIFI columns;
- i) Catenary lighting system;
- j) Lighting columns;

- k) Drinking fountain;
- l) Wayfinding signage; and
- m) Cycle racks.

Reason: To ensure that the Local Planning Authority is satisfied that the details and approach adopted will secure high quality design and detailing in accordance with London Plan Policy 7.6 and Local Plan Policies BN.1, BN.3, BN.5, BN.8 and BN.10.

#### Planting Details

- 3) Prior to the commencement of the relevant part of the public realm works hereby permitted, a landscaping scheme shall be submitted to and approved in writing by the local planning authority. The development hereby permitted shall be thereafter carried out in accordance with the approved details within 6 months of the date of occupation of Building S9. All tree, shrub and hedge planting included within the above specification shall accord with BS3936:1992, BS4043:1989 and BS4428:1989 (or subsequent superseding equivalent) and current Arboricultural best practice. The submitted details are expected to demonstrate the following:
- a) The quantity, size, species, position, density and the proposed time of planting of all trees, lawns and planting.
  - b) Details of all tree pits to include organic material within the tree pit/connected system.

Reason: In order to ensure high quality soft landscaping in and around the site in the interests of the ecological value of the site and in the interests of visual amenity in accordance with London Plan Policy 7.6 and Local Plan Policies BN.1, BN.3, BN.5, BN.8 and BN.10.

#### Wind Mitigation Study

- 4) Prior to the commencement of development hereby permitted, a further wind impact assessment (which takes into account the findings of the FD Global IQL Wind Microclimate Assessment: Phase 3 Public Realm Redman Place - 10<sup>th</sup> May 2018) demonstrating suitable wind conditions at all Building S9 ground floor entrances and areas of outdoor seating during the temporary and completed landscaping phases, shall be submitted to approved in writing by the Local Planning Authority.

Reason: In order to ensure a high quality of design and public realm with regards to wind safety and comfort in accordance with Local Plan Policy BN.10.

*Pre-commencement justification: to ensure appropriate measures are taken to ensure appropriate wind conditions.*

#### Wind Mitigation Measures

- 5) Prior to the commencement of development hereby permitted, details of any wind mitigation measures (including any required during temporary landscaping phases) shall be submitted to and approved in writing by the Local Planning Authority. The submitted details shall demonstrate that the proposed mitigation results in acceptable conditions in terms of safety and comfort within and around the development. The development shall be carried out in accordance with the approved details and mitigation measures retained thereafter.

Reason: In order to ensure a high quality of design and public realm with regards to wind safety and comfort in accordance with Local Plan Policy BN.10.

*Pre-commencement justification: to ensure appropriate measures are taken to ensure appropriate wind conditions.*

#### Secure by Design

- 6) The development shall be constructed and operated thereafter to 'Secured by Design Standards' or to an alternative security standard (based on 'Secured by Design' principles) as agreed with the Metropolitan Police. A certificate of accreditation to Secured by Design Standards (or confirmation in writing from the Metropolitan Police to an alternative standard) shall be submitted to the Local Planning Authority for approval in writing prior to the first use of the public realm.

Reason: To ensure that the development maintains and enhances community safety in accordance with London Plan Policy 7.3.

#### Estate Management

- 7) Six months prior to the first use of the public realm hereby approved, a site-specific Estate Management Plan shall be submitted to the Local Planning Authority for approval. The site-specific Estate Management Plan shall include details of the roles and responsibilities to be held by an Estate Management Company for the site, and the location of the Estate Management Company office. The details approved in the submitted Estate Management Plan shall be put in place as part of the operation of the site, prior to first use of the public realm.

Reason: In order to ensure that the outside spaces on the site and the new public realm is maintained and managed appropriately.

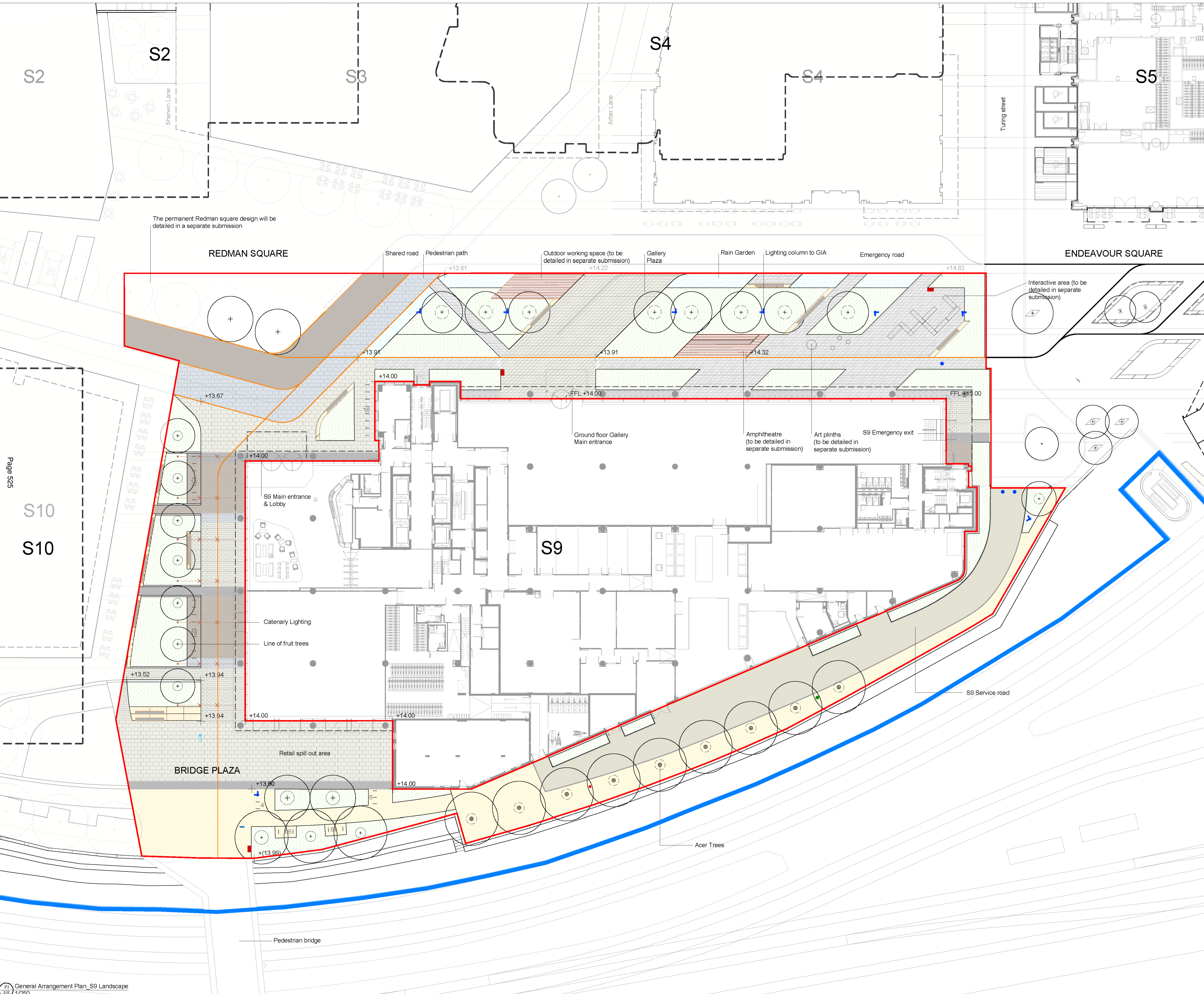
### **14. PLANNING INFORMATIVES**

- 1) You are advised that the London Fire Brigade have advised that sprinklers are used within any structures/pavilions within the public realm. Any queries in relation to this should be addressed to Andrew Reeves ([FRS-AdminSupport@london-fire.gov.uk](mailto:FRS-AdminSupport@london-fire.gov.uk) or 020 8555 1200).
- 2) In relation to Condition 6, you are advised to seek the advice of the Metropolitan Police Service Designing Out Crime Officers (DOCOs). The services of MPS DOCOs are available free of charge and can be contacted via [docomailbox.ne@met.police.uk](mailto:docomailbox.ne@met.police.uk) or 0208 217 3813.
- 3) You are reminded that Condition B10 of the parent consent (ref: 10/90641/EXTODA) requires full details of hard landscaping materials to be submitted and approved by the Local Planning Authority prior to their use within the site. It is expected that any application pursuant to this condition includes details of the Resin Bound Gravel to be used within Bridge Plaza and that this material shall match the same material used within the East Bank site.

### **Appendices**

- Appendix 1: General Arrangement Plan
- Appendix 2: Phasing Plan
- Appendix 3: Hardscape Plan
- Appendix 4: Tree Plan

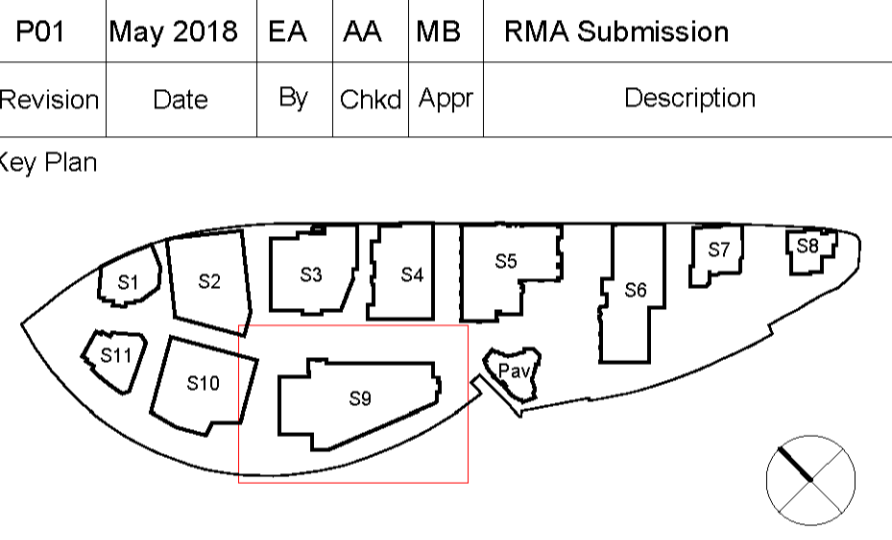
- Appendix 5: Planting Plan
- Appendix 6: Landscape Character Areas
- Appendix 7: Landscape Spaces and Uses
- Appendix 8: Gallery Gardens Image
- Appendix 9: Bridge Plaza Image
- Appendix 10: Arrival from Endeavour Square Image
- Appendix 11: QRP Report



- Legend:**
- Boundary:** IQL Boundary line (Blue line), RMA Boundary (Red line)
  - Levels:** Proposed levels (+14.30), Existing levels (+14.30)
  - Building Footprints:** Emerging building footprints (Dashed line), Consented ZMP Buildings (Dotted line)
  - SOFTSCAPE:** Proposed tree (Circle with cross), Planted areas (Green stippled), Rain garden (Blue stippled)
  - FURNITURE:** Indicative area for outdoor working space/ amphitheatre (Brown hatched), Precast benches (Yellow hatched), Timber bench top (Brown solid), Bollard (Blue dot), Feeder Pillar (Orange rectangle), CCTV Column (Red dot), WIFI/CCTV Column (Green dot), Bin (Red square), Catenary lighting (Red X), Catenary lighting poles- by GIA Equation (Red circle), Catenary lighting- by GIA Equation (Red cross), Lighting columns- by GIA Equation (Blue triangle)
  - HARDSCAPE:** Metal lines (Orange line), Rhomboid pavers (Grey hatched), Pavers (White hatched), Bound gravel (White stippled), Setts (Grey stippled), Asphalt (Black stippled), Pebbles (Grey stippled), Drinking fountain (Blue square), Wayfinding (Blue line), Cycle racks (Blue line), 70 Total number of cycle (Blue line)

Note:  
-For detailed paving dimensions refer to TIQ-L-GUS-SW-S9-DR-90-P300

Revision	Date	By	Chkd	Appr	Description
P01	May 2018	EA	AA	MB	RMA Submission



Landscape Architect: **Gustafson Porter + Bowman**

Architect: **Rogers Stirk Harbour + Partners**

Engineer: **BURO HAPPOLD ENGINEERING**

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292 IQL - International Quarter - London

Drawing Title

**General Arrangement Plan S9 Landscape- For Approval**

Drawing Number

**TIQ-L-GUS-SW-S9-DR-90-P101**

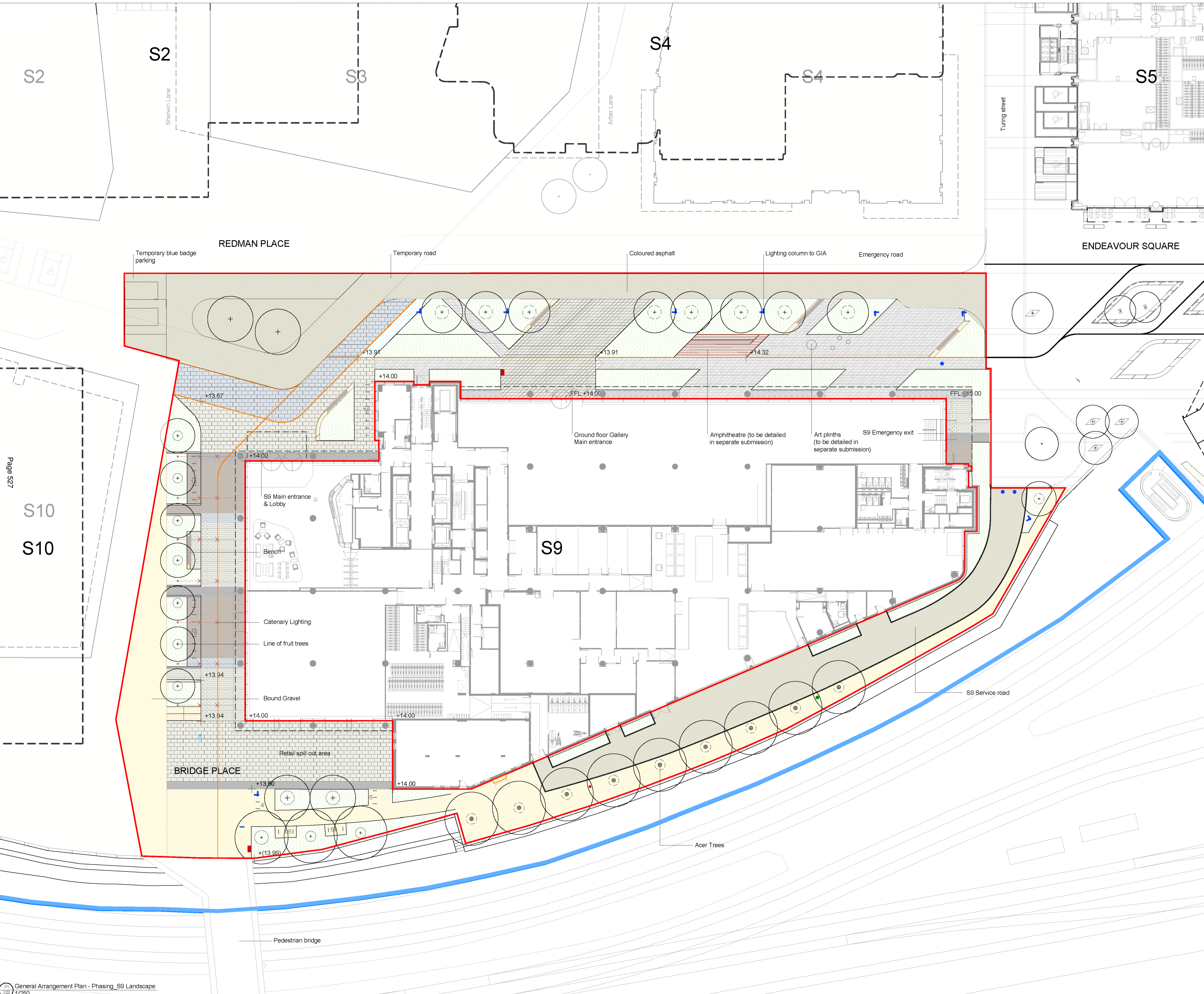
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EA	AA	MB

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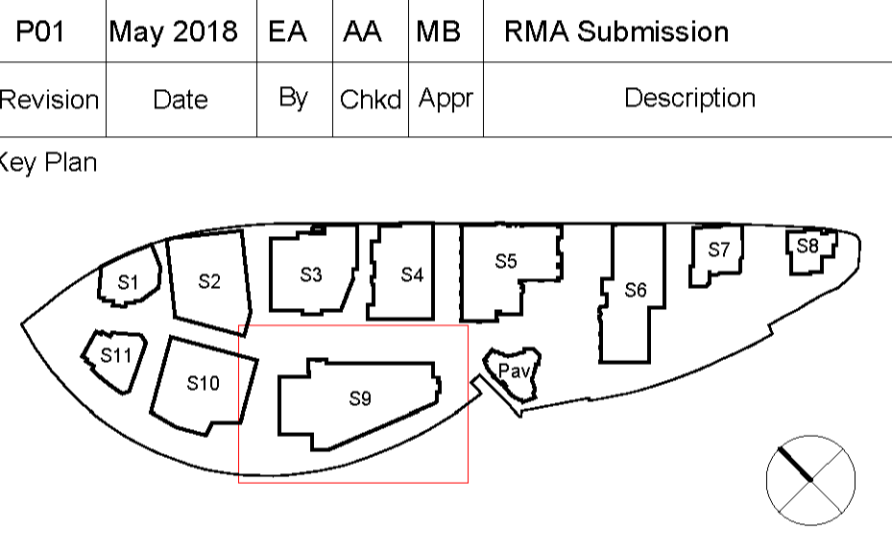




- IQL Boundary line
- RMA Boundary
- Proposed levels
- Existing levels
- Emerging building footprints
- Consented ZMP Buildings
- SOFTSCAPE
- Proposed tree
- Planted areas
- Rain garden
- FURNITURE
- Indicative area for outdoor working space/ amphitheatre
- Precast benches
- Timber bench top
- Bollard
- Feeder Pillar
- CCTV Column
- WIFI/CCTV Column
- Bin
- x Catenary lighting
- o Catenary lighting poles- by GIA Equation
- + Catenary lighting- by GIA Equation
- + Lighting columns- by GIA Equation

Note:  
For detailed paving dimensions refer to TIQ-L-GUS-SW-S9-DR-90-P300

Revision	Date	By	Chkd	Appr	Description
P01	May 2018	EA	AA	MB	RMA Submission



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**General Arrangement Plan\_Phasing S9 Landscape- For Approval**

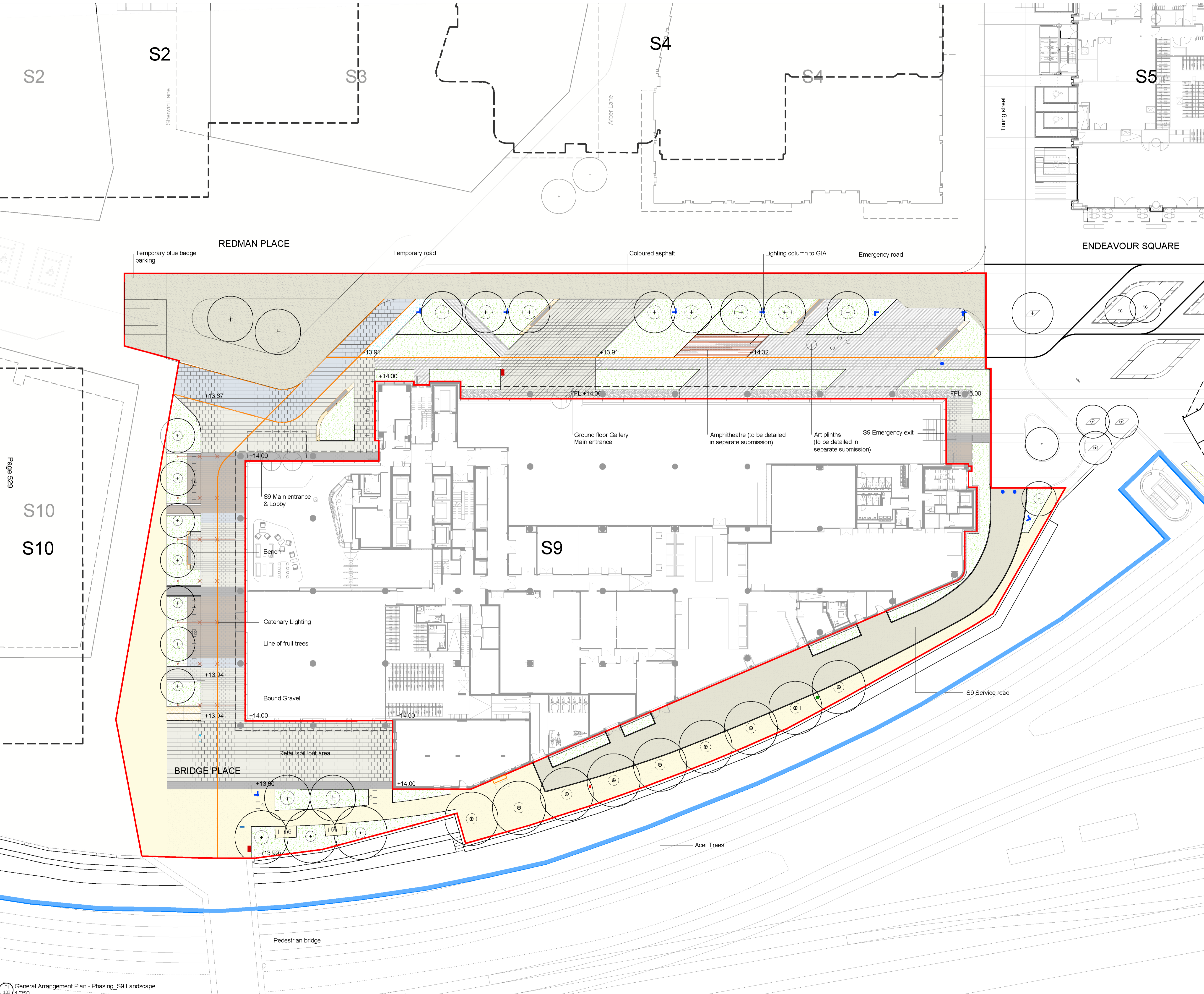
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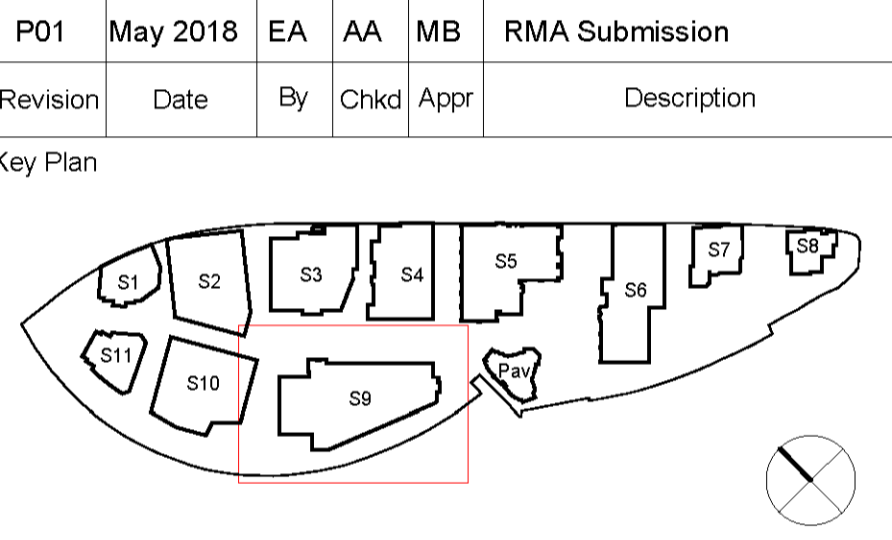
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- Legend**
- IQL Boundary line
  - RMA Boundary
  - Proposed levels
  - Existing levels
  - Emerging building footprints
  - Consented ZMP Buildings
  - SOFTSCAPE
  - Proposed tree
  - Planted areas
  - Rain garden
  - FURNITURE
  - Indicative area for outdoor working space/ amphitheatre
  - Precast benches
  - Timber bench top
  - Bollard
  - Feeder Pillar
  - CCTV Column
  - WiFi/CCTV Column
  - Bin
  - Catenary lighting
  - Catenary lighting poles- by GIA Equation
  - Catenary lighting- by GIA Equation
  - Lighting columns- by GIA Equation
- HARDSCAPE**
- Metal lines
  - Rhomboid pavers
  - Pavers
  - Bound gravel
  - Setts
  - Asphalt
  - Pebbles
  - Drinking fountain
  - Wayfinding
  - Cycle racks
  - 70 Total number of cycle spaces provided

Note:  
For detailed paving dimensions refer to TIQ-L-GUS-SW-S9-DR-90-P300

Revision	Date	By	Chkd	Appr	Description
P01	May 2018	EA	AA	MB	RMA Submission



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**General Arrangement Plan\_Phasing S9 Landscape- For Approval**

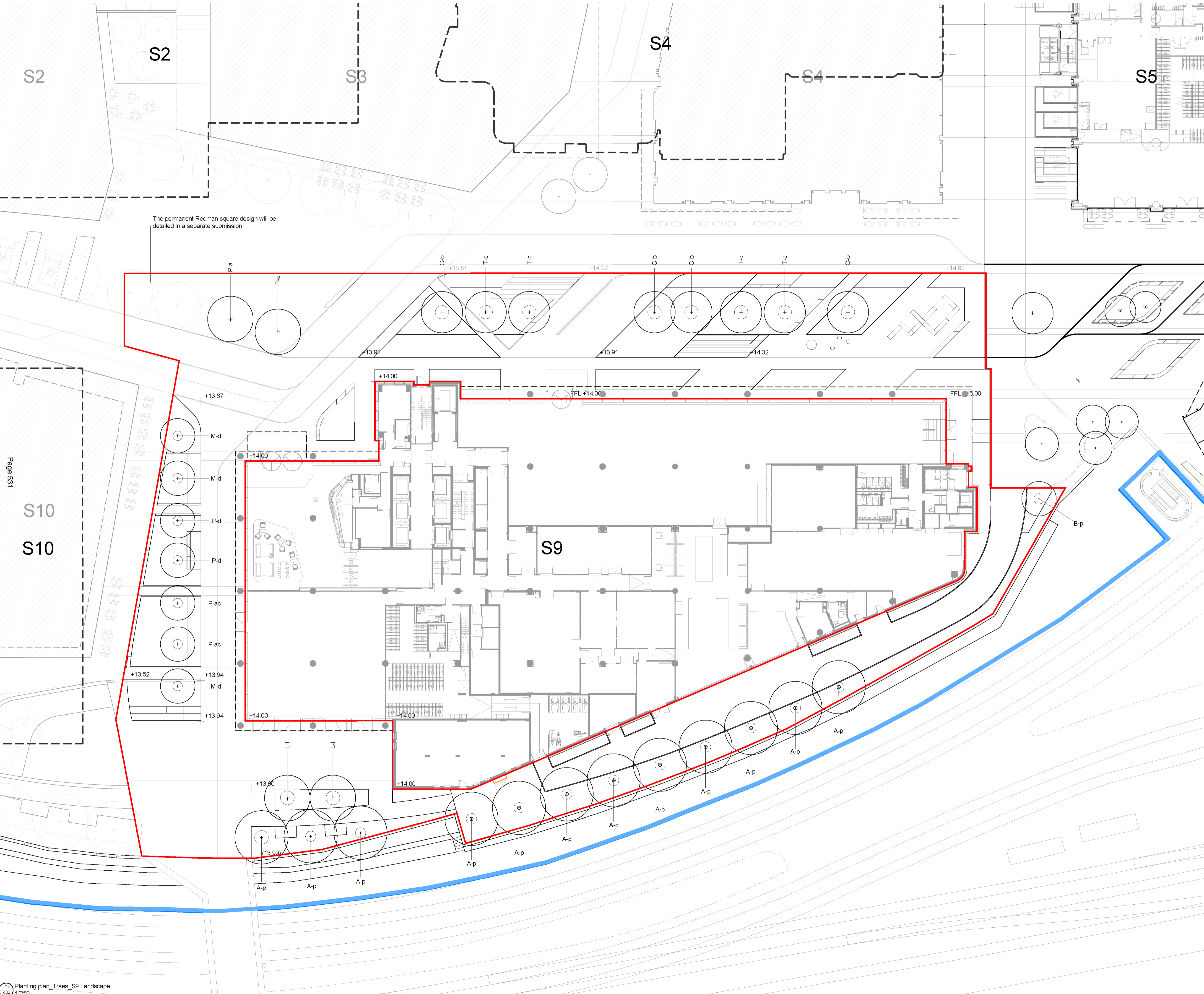
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- IQL Boundary line
- RMA Boundary
- + Proposed levels
- + Existing levels
- Emerging building footprints
- Consented ZMP Buildings
- TREES
- Proposed tree
- A-p Acer platanoides height 12-14m
- B-p Betula Pendula height 10-12m
- C-b Carpinus betulus height 10-12m
- T-c Tilia cordata height 10-12m
- M-d Malis domestica fruit cultivars height 6-8 m
- P-a Prunus avium height 10-12 m
- P-ac Prunus avium fruit cultivars height 6-8 m
- P-d Prunus domestica fruit cultivars height 6-8 m
- L-t Liliodendron tulipifera height 8-10 m

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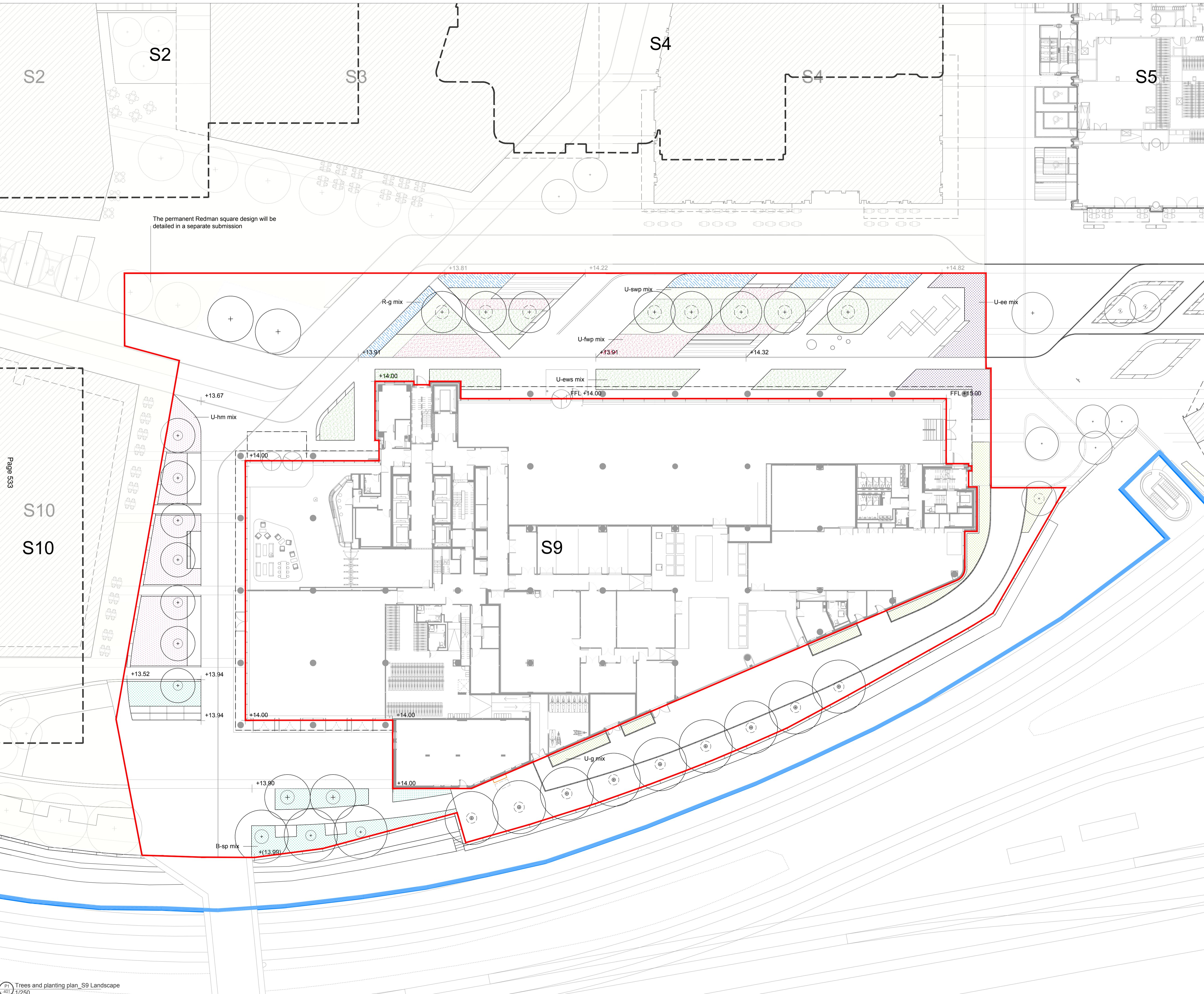
**Planting plan\_Trees**  
S9 Landscape- For Approval

Drawing Number

**TIQ-L-GUS-SW-S9-DR-90-P400**

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The permanent Redman square design will be detailed in a separate submission

- IQL Boundary line
- RMA Boundary
- + Proposed levels
- + Existing levels
- Emerging building footprints
- Consented ZMP Buildings
- PLANTING MIXES**
- Rain garden mix (R-g mix)
- Understorey mix- evergreen woodland shrubs (U-ews mix)
- Understorey mix- structural woodland perennial (U-swp mix)
- Understorey mix- flowering woodland perennials and bulbs (U-fwp mix)
- Understorey mix- eastern embrace (U-ee mix)
- Understorey mix- herbs and medicinal flower garden (U-hm mix)
- Understorey mix-grass mix (U-g mix)
- Sun loving perennials (B-sp mix)

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Revision	Date	By	Chkd	Appr	Description
P01	May 2018	EA	AA	MB	RMA Submission



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Drawing Title  
**Planting Plan\_Planting S9 Landscape- For Approval**

Drawing Number  
**TIQ-L-GUS-SW-S9-DR-90-P401**

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EA	AA	MB

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LANDSCAPE CHARACTER AREAS

Gustafson  
Porter +  
Bowman



- |                   |                          |                     |
|-------------------|--------------------------|---------------------|
| Main squares      | Streetscape and pathways | Buffer planting     |
| Secondary squares | Retail areas             | Ornamental planting |
| Playable areas    | Tertiary space           | Lawn                |

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- |                                   |                    |
|-----------------------------------|--------------------|
| 1-Redman Square                   | 7-Cinema Entrance  |
| 2-S9 Entrance                     | 8-Endeavour Square |
| 3-Redman Place                    | 9-Arber Lane       |
| 4-Light/reflection Artwork Pocket | 10-Turing Street   |
| 5-Social Working space and Bar    | 11-Sherwin Lane    |
| 6-Digital Gallery                 | 12-Brewery Place   |
|                                   | 13-Amphitheatre    |

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Gallery Garden looking towards British Council entrance (Outdoor working shelters will be submitted as part of O4 conditions)

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COMING FROM THE PEDESTRIAN BRIDGE



Bridge Plaza coming from CED pedestrian bridge

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ARRIVING FROM ENDEAVOUR SQUARE

Gustafson  
Porter +  
Bowman



IOL REDMAN PLACE - RMA Page 72

Coming from Endeavour Square looking toward Redman place gardens

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**London Legacy Development Corporation Quality Review Panel**

**Report of Formal Review Meeting: IQL Redman Place**

Thursday 22 March 2018

Level 10, 1 Stratford Place, Montfichet Road, London E20 1EJ

**Panel**

Peter Studdert (chair)  
Neil Deely  
Johnny Winter

**Attendees**

Josh Hackner	LLDC Planning Policy and Decisions Team
Tessa Kordeczka	Frame Projects

**Apologies / report copied to**

Anthony Hollingsworth	LLDC Planning Policy and Decisions Team
Catherine Smyth	LLDC Planning Policy and Decisions Team
Richard McFerran	LLDC Planning Policy and Decisions Team
Steve Tomlinson	London Legacy Development Corporation
Ben Hull	London Borough of Newham

**Confidentiality**

This is a pre-application review, and therefore confidential. As a public organisation the LLDC is subject to the Freedom of Information Act (FOI), and in the case of an FOI request may be obliged to release project information submitted for review.

# CONFIDENTIAL

## 1. Project name and site address

International Quarter London (IQL) South, Zone 2, Stratford City

Redman Place, comprising public realm between Plot S4 and Plot S9.

## 2. Presenting team

Mary Bowman	Gustafson Porter + Bowman
Tom van de Bospoort	Gustafson Porter + Bowman
Tim Makower	Makower Architects
Laura Pellegrinelli	Lendlease
Matthew Eyre	Quod
Steffan Rees	Quod

## 3. Planning authority's views

The Quality Review Panel had reviewed the revised masterplan for International Quarter London (IQL) South in November 2017. Revision of the masterplan has required a new approach to both the public realm and the design of buildings on individual IQL plots. The panel considered the revised masterplan to be a significant improvement on its earlier iteration.

New proposals for the public realm associated with Plot S4 and Plot S9 – now renamed Redman Place – have been developed to respond to the revised masterplan and a new design for the building on Plot S4. The planning authority is keen to ensure that Redman Place provides high quality public space at the heart of IQL South.

## 4. Quality Review Panel's views

### *Summary*

The design of the IQL South public realm, including Redman Place, has developed well and shows considerable promise. The Quality Review Panel suggests that the principal route through IQL from Arber Lane, between Plot S3 and Plot S4, to Bridge Place might be reinforced by the landscape design, including choice of materials. The 'garden gallery' in front of the building on Plot S9 responds particularly well to a building that will be occupied by the British Council, with a gallery at ground floor level. The panel thinks that the proposed lighting strategy has the potential to be highly effective. These comments are expanded below.

This report should be read together with the report of the panel's review of IQL Plot S4, also on 22 March 2018, which considered the public realm between Plot S3 and Plot S4.



# CONFIDENTIAL

## *Response to masterplan*

- Previous presentations of the revised masterplan for IQL South included clear diagrams of the spaces to be created. It would be helpful to see those diagrams overlaid with the detailed designs for the public realm, including Redman Square and Redman Place. This would allow a closer interrogation of the relationship between functionality and character.

## *Public realm and landscape design strategy*

- The panel welcomes the clear description of the design for the public realm between Plot S4 and Plot S9, to be named Redman Place. This has developed well.
- It repeats its support for the move of the vehicle access route through the IQL site from southwest to northeast – passing in front of building S4 instead of building S9. This is indicated by natural stone setts within a shared surface.
- The panel recommends that the principal pedestrian route to be established from Arber Lane, between Plot S3 and Plot S4, across Redman Place and Redman Square to Bridge Place be reinforced by the landscape design, including in the choice of materials. (See also the report of the review of Plot S4 on 22 March 2018.)
- The panel also repeats its support for the interpretation of historic uses (railway lines) within the landscape design. It would encourage continuity of this distinctive motif through the public realm from Endeavour Square, including Redman Place.
- The presence of the British Council in the building on Plot 9, and its ground floor gallery, will be especially important for the success of IQL as a new city quarter. The panel thinks that the ‘garden gallery’ in Redman Place responds particularly well to building S9.
- Pedestrians will be able to both filter through and also linger in the ‘garden gallery’. Both the proposed ‘amphitheatre’ and the bar / workspace incorporated into the ‘garden gallery’ can be expected to be popular.
- The panel raises a minor, detailed point: a strong direction of travel to the right from the entrance to building S9 is established; the panel suggests that there will also be a desire line to the left and that this might be more strongly reflected in the landscape design.



## CONFIDENTIAL

- The panel repeats a point made in its review of building S4. It suggests reconsideration of the low clipped hedge around the entrances to the proposed café and cinema in building S4. The panel thinks that this hedge is unnecessary and does not fit with the more civic character sought for Redman Place and the wider IQL public realm.

### *Lighting strategy*

- The panel fully supports the proposed lighting strategy, which includes catenary lighting on major pedestrian routes. It suggests that this might be used more extensively.

### *Next steps*

- The Quality Review Panel offers its warm support for the design developed for Redman Place.
- It looks forward to having the opportunity to comment in more detail on the design of the public realm for Redman Square.





**Subject:** The International Quarter London, Building S4 (Substructure) - 18/00355/REM and (Superstructure) 18/00354/REM

**Meeting date:** 25 September 2018

**Report to:** Planning Decisions Committee

**Report of:** Daniel Davies – Principal Planning Development Manager

FOR DECISION

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**This report will be considered in public**

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## **1. EXECUTIVE SUMMARY**

- 1.1. The report considers two reserved matters applications for Building S4 in Zone 2, known as International Quarter London South (IQL South), of the Stratford City Outline Planning Permission (10/90641/EXTODA) ("Stratford City OPP"). Building S4 benefits from two previous reserved matters consents; application ref: 16/00339/REM which relates to the substructure (comprising below ground works) and 16/00342/REM which relates to the superstructure (above ground works). Applications 18/00355/REM and 18/00354/REM are new proposals for the development plot and comprise new substructure and superstructure details. The new scheme would provide a 21 storey (+109.8 AOD) mixed-use building with basement comprising office (44,916 sq. m), retail (825 sq. m) and leisure uses (1,170 sq. m) and associated landscaping.
- 1.2. IQL South is a multi-phased masterplan development and a number of buildings have been built out and are occupied. This includes Glasshouse Gardens which are two residential buildings and Buildings S5 and S6 which are office buildings housing tenants such as Transport for London and the Financial Conduct Authority. The areas of public realm which surround these buildings have also been delivered including the Entrance Plaza (otherwise known as The Stitch), Turing Street and Endeavour Square. Construction activities associated with Building S9 are partially completed and it is anticipated that Building S9 will be occupied in summer 2019.
- 1.3. The main issues to be considered in relation to the applications are:
  - Overall conformity with the Stratford City Outline Planning Permission and approved Zonal Masterplan;
  - Access and inclusive design;
  - External appearance and compliance with local plan policy BN.10; and
  - Environmental impacts.
- 1.4. Officers have assessed the documents submitted under both these applications and are satisfied that the scheme is in conformity with the Stratford City OPP, Zonal Masterplan and Site Wide Strategies and will not give rise to new or different significant environmental effects. For this reason an EIA is not required.

- 1.5. The submission demonstrates that adequate access and circulation arrangements have been made through the siting of the building, its associated below ground infrastructure, and ground floor entrances which would be accessible. The scheme is suitably designed and would accord with the principles of the Stratford City Outline Planning Permission and Zonal Masterplan. The public realm would provide an appropriate amount of cycle parking and motorbike parking.
- 1.6. Minor deviations from the approved Zonal Masterplan are proposed which officers have assessed to be acceptable, observing that the proposed scheme would improve conditions compared to previously approved reserved matters proposals. Detailed landscaping proposals are set out under a separate application for Redman Place which is being presented alongside this scheme at planning committee (application reference: 18/00255/REM). The design development of these proposals have taken place concurrently at during pre-application discussions and both officers and the Quality Review Panel concluded that the landscaping proposals appropriately respond to the proposed building.
- 1.7. Officers have assessed the design quality of the building which is considered to respond appropriately to its context and successfully incorporates a 'Point of Orientation' as required by the Zonal Masterplan on its Westfield Avenue elevation. It is considered to be of a high design standard, has a well-articulated facade and massing that is distinctive and offers unique character to the series of office buildings consented in Zone 2. Officers consider that the proposal, subject to conditions, meets the local plan tall building policy criteria of Policy BN.10. The scheme was also reviewed by LLDC's Quality Review Panel on three occasions who endorse the proposal.
- 1.8. No objections have been raised by members of the public or statutory consultees. It is therefore recommended that both reserved matters applications are acceptable and recommended for approval subject to the conditions set out in section 13 of this report.

## **2. RECOMMENDATIONS**

### **2.1 The Committee is invited to:**

- a) **APPROVE application 18/00354/REM with the FULL DISCHARGE of conditions B1, B8, B9, Q1 and Q4 of the Stratford City Outline Planning Permission (10/90641/EXTODA) in relation to reserved matters for superstructure works for Plot S4, subject to the conditions set out in this report;**
- b) **APPROVE application 18/00355/REM with the FULL DISCHARGE of conditions B1, B8 and B9 of the Stratford City Outline Planning Permission (10/90641/EXTODA) in relation to the substructure works for Building S4; and**
- c) **AGREE TO DELEGATE AUTHORITY to the Director of Planning Policy and Decisions to make any refinements, amendments, additions and/or deletions to the proposed draft conditions as the Director of Planning Policy and Decisions considers reasonably necessary.**

## **3. FINANCIAL IMPLICATIONS**

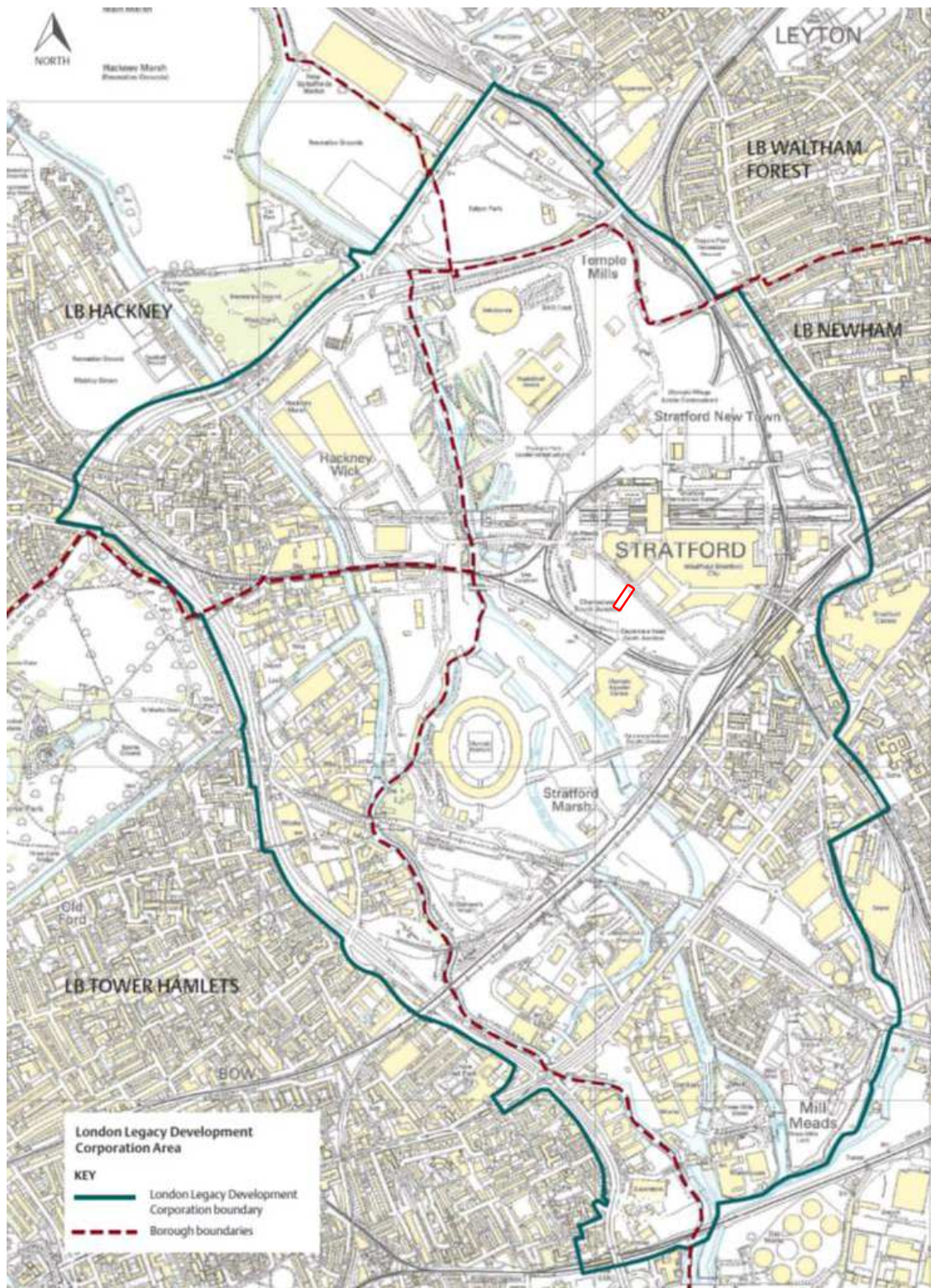
- 3.1. None

## **4. LEGAL IMPLICATIONS**

- 4.1. None



## Site plan



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**Location:** Plot S4, Stratford City Zone 2, The International Quarter London (IQL) South, Land adjacent to Westfield Avenue, Stratford City.

**London Borough:** Newham

**Proposal:** 18/00354/REM

Application for the approval of Reserved Matters pursuant to conditions B1, B8, B9, Q1 and Q4 of the Stratford City Outline Planning Permission (Ref: 10/90641/EXTODA) comprising the construction of a 21 storey building with basement to provide office (Use Class B1), complementary retail (Use Classes A1-A5) and leisure uses (Use Class D1/D2) together with associated public realm (including Secondary Road S2 (part), other areas of open space, associated parking for cars, motorcycles and bicycles, and associated works .

**18/00355/REM**

Application for the approval of Reserved Matters pursuant to Conditions B1, B8 and B9 of Outline Planning Permission reference 10/90641/EXTODA being details of the layout, scale, siting and access including engineering works for the below ground basement, in-ground drainage, services and utilities and associated substructure works for Building S4.

**Applicants:** Stratford City Business District Limited (SCBD Ltd)  
**Agent:** Quod Ltd  
**Architect:** Arney Fender Katsalidis

## **5. SITE & SURROUNDINGS**

- 5.1. The application site, measuring 0.38hectares, is a rectangular plot, bounded by Westfield Avenue to the north east, Redman Place to the south west and is immediately neighboured by building S5 along Westfield Avenue. It is located within Zone 2 of the wider Stratford City development and forms part of the IQL South. Zone 2 lies adjacent to Zone 1 which is the Westfield shopping centre, and is bounded by Westfield Avenue to the north east, Montfichet Road to the south east and railway lines to the south west.
- 5.2. IQL South (Zone 2) is connected by Bridge F10 to the Queen Elizabeth Olympic Park to the south west and Stratford Waterfront (Planning Delivery Zone 1) which is immediately adjacent to the railway lines. Stratford Waterfront is subject to current Cultural and Education District proposals known the East Bank.
- 5.3. IQL South is a multi-phased masterplan development and a number of buildings have been built out and are occupied. This includes Glasshouse Gardens which are two residential buildings and Buildings S5 and S6 which are office buildings housing tenants including Transport for London and the Financial Conduct Authority. The areas of public realm which surround these buildings have also been delivered including the Entrance Plaza (otherwise known as The Stitch), Turing Street and Endeavour Square. Construction activities associated with Building S9 are partially completed and it is anticipated that Building S9 will be occupied in Summer 2019.
- 5.4. The remaining plots to the north and north west (S10, S1 and S2) have outline planning permission for commercial development but remain undeveloped. The applicant has been in discussions with LLDC PPDT in relation to how these parts of Zone 2 might come forward, including the possible introduction of residential buildings at the northern end of the site. Further details of the revised masterplan will be presented to committee as applications for these northern plots come forward.
- 5.5. The application site is not located within a conservation area nor are there any designated or non-designated heritage assets within the site or surrounding area. The site has excellent public transport connections with both Stratford International Station and Stratford Regional Station in close walking distance and direct access to bus routes on Westfield Avenue. This is reflected by its high public transport accessibility level rating (PTAL) of 6b.

## **6. PLANNING PROPOSAL**

- 6.1 The proposal is seeking permission to construct a single 21 storey building and basement that will provide a total of 44,916 sq. metres of floorspace (GEA) comprising of predominately office space from level 1 to 21, retail at ground floor (825sq.m) and leisure uses (1,170 sq. metres) spread across portions of the first and second floor of the building. The leisure use proposed is a cinema (Class D2). An overview of the distribution of these uses within the building is set out below:

### **Basement level**

- Plant, bike shed and changing rooms

### **Ground Floor**

- Retail units
- Main entrance lobby
- Cinema Lobby
- Plant rooms, loading bay and storage

### Level 1

- Cinema Screens and pre-function rooms
- Offices
- Facility managers office

### Level 2

- Cinema Screens
- Offices
- Terraces

### Level 3, 19, and 21

- Offices
- Terraces

### Level 4-18

- Typical Office floors

### Roof level

- Plant
- Building Maintenance Unit

- 6.2 The scheme will deliver cycle parking (387 spaces), motor cycle parking (14 spaces), car parking (5 spaces) and landscaping on the surrounding land directly adjacent to the site. Further details on these elements are provided later within this report.
- 6.3 Plot S4 will be the fourth commercial building to come forward in The International Quarter (IQL) and the applicant has indicated that the building is likely to be occupied by multiple tenants, unlike Plots S5 and S6 for which tenants had been secured in advance.
- 6.4 The conditions of the Stratford City Outline Planning Permission that are relevant to these applications are summarised below:

<b>Condition B1</b>	Details of design, appearance, access and landscaping.
<b>Condition B8</b>	Requirements for reserved matters applications including the submission of a ZMP conformity statement.
<b>Condition B9</b>	Details of reprofiling and earthworks.
<b>Condition Q1</b>	Requirement for landscaping in each zone to take account of landscaping in adjacent zones.
<b>Condition Q4</b>	Details of open space and design and landscaping.
<b>Condition A4</b>	All reserved matters to be in accordance with the ZMP unless minor variations are agreed in writing with the LPA.

Two reserved matters applications have been submitted for approval in order to construct the building S4. These applications are described in turn as:

**a) Substructure (18/00355/REM)**

- 6.5 This application relates to the substructure works only for plot S4 and is seeking approval for details of the siting and engineering works, in-ground drainage services and utilities. The substructure works include all below ground works including the building foundations, basement, piling and in-ground drainage services and utilities.
- 6.6 The submitted drawings for approval and documentation confirm how the substructure (and subsequent building) will be consistent with the Stratford City OPP and the Zonal Masterplan (subject to minor deviations), including parameter plans and approved site wide strategies. Documentation also details the environmental impacts resulting from the substructure works, including a piling risk assessment and an underground drainage design statement.

**b) Superstructure (18/00354/REM)**

- 6.7 This application relates to the above ground works for plot S4 and specifically the floorspace, uses, massing and associated landscaping.
- 6.8 These applications have been submitted separately to remove the risk of programme delay of below-ground construction works if there were any significant delays in the consideration of the superstructure. For the purposes of this report, these applications are described interchangeably as the “RMA applications”, where appropriate.

**7. RELEVANT PLANNING HISTORY**

Stratford City Outline Planning Permission

- 7.1 The Stratford City OPP was granted in February 2005 (reference P/03/0603) by the London Borough of Newham for comprehensive redevelopment of the rail lands site for a range of residential, office and retail uses as part of the regeneration of Stratford City. This is the ‘parent’ planning permission for this site and has subsequently been varied and approved on the 30th of March 2012 altering the quantum of development, various conditions and parameters (planning reference: 10/90641/EXTODA).

Zone 2 Stratford City -The International Quarter (IQL)

- 7.2 The IQL South forms part of the Stratford City Outline Planning Permission covering an area that is known as “Zone 2” of the Stratford City OPP. A Zonal Masterplan for Zone 2 sets out parameters for development and describes the principles that new development should comply in accordance with Condition A1 of the Stratford City OPP. The Zonal Masterplan was approved on 28<sup>th</sup> March 2012 (ref: 11/90463/AODODA) and was later superseded by an updated Zonal Masterplan which was approved on 22<sup>nd</sup> September 2015 (Planning reference 15/00005/AOD).

Zone 2 Reserved Matters Applications (RMA's)

- **Plot 4** –Reserved matters applications were approved in November 2017 for the construction of an office building (71,923sqm) with complementary retail uses (Use Classes A1-A5) up to a maximum height of 21 storeys (+109.9m AOD) with associated parking for cars, motorcycles and bicycles. That planning permission has not been implemented. Should the current RMA applications be found acceptable by the Planning Decisions Committee, the applicant intends to implement the new scheme.
- **Pavilion Building** – Members resolved to grant Reserved Matters approval at Committee in July 2018 (18/00252/REM)

- **Plot S9** – Reserved matters approval was granted for a commercial building in April 2017 (16/00672/REM and 16/00671/REM). This building is currently under construction and is due to be occupied by the British Council and Cancer Research UK.
- **Plot S8 and S7 (Glasshouse Gardens)** - Reserved matters approval was granted in January 2014 (13/00409/REM) for two residential buildings up to a maximum height of 30 storeys, providing 333 residential units. Occupation of the building commenced in October 2016. This site includes Balcony Park which has a Multi-Use Games Area (MUGA) and a Local Equipped Area of Play (LEAP).
- **Plot S6 (Transport for London – TfL)** - Reserved matters approval for substructure works granted in February 2015 (14/00483/REM), with later approval for the superstructure under reference 15/00003/REM in October 2015 for a commercial building up to a maximum of 12 storeys (+7Om AOD approx.) to provide 33,688.8 sq. m of floorspace, consisting of 32,796.6sqm of office floorspace (Class B1), 591.5sqm of complementary retail (Class A1-A3) and a 300.7 sq. m creche (Class D1 ). Constructed and occupied by TfL.
- **Plot S5 (Financial Conduct Authority - FCA)** -Reserved matters approval for substructure works granted in February 2015 (14/00482/REM), with later approval for the superstructure under reference 15/00002/REM in October 2015 for a commercial building up to a maximum of 20 storeys (+107m AOD approx.) to provide 62,120sqm floorspace, consisting of 61,233sqm of office floorspace (Class B1) and 887sqm of complementary retail (Class A1-A5). Constructed and occupied by the FCA.
- **Services and Utilities** - Zone 2 Site-Wide In-Ground Services and Utilities - Reserved matters approval was granted in February 2015 under application 14/00479/REM.
- **Carpenter’s Square** – (public realm) application 16/00683/REM for landscaping around future buildings, comprising public open space including hard and soft landscaping and associated works. The consent was not implemented and revised landscaping proposals for S9 are subject of an application that is being presented alongside this scheme at planning committee (planning ref: 18/00255/REM).

#### Environmental Impact Assessment

- 7.3 The proposed development has been subject to a Screening Opinion (18/00357/SCRES) which is being reviewed by PPDT’s environmental consultants and will be reported in an update report to planning committee. Subject to the final confirmation from our EIA consultants, our view is that an EIA is unlikely to be required as the previously approved RMAs were screened negatively and this scheme would not give rise to new or different significant environmental effects (16/00340/REM and 16/00349REM).

#### Other Relevant Planning Applications

- 7.4 Applications to discharge pre-commencement conditions have been submitted for approval with respect to building S4 to discharge Conditions G1 (Construction Method and Management Statement), G8 (Telephone Inquiry Line), K6 (BREEAM), L1 (Foul and Surface Water Drainage), L5 (Storage Facilities for Oils, Fuels and Chemicals), L7 (Sewerage Infrastructure), L8 (Hazardous Substances), L12 (Piling Works), O4 (Noise Assessment and Mitigation), P2 (Wheel Washing), X1 and X2 (Heights of Buildings, Cranes and Plant) of the outline planning permission 10/90641/EXTODA (planning ref: 18/00356/AOD). These details update information previously submitted and approved in connection with the previous RMA for building S4 (16/00353/AOD).

- 7.5 Various S96A applications have been submitted to make non-material changes to the parent planning permission. If approved these changes would redistribute leisure floorspace from IQL North to IQL South (planning reference: 18/00335/NMA); enable a greater proportion of food and beverage retail within IQL South (18/00336/NMA) and transfer commercial and complementary retail floorspace from Zone's 3 and 4, respectively, to Zone 2 (18/00337/NMA). These applications are pending and the intention is for them to be determined under delegated powers. An update of their status will be presented as part of an update report to planning committee.
- 7.6 A pedestrian bridge (Carpenter's Bridge) is proposed to connect Zone 2 to Stratford Waterfront, from the north west of IQL South. The bridge is part of the Cultural and Education District proposals, which are expected to be submitted to the Local Planning Authority as an application in autumn 2018.
- 7.7 The revised building S4 application and proposed revisions to the masterplan have been subject to pre-application briefings with the planning decisions committee. A broad level of support has been given to the principle of these changes to the scheme and wider masterplan.

## **8. POLICIES & GUIDANCE**

### National Planning Policy Framework (2018)

- 8.1 The National Planning Policy Framework sets out national planning policy and is a material consideration in planning decisions. It sets out a presumption in favour of sustainable development which Plans and decisions should apply. This requires that in order to achieve this, development proposals that accord with an up to date development plan should be approved without delay. Where there are no relevant development plan policies or the policies that are most relevant are out of date, that permission is granted unless the application of policies within the NPPF that protect areas or assets of particular importance provides a clear reason for refusing development, or any adverse impacts of granting permission would significantly or demonstrably outweigh the benefits, when assessed against the policies in the NPPF as a whole. The NPPF should be read in conjunction with the Planning Practice Guidance, a web-based resource for all users of the planning system. This set out detailed guidance in support of the policy areas in the NPPF, including the importance of good design and how this can be achieved through planning decisions.
- 8.2 The following parts of the framework are relevant to these submissions:
- (Section 6) Building a strong, competitive economy
  - (Section 7) Ensuring the vitality of town centres
  - (Section 9) Promoting sustainable transport
  - (Section 12) Achieving well-designed places
  - Section 16) Conserving and enhancing the natural environment

### The London Plan (March 2016)

The relevant policies are listed below:

Policy 2.4 The 2012 Games and their Legacy

Policy 2.9 Inner London

Policy 2.13 Opportunity Areas and Intensification Areas

Policy 2.14 Areas for regeneration

Policy 4.1 Developing London's economy

Policy 4.2 Offices

Policy 4.3 Mixed use development and offices  
 Policy 4.12 Improving opportunities for all  
 Policy 5.2 Minimising carbon dioxide emissions  
 Policy 5.3 Sustainable design and construction  
 Policy 5.6 Decentralised energy in development proposals  
 Policy 5.9 Overheating and cooling  
 Policy 5.10 Urban Greening  
 Policy 5.12 Flood Risk Management  
 Policy 5.13 Sustainable drainage  
 Policy 5.14 Water quality and wastewater infrastructure  
 Policy 5.15 Water use and supplies  
 Policy 5.21 Contaminated land  
 Policy 6.3 Assessing effects of development on transport capacity  
 Policy 6.9 Cycling  
 Policy 6.10 Walking  
 Policy 7.2 An Inclusive environment  
 Policy 7.4 Local character  
 Policy 7.5 Public realm  
 Policy 7.6 Architecture  
 Policy 7.7 Location and design of tall buildings  
 Policy 7.11 London view management framework  
 Policy 7.12 Implementing the London View Management Framework  
 Policy 7.14 Improving Air Quality  
 Policy 7.15 Reducing Noise and Enhancing Soundscapes  
 Policy 7.19 Biodiversity and access to nature

#### The Draft London Plan (December 2017)

The Mayor of London Published a draft London Plan in November 2017 for the purposes of public consultation. The policies in the draft new London Plan currently only have limited weight when making planning decisions. This report may make reference to policies within the new London Plan where they are materially relevant to the assessment of the proposal.

#### London Legacy Development Corporation Local Plan (July 2015)

The relevant policies are listed below:

Policy SP .1 Building a strong and diverse economy  
 Policy S.1 Health and Wellbeing of prospective residents  
 Policy B.1 Location and maintenance of employment uses  
 Policy B.2 Thriving town, neighbourhood and local centres  
 Policy B.5 Increasing local access to jobs, skills and employment training  
 Policy SP.3 Integrating the built and natural environment  
 Policy BN.1 Responding to place  
 Policy BN.5 Requiring inclusive design  
 Policy BN.10 Proposals for tall buildings  
 Policy BN.11 Reducing noise and improving air quality  
 Policy BN.13 Improving the quality of land  
 Policy T.4 Managing development and its transport impacts  
 Policy T.5 Street Network  
 Policy T.6 Facilitating local connectivity  
 Policy T.8 Parking and parking standards in new development  
 Policy T.9 Providing for pedestrians and cyclists  
 Policy S.1 Health and wellbeing



Policy S.4 Sustainable design and construction  
Policy SP.5 A sustainable and healthy place to live and work  
Policy S.6 Waste reduction  
Policy S.7 Overheating and urban greening  
Policy S.8 Flood risk and sustainable drainage measures

The Draft London Legacy Development Corporation Local Plan (September 2018)

LLDC has carried out consultations on the scope of the changes to the Local Plan and is preparing a Draft Revised Local Plan for the purposes of public consultation. The policies in the draft Revised Local Plan only have limited weight when making planning decisions.

Other relevant material considerations:

- Mayor of London – Accessible London (2014)
- Mayor of London –Olympic Legacy SPG (2012)

## **9. CONSULTATIONS**

9.1. Both applications were advertised in the Newham Recorder press and site notices were erected on the boundary of the site on Westfield Avenue. Letters were also sent to the following consultees. Any responses received have been summarised in the table below:

- LB Newham Planning;
- LB Newham Highways;
- LB Newham Transport;
- LB Newham Waste Management;
- Natural England;
- Environment Agency;
- Network Rail;
- HS1;
- DLR;
- London Underground;
- TfL;
- Docklands Light Railway;
- Thames Water;
- National Grid;
- EDF Energy;
- Metropolitan Police;
- Westfield;
- Holiday Inn;
- LS185; and
- London Cycling Campaign.

<b>Consultee</b>	<b>Response</b>
• LB Newham Planning	No written comments received.
• LB Newham Highways	No written comments received.
• LB Newham Transport	No written comments received.
• LB Newham Environmental Health	No objection to the scheme.
• LB Newham Waste Management	No written comments received.
• Natural England	No written comments received.
• Environment Agency	No written comments received.
• Network Rail	No written comments received.
• HS1	No written comments received.
• DLR	No written comments received.
• London Underground	No written comments received.
• TfL	No written comments received.
• Docklands Light Railway	No written comments received.
• Thames Water	No written comments received.
• National Grid	No written comments received.
• EDF Energy	No written comments received.
• Metropolitan Police	No written comments received.
• Westfield	No written comments received.
• Holiday Inn	No written comments received.
• LS185	No written comments received.
• London Cycling Campaign	No written comments received.

#### Neighbour Responses

9.2. No comments received.

#### Internal Consultees

9.3. PPDT's Environmental Consultants (ARUP)

PPDT's Environmental Consultant (ARUP) were consulted on the application and originally sought clarification on daylight and sunlight, wind, ecology and air quality issues. Further information was provided by the applicant on these issues which at the time of writing report were under review. Whilst there appears to be no significant matters related to these issues, they are likely to lead to further conditions or possible minor amendments to proposal which will be described in an update report to Committee. Based on the acceptability of these details or subsequent approval of conditions imposed, officers are satisfied that the environmental information provided to support the reserved matters application is acceptable.

9.4. PPDT's Transport Consultant (CH2M)

PPDT Transport Consultants have reviewed the documents submitted as part of the RMA and are satisfied that the anticipated transport effects do not raise significant concerns from a transport perspective and are acceptable. Clarification was sought on the gradient of the internalised cycle ramp within the building which they have advised should be amended to have a gradient of no more than 7%.

9.5. Quality Review Panel

The Quality Review Panel reviewed Building S4 three times at pre-application stage (January 25<sup>th</sup> March 22<sup>nd</sup> and May 3<sup>rd</sup> 2018) . The panel were supportive of the proposals

and consider that it has evolved in a positive direction. They acknowledged that the relationship of the building with the adjoin plot S3 was important and should be captured in design codes. A summary of the Quality Review Panel's views in respect of this scheme are set out below:

*“The Quality Review Panel welcomes the revisions made to the design of the building on Plot S4, as well as the projection of the design of the building on neighbouring Plot S3. A much clearer route through IQL is established – in line with the principles of the IQL South masterplan. The panel recommends further exploration of how Turing Street might become more activated. The proposed architectural expression of building S4, including the articulation of façades and materials, shows promise; its success will depend on the quality of detailing, materials and construction. The panel repeats its support for the landscape design and public realm associated with Plot S4. It recommends continuing analysis of microclimatic conditions across IQL South”*

The panel welcomed the opportunity to see how the details of the design are developed in order to be able to advise on compliance with Policy BN.10.

## **10. ASSESSMENT OF PLANNING ISSUES**

### **Principle of Development**

- 10.1. The application site, measuring 0.38ha, is in an Opportunity Area and Metropolitan Centre where development is expected to support wider regeneration aspirations and provide Grade A B1 office space in accordance with London Plan (Policy 2.13 – Opportunity Areas and Intensification Areas) and the Local Plan (Policy B.2 – Thriving Town, neighbourhood and local centres). The site benefits from outline planning permission by way of Stratford City OPP (ref: 10/9064/EXTODA) and so the principle of comprehensive redevelopment is established. Development of this site is required to be in accordance with the approved parameters of the Stratford City OPP, the approved Zonal Masterplan (ZMP) for Zone 2 and accompanying Site Wide Strategies
- 10.2. These reserved matters applications seek to demonstrate compliance with the parent consent and to justify any deviations from it. The parent planning permission makes clear provisions for office space throughout the IQL South site and as such the principle of this development, in land use terms is acceptable and it would make a significant contribution towards the achievement of strategic London Plan and local regeneration objectives through the creation of jobs and stimulating the local economy (Local Plan strategic policy SP.1 'Building a Strong and Diverse Economy). The addition of leisure floorspace is considered likely to be non-material and acceptable in the context of the Stratford Centre OPP as this allows for leisure floorspace within the Stratford Metropolitan town centre boundary.

### **Overall Conformity with Parameter Plans, Stratford City OPP and Zonal Masterplan**

- 10.3. Condition A4 of the Stratford City OPP requires that all RMAs be submitted in accordance with the approved Zonal Masterplan. The relevant ZMP for Zone 2 was approved in September 2015 (15/00005/AOD), having been updated from earlier iterations. The Applicant has submitted a statement setting out how this proposal conforms with the approved ZMP and parameters set out in the Stratford City OPP. The statement also sets out where the scheme makes minor departures from the ZMP.

### **Siting of the building**

- 10.4. The consented parameter plans define the plot boundaries within which buildings should be located, and establish minimum distances between the elevations of buildings on adjoining plots. Typically a minimum of 15 metres is required between buildings although

this can be reduced to 13 metres where there are protruding facade elements such as staircases. The Parameter plans also require that buildings and their public realm should be designed to enhance legibility of movement with the ZMP Design Statement indicating that buildings should include a Point of Interest as part of the frontage of the building. Four 'Points of Orientation' are identified in the approved Zonal Masterplan and one is included on the Westfield Avenue frontage of Building S4. The purpose of such Points of Orientation is to help create interest and activity along key pedestrian routes, and to assist with legibility and integration of IQL with its context.

- 10.5. The proposed siting of the building has been informed by these requirements and emerging proposals for a new masterplan for the remaining parts of IQL South. The evolving masterplan is being developed by newly appointed architects who are exploring how to modify the development framework in order to positively respond to the lessons learnt from the previously delivered plots and to enable the wider masterplan plan to adapt to emerging changes in the market, including a diversification of the permitted uses to enhance place-making. Whilst those changes are not the subject of this planning application, a number of key principles have been established through discussions with officers and the QRP which has informed the siting, orientation and layout of the proposed S4 building. One of the overarching aims for building S4 is to achieve variety in the urban grain with compressed lanes and more public open spaces.
- 10.6. The proposed S4 building has a much smaller footprint than the extant S4 building (circa 35%) the result of which would be the creation of Arber Lane. Arber Lane is intended to be lined with retail frontages, sensitively landscaped and would form a key route linking pedestrians to Westfield and Stratford Waterfront via the new Carpenters Land Bridge. The angled ground floor of the building would create a desire line between these destinations strengthening the linkages and would add a much finer urban character when compared to the previous scheme.
- 10.7. Buildings S4 and S5 (Financial Conduct Authority) are separated by a minimum distance of 13.6 metres between the stair cores on the north-west elevation and up to 18.3 metres on the south-east façade which would comply with the approved ZMP requirements. The distance of separation with S9 is in excess of 27 metres and so would also comply with the ZMP requirements.
- 10.8. The consented parameter plans do not define constraints for the siting of utilities but they provide the context for the existing, proposed and optional local and wider utility networks that serve the Stratford City site. Whilst there are no specific ZMP drawings for services in Zone 2, the ZMP Design Statement states that a significant body of major utilities infrastructure has been provided within Stratford City and the Olympic Park, specifically recognising that as developments are brought forward within Zone 2, new connections will be made into this inherited infrastructure.
- 10.9. The drawings submitted confirm that the location of proposed piles, building foundations, basement drainage and utilities works would fall within the Development Plot boundaries in which building S4 would be sited. Their design has been coordinated in tandem with the design and siting of the superstructure.
- 10.10. Based on this analysis the proposed siting and routing of in-ground services are consistent with the existing and proposed networks and in this respect are considered to comply with the approved parameter plans and are sited comfortably within the approved Plot boundaries. The separation distance of building S4 with existing and proposed buildings also complies with the ZMP requirements demonstrating that the siting of the building is acceptable.

### **Means of access**

- 10.11. The ZMP and parameter plans detail the location of pedestrian and vehicular accesses, shared surfaces, cycle routes and servicing arrangements. There are no specific requirements in terms of the location of pedestrian access points to Building S4, but it is noted that Turing Street was envisaged as the primary pedestrian route within this development. This route would sit between building S5 and building S4.
- 10.12. The provision of a cycle route along Westfield Avenue is set out in the ZMP, which has now been built. Access to this cycle route is explained in the ZMP to be provided by way of cycle linkages through Zone 2 on a route to be shared with pedestrians between buildings S4 and S5. Turing Street was envisaged as a shared surface accommodating low levels of vehicular traffic, pedestrians, cyclists and servicing. The provision of a cycle route shared with pedestrians between buildings S4 and S5 has already been delivered through the Endeavour Square RMA (16/00523/REM). A separate RMA continuing this route is proposed under the Redman Place RMA (18/00255/REM) which is reported elsewhere on this agenda.
- 10.13. Under this proposal Turing Street will accommodate 5 car parking spaces, 2 of which have been designed as blue badge parking bays. The remaining 3 spaces would be allocated for tenants of the S4 office space, which is significantly less than the maximum of 71 car parking spaces permissible under the Stratford City OPP. 5% of spaces are required to be in form of accessible parking bays in accordance with the ZMP Design Statement. At 40%, the provision of accessible car parking bays is proportionally high and is compliant with the ZMP.
- 10.14. Cycle parking would be predominately provided within the basement of the building S4 (330 spaces) and accessed via a dedicated ramp and entry point near the corner of Turing Lane and Westfield Avenue. Shower and changing facilities are to be provided within this part of the building, which is supported as it would encourage users of the building to make use of these facilities. A further 57 cycle spaces would be delivered within the public realm, principally along Turing Lane along with 14 motorbike parking spaces.
- 10.15. At 330 cycle parking spaces the amount of cycle parking provided within the building is well above the 180 spaces required by the Stratford City s.106 agreement and the adopted London Plan. The amount of visitor parking provided at grade within the curtilage of the building would also exceed the minimum of 44 required by the s.106 and the adopted London Plan, a significant level is also being provided at grade.
- 10.16. Based on the s.106 at least 30 motorcycle parking spaces would normally be expected but only 14 are proposed. The applicant states that the calculation contained in the s.106 agreement relates to IQL in its entirety and not individual buildings in isolation and that the same number of motorbike spaces were consented for the previous Building S4 RMA proposals which were found to be acceptable. That building was much larger than the current S4 building and so reasonably would have required a higher level of provision to be made.
- 10.17. Officers acknowledge that there is a shortfall proposed based on the pre-existing formula but recognise that it would be difficult to accommodate further motorbike parking facilities without adversely impacting on either the delivery of active ground floor frontages, quality of landscaping or the amount and location of visitor cycle parking. Servicing also takes place on Turing Street which is a further constraint on parking provision. These factors are considered to be relevant material considerations that weigh in favour of accepting the shortfall in officers' view, as more weight is placed on the planning merits contributed by these aspects of the proposal.
- 10.18. Servicing would take place on Turing Street and has been staggered so as not to conflict with the operation of the existing servicing yard for building S5. Service access is directly

from Westfield Avenue via a loading bay which is linked to a goods lift located in the rear core of the building. Servicing in this way is consistent with indicative locations set out in the ZMP and consistent with approved parameters.

- 10.19. Pedestrian access to the office element of the building is proposed via main entrances on Westfield Avenue, with other entrances to the building and ground floor uses on Arber Lane and Redman Place. All main entrances have been designed to be accessible and to meet the requirements of Approved Document M as a minimum which in planning terms is acceptable
- 10.20. It is noted that the proposed scheme would make minor departures from the ZMP in two respects. Firstly, it relocates the primary pedestrian route from Turing Street to what is now described as Arber Lane (between Building S4 and Building S3), creating a more direct key pedestrian link, leading visitors through from Westfield, through the IQL South and towards the bridge linking to the proposed East Bank development. Secondly, there would be a deviation from the position of the vehicular plot access and shared surface within Plot P01, removing vehicular plot access leading from Redman Place up through Arber Lane between Buildings S3 and S4. This effectively means that Arber Lane is capable of being pedestrian only which is considered a significant improvement from the consented position. Turing Street would still be accessible to pedestrians but in creating a more direct pedestrian focused route there would now be a much more inviting and pleasant vehicle free route that offers a more intimate characterful walking experience which is considered a significant planning benefit.
- 10.21. The layout and orientation of building S4 would guide pedestrians visually towards Redman Place, towards building S9 and the new Carpenters Land Bridge which would represent an improvement upon the consented position and is in keeping with the broader healthy streets aspirations. As such, there are key material considerations, which weigh in favour of accepting these specific, but minor, departures from the approved ZMP.

#### **Design and use of the building**

- 10.22. Condition D2 of the Stratford City OPP sets out the maximum permissible floorspace for uses in Zone 2. Table 1 shows that the scheme would not exceed the total floorspace requirements of Zone 2 and in this respect complies with the constraints of the permission. There is sufficient B1 floorspace within the parameters of the Stratford City OPP for the remaining buildings to come forward. However, it is noted that the scheme seeks to introduce leisure uses into Zone 2 which were not originally envisaged under the parent planning permission.
- 10.23. A series of S96a applications have been submitted which seek to vary the amount and type of floorspace permitted within Zone 2. If approved, these amendments would enable the delivery of leisure uses within Zone 2, amongst other things, to be consistent with the ZMP. Table 1 below sets out the total quantum of floorspace permitted in Zone 2 on the basis that those S96A applications are approved. The information has been presented in this way as those applications had not been determined at the time of writing this report.

**Table 1: Zone 2 maximum land use quantum taking account of the proposed S96A applications (excluding residential uses) (sq. metres).**

Land Use	Condition D2 Total Zone 2 (sq. metres)	S5 (TfL Building)	Building S6 (FCA Building)	Building S9 (BC)	Pavilion	S4 (% of total)	Total	Remaining
Complimentary Retail (A1-A5)	5,813	887	592	381	1,343	825 sq. m (14%)	4,028	+1785
Commercial (Use Class B1)	283,399	61,233	32,797	33,536	0	44,916 sq. m (16%)	172,482	+110,917
Leisure uses (Class D2)	2000	0	0	0	0	1,170 sq. m (59%)	1,170	+830
Community uses (Class D1)	309	0	301	0	0	0	301	+9

10.24. Based on the pending S96A applications, building S4 represents 16% of the total commercial floorspace allocated across Zone 2, 14% of complementary retail and 59% of the proposed leisure uses. Illustrative plans indicate that the 3-screen cinema is prominently expressed in the architectural expression on the façade of the building and the corners of the building facing S9 and Redman Place. This could be a complementary addition to the masterplan adding visual interest to the family of consented buildings. However officers are mindful that should a cinema provider not be brought on board, or in the case of the S96a applications not being approved, the space and the external elevations may need to be adapted to allow the building to transform in response to the needs of the end user.

10.25. In such an event, officers are satisfied that there is sufficient flexibility in the detailed design of the building and space to ensure that it could be transformed to be fit for an alternative purpose. Should Members find the scheme to be acceptable in all other respects, it is recommended that such changes could be managed by a suitably worded condition, requiring the submission of details for approval by the local planning authority. Alternatively, the applicant would have the option of making a further application to agree changes to the façade design of the building.

10.26. Turning to the distribution of uses more generally, retail frontages and active entrances would dominate the ground floor, with entrances located at prominent locations on Westfield Avenue, Arber Lane and Redman Place. A 'Third Space' is proposed at a prominent part of Westfield Avenue which is intended to function as a less formal place where people can meet to work, have informal meetings, use wifi or simply have a coffee. The idea of a third space, is increasingly popular in modern workspaces and they are typically comfortable inviting and contemporary in their look and feel and destinations in their own right. The space has been designed to have the right balance of functionality and flexibility with the adjoining office and is capable of enlivening Arber Lane drawing pedestrians towards through into IQL. The principle of locating visually interesting and active spaces at these prominent parts within the building is strongly supported, however it is likely that its success will rely on securing the right tenant that has a vision that will mean it is well utilised and inviting.

10.27. The Zonal Masterplan alludes to the need for there to be a 'Point of Orientation' on Westfield Avenue which is in considered to be fulfilled by the combination of the angled

building line, arrangement of the ground floor entrances and third space activation. This aspect of the scheme is considered to comply with the ZMP.

#### Public Realm and Landscaping proposals

- 10.28. The substantive public realm for Building S4 is set out in proposals for Redman Place which is being presented to the Planning Decisions Committee alongside this application (Planning reference 18/00255/REM). The design development of the public realm and its interface with the proposed S4 building has been coordinated to ensure a joined up working approach and has been carried out by the same landscape architects. Appropriate detailing of the public realm has been provided for illustrative purposes and officers are satisfied that the landscaping scheme proposed responds appropriately to the building. The approach is comprehensive although ongoing discussions are continuing with the applicant to ensure that the location and siting of trees and landscaping appropriately takes into account the siting of utilities and microclimate considerations.
- 10.29. Extensive biodiverse roof planting is proposed on the terraces of the building. Details of this planting including construction drawings, plants species and, where appropriate, irrigation systems will be secured by condition.

#### Height and massing

- 10.30. The Zonal Masterplan plan establishes a maximum height for building S4 at +110 AOD. At 109.8m AOD, inclusive of roof plant, the proposed building S4 complies with the height parameters in the Zonal Masterplan. The approach to massing builds on the vernacular of the consented Roger Stirk and Harbour buildings, respecting their established character whilst also introducing a new vernacular that seeks to more clearly distinguish between the base, middle and top to the building through the proportions and sub-division of glazing which adds interest and variety to the emerging family of buildings. The building volume is composed of a series of distinct elements which would successfully break down its perceived bulk and has an interesting and visually coherent composition from different vantage points both in and outside IQL South.

#### **Urban Design & Architectural Expression (including external appearance and an assessment of compliance with Policy BN.10 of the local plan)**

- 10.31. Neither the Stratford City OPP nor the Zonal Masterplan contain prescriptive design guidance or codes on the detailing of the buildings beyond the parameters that the proposal has been assessed against above. However, the approved Design Statement does describe the following:
- Any submissions relating to less than all of the buildings in each plot, should explain how they relate to the future neighbours in that plot as part of the submission;
  - If details of the design of individual buildings are submitted in advance of the submissions relating to adjacent public realm and streetscape, appropriate details should be provided with those reserved matters applications;
  - All materials in the public realm, including roads, footpaths, parks and open spaces should have an integrity and relationship of materiality and street furniture; and
  - Materials used on the external surfaces of buildings should be high quality and complement, where appropriate, neighbouring buildings.
- 10.32. The London Plan requires that buildings be of the highest architectural quality, and to be of a proportion, composition, scale and orientation that enhances and activates the public realm. (Policy 7.6 - Architecture). Policy BN.10 of the Legacy Corporation's Local Plan for tall buildings applies to this scheme as the proposed building exceeds the



prevailing height of 30m for buildings in Sub-Area 3. Policy BN.10 provides clear design criteria that proposals for tall buildings should be assessed against. An assessment of the scheme against BN10 is set below:

<b>BN. 10 Criteria</b>	<b>Officer Comments</b>
<p>1. Exhibit outstanding architecture and incorporate high-quality materials, finishes and details</p>	<p>The overall material strategy for the building is broadly supported, with dark metal and concrete complementing the palette of materials already established on the IQL site. However, there are some areas of further information relating to the detail that should be addressed in order for the building to demonstrate full compliance with this criteria. This includes the detailing of the large 'textured solid insulated panels', glazing system, various façade types and prominent appearance of the roof top plant.</p> <p>QRP noted that the success of the building will depend on how well it is crafted with 'exceptional detailed design, materials and construction' essential to achieving the promised level of quality. The panel advised that they would want to see the details of how the design are developed in order to advise on whether the buildings meets the BN.10 test.</p>
<p>2. Respect the scale and grain of their context</p>	<p>The scheme successfully responds to the grain and scale of the emerging masterplan context, by adopting the same façade modules and by adding a finer grain of building which provides variation and relief in townscape terms.</p>
<p>3. Relate well to the street width and make a positive contribution to the streetscape</p>	<p>Arber Lane will be a dedicated pedestrian street which links Westfield Avenue to Redman Place creating a direct and more please route from pedestrians through to the proposed Carpenters Bridge and East Bank Development.</p> <p>QRP remarked that the key principles for the design of Building S3 should be safeguarded in the design codes that will be developed as an addendum to the IQL outline planning permission and that these should include specifying a diagonal along Arber Lane; the height of the base and the undercut; and the extent of the active frontage and public realm.</p>
<p>4. Generate an active street frontage</p>	<p>The scheme would deliver active frontages through the provision of retail, leisure and offices uses along Westfield Avenue, Arber Lane and Redman Plan and some activation on Turing Street.</p>
<p>5. Provide accessible public space within their curtilage</p>	<p>High quality landscaping and public realm would be delivered with the building which has been designed in conjunction with updated landscape proposals for building S9. In the interim period, before S3 is constructed, temporary landscaping is proposed along the hoarding line which the applicant is proposing to programme with temporary events and 'pop-up' activities.</p>

6. Incorporate sufficient communal space	Terraces have been incorporated at the upper and lower levels of the building for the use of commercial tenants.
7. Contribute to defining public routes and spaces	The proposed landscaping design incorporates the IQL stainless steel 'railway line' vernacular which current runs through from Endeavour Square to Redman Place. These lines are proposed to be continued along Arber Lane to help guide pedestrians from Westfield Avenue to Redman Square and through to Bridge Place and on to the East Bank
8. Promote legibility	Illustrative material showing the projected relationship of building S4 and building S3 indicates how pedestrians arriving from John Lewis can get visual glimpses of building S9 and beyond further along Arber Lane, improving legibility.
9. Create new or enhance existing views, vistas and sightlines	The scheme would improve the visual amenity of the site by creating active frontages and through the design of the building which add interest in its façade design and its proposed relationship to the proposed S3 building.
10. Preserve or enhance heritage assets and views to/from these, and contribute positively to the setting of heritage assets, including conservation areas	The site lies within the background of the key strategic view of St Paul's from King Henry's Mound as identified in the London View Management Framework. A verified views assessment has been carried out which demonstrates that this S4 building would not be visible in this view. There are no other heritage assets, including conservation areas that would be affected by the construction of this building.
11. Micro-climatic conditions (specifically down-draughts and lateral winds over public spaces)	Subject to the appropriate installation of wind mitigation measures, building S4 can be constructed in a manner that results in safe and comfortable wind environment at ground level and on the proposed terraces.
12. Impacts to the surrounding area (including open spaces and other buildings and waterways) that relate to: Overlooking Daylight Overshadowing Light spill/reflection Wider amenity	The proposal would not have a detrimental impact on the amenities of surrounding occupiers in terms of daylight, sunlight or loss of privacy and would not result in overlooking or excessive overshadowing. The scheme will provide new publicly accessible, high quality landscaped public space. Solar glare issues are not anticipated to arise from this scheme.
13. Existing views of landmarks, parkland, heritage assets, waterways, and views along street corridors	The development is considered to significantly improve views within IQL and along Westfield Avenue
14. Tall buildings should be located within the Centre boundaries outlined within the Local Plan. In order of hierarchy these are: • Stratford Metropolitan Centre	The building is within the Stratford Metropolitan Centre which, in policy terms, is an appropriate location for a tall building.

<ul style="list-style-type: none"> <li>• Bromley-by-Bow District Centre</li> <li>• Hackney Wick Neighbourhood Centre</li> <li>• Pudding Mill Local Centre</li> <li>• East Village Local Centre</li> </ul>	
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Summary of BN.10 assessment

- 10.33. The design of the S4 building has been developed in response to the evolving masterplan which has led to the creation of a finer grain building and intimate streets which are more pedestrian focused and provide new direct linkages to key destinations. Westfield Avenue, Arber Lane and Redman Place are lined with extensive active frontages and are capable of being occupied by a diverse range of engaging ground floor uses that would enliven this part of the masterplan during the day and night. The building would add interest to the townscape and would not be visible in protected strategic views.
- 10.34. The detailed comments of the QRP are provided at appendix 8. They indicate support for the scheme and the safeguarding of key principles for the design of Building S3 through design coding. Officers are working with the applicant to progress these as part of ongoing discussions.
- 10.35. The principle of the materials proposed is broadly acceptable however officers recommended that detailing be reserved by condition. Deferring resolution on materiality and detailed façade design is considered appropriate in this instance as it would enable a comprehensive and focused review of all external materials on site alongside other buildings. It would also provide the opportunity for further interrogation which is considered appropriate in this instance owing to the complexity of the façade design and number of façade types and overlapping junctions. The principle of reserving such details is firmly established under condition B10 of the Stratford City OPP.
- 10.36. The proposed building is considered to be of a high architectural standard, with strong urban design and a well- articulated façade that would provide a distinctive design that positively contributes to the family of buildings already approved and under construction. It will improve the street scene and contribute towards successful place-making, through the distribution and quantum of uses adding visual interest to the public realm. Officers are satisfied that these elements combined will produce an outstanding building that complies with Policy BN10.

Environmental Compliance

Daylight, Sunlight and Overshadowing

- 10.37. Conditions V1 and V2 of the Stratford City OPP require the development to be designed according to the BRE 'Site Layout Planning for Sunlight and Daylight: A Guide to Good Practice' and the guidance in BS8206: Part II and the Applications Manual: Day Lighting and Window Design - Lighting Guide LG10 (1999). The Applicant has submitted a sunlight and daylight assessment to demonstrate that the reserved matters applications for Building S4 complies with Conditions V1 and V2 of the Stratford City OPP. The following effects have been tested:
- The proposed development on the surrounding buildings;
  - The proposed development on surrounding external spaces;
  - The proposed development on future users of S4.

#### Impact of proposed development on the surrounding buildings

- 10.38. The likely effects of Building S4 on daylight and sunlight at surrounding buildings has been assessed taking account of Buildings S5 and S6 (occupied), Building S9 (under construction) and the remaining adjacent buildings still to come forward in the masterplan (the Pavilion and Building S2). The findings of this study indicate that these buildings will not experience a noticeable change in daylight and sunshine hours. Based on this analysis officers are satisfied that acceptable conditions would be achieved that are in accordance with the standards approved in the ZMP.

#### Impact of proposed development on surrounding external spaces

- 10.39. The likely effects of building S4 on sunlight to external amenity spaces has been assessed taking account the buildings already delivered and proposed to be delivered under the masterplan. Sunlight penetration to the public spaces surrounding the site would achieve at least the minimum of more than 2 hours of sunlight on the 21st of March based on illustrative scheme. The experience of sunshine on Arber Lane, the principal space affected by building S4 can only be confirmed at the point when the RMA for S3 is submitted for approval.

#### Quality of daylight for future occupiers of the S4 building

- 10.40. The findings indicate that average daylight factor (ADF) targets are met on all floors, apart from open plan areas on level 1, where a lower figure of 1.72% is achieved. Having regard to the buildings compliance with ZMP separation distances, the high level of glazing across the building and its overall daylight performance, the conditions achieved are comparable with the previous consented proposal. The area affected is identified as a facilities management area and limited to a modest part of the overall floor plate. Notwithstanding this, the standard of daylight achieved is not considered to be harmful to the extent that it would warrant the refusal of planning permission.

#### Solar Glare

- 10.41. A sunlight reflection report has been submitted which examines the likelihood of facade reflectivity issues arising from the construction of Building S4. This includes solar glare analysis, and an assessment of the effects of reflected and direct sunlight from the building. The analysis concludes that during the summer months there are short periods of time in the early hours where the path of the sun would generate reflections. However the predicted effects are not above the disability glare thresholds that would apply to be considered harmful and the predicted effects would have a very short duration and would not result in harmful impacts.
- 10.42. Policy BN.10 that states that new development should not have adverse effects, including the impact of reflection and officers are satisfied that the study submitted demonstrates that no unacceptable harm is anticipated as result of this proposal.

#### Wind

- 10.43. Conditions V3 and V4 of the Stratford City OPP require wind tunnel testing for new proposals. This is to ensure that all outdoor spaces will have a comfortable wind environment for occupiers and users of the area. The applicant has carried out a study to assess wind effects and to determine the likelihood of wind speeds exceeding comfortable thresholds for pedestrian activities. PPDT's environmental consultants have reviewed this information and conclude that subject to the appropriate mitigation measures, Building S4 can be constructed in a manner that results in a safe and comfortable wind environment at ground level and on the proposed terraces.

**Site wide strategic compliance – including Energy, Waste, Water, Ecology, Materials and Microclimate; and Sustainable Design and Construction.**

- 10.44. Condition C3 of the Stratford City OPP requires all works to be carried out in accordance with the approved Site Wide Strategies. These strategies cover matters relating to energy, building design, waste, water, ecology, materials, microclimate and air quality. These strategies were approved by the ODA in November 2007 and continue to apply to the construction of this building.
- 10.45. The applicant has provided an Environmental Compliance Statement which confirms that:
- Energy – Building S4 will be connected to the existing district heating infrastructure
  - Building design – Building S4 is targeting a BREEAM rating of ‘Excellent’ in accordance with the Site Wide Strategy Sustainability Target
  - Waste – Both a site-wide and building specific waste strategy have been developed which, upon implementation will ensure that residual wastes and recyclables can be collected and stored separately. Furthermore, construction waste will be managed in accordance with the Zone 2 Construction Method and Management Statement required by Condition G1 of the Stratford City OPP.
  - Water – Significant reductions in water consumption will be achieved, with a target of 25% improvement compared to the baseline of 20%. This will be achieved through a combination of water efficient fittings and appliances, as well as rainwater recycling to feed WCs.
  - Ecology –Extensive green roofs are proposed at the level 2 and 21 roof terraces.
  - Materials - The key Site Wide Strategy Sustainability Target (M2) regarding materials for the plot is to achieve a minimum BREEAM rating of Very Good for all buildings. Building S4 will achieve BREEAM Excellent which exceeds that target.
  - Microclimate - The results of microclimate testing and its implication for the building design and landscaping will be reported in an update report at Planning Committee.
  - A site specific Construction Method and Management Statement (CMMS) has been submitted which details proposed pilling, noise mitigation works, wheel-washing, details of cranes and plant which has been assessed alongside this application. Officers are satisfied that the construction works and methods proposed would be undertaken in accordance with the approved Site Wide Construction Method and Management Strategy and that there are suitable safeguards in place that would minimise effects on air quality, dust, noise and general environmental amenity. These details form part of package pre-commencement conditions submitted for approval which have been assessed and found to be acceptable. On the basis of the information submitted these details have been discharged and recommended for approval under delegated powers (planning ref. 18/00356/AOD).
- 10.46. In summary, building S4 has been designed in accordance with the approved site wide strategies for Zone 2 ZMP. Appropriate measures are in place to ensure that that environmental effects of the building during construction and operation are being appropriately managed and would not result in significant adverse impacts. The proposed substructure works are also considered and would be carried out in full compliance with the ZMP and relevant Stratford City OPP conditions and in accordance with policies 5.3 and 5.12 of the London Plan and policies BN.13 and T.4 of the Local Plan. This is a positive aspect of the scheme will contribute towards wider sustainable design and construction objectives.

## **Estate Management and Maintenance**

- 10.47. Condition C1 of the Stratford City OPP requires that a series of Site Wide Strategies are developed and approved by the Local Planning Authority, including a Site Wide Estate Management Strategy. The Estate Management Strategy has been produced to cover the Stratford City area and was approved by the ODA in November 2007. It sets out the principles for access, ownership and maintenance, security, prevention of environmental crime, parking and traffic management.
- 10.48. The strategy describes the intention for Estate Management Offices to be established in each zone in Stratford City. Estate Management Companies should then be set up to take responsibility for the management and maintenance of sites at their complete / operational phase. These Estate Management Companies can contract organisations to become responsible for managing specific aspects of the site, including open spaces such as International Square.
- 10.49. In order to ensure that an appropriate management and maintenance scheme is put into operation for Building S4 and surrounding landscape areas, a condition is recommended, similar to that used for Plots S5 and S6 and S9 to secure the establishment of an Estate Management Company and its management of the land around this plot.

## **11. HUMAN RIGHTS & EQUALITIES IMPLICATIONS**

Members should take account of the provisions of the Human Rights Act 1998 as they relate to the application and the conflicting interests of the Applicants and any third party opposing the application in reaching their decisions. The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report. In particular, Article 6 (1 ), of the European Convention on Human Rights in relation civil rights and a fair hearing; Article 8 of the ECHR in relation to the right to respect for private and family life and Article 1 Protocol 1 of the ECHR in relation to the protection of property have all been taken into account

- 11.1 In addition the Equality Act 2010 provides protection from discrimination in respect of certain protected characteristics namely: age, disability, gender reassignment, pregnancy and maternity, race, religion, or beliefs and sex and sexual orientation. It places the Local Planning Authority under a legal duty to have due regard to the advancement of equality in the exercise of its powers including planning powers. Officers have taken this into account in the assessment of the application and Members must be mindful of this duty inter alia when determining all planning applications. In particular Members must pay due regard to the need to:
- Eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Equality Act;
  - Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and
  - Foster good relations between persons who share a relevant protected characteristic and persons who do not share it
- 11.2 Officers are satisfied that the application material and Officers' assessment has taken into account these issues. Officers consider that the effects of the proposal would not be so adverse as to cause harm and justify a refusal of consent or permission.

## **12. CONCLUSION**

- 12.1 Building S4 is the fourth commercial building that will be delivered as part of IQL South development and will contribute towards economic growth and job creation in the Metropolitan Centre of Stratford, meeting the strategic objections of the Legacy Corporation and set out in the London Plan and Local Plan.
- 12.2 The RMA applications are in conformity with the Stratford City OPP and Zonal Masterplan and do not generate any new significant environmental effects. The proposed design and appearance of the building is considered to be of a high architectural quality, meeting the requirement of Policy 7.6 of the London Plan and criteria of Policy BN.10 of the Local Plan. Conditions are recommended that will ensure that a high standard of external detailing, materials and finishes to the building is achieved.
- 12.3 On this basis, it is recommended that permission is granted for the submitted Reserved Matters Applications, as set out in the recommendation in Section 1 of the report.

## **13. PLANNING CONDITIONS**

### **Substructure Reserved Matters Application Ref: 18/00355/REM**

#### **1. Approved Plans**

The development shall be carried out and retained thereafter in accordance with the following drawings:

(to be confirmed and inserted prior to the decision notice being issued)

together with the description of the proposal contained in the application and any other plans, drawings, documents, details, schemes or strategies which have been approved in writing by the Local Planning Authority pursuant to these conditions.

*Reason: To ensure that all works are properly implemented and retained.*

### **Superstructure Reserved Matters Application Ref: 18/00354/REM**

#### **1. Approved Plans**

The development shall be carried out and retained thereafter in accordance with the following drawings:

(to be confirmed and inserted prior to the decision notice being issued)

together with the description of the proposal contained in the application and any other plans, drawings, documents, details, schemes or strategies which have been approved in writing by the Local Planning Authority pursuant to these conditions.

*Reason: To ensure that all works are properly implemented and retained.*

2. No development shall commence unless the non-material amendment application with reference 18/00335/NMA which permits the use of leisure floorspace in Zone 2 of the Stratford City Outline Planning Permission (10/90641/EXTODA) has been granted by the local planning authority.

*Reason: To ensure that the leisure use element of the development is permitted within Zone 2 before any development commences*

### **3. Detailed drawings**

Prior to the commencement of the relevant part of the development, detailed section drawings (at 1:5/1:10/1:20 or at another scale as may be agreed with the Local Planning Authority) through each façade type (such details to include the junction between each façade type) shall be submitted to and approved by the Local Planning Authority. Thereafter, the development shall be constructed in accordance with the approved details.

*Reason: To ensure that the Local Planning Authority is satisfied that the details and approach adopted will secure high quality design and detailing in accordance with London Plan Policy 7.6 and Local Plan Policy BN.1 and BN.10.*

### **4. Materials**

Prior to the commencement of the relevant part of the development, detailed section drawings (at 1:5/1:10/1:20 or at another scale as may be agreed with the Local Planning Authority) through each façade type (such details to include the junction between each façade type) shall be submitted to and approved by the Local Planning Authority. Thereafter, the development shall be constructed in accordance with the approved details.

*Reason: To ensure that the Local Planning Authority is satisfied that the details and approach adopted will secure high quality design and detailing in accordance with London Plan Policy 7.6 and Local Plan Policy BN.1 and BN.10.*

### **5. Estate Management**

Six months prior to occupation of Building S4, a site specific Estate Management Plan shall be submitted to the Local Planning Authority for approval. The site specific Estate Management Plan shall be consistent with the approved Estate Management Plan for Endeavour Square and include details of the roles and responsibilities to be held by an Estate Management Company for the site, and the location of the Estate Management Company office.

The details approved in the submitted Estate Management Plan shall be put in place as part of the operation of the site, prior to occupation of the development.

*Reason: In order to ensure that the outside spaces on the site and the new public realm is maintained and managed appropriately.*

### **6. Use Class A3 -A5 - Food**

Prior to the occupation of any complimentary retail units in A3, A4 or A5 use, full details of the grease trap or grease digester system to be installed for the commercial kitchen shall be submitted to and approved by the Local Planning Authority. Details should include plans & sectional drawings with measured drain sizes and full manufactures specifications. The approved scheme is to be completed prior to occupation of the development & shall be permanently maintained thereafter.

*Reasons: To protect the amenity of future occupants and/or neighbours & with regard to Local Plan Policy BN.11.*

### **7. Wind Mitigation Study**

Twelve months prior to the occupation of the development, a further wind mitigation study (which shall for the avoidance of doubt review the mitigation measures presented within the FD Global assessment IQL-Y-FDG-S4-XXX-RP-00-001 Revision: P02) shall be submitted to the Local Planning Authority for approval that demonstrates the mitigation measures required to achieve safe and comfortable wind conditions around the development.



The development shall be carried out in accordance with the approved details prior to the occupation of the building (or another timescale that may be agreed with the Local Planning Authority).

*Reason: In order to ensure a high quality of design and public realm with regards to wind safety and comfort in accordance with Local Plan Policy BN.10.*

#### **8. Biodiverse roof spaces**

Details of the biodiverse roof spaces shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of works above the ground floor (excluding the core) in connection with this permission. The development shall not be carried out other than in accordance with any such approval given.

*Reason: So that the local authority may be satisfied that the details of the biodiverse roof will achieve a high standard in accordance with policy S.4 Sustainable design and construction of the Local Plan (2015).*

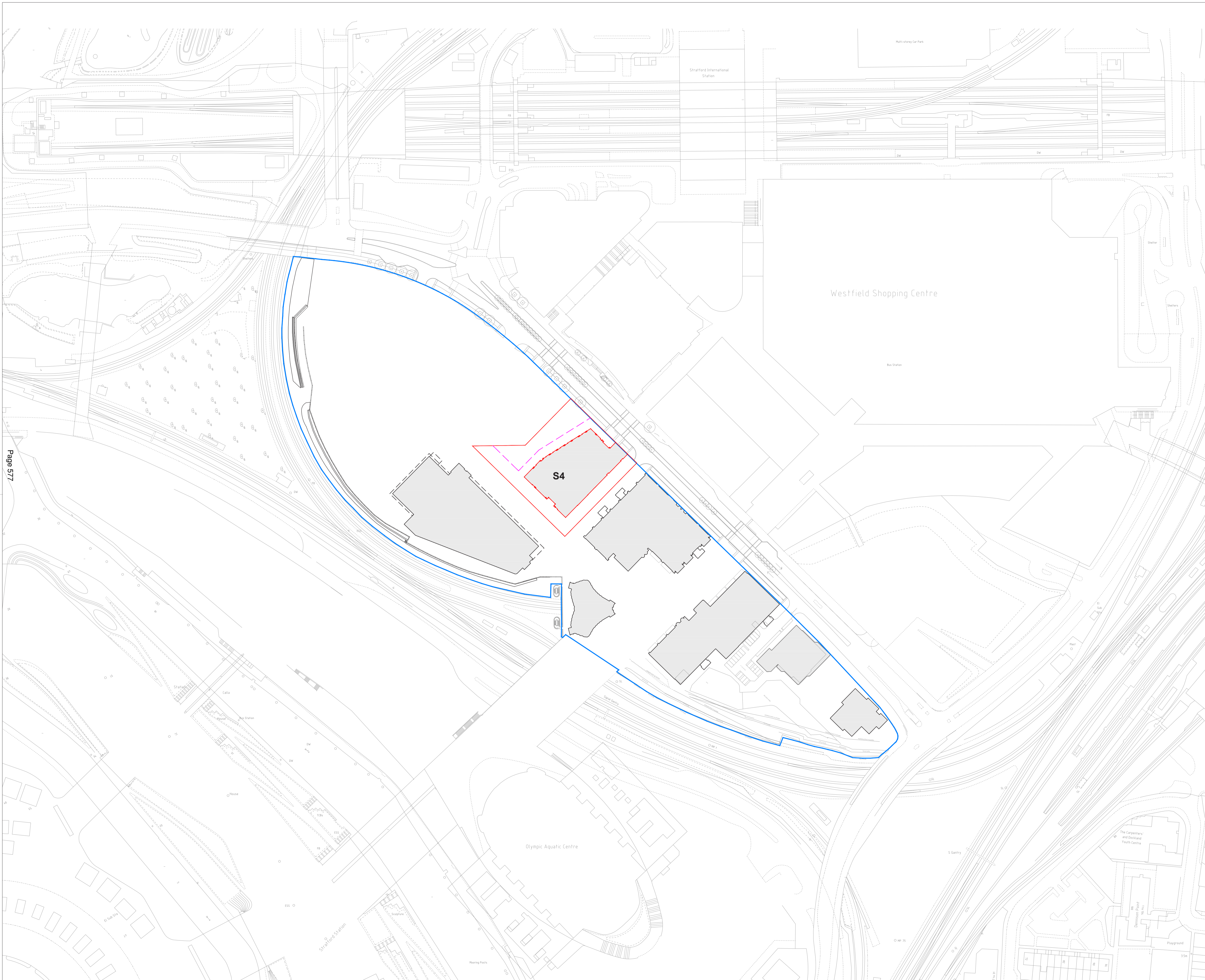
### **PLANNING INFORMATIVES**

1. Condition B10 of the Stratford City Outline Planning Permission (ref: 10/90641/EXTODA) requires full details (including samples) of all materials uses on all external surfaces to be submitted to and approved by the Local Planning Authority pursuant to Condition B1 prior to the commencement of the relevant part of the development.
2. In accordance with Condition Q2 of the Stratford City Outline Planning Permission (ref: 10/90641/EXTODA) prior to the occupation of the building hereby approved, a landscape management and maintenance plan should be submitted for approval in writing to the local planning authority and shall be managed and maintained in accordance with the approved landscape management and maintenance plan

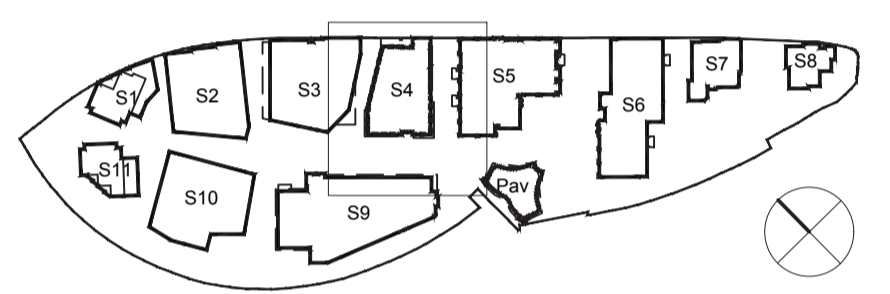
#### **Appendices**

- Appendix 1 Site Location Plan
- Appendix 2 Plot Boundary Parameter Plan
- Appendix 3 CGI of Building S4 viewed from Redman Place
- Appendix 4 Redman Place Elevation
- Appendix 5 Westfield Avenue Elevation
- Appendix 6 Ground Floor Arrangement Plan
- Appendix 7 Building Section BB
- Appendix 8 Quality Review Panel Comments

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- IQL Boundary line
- RMA Phase Boundary
- RMA Phase Boundary - Temporary Landscape
- Existing buildings at IQL



Page 577

Revision	Date	By	Chkd	Appr	Description
P00	07/06/2018	TB	MB	MB	For Planning

Landscape Architect  
**Gustafson  
 Porter +  
 Bowman**

Architect <b>Arney Fender Katsalidis</b>	Engineer <b>BURHAPPOLD ENGINEERING</b>
Project Manager	Specialist

Client

Project  
 International Quarter London

Drawing Title  
**Site Location Plan  
 For Approval**

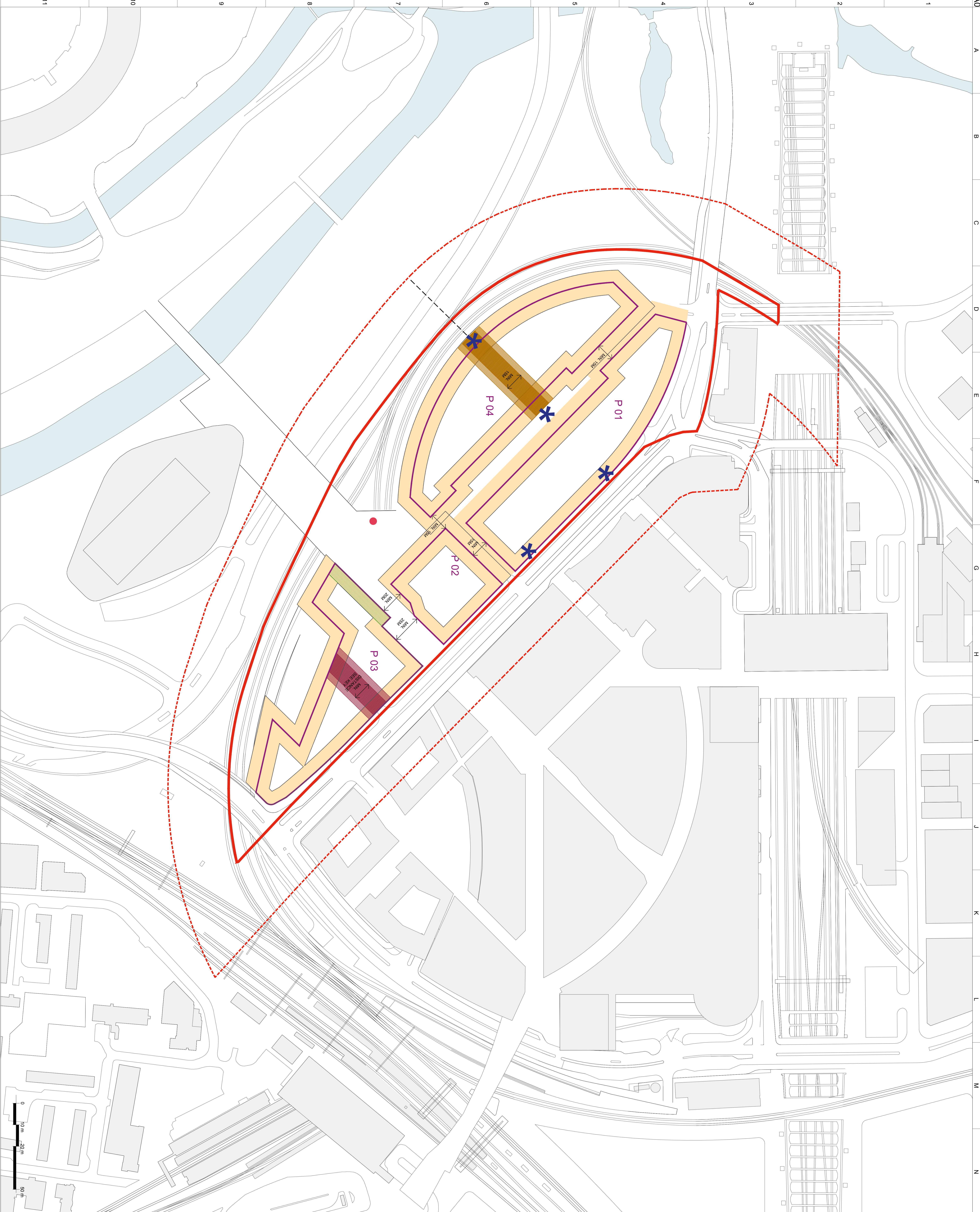
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1:2500@A3		

Drawn by TB	Checked by MB	Approved by MB
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- LEGEND**
- ZONE 2 BOUNDARY
  - 5m OFFSET
  - PLOT BOUNDARY
  - L.O.D. "4"-10m"
  - INDICATIVE LOCATION FOR POTENTIAL PAVILION
  - SAFEGUARDED BREAK IN BLOCK AND ACCESS ROUTE (INDICATIVE) BREAK
  - SAFEGUARDED BREAK IN BLOCK (INDICATIVE)
  - MIN. DISTANCE BETWEEN ROOMS - 5m FOR NON HABITABLE ROOMS
  - 5m DEVIATION TO SAFEGUARDED BREAK
  - 12m HIGH UNDERCUT TO BUILDINGS TO BE SAFEGUARDED
  - ★ BUILDING / PUBLIC REALM DESIGN TO ENHANCE LEGIBILITY WITHIN THE ZONE AS PART OF FUTURE RESERVED MATTERS APPLICATIONS
  - POSSIBLE FUTURE PEDESTRIAN BRIDGE (BY OTHERS)
- \* PROTRUDING FACADE ELEMENTS SUCH AS SPANDRILLS LOCALLY REDUCE THE MINIMUM DISTANCE BETWEEN PLOTS AND BUILDINGS WITHIN PLOTS TO 13 M.

**DESIGN TEAM:**  
 RSHF - VOGT - BIURO HAPPOLD

Issue	Date	By	CHKd	Appd
D	27.02.2015	ES	ES	MS
C	25.11.2014	SA	ES	MS
B	07.11.2011	EA	ES	EE
A	22.07.2011	EA	EE	EE

**LEND LEASE**

13 Fletroy Street  
 London W1T 4BQ  
 Tel: +44 (0)20 7638 1531  
 www.rshf.co.uk  
 Client: [redacted]

**THE INTERNATIONAL QUARTER**

**Drawing Title**  
 ZMP PLAN ZONE 2  
 PLOT BOUNDARIES

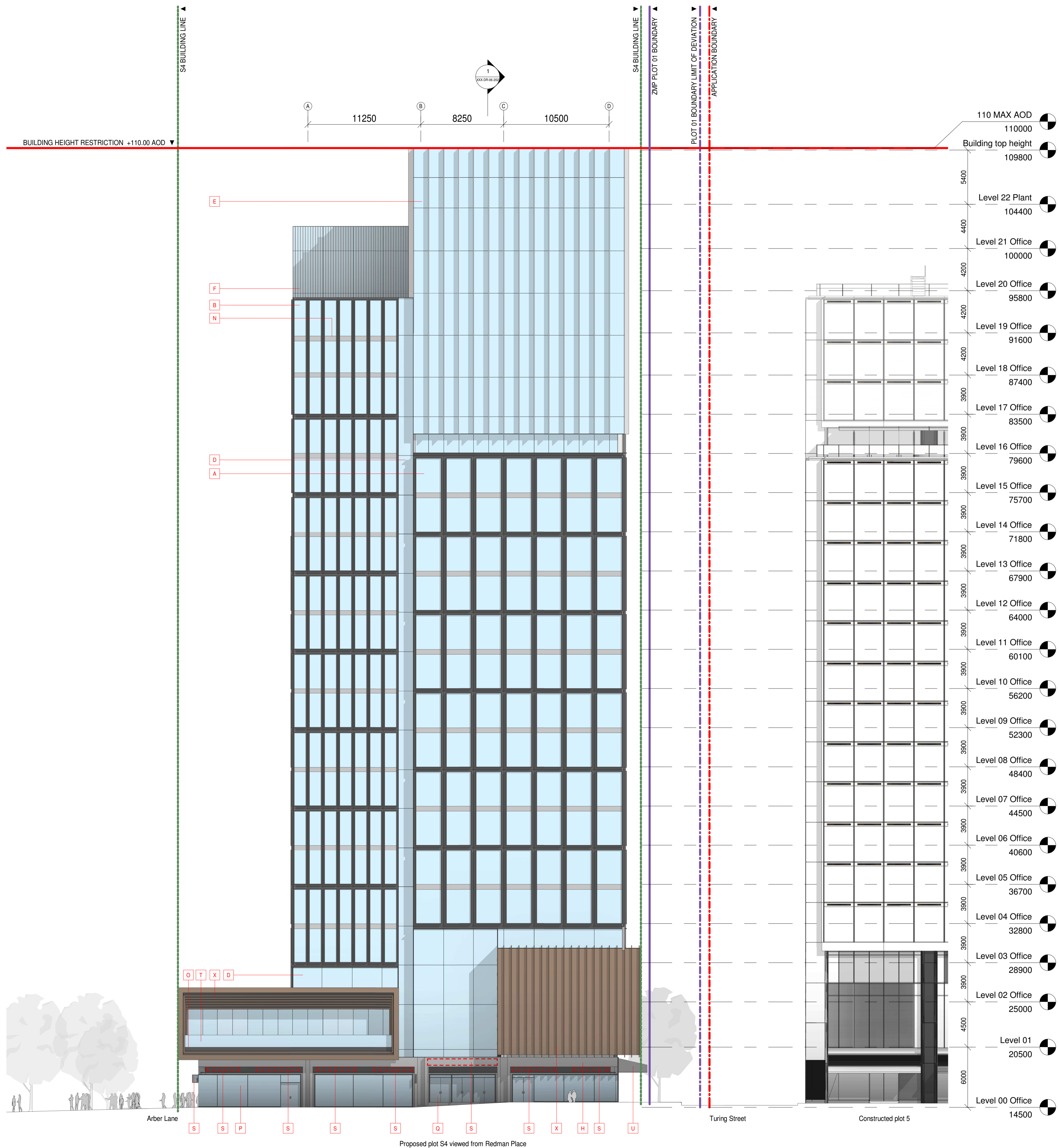
Scale at A1 1:1000 @ A0  
 Discipline URBAN DESIGN  
**FOR APPROVAL**  
 Job No 239652-00  
 Drawing No TIQ-AUD-06-X-012  
 Issue D

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**KEY**

- Application boundary
- 50 m Zone (offset from application boundary)
- SCBD Ltd Land ownership
- S4 Building Line
- 110 m AOD - Max. Height Limit
- ZMP Plot 01 Boundary
- Plot 01 Boundary Limit of Deviation +/- 10m

**NOTE**

Material colours to be determined to allow for future flexibility

- A 3.0m CCF with frame (Facade Type 03)
- B 1.5m CCF with frame (Facade Type 04)
- C 1.5m DGU with frame (Facade Type 05)
- D DGU (Facade Types 06, 07, 12, & 13)
- E DGU with fins (Facade Types 01 & 08)
- F 1.5m DGU with fritted glass (Facade Type 02)
- G Painted steel balustrade
- H Precast concrete cladding
- I Integrated revolving doors
- J Plant
- K Textured solid insulated panel
- L Back solid insulated spandrel
- M 1.5m DGU with frame and fritted glass (Facade Type 05)
- N Roof terrace
- O Leisure box terrace
- P Storefront glass panel
- Q Storefront integrated door
- R Solid insulated panel expressed joints (Facade Type 22)
- S Future tenant signage subject to separate application
- T Glass panel balustrade
- U Metal canopy
- V Metal loading bay shutter
- W Metal louvres
- X Metal Fins

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REV 00/00/00 DESCRIPTION  
JR PV  
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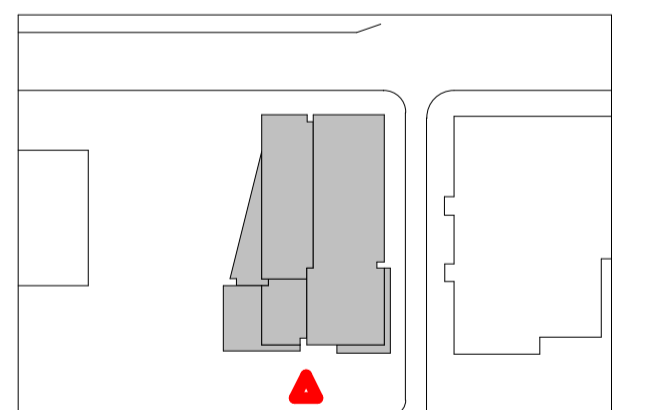


**LOCATION PLAN**



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**KEY PLAN**



**Arney Fender Katsalidis**

Arney Fender Katsalidis  
T +44 (0) 20 3772 7000  
E info@afk.co.uk  
W afk.co.uk  
44-48 Scrutton Street, London, EC2A 4HH

PROJECT: IQL -S4 PROJECT No.: 44079

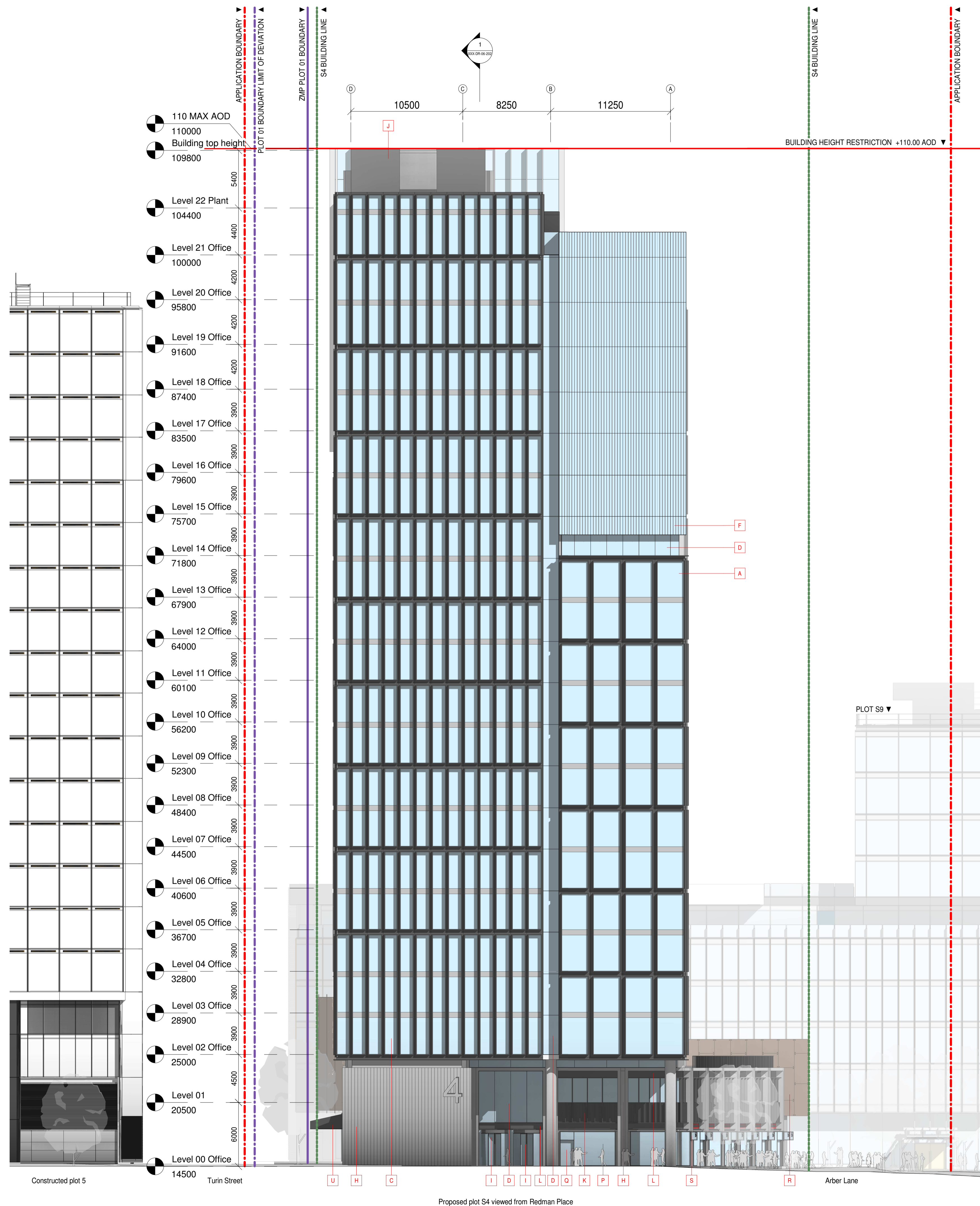
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STATUS: For Planning

DRAWING DATE: 06/07/18 SCALE: 1:200 @ A1

DRAWING No.: IQL-A-AFK-S4-XXX-DR-06-301 REVISION: P00

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**KEY**

- Application boundary
- 50 m Zone (offset from application boundary)
- SCBD Ltd Land ownership
- S4 Building Line
- 110 m AOD - Max. Height Limit
- ZMP Plot 01 Boundary
- Plot 01 Boundary Limit of Deviation +/- 10m

**NOTE** Material colours to be determined to allow for future flexibility

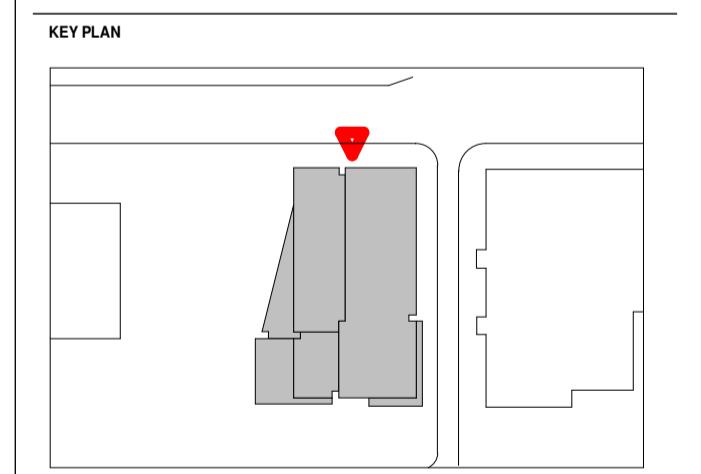
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- B 1.5m CCF with frame (Facade Type 04)
- C 1.5m DGU with frame (Facade Type 05)
- D DGU (Facade Types 06, 07, 12, & 13)
- E DGU with fins (Facade Types 01 & 08)
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- T Glass panel balustrade
- U Metal canopy
- V Metal loading bay shutter
- W Metal louvres
- X Metal Fins

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 REV 00/00/00 DESCRIPTION JR PV  
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**Arney Fender Katsalidis**

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 44-48 Scrutton Street, London, EC2A 4HH

**PROJECT**  
 IQL -S4

**PROJECT No.**  
 44079

**DRAWING TITLE**  
 North East Elevation

**STATUS**  
 For Planning

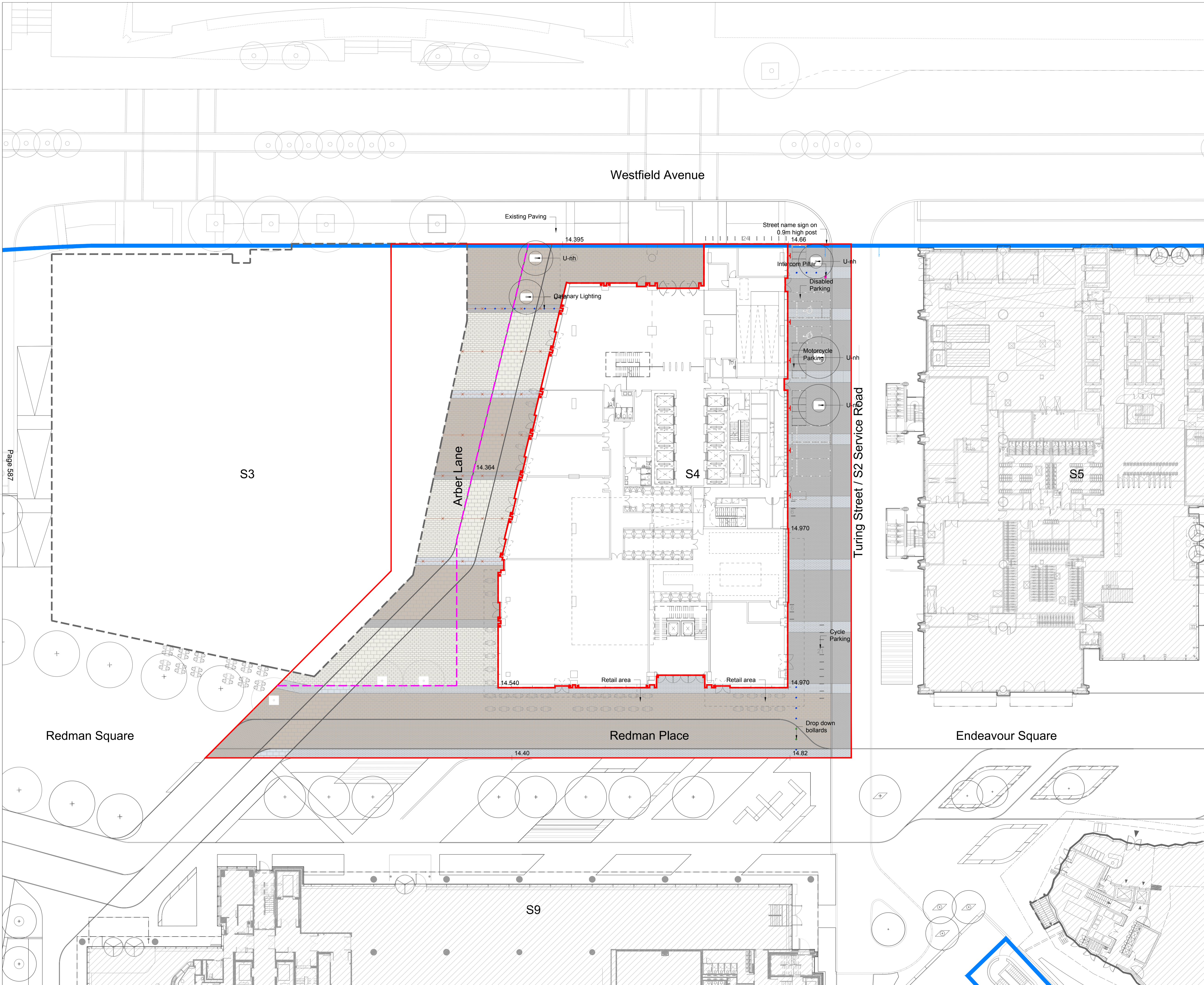
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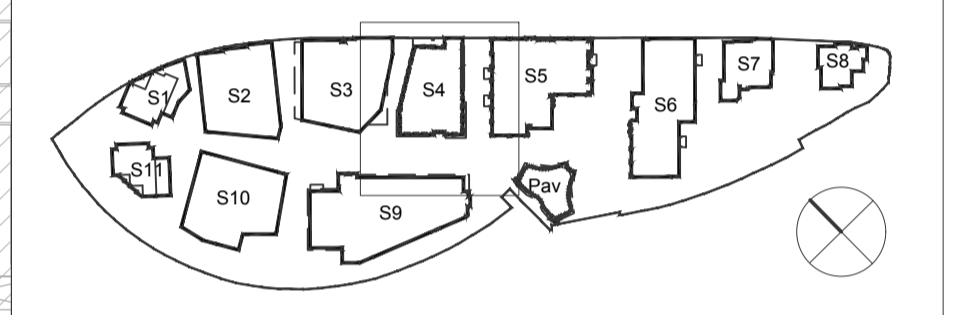
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 IQL-A-AFK-S4-XXX-DR-06-303

**REVISION**  
 P00

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- IQL Boundary line
- Phase boundary
- RMA Phase Boundary - Temporary Landscape
- Emerging IQL Masterplan (subject to future Masterplan applications)
- Consented ZMP Buildings
- Existing buildings at IQL
  
- + Proposed levels
- + Existing levels
  
- HARDSCAPE**
- Metal lines
  
- FURNITURE**
- + Signage
- + Intercom / Access pillar
- Cycle racks
- P68 Static bollards
- Bollards
- x Catenary lighting
  
- SOFTSCAPE**
- Proposed tree
  
- HARDSCAPE**
- Manhole covers
- Sandstone paving/setts
- Resin Bound gravel\_Light buff



P00	06/07/2018	TB	MB	MB	For planning
Revision	Date	By	Chkd	Appr	Description

Landscape Architect

**Gustafson  
Porter +  
Bowman**

Architect <b>Arney Fender Katsalidis</b>	Engineer <b>BUROHAPPOLD ENGINEERING</b>
Project Manager	Specialist

Client

**lendlease LCR**

Project  
International Quarter London

Drawing Title  
**General Arrangement Plan**

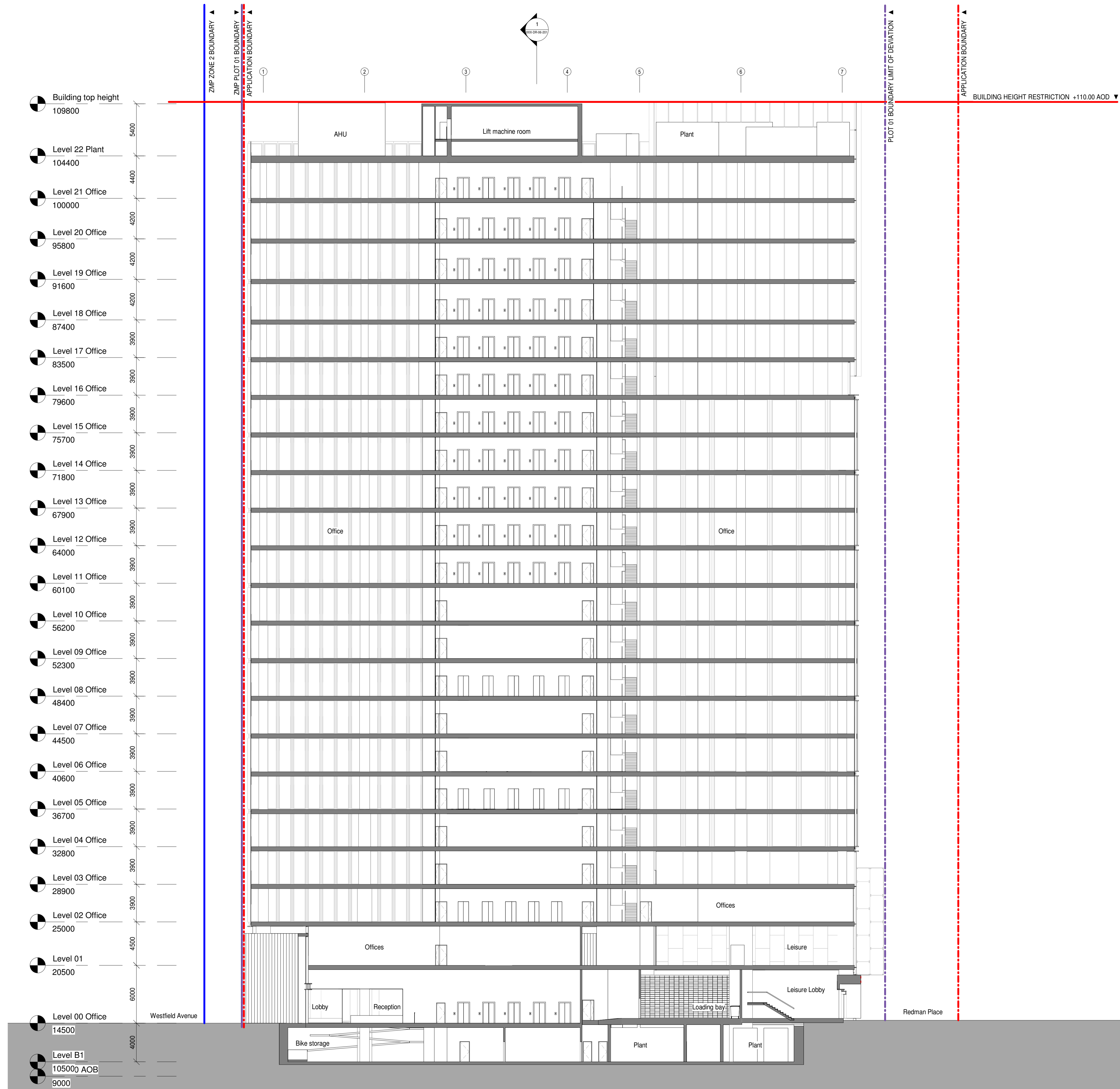
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Scale 1:250@A1 1:500@A3	Date 06/07/2018	Revision P00
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**KEY**

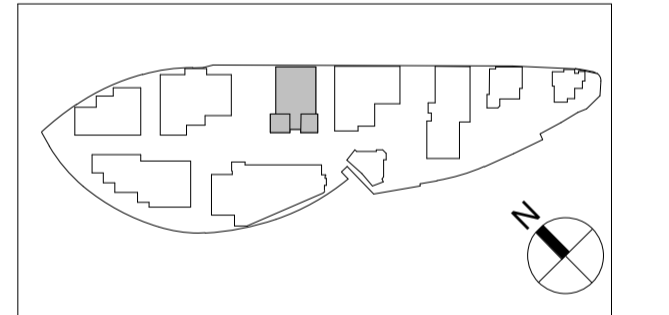
- Application boundary
- 50 m Zone (offset from application boundary)
- SCBD Ltd Land ownership
- S4 Building Line
- 110 m AOD - Max. Height Limit
- ZMP Plot 01 Boundary
- Plot 01 Boundary Limit of Deviation +/- 10m

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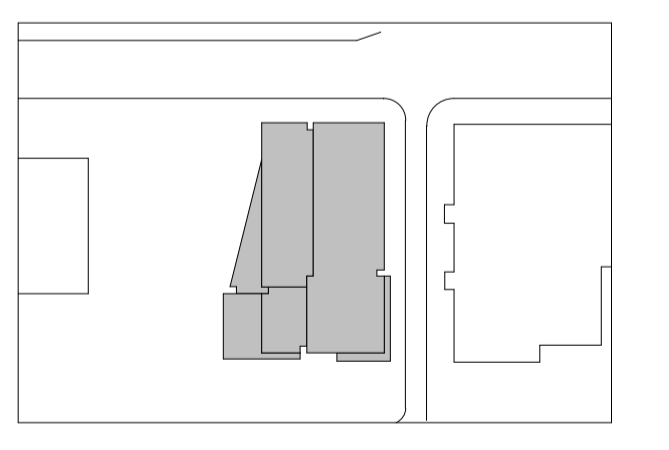


**LOCATION PLAN**



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**KEY PLAN**



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 44-46 Scutcheon Street, London, EC2A 4HH

**PROJECT** IQL -S4 **PROJECT No.** 44079

**DRAWING TITLE** Building Section BB

**STATUS** For Planning

**DRAWING DATE** 06/07/18 **SCALE** 1:200 @ A1

**DRAWING No.** IQL-A-AFK-S4-XXX-DR-06-202 **REVISION** P00

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FRAME PROJECTS

## **London Legacy Development Corporation Quality Review Panel**

### **Report of Formal Review Meeting: IQL Plot S4**

Thursday 3 May 2018

Level 10, 1 Stratford Place, Montfichet Road, London E20 1EJ

#### **Panel**

Peter Studdert (chair)  
Simon Henley  
Mike Martin  
Kelvin Campbell  
David Bonnett

#### **Attendees**

Daniel Davies	LLDC Planning Policy and Decisions Team
Richard McFerran	LLDC Planning Policy and Decisions Team
Sophie Backhouse	London Legacy Development Corporation
Tessa Kordeczka	Frame Projects

#### **Apologies / report copied to**

Anthony Hollingsworth	LLDC Planning Policy and Decisions Team
Catherine Smyth	LLDC Planning Policy and Decisions Team
Ben Hull	London Borough of Newham

## 1. Project name and site address

Plot S4, Zone 2, Stratford City (known as International Quarter London (IQL))

## 2. Presenting team

Earle Arney	Arney Fender Katsalidis
Peter Vrahimis	Arney Fender Katsalidis
James Cheung	Arney Fender Katsalidis
Mary Bowman	Gustafson Porter + Bowman
Tim Makower	Makower Architects
Adam Peavoy	Makower Architects
Harry Hon	Lendlease
Jonathan Pinkney	Lendlease
Steffan Rees	Quod

## 3. Planning authority's views

The planning authority has had several meetings with the design team and considers that the design for IQL Plot S4 is developing in the right direction. Revisions to the design have responded to previous comments by the Quality Review Panel, including by improving legibility through IQL, notably through Arber Lane.

Building S4 will be required to meet the requirements of LLDC Local Plan Policy BN.10 on tall buildings, including 'outstanding' architecture.

## 4. Quality Review Panel's views

### *Summary*

The Quality Review Panel welcomes the revisions made to the design of the building on Plot S4, as well as the projection of the design of the building on neighbouring Plot S3. A much clearer route through IQL is established – in line with the principles of the IQL South masterplan. The panel recommends further exploration of how Turing Street might become more activated. The proposed architectural expression of building S4, including the articulation of façades and materials, shows promise; its success will depend on the quality of detailing, materials and construction. The panel repeats its support for the landscape design and public realm associated with Plot S4. It recommends continuing analysis of microclimatic conditions across IQL South. These comments are expanded below.

### *Response to masterplan*

- The panel commends the refinements made to the design of building S4. These represent a significant improvement on earlier iterations. The simplification of the design, together with the projection of the design of building S3, are a rational response to reinforcing legibility through IQL. A natural flow of movement is established.



- The panel welcomes the fact that key principles for the design of building S3 will be safeguarded in the design codes to be developed as an addendum to the IQL outline planning permission. These will include specifying a diagonal along Arber Lane; the height of the base and the undercut; and the extent of the active frontage and public realm. The projected design of building S3 features a row of columns – the panel would be interested to see how these might be developed.
- The clarity of the entrance to office accommodation on Westfield Avenue, with an entrance lobby wrapping around into Arber Lane, as well as retail continuing along the length of Arber Lane, are successful moves.
- The panel would encourage initiatives to maximise transparency and activation along Turing Street. It suggests exploring possible ways of locating inactive uses further into the interior of the building and active uses to the frontage along Turing Street (see also comment below).
- Further clarity of how obvious the entrance to building S9 will be from the route along Arber Lane would be helpful.

#### *Architectural expression*

- The panel repeats its support for the device of separate ‘zones’ for the buildings: base / podium; middle; and upper. The base zone allows more flexibility in the design of the public realm and results in a stronger human scale at street level.
- The architectural expression developed for building S4 displays a clear narrative. The fragmented form of the massing and the modular articulation of the façades work well. The panel would be interested to see further details of the treatment of the façades.
- The panel broadly supports the proposed materials, including dark metal and concrete. These have the potential to result in a distinctive building that differs from, but sits well with, the other IQL buildings.
- The building’s success will depend on how well it is crafted: exceptional detailed design, materials and construction will be essential to achieve the quality sought. Detailing should be as crisp as possible, including for the proposed projecting ‘boxes’. The panel would welcome the opportunity to see how details of the design are developed in order to be able to advise on compliance with Policy BN.10.
- The panel recommends that signage be incorporated at this stage of the design.



### *Landscape design and public realm*

- The panel repeats its broad support for the landscape and public realm strategy associated with Plot S4, which can be expected to result in delightful public spaces.
- The panel's principal concern in relation to the landscape and public realm was successful interpretation of a masterplan that envisages Arber Lane as a major thoroughfare. The revised design is a marked improvement in this respect.
- The panel repeats the importance of not neglecting how Turing Street will be used and experienced by pedestrians. While this is a service road, necessarily wide, the panel would support interventions to enliven this street and encourage use by pedestrians. In this respect, it welcomes the inclusion of the ramp to basement cycle storage from Turing Street.

### *Microclimatic conditions*

- The panel recommends continuing analysis of microclimatic conditions, including wind levels through IQL South, and consideration of mitigating factors, including through the buildings' architecture.

### *Inclusive design*

- The panel points to the comparatively long distances to be navigated through IQL South and seeks assurances that access by taxis will be efficiently managed.

### *Next steps*

- The Quality Review Panel encourages the design team to continue to refine the design for IQL Plot S4, taking into account the comments above and in consultation with planning officers.
- It would welcome the opportunity to provide final comments, after submission of a planning application, including on compliance with Policy BN.10.





**Subject: Decisions made under Delegated Authority**

**Meeting date: Tuesday 25 September 2018**

**Report to: Planning Decisions Committee**

**Report of: Anthony Hollingsworth, Director of Planning Policy and Decisions**

FOR NOTING

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**This report will be considered in public**

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**1. SUMMARY**

This report appends a list of all decisions taken by officers between 1 - 31 July 2018 and 1 – 31 August 2018.

**2. RECOMMENDATION**

**2.1 The Committee is asked to note the report and the attached Appendix 1 and 2.**

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**LLDC Delegated Decisions Report - July 2018**

**Appendix 1**

<u>Application Number</u>	<u>Application Type</u>	<u>Registration Date</u>	<u>Location</u>	<u>Full Development Description</u>	<u>Applicant</u>	<u>Decision Date</u>	<u>Decision Description</u>	<u>Officer Name</u>	<u>Officer Comments</u>
18/00243/106	Section 106 Details	08/05/2018	Bobby Moore Academy (Secondary), Planning Delivery Zone 4, Land east of Lea Navigation River and west of the Olympic Stadium, (Stadium Island)	Details submitted pursuant to Section 106 Agreement – Schedule 1, Clause 2 (School Facilities Dual Use Agreement) in relation to full planning permission ref: 16/00035/FUL dated 27 January 2017.	Savills	02/07/2018	S106 Response Letter	Russell Butchers	Agrees details of facilities within Bobby Moor academy available for use by community organisations and the hours, as required by the s106.
18/00203/AOD	Approval of details (conditions)	20/04/2018	Land at Cooks Road, Pudding Mill Lane, Stratford, London, E15 2PW	Submission of details pursuant to Condition 27 (Servicing Management Plan) of planning permission reference 15/00392/FUL dated 12th August 2016 as varied by 16/00534/VAR as it relates to the development at Cooks Road.	Bellway Homes (Thames Gateway) C/O Agent	03/07/2018	Approve	Richard McFerran	
18/00245/FUL	Full planning application	11/05/2018	Angel Lane, Westfield, Zone 1, London, E15 1BB	Full planning permission for the construction of a temporary station escape stair and associated works.	Westfield UK Property Developments Ltd	04/07/2018	Approve	Anne Ogundiya	Temporary permission (1st October 2021) for the installation of a temporary escape stair to accommodate a direct means of escape along Great Eastern Road from Stratford Station underground tunnels. The works are temporary until such time as the new station escape approved as part of the Angel Lane Office development (Phase 2 of planning permission 16/00524/FUL) is delivered. The development is of a minor nature and as such it is appropriate for this application to be delegated.
18/00204/AOD	Approval of details (conditions)	20/04/2018	Land at Cooks Road, Pudding Mill Lane, Stratford, London, E15 2PW	Approval of Details submitted pursuant to Condition 28 (Car Parking Management (Blue Badge)) of planning permission reference 15/00392/FUL dated 12th August 2016 as varied by 16/00534/VAR as it relates to the development at Cooks Road.	Bellway Homes (Thames Gateway) C/O Agent	04/07/2018	Approve	Richard McFerran	
16/00454/AOD	Approval of details (conditions)	13/09/2016	Chobham Farm Development Site, (Zone 4), Leyton Road, Stratford, E15 1DN	Submission of details pursuant to condition AZ.32 (Piling Strategy) of planning permission 12/00146/FUM in relation to Chobham Farm Zone 4.	Telford Homes	05/07/2018	Approve	Hilary Wrenn	
17/00644/106	Section 106 Details	07/12/2017	Zone 2, Chobham Farm Development, Thornham Grove, Penny Brookes Street, Stratford, London	Submission of details of compliance in relation to Schedule 3 Clauses 5.1.1 (Affordable Housing Statement) and 5.1.2 (Affordable Housing Management Scheme), Schedule 5 Clauses 1.5.1, 1.5.2, 1.5.3 (Zonal Family Housing Report) associated with the S106 agreement for planning consent 12/00146/FUM as amended by 14/00439/NMA and 14/00440/NMA and planning consent 17/00175/REM for Chobham Farm Zone 2.	C/O Agent	05/07/2018	S106 Response Letter	Hilary Wrenn	This approves the amount of 3+ bedroom units as 44.1% of all units; overall affordable housing within the scheme is 35% of units (of which 60% are affordable rent and 40% shared ownership) Affordable Housing provider is London and Quadrant
18/00202/AOD	Approval of details (conditions)	20/04/2018	Land at Cooks Road, Pudding Mill Lane, Stratford, London, E15 2PW	Partial approval of details (Residential Signage Only) submitted pursuant to Condition 5 (Signage Details) of planning permission reference 15/00392/FUL dated 12th August 2016 (as varied by 16/00534/VAR).	Bellway Homes (Thames Gateway) C/O Agent	05/07/2018	Approve	Richard McFerran	
15/00513/AOD	Approval of details (conditions)	15/10/2015	Chobham Farm Site Zone 4, Leyton Road, Stratford, London, E15 1DR	Application for Approval of Details pursuant to Conditions AZ.25 (Site Specific Remediation Strategy) AZ.26 (Remediation Method Statements) & AZ.33 (Piling & Groundwater Impacts) of planning permission reference 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA.	Telford Homes Plc	05/07/2018	Approve	Hilary Wrenn	
15/00433/AOD	Approval of details (conditions)	24/08/2015	Chobham Farm Site, Zone 4, Leyton Road	Submission of details pursuant to condition AZ.24 12/00146/FUM Intrusive Investigation Method Statement relating to Contamination.	Telford Homes	05/07/2018	Approve	Hilary Wrenn	
17/00285/AOD	Approval of details (conditions)	30/06/2017	Zone 2, Chobham Farm Development, Thornham Grove, Penny Brookes Street, Stratford, London	Approval of Details submitted pursuant to Condition AZ.74 (Details of Public Realm Lighting) of planning permission reference 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA.	Higgins Homes Plc	05/07/2018	Approve	Grant McClements	
18/00269/106	Section 106 Details	21/05/2018	25-37 Rothbury Road, Hackney Wick, London, E9 5EN	Submission of details pursuant to Schedule 1 (Affordable Housing), Clause 2.1 (Affordable Housing Providers) of the Section 106 Agreement associated with planning permission 16/00441/FUL dated 12 December 2017.	Aitch Group	09/07/2018	S106 Response Letter	Hilary Wrenn	Housing provider is The Home Group and LBTH Housing indicated they have agreed this.
18/00253/SCRES	Screening Opinions	16/05/2018	The Pavilion, Endeavour Square, International Quarter London (IQL), Stratford City Zone 2, London, E15 2EG	Formal Request for an Environmental Impact Assessment (EIA) Screening Opinion under Regulation 6 of the Town and County Planning (Environmental Impact Assessment) Regulations 2017 (as amended) insofar as it relates to the approval of Reserved Matters pursuant to conditions B1, B8 and B9 of the Stratford City Outline Planning Permission (Ref: 10/90641/EXTODA) comprising details of layout, scale, appearance, access and landscaping of the IQL Pavilion, comprising a part two, part three storey building for complementary retail (Use Classes A1-A5) with associated works.	Stratford City Business District Limited	09/07/2018	Screening not required	Josh Hackner	The nature of this application is such that a delegated decision is appropriate.
18/00074/AOD	Approval of details (conditions)	19/02/2018	Cherry Park, Westfield Avenue/Montfichet Road, Zone 1, Stratford City, London	Application for Approval of Details pursuant to Condition A2 (Construction Transport Management Plan) of planning permission reference 15/00358/OUT dated 13 February 2018.	Stratford City Developments Limited	09/07/2018	Approve	Russell Butchers	
18/00072/AOD	Approval of details (conditions)	19/02/2018	Cherry Park, Westfield Avenue/Montfichet Road, Zone 1 Stratford City, London	Application for Approval of Details pursuant to Condition O2 (Reserved Matters – Phasing) of planning permission reference 15/00358/OUT dated 13 February 2018.	Stratford City Developments Limited	09/07/2018	Approve	Russell Butchers	
18/00248/FUL	Full planning application	16/05/2018	Unit SU2013A, Westfield Stratford City, Montfichet Road, London, E20 1EJ	Change of use of unit SU2013A from retail (Use Classes A1 A2 and A3) to use for the sale of cosmetic treatments (including cosmetic surgery) (Sui Generis Use).	Westfield Europe Ltd c/o Agent	09/07/2018	Approve	Sara Dawes	A change of use of a shop unit within Westfield; the new use was considered to be acceptable in the town centre, minor in nature and non-controversial, so a delegated decision was appropriate.

**LLDC Delegated Decisions Report - July 2018**

**Appendix 1**

<u>Application Number</u>	<u>Application Type</u>	<u>Registration Date</u>	<u>Location</u>	<u>Full Development Description</u>	<u>Applicant</u>	<u>Decision Date</u>	<u>Decision Description</u>	<u>Officer Name</u>	<u>Officer Comments</u>
17/00297/AOD	Approval of details (conditions)	04/08/2017	Zone 2, Chobham Farm Development, Thornham Grove, Penny Brookes Street, Stratford, London	Approval of Details submitted pursuant to Conditions AZ.73 (Site-Wide Inclusive Access Strategy) of planning permission reference 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA as it relates to Zone 2 of the Chobham Farm development.	Higgins Homes Plc	09/07/2018	Approve	Grant McClements	
17/00681/106	Section 106 Details	12/12/2017	Zone 2, Chobham Farm Development, Thornham Grove, Penny Brookes Street, Stratford, London	Submission of details pursuant to Schedule 10 (Zonal Travel Plans), Clauses 1.1.1 (Zonal Travel Plan) and 1.1.2 (Travel Plan Monitoring Officer) of the Section 106 Agreement associated with planning permission 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA and planning consent 17/00175/REM for Chobham Farm Zone 2.	Lichfields	09/07/2018	S106 Response Letter	Grant McClements	This application involved the approval of a Zonal Travel Plan for Chobham Farm Zone 2 and evidence that a Travel Plan Monitoring Officer had been appointed.  Decision delegated as officers were satisfied that the submitted details adhered to the obligation requirements.
18/00242/AOD	Approval of details (conditions)	10/05/2018	206-214 High Street, Stratford, London, E15 2JA	Submission of Details submitted pursuant to condition 25 (Noise Assessment/ Noise Mitigation Measures) of planning permission reference 13/00404/FUM dated 29 July 2014 (as varied by non-material amendments references 14/00357/NMA dated 22 October 2014 and 15/00102/NMA dated 19 May 2015.	Alumno Developments	09/07/2018	Approve	Russell Butchers	
17/00475/AOD	Approval of details (conditions)	18/09/2017	415 Wick Lane, London, E3 2JG	Submission of details to partially discharge conditions 12 (Contamination- Risk Assessment) and 15 (Contamination – Monitoring and Maintenance)of planning permission 16/00685/FUL dated 13th September 2017.	Taylor Wimpey East London	11/07/2018	Approve	Richard McFerran	
17/00618/AOD	Approval of details (conditions)	29/11/2017	415 Wick Lane, London, E3 2JG	Submission of details to partially discharge condition 11 (Piling Method Statement) of planning permission 16/00685/FUL dated 13th September 2017.	Taylor Wimpey	11/07/2018	Approve	Richard McFerran	
18/00246/AOD	Approval of details (conditions)	11/05/2018	25-37 Rothbury Road, London, E9 5LN	Approval of Details submitted pursuant to Condition 10 (Detailed Drawings) of planning permission reference 16/00441/FUL dated 12 December 2017.	Aitch Group	12/07/2018	Approve	Hilary Wrenn	
18/00288/AOD	Approval of details (conditions)	25/05/2018	Land at Cooks Road & Bow Back River, Pudding Mill Lane, London, E15 2PW	Partial approval of details pursuant to Condition 31 (BREEAM) of planning permission reference 15/00392/FUL dated 12th August 2016, as varied by 16/00534/VAR as it relates to the development at Cooks Road.	Bellway Homes (Thames Gateway) C/O Agent	12/07/2018	Approve	Daniel Davies	
17/00661/AOD	Approval of details (conditions)	22/12/2017	206-214 High Street, Stratford, London, E15 2JA	Submission of Details submitted pursuant to conditions 6 (Shopfront Glazing) 12 (Energy Strategy) and 29 (Energy Modelling) of planning permission reference 13/00404/FUM dated 29 July 2014.	Alumno Developments	12/07/2018	Approve	Russell Butchers	
17/00371/AOD	Approval of details (conditions)	09/08/2017	Zone 2, Chobham Farm Development, Thornham Grove, Penny Brookes Street, Stratford, London	Approval of Details submitted pursuant to Condition AZ.56 (Landscaping) of planning permission reference 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA as it relates to Zone 2 of the Chobham Farm development.	Higgins Homes Plc	13/07/2018	Approve	Hilary Wrenn	
18/00263/FUL	Full planning application	24/05/2018	Unit 1, 39, Autumn Street, London, E3 2TT	Alterations to rear elevation of Unit 1 to replace two existing windows and one existing door with two doubled glazed windows and a double glazed door.	Studio Mama	16/07/2018	Approve	Russell Butchers	Minor works to the property.The development was consistent with the relevant policies of the Local Plan.
18/00268/AOD	Approval of details (conditions)	24/05/2018	Bridge Wharf, 1, High Street, Stratford, LONDON, E15 2QA	Approval of details to discharge condition 4 (details of finish) pursuant to advertisement consent reference 17/00553/ADV dated 31st January 2018	JCDecaux UK Limited	17/07/2018	Approve	Anne Ogundiya	
18/00349/AOD	Approval of details (conditions)	10/07/2018	Eastwick Phase 1, Development Parcels 5.5 and 5.9, Planning Delivery Zone 5, Queen Elizabeth Olympic Park, London	Submission of details pursuant to Condition LCS0.255 (Notice of Commencement) of outline planning permission 11/90621/OUTODA dated 28 September 2012 (as varied by planning references 14/00036/VAR dated 11 August 2014 and 17/00236/VAR dated 03 May 2018) of the Legacy Communities Scheme, in respect of the Zonal Masterplan for Planning Delivery Zone 5 (East Wick).	East Wick and Sweetwater Projects & LLDC	17/07/2018	Approve	Anne Ogundiya	
18/00327/NMA	Non-Material Amendment (Section 96A applications)	14/06/2018	London Aquatics Centre, Stratford, London, E20 2ZQ	Application for Non-Material Amendments to vary wording of condition 1 (approved drawings) of planning permission reference 08/90026/REMODA - dated 05 June 2008 for alterations to the entrance doors.	LLDC- Development	17/07/2018	Granted NMA	Anne Ogundiya	Application to remove the venue's existing double height main entrance doors sliding glass doors and replace with standard height glass doors. The nature and scale of the proposed development would not impact on the original architectural intent approved under the parent planning permission (08/90026/REMODA). The proposed changes and the decision could therefore be delegated.
18/00300/AOD	Approval of details (conditions)	04/06/2018	Eastwick Phase 1, Development Parcels 5.5 and 5.9, Planning Delivery Zone 5, Queen Elizabeth Olympic Park, London	Submission of details pursuant to Condition LCS0.213 (Cycle Parking Details) of outline planning permission 11/90621/OUTODA dated 28 September 2012 (as varied by planning references 14/00036/VAR dated 11 August 2014 and 17/00236/VAR dated 03 May 2018) of the Legacy Communities Scheme, in respect of the Zonal Masterplan for Planning Delivery Zone 5 (East Wick).	East Wick and Sweetwater Projects & LLDC	17/07/2018	Approve	Anne Ogundiya	
18/00259/AOD	Approval of details (conditions)	17/05/2018	Eastwick Phase 1, Development Parcels 5.5 and 5.9, Planning Delivery Zone 5, Queen Elizabeth Olympic Park, London	Application for the Discharge of Condition LCS0.61 (Construction Dust); and LCS0.78 (Construction Vibration) of the Legacy Communities Scheme planning permission 11/90621/OUTODA, (as varied by 14/00036/VAR dated 11th August 2014 and 17/00236/VAR dated 03 May 2018) in so far as it relates to Eastwick Phase 1, PDZ5.	East Wick and Sweetwater Projects and LLDC	17/07/2018	Approve	Anne Ogundiya	



**LLDC Delegated Decisions Report - July 2018**

**Appendix 1**

<u>Application Number</u>	<u>Application Type</u>	<u>Registration Date</u>	<u>Location</u>	<u>Full Development Description</u>	<u>Applicant</u>	<u>Decision Date</u>	<u>Decision Description</u>	<u>Officer Name</u>	<u>Officer Comments</u>
18/00292/NMA	Non-Material Amendment (Section 96A applications)	29/05/2018	Endeavour Square, Westfield Avenue, Stratford, London, E20 1GL	Application under Section 96A of the Town and Country Planning Act 1990 for non-material amendments to planning permission 16/00523/REM comprising: Updated Pavilion location and design; Redesign of the space behind the pavilion; removed bike parking location behind the pavilion; updated number (10) and bike parking location on the right side of the pavilion; number of Birches to the right of the pavilion increased from 5 to 6; updated lighting column location associated with birches cluster relocation (right side of the pavilion); updated drinking fountain location (right side of the pavilion); updated legible London monolith location (right side of the pavilion); updated bin location (right side); added PT5 Cabinet (Air quality monitoring station); reduced bollards number (4) , location and alignment of bollards (left side of the pavilion); integrated bike parking aligned with Automatic rising bollards to the left of the pavilion; updated metal paving lines arrangement across the square; increased number of birches (1) to the left side of the pavilion; updated central turning island design – levels, location, and surface treatment; relocated Acer campestre tree from the Turing Street to the left side area of the pavilion; artwork by Troika relocated from Phase 1B to Phase 1C; Updated paving layout on Turing Street; added bird, bat and bug boxes on Pin Oaks and Lime trees; catenary lighting shown in the Olympic Promenade; added retail pergolas, planters, tables and chairs at the Olympic Promenade; integrated wind mitigation screen and canopy (Bike shelter) on Turing Street; motorcycle and permit holders parking signs integrated on Turing Street; updated bins location and number across the square; updated design of tree grilles across the square; Westfield Avenue pavilion reinstatement included inside red line boundary and blue badge parking for pavilion added on Turing Street.	Stratford City Business District Ltd.	18/07/2018	Granted NMA	Josh Hackner	The amendments related to the public realm within Endeavour Square to acknowledge the footprint of the Pavilion building which was approved at Planning Decisions Committee in July. The nature of the amendments were considered appropriate for delegated decision. Alterations would not alter the original assessment or outcome of the planning permission, as granted.
18/00276/106	Section 106 Details	21/05/2018	25-37 Rothbury Road, Hackney Wick, London, E9 5EN	Submission of details pursuant to Schedule 7 (Design Monitoring), Clause 2.2 (Retention of Design Team) of the Section 106 Agreement associated with planning permission 16/00441/FUL dated 12 December 2017.	Aitch Group	18/07/2018	S106 Response Letter	Hilary Wrenn	Confirmation that Hawkins Brown architects have been retained through the construction period to oversee design/materials development
18/00052/106	Section 106 Details	02/02/2018	Chobham Farm Site Zone 4, Leyton Road, Stratford, London, E15 1DR	Submission of details pursuant to the (Owner's Covenants with the LPA) Anticipated Commencement Date, Paragraphs 5.1.4, 5.1.5 and 5.1.6 of the Section 106 Agreement associated with planning permission 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA.	Telford Homes Plc	18/07/2018	S106 Response Letter	Hilary Wrenn	Telford Homes notifying of construction commencement 10th December 2015 as required by s106 (late notification)
18/00205/106	Section 106 Details	18/04/2018	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details pursuant to section 8 (security planters and bollards), part 4 of schedule 1 of the S106 Legal Agreement associated with planning permission 10/90285/FUMODA as amended by 13/00579/VAR, dated 28 October 2014.	Manhattan Loft Gardens Ltd & LCR	18/07/2018	S106 Response Letter	Sara Dawes	Application confirming compliance with a clause in the S106. Appropriate for delegated decision.
18/00121/106	Section 106 Details	26/02/2018	1 Smeed Road and 79-85 Monier Road, London, E3 2PS	Submission of details to discharge Schedule 4 (Travel Plan) of the Section 106 Agreement associated with planning permission (approved under Full ref: 14/00374/FUL as amended by 17/00227/VAR).	Weston Homes Plc	19/07/2018	S106 Response Letter	Josh Hackner	Details submitted were considered to comply with the aims and terms of the S106 requirements.
18/00120/106	Section 106 Details	26/02/2018	1 Smeed Road and 79-85 Monier Road, London, E3 2PS	Submission of details pursuant to Schedule 3 (Sustainable Transport) Clause 2 (Car Club) of the Section 106 Agreement associated with planning permission (approved under Full ref: 14/00374/FUL as amended by 17/00227/VAR).	Weston Homes Plc	19/07/2018	S106 Response Letter	Josh Hackner	Details submitted were considered to comply with the aims and terms of S106 requirements.
17/00362/AOD	Approval of details (conditions)	07/08/2017	Endeavour Square, Westfield Avenue, Stratford, London, E20 1GL	Partial Approval of Details submitted pursuant to conditions J3 (Ecological Management Plan) and Q2 (Landscape Management and Maintenance Plan) attached to the outline planning permission 10/90641/EXTODA dated 30/03/2012 in so far as it relates to Endeavour Square (Formerly International Square).	Stratford City Business District Limited	19/07/2018	Approve	Josh Hackner	
18/00183/106	Section 106 Details	10/04/2018	206-214 High Street, Stratford, London, E15 2JA	Submission of details pursuant to Schedule 2 (Contributions and General Obligations), Paragraph 5 (Residential and Facilities Management) and Schedule 3 (Affordable Workspace and Local Employment), Paragraph 3 (Local supplier and contractors), of the s106 agreement for planning permission 13/00404/FUM dated 29 July 2014.	Alumno Developments (Stratford) Ltd.	19/07/2018	S106 Response Letter	Russell Butchers	The requirements of the s106 agreement were met and the relevant obligations discharged.
18/00261/106	Section 106 Details	24/05/2018	25-37 Rothbury Road, Hackney Wick, London, E9 5EN	Submission of details pursuant to Schedule 2, paragraph 2 (Evidence of Commencement) of the Section 106 Agreement associated with planning permission 16/00441/FUL dated 12 December 2017 - mixed use development at 25-37 Rothbury Road.	Rothwick LLP	20/07/2018	S106 Response Letter	Hilary Wrenn	Evidence that commencement of development began on 13th January 2018 as required by s106 agreement
18/00294/AOD	Approval of details (conditions)	04/06/2018	33-35 Monier Road, London, E3 2PR	Approval of Details submitted to fully discharge condition 16 (Approval of Road Works Necessary) associated with planning permission 15/00212/FUL dated 24 March 2016 in so far as it relates to the approved development known as 33-35 Monier Road.	Monier Road Ltd c/o Agent	20/07/2018	Approve	Anne Ogundiya	
15/00560/AOD	Approval of details (conditions)	03/11/2015	Chobham Farm Site Zone 4, Leyton Road, Stratford, London, E15 1DR	Application for Approval of Details pursuant to Conditions AZ.2 (Notice of Commencement) AZ.7 (Hours of Work) AZ.8 (On-Site Construction Facilities) AZ.10 (Inspection of Construction Work) AZ.11 (Fencing) AZ.12 (Temporary Highway Access) AZ.13 (Construction Delivery Arrangements) AZ.14 (Construction Dust) AZ.15 (Construction Noise & Vibration) AZ.19 (Vibration) AZ.20 (Demolition and Site waste management strategies) & AZ.100 (Storage of Hazardous Materials) of planning permission reference 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA.	Telford Homes	20/07/2018	Approve	Hilary Wrenn	
18/00328/NMA	Non-Material Amendment (Section 96A applications)	26/06/2018	London Stadium, Queen Elizabeth Olympic Park, London, E20 2ST	Application for non-material amendments to the wording of condition 3 (Phasing) of planning permission 15/00223/FUL dated 28th July 2015, to assist in the deliverability of the panels within Champions Place.	E20 Stadium LLP	20/07/2018	Granted NMA	Daniel Davies	The application was considered to be non-material as the changes proposed would not result in a scheme that would be material different to the parent scheme.

**LLDC Delegated Decisions Report - July 2018**

**Appendix 1**

<u>Application Number</u>	<u>Application Type</u>	<u>Registration Date</u>	<u>Location</u>	<u>Full Development Description</u>	<u>Applicant</u>	<u>Decision Date</u>	<u>Decision Description</u>	<u>Officer Name</u>	<u>Officer Comments</u>
16/00663/106	Section 106 Details	02/12/2016	Here East, Waterden Road, Queen Elizabeth Olympic Park, London, E20 3BS	Submission of details pursuant to Schedule 5 (Community Uses) of the Section 106 Agreement for planning permission 13/00534/FUM and 13/00536/COU dated 1st April 2014.	ICITY	20/07/2018	S106 Response Letter	Sophie Hockin	Application confirmed compliance with a clause in the S106.
18/00237/106	Section 106 Details	08/05/2018	Queen Elizabeth Olympic Park, Stratford, London	LCS BAP monitoring report for 2015 and 2016 pursuant to paragraph 1.3.1 (Monitoring Report), Schedule 10 (Green Infrastructure- BAP and Publicly Accessible Open Space) of the LCS Section 106 Agreement (LPA ref. 11/90621/OUTODA as varied by 14/00036/VAR and 17/00236/VAR).	LLDC- Development	23/07/2018	S106 Response Letter	Russell Butchers	The submitted BAP monitoring report was satisfactory and met the requirements of the obligation.
18/00291/AOD	Approval of details (conditions)	29/05/2018	Legacy Tower, 88 Great Eastern Road, London, E15 1DE	Submission of Details to partially discharge condition 21 (Code for Sustainable Homes) in relation to full planning permission ref: 13/00322/FUL in so far as it relates to the Legacy Tower only.	Telford Homes Plc	23/07/2018	Approve	Anne Ogundiya	
18/00219/106	Section 106 Details	12/04/2018	Bobby Moore Academy (Secondary), Planning Delivery Zone 4, Land east of Lea Navigation River and west of the Olympic Stadium, (Stadium Island)	Details submitted pursuant to Legacy Communities Scheme (LCS) Section 106 Agreement – Schedule 8, Paragraph 4.12.1 (Anticipated School Opening Date) in relation to outline planning permission ref: 11/90621/OUTODA as varied by 14/00036/VAR, insofar as it relates to the Bobby Moore Academy (Secondary) only.	LLDC- Development	23/07/2018	S106 Response Letter	Russell Butchers	Notice given of opening of secondary school on 10 September 2018 as required by s106
17/00404/AOD	Approval of details (conditions)	22/08/2017	Duncan House, High Street, Stratford, London, E15 2JB	Submission of details pursuant to fully discharge conditions 21 (Landscaping Plan) 23 (Tree Planting) and 26 (Playspace) attached to full planning permission reference 15/00598/FUL dated 25 August 2016 in so far as it relates to the approved development at the Duncan House site.	Watkin Jones Group Ltd	23/07/2018	Approve	Grant McClements	
18/00319/AOD	Approval of details (conditions)	15/06/2018	Hackney Wick Overground Station, Wallis Road, Hackney, London, E9 5ER	Application for approval of details pursuant to condition 16-vii (Sample Materials & Details) of planning permission 14/00275/FUL.	VolkerFitzpatrick Ltd	25/07/2018	Approve	Anne Ogundiya	
18/00190/106	Section 106 Details	26/03/2018	Plot N24 (Manhattan Loft Gardens), Zone 3, Stratford City Development, Stratford, London	Submission of details pursuant to Part 1, Clause 1.6 of Section 106 Agreement of planning permission 10/90285/FUMODA dated 11 June 2010 as amended by 13/00423/VAR dated 21 March 2014 in respect of a Residential Waste amangement Plan.	Manhattan Loft Gardens Ltd	25/07/2018	S106 Response Letter	Russell Butchers	The submitted residential waste management plan was acceptable.
17/00576/AOD	Approval of details (conditions)	09/11/2017	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details to discharge Condition 7 (Proposed Disabled Toilets) attached to planning permission 10/90285/FUMODA as amended by 13/00423/VAR, dated 21 March 2014 insofar as it relates to the ground floor and mezzanine only.	Manhattan Loft Gardens Ltd and LCR	25/07/2018	Approve	Daniel Davies	
18/00083/FUL	Full planning application	21/02/2018	Unit 2, Maverton Road Industrial Estate, Maverton Road, LONDON, E3 2JE	Full planning permission for use of the existing premises to allow for flexible uses within Classes B1(c) and/or Class B2 and/or Class B8, and/or plant/tool hire (Sui Generis) including the use of the forecourt as open storage.	LaSalle Investment Management on behalf	25/07/2018	Approve	Daniel Davies	There are no significant transport effects anticipated to arise from the propose scheme and subject to restrictions on hours of operation of B2 uses and storage of materials externally, officers are satisfied that site can function without harm to the emerging residential context.
18/00036/AOD	Approval of details (conditions)	30/01/2018	Plot M7, Zone 1, Stratford City	Submission of details pursuant to conditions O3 (Perimeter noise monitoring) and O4 (Noise assessment and mitigation measures for permanent plant) of planning permission 10/90641/EXTODA.	Westfield Europe Ltd	25/07/2018	Approve	Sara Dawes	
18/00284/T16	Part 16 Prior Notification Telecomms	04/06/2018	Monier Road, London, E3 2NR	Installation of electronic communications apparatus under Class A of Part 16 of Schedule 2 of the Town and Country Planning (General Permitted Development) Order 2015 (as amended).	Telefonica and Vodafone (CTIL)	26/07/2018	Objection to Prior Notification	Grant McClements	Prior approval for a new telecoms mast at Monier Road. Refused as officers were not satisfied that the proposal was within the height allowed by the GDPO.  Delegated as the application was for prior approval only.
18/00191/106	Section 106 Details	13/04/2018	Queen Elizabeth Olympic Park, London	Details submitted pursuant to Legacy Communities Scheme (LCS) Section 106 Agreement – Schedule 9, Paragraphs 4.3 and 4.4 (First Instalment of the LCSCPG Contribution) in relation to outline planning permission ref: 11/90621/OUTODA as varied by 14/00036/VAR.	LLDC- Development	27/07/2018	S106 Response Letter	Grant McClements	The application outlined that spend on the LCSCPG funding had been ringfenced with the first instalment of the LCSCPG contribution deposited into the LCSCPG account. Approved as the application outlined spend and apportionment as required by the obligation.
18/00192/106	Section 106 Details	16/04/2018	Queen Elizabeth Olympic Park, London	Details submitted pursuant to Legacy Communities Scheme (LCS) Section 106 Agreement – Schedule 8, Paragraph 7.2 (Post Education Contribution Update Report) in relation to outline planning permission ref: 11/90621/OUTODA as varied by 14/00036/VAR.	LLDC- Development	27/07/2018	S106 Response Letter	Grant McClements	The applicant submitted a Post Education Contribution Update Report to satisfy the planning obligation. Approved as the detailed spend apportionments for the Post Education Contribution as required by the obligation.
18/00048/AOD	Approval of details (conditions)	05/02/2018	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details to discharge Condition 27 (Plant Noise Assessment and Noise Mitigation Measures) attached to planning permission 10/90285/FUMODA as amended by 13/00423/VAR, dated 21 March 2014, insofar as it relates to the residential part of the development.	Manhattan Loft Gardens Ltd and LCR	27/07/2018	Approve	Daniel Davies	
18/00165/AOD	Approval of details (conditions)	29/03/2018	Plot N24, Zone 3, Stratford City, Stratford, London, E20 1YY	Submission of details pursuant to condition 33 of planning permission 13/00579/VAR associated with Plot N24, insofar as they relate to external lighting at levels 25 and 36, and lighting within the bike store at ground floor level	Manhattan Loft Gardens Ltd	27/07/2018	Approve	Daniel Davies	

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**Appendix 1**

<u>Application Number</u>	<u>Application Type</u>	<u>Registration Date</u>	<u>Location</u>	<u>Full Development Description</u>	<u>Applicant</u>	<u>Decision Date</u>	<u>Decision Description</u>	<u>Officer Name</u>	<u>Officer Comments</u>
17/00574/106	Section 106 Details	09/11/2017	Queen Elizabeth Olympic Park, London	Details submitted pursuant to Legacy Communities Scheme (LCS) Section 106 Agreement - Schedule 9, Paragraph 7.1 and 7.2 (Legacy Careers Programme 2016/17 Monitoring) attached to planning application ref: 11/90621/OUTODA varied by 14/00036/VAR.	LLDC	27/07/2018	S106 Response Letter	Grant McClements	The application required a review report of the Legacy Communities Scheme Careers Programme Group to be submitted to the LPA – this application sought to discharge the monitoring report for 2016/17. Approved as the application outlined progress in achieving targets as required by the obligation.
17/00577/AOD	Approval of details (conditions)	09/11/2017	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details to discharge Condition 8 (Accessible Hotel Rooms and Leisure Facilities) attached to planning permission 10/90285/FUMODA as amended by 13/00423/VAR, dated 21 March 2014, insofar as it relates to the hotel rooms only.	Manhattan Loft Gardens Ltd and LCR	27/07/2018	Approve	Daniel Davies	
17/00660/AOD	Approval of details (conditions)	21/12/2017	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details to discharge Condition 39 (Façade Cleaning & Maintenance Strategy) attached to planning permission 10/90285/FUMODA as amended by 13/00423/VAR, dated 21 March 2014.	Manhattan Loft Gardens Ltd	27/07/2018	Approve	Daniel Davies	
18/00310/PNCOU	Prior Notification for Change of Use	06/06/2018	Unit C1, 417, Wick Lane, LONDON, E3 2JG	Prior Approval for change of use from Use Class B1(a) (Office) to Use Class C3 (residential) under Schedule 2, Part 3, Class O of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) to create 1 X 1 bedroom unit and 1 X studio unit at lower ground floor level.	RMF Construction Services	30/07/2018	Prior Notification was required and was approved	Russell Butchers	The development complied with the requirement of the GPDO and the LPA was obliged to approve the application.
18/00305/VAR	Variation of conditions (Section 73 applications)	05/06/2018	445-453 Wick Lane, Bow, London, E3 2TB	Amendments to Condition 2 (plan numbers condition) of planning permission ref. 17/00608/FUL dated 25 January 2017 under Section 73 of the Town and Country Planning Act 1990 (as amended) and seeking approval for the deletion of condition 3 (Matching external materials).	Shell UK	30/07/2018	Approve	Grant McClements	Applicant sought to change two conditions to allow a change of roof materials and a timber fence for an approved petrol station extension. Both were concluded to have very little impact on amenity, access or safety.
17/00144/106	Section 106 Details	27/03/2017	Here East, Waterden Road, Queen Elizabeth Olympic Park, London, E20 3BS	Submission of details pursuant to Schedule 6 (Managed Workspace) of the Section 106 Agreement for planning permission 13/00534/FUM and 13/00536/COU dated 1st April 2014.	Innovation City (London) Ltd. ICITY trading as Here East	30/07/2018	S106 Response Letter	Sophie Hockin	Application confirmed compliance with a clause in the S106.
18/00215/FUL	Full planning application	24/04/2018	Site known as Stratford Waterfront, bound by the Waterworks River to the south-west, London Aquatics Centre and F10 Bridge to the south-east, and Carpenters Road to the north and east.	Detailed planning permission is sought for the enabling works to facilitate the development of Planning Delivery Zone (PDZ1.1) including site clearance and removal of existing utilities and structures, ground investigation and excavation to formation levels, the installation of temporary and permanent utilities and substations, road adjustments and the construction of temporary vehicle and pedestrian access.	LLDC- Development	31/07/2018	Approve	Richard McFerran	A non-strategic application with limited visual, environmental or transport impacts. The works would help facilitate the future redevelopment of the development parcel in accordance with the LCS or any other future permission.
17/00663/AOD	Approval of details (conditions)	22/12/2017	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details to discharge Conditions 3 (External Material Samples and Condition 4 (Hard and Soft Landscaping) in so far as it relates to Level 7 and 36 and the discharge of Condition 33 (Detailed Drawings) in so far as it relates to external lighting associated with level 25, 36 and bike storage at ground floor level, all in connection with planning permission 10/90285/FUMODA as amended by 13/00423/VAR, dated 21 March 2014.	Manhattan Loft Gardens Ltd and LCR	31/07/2018	Approve	Daniel Davies	
18/00309/AOD	Approval of details (conditions)	04/06/2018	Zone 2, Chobham Farm Development, Thornham Grove, Penny Brookes Street, Stratford, London	Submission of details to partially discharge condition AZ.41 (BREEAM – D1, D2, B1 uses certification) of planning permission 12/00146/FUM, in so far as it relates to Zone 2 of the Chobham Farm Development	Higgins Construction PLC	31/07/2018	Approve	Grant McClements	
17/00336/AOD	Approval of details (conditions)	24/07/2017	Manhattan Loft Gardens, Plot N24, Zone 3, Stratford City, London, Stratford, E20 1YY	Submission of details to discharge Conditions 3 and Condition 4 in so far as it relates to the Level 25 sky garden only, and to discharge Condition 15 (Biodiversity Enhancement Measures) attached to planning permission 10/90285/FUMODA as amended by 13/00423/VAR, dated 21 March 2014.	Manhattan Loft Corporation Ltd	31/07/2018	Approve	Daniel Davies	
17/00569/AOD	Approval of details (conditions)	06/11/2017	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details to discharge Condition 14 (Photovoltaic Panels) attached to planning permission 10/90285/FUMODA as amended by 13/00423/VAR, dated 21 March 2014.	Manhattan Loft Gardens Ltd and LCR	31/07/2018	Approve	Daniel Davies	
17/00412/AOD	Approval of details (conditions)	25/08/2017	80-84 & 90b Wallis Road, Hackney, London, E9 5LW	Submission of details pursuant to Condition 15 (Construction Waste Management Plan) of planning permission 14/00387/FUL as varied by 16/00467/VAR.	Telford Homes Plc	01/08/2018	Approve	Russell Butchers	

**LLDC Delegated Decisions Report - July 2018**

**Appendix 1**

<u>Application Number</u>	<u>Application Type</u>	<u>Registration Date</u>	<u>Location</u>	<u>Full Development Description</u>	<u>Applicant</u>	<u>Decision Date</u>	<u>Decision Description</u>	<u>Officer Name</u>	<u>Officer Comments</u>
18/00227/LBC	Listed building consent	02/05/2018	Gainsborough Primary School, Berkshire Road, Hackney, London, E9 5ND	Refurbishment works comprising; - replacement of yellow and red bricks where damaged or missing to the elevations of main school building and caretaker house, as well as the chimney stacks on the roof; - replacement of gauged, corbel and moulded bricks to main school building where condition has severely deteriorated or bricks are missing; - installation of new lime mortar flaunching at high level of main school building; - Re-roofing of existing roof turrets due to severe weathering. Stripping off existing roof tiles. Timber splice repairs to Louvres where damaged associated with the main school building; - Miscellaneous repairs to cast iron rainwater goods to securely fix them to the external facade and seal the connections where severely eroded, cracked and damaged beyond repair associated with the main school building; - Install new Garland high-performance felt roofing membrane in colour dark grey to newly completed roof deck on outbuilding, where the existing roofing is damaged beyond repair; - Removal of the existing damaged chain-link fence to install a new freestanding system. Rebuilding the masonry boundary wall to the northwest boundary wall to match existing, where the wall is leaning into an adjoining property and structurally unsafe; - Installing new strip foundations and new steel posts to support new perimeter fencing on the entire length of the north boundary, to replace the existing chainlink fence previously causing damage to this section of wall; - Repairs to stonework on the return waves and apex of the gables to the roof of the children's centre, where severely damaged and in danger of falling, or to prevent further deterioration; and - Roof tiles to be refixed and like for like replacements installed on the children's centre where the tiles have either slipped or are missing. Proposed works are association with Full Planning application reference 18/00226/FUL.	London Borough of Hackney	02/08/2018	Approve	Josh Hackner	The proposed refurbishment works were considered to represent less than substantial harm to the significance of the Listed Building. The restoration and repair of the building would preserve the building and secure its long term future as an operational asset for education.  The proposals were considered to be policy compliant and of a nature suitable for a delegated decision.
18/00226/FUL	Full planning application	02/05/2018	Gainsborough Primary School, Berkshire Road, Hackney, London, E9 5ND	Refurbishment works comprising; - replacement of yellow and red bricks where damaged or missing to the elevations of main school building and caretaker house, as well as the chimney stacks on the roof; - replacement of gauged, corbel and moulded bricks to main school building where condition has severely deteriorated or bricks are missing; - installation of new lime mortar flaunching at high level of main school building; - Re-roofing of existing roof turrets due to severe weathering. Stripping off existing roof tiles. Timber splice repairs to Louvres where damaged associated with the main school building; - Miscellaneous repairs to cast iron rainwater goods to securely fix them to the external facade and seal the connections where severely eroded, cracked and damaged beyond repair associated with the main school building; - Install new Garland high-performance felt roofing membrane in colour dark grey to newly completed roof deck on outbuilding, where the existing roofing is damaged beyond repair; - Removal of the existing damaged chain-link fence to install a new freestanding system. Rebuilding the masonry boundary wall to the northwest boundary wall to match existing, where the wall is leaning into an adjoining property and structurally unsafe; - Installing new strip foundations and new steel posts to support new perimeter fencing on the entire length of the north boundary, to replace the existing chainlink fence previously causing damage to this section of wall; - Repairs to stonework on the return waves and apex of the gables to the roof of the children's centre, where severely damaged and in danger of falling, or to prevent further deterioration; and - Roof tiles to be refixed and like for like replacements installed on the children's centre where the tiles have either slipped or are missing. Proposed works are association with Listed Building Consent application reference 18/00227/LBC.	London Borough of Hackney	02/08/2018	Approve	Josh Hackner	The proposed refurbishment works were considered to represent less than substantial harm to the significance of the Listed Building. The restoration and repair of the building would preserve the building and secure its long term future as an operational asset for education.  The proposals were considered to be policy compliant and of a nature suitable for a delegated decision.
16/00679/AOD	Approval of details (conditions)	31/03/2017	1 Smeed Road and 79-85 Monier Road, London, E3 2PS	Application for the approval of details pursuant to condition 18 (Green/Brown Roofs), 19 (Landscaping) and approval of condition 14 (Cycle Storage) of planning permission 14/00374/FUL, in relation to the residential units only.	Weston Homes Plc	02/08/2018	Approve	Josh Hackner	
17/00430/FUL	Full planning application	06/09/2017	The Lock Building, 72, High Street, Stratford, E15 2QB	Construction of 9 flats (1 x studio, 3 x 1 bed, 4 x 2 bed, 1 x 3 bed) in rear / side extension to existing building. Extensions and alterations to the commercial area of the building, including the insertion of a mezzanine floor and division of the commercial space into four units with a flexible A1/A2/A3 and/or B1 use class and the installation of glazed shopfronts	Renegade Investment Properties Ltd	03/08/2018	Approve	Russell Butchers	Planning permission had been previously approved for similar schemes on the site. The new scheme complied with Local Plan policies and officers were satisfied with the design and residential amenity of the proposed units.
18/00331/106	Section 106 Details	29/06/2018	Land to the east of Hancock Road and west of the River Lea Navigation, Bromley by Bow E3, (Bow River Village Phase 2)	Submission of details pursuant to Schedule 1 (Site Specific Covenants), Clause 5.8 (Affordable Housing Provider) of the s106 agreement attached to planning application PA/11/02423/LBTH in relation to Phase 2 of the development.	Higgins Construction PLC	07/08/2018	S106 Response Letter	Russell Butchers	The proposed affordable housing provider was acceptable and is a provider that is approved by LBTH.
18/00353/NMA	Non-Material Amendment (Section 96A applications)	11/07/2018	International Square, Westfield, Zone 1 Stratford City, London	Application for non-material amendments to the approved planning permission 16/00486/VAR, seeking alterations to amend the design of the cycle parking, to provide conventional Sheffield stands.	Westfield Europe Ltd.	07/08/2018	Granted NMA	Russell Butchers	The amendments to the cycle parking were of a non-material nature and were acceptable.
18/00369/NMA	Non-Material Amendment (Section 96A applications)	25/07/2018	Land to the east of Hancock Road and west of the River Lea Navigation, Bromley by Bow E3, (Bow River Village Phase 2)	Application for non-material amendment for the removal of condition C28b (Code for Sustainable Homes Assessment) of planning permission PA/11/02423/LBTH dated 27th September 2012 (as varied by planning reference 16/00427/VAR dated 14 November 2016) as it relates to Phase 2 of the development.	Southern Housing Group	07/08/2018	Granted NMA	Russell Butchers	The Code for Sustainable Homes is no longer a planning requirement and the proposal to amend the condition was acceptable.

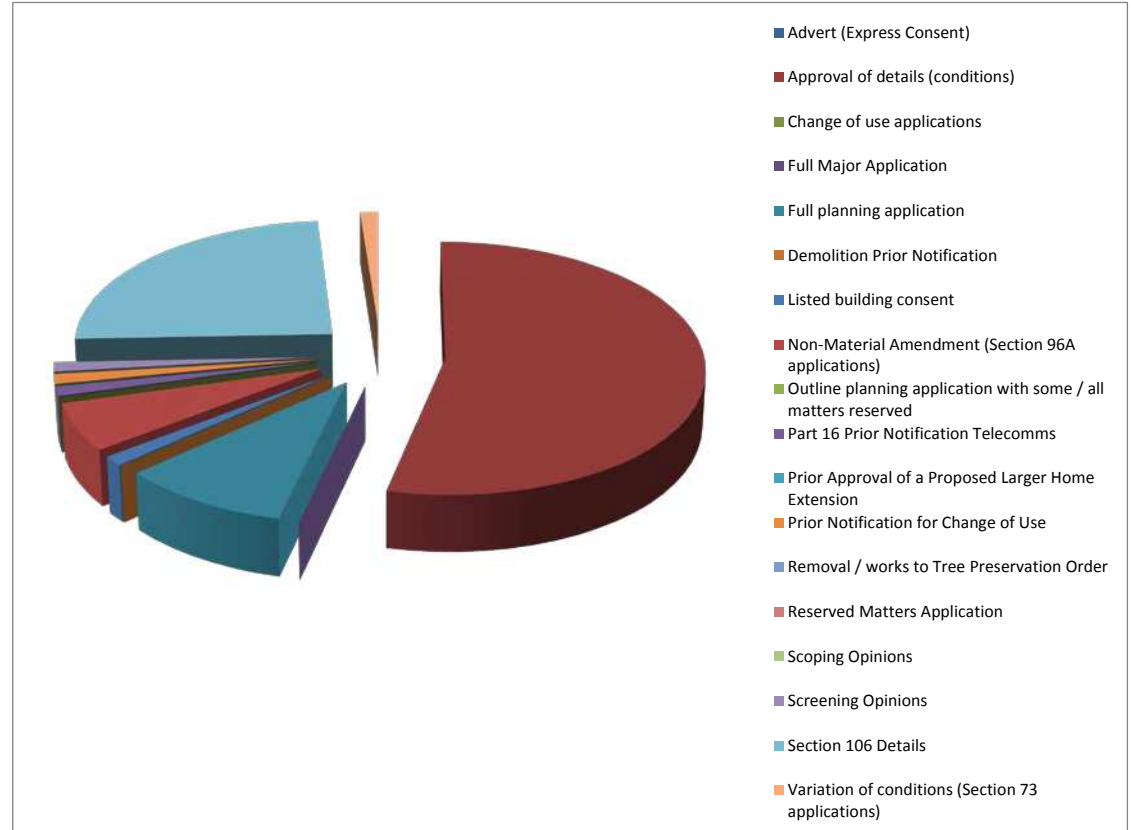
**LLDC Delegated Decisions Report - July 2018**

**Appendix 1**

<u>Application Number</u>	<u>Application Type</u>	<u>Registration Date</u>	<u>Location</u>	<u>Full Development Description</u>	<u>Applicant</u>	<u>Decision Date</u>	<u>Decision Description</u>	<u>Officer Name</u>	<u>Officer Comments</u>
18/00015/AOD	Approval of details (conditions)	18/01/2018	Hackney Wick Overground Station, Wallis Road, Hackney, London, E9 5ER	Submission of details pursuant to fully discharge condition 19 (Surface Water Drainage) of planning permission 14/00275/FUL dated 23rd September 2014.	VolkerFitzpatrick Ltd	08/08/2018	Approve	Anne Ogundiya	
18/00321/AOD	Approval of details (conditions)	14/06/2018	68 Wallis Road, Hackney, London, E9 5LH	Submission of details to fully discharge condition 11 (Short Stay-Cycle Parking Scheme) of planning permission 17/00391/FUL dated 17th November 2017.	Woodridings Ltd	08/08/2018	Approve	Grant McClements	
18/00322/AOD	Approval of details (conditions)	14/06/2018	68 Wallis Road, Hackney, London, E9 5LH	Submission of details to fully discharge condition 5 (Flood Risk Management) of planning permission 17/00391/FUL dated 17th November 2017.	Woodridings	08/08/2018	Approve	Grant McClements	
18/00316/FUL	Full planning application	08/06/2018	Unit NR2, Block 1D-D, Chobham Manor, Abercrombie Road, PDZ 6.1, Stratford	Application for proposed change of use from the permitted retail use (A1-A5 Use) to office use (B1(a) Use).	Chobham Manor LLP	09/08/2018	Approve	Grant McClements	The applicant sought to change the use of a unit from retail to office at Chobham Manor as no successful retail occupants had come forward over 14 months of marketing. Temporary permission granted.  Decision delegated as the unit size was small and the proposed use would be appropriate in a predominantly residential area.
18/00085/AOD	Approval of details (conditions)	26/03/2018	Chobham Farm Site Zone 4, Leyton Road, Stratford, London, E15 1DR	Submission of details pursuant to Condition AZ.79 (Interim Uses and Boundary Treatment Strategy) associated with planning permission 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA.	Telford Homes Plc	09/08/2018	Approve	Grant McClements	
18/00228/AOD	Approval of details (conditions)	02/05/2018	Endeavour Square, The International Quarter London (IQL) South, Land adjacent to Westfield Avenue, Zone 2 Stratford City, London	Approval of details pursuant to Conditions B10 (Material Samples), Q4 (Landscape) part d) (lighting) and part e) (any features or artworks) of the Stratford City Outline Planning Permission (SC OPP) (ref: 10/90641/EXTODA) in so far as it relates to the artwork within Endeavour Square.	Stratford City Business District Limited	10/08/2018	Approve	Josh Hackner	

## LLDC Delegated Decisions Report July 2018 - Application Type Totals

Application Type	Total
Advert (Express Consent)	0
Approval of details (conditions)	44
Change of use applications	0
Full Major Application	0
Full planning application	8
Demolition Prior Notification	0
Listed building consent	1
Non-Material Amendment (Section 96A applications)	5
Outline planning application with some / all matters reserved	0
Part 16 Prior Notification Telecomms	1
Prior Approval of a Proposed Larger Home Extension	0
Prior Notification for Change of Use	1
Removal / works to Tree Preservation Order	0
Reserved Matters Application	0
Scoping Opinions	0
Screening Opinions	1
Section 106 Details	20
Variation of conditions (Section 73 applications)	1
<b>Total Applications Determined this Month</b>	<b>82</b>



LLDC Delegated Decisions Report - August 2018

Appendix 2

Application Number	Application Type	Registration Date	Location	Full Development Description	Applicant	Decision Date	Decision Description	Officer Name	Officer Comments
17/00412/AOD	Approval of details (conditions)	25/08/2017	80-84 & 90b Wallis Road, Hackney, London, E9 5LW	Submission of details pursuant to Condition 15 (Construction Waste Management Plan) of planning permission 14/00387/FUL as varied by 16/00467/VAR.	Telford Homes Plc	01/08/2018	Approve	Russell Butchers	
18/00227/LBC	Listed building consent	02/05/2018	Gainsborough Primary School, Berkshire Road, Hackney, London, E9 5ND	Refurbishment works comprising: - replacement of yellow and red bricks where damaged or missing to the elevations of main school building and caretaker house, as well as the chimney stacks on the roof; - replacement of gauged, corbel and moulded bricks to main school building where condition has severely deteriorated or bricks are missing; - installation of new lime mortar flaunching at high level of main school building; - Re-roofing of existing roof turrets due to severe weathering. Stripping off existing roof tiles. Timber splice repairs to Louvres where damaged associated with the main school building; - Miscellaneous repairs to cast iron rainwater goods to securely fix them to the external facade and seal the connections where severely eroded, cracked and damaged beyond repair associated with the main school building; - Install new Garland high-performance felt roofing membrane in colour dark grey to newly completed roof deck on outbuilding, where the existing roofing is damaged beyond repair; - Removal of the existing damaged chain-link fence to install a new freestanding system. Rebuilding the masonry boundary wall to the northwest boundary wall to match existing, where the wall is leaning into an adjoining property and structurally unsafe; - Installing new strip foundations and new steel posts to support new perimeter fencing on the entire length of the north boundary, to replace the existing chainlink fence previously causing damage to this section of wall; - Repairs to stonework on the return waves and apex of the gables to the roof of the children's centre, where severely damaged and in danger of falling, or to prevent further deterioration; and - Roof tiles to be refixed and like for like replacements installed on the children's centre where the tiles have either slipped or are missing. Proposed works are association with Full Planning application reference 18/00226/FUL.	London Borough of Hackney	02/08/2018	Approve	Josh Hackner	The proposed restoration and repair works would serve to enhance the non-designated heritage asset, which is considered to represent a significant public benefit by preserving the building and securing its long-term future. As such the proposal were considered to be in accordance with paragraph 134 of the NPPF and Policy BN.16 (Preserving and enhancing heritage assets) of the Local Plan.  Overall, the proposals were considered to be policy compliant and therefore a delegated decision was deemed appropriate.
18/00226/FUL	Full planning application	02/05/2018	Gainsborough Primary School, Berkshire Road, Hackney, London, E9 5ND	Refurbishment works comprising: - replacement of yellow and red bricks where damaged or missing to the elevations of main school building and caretaker house, as well as the chimney stacks on the roof; - replacement of gauged, corbel and moulded bricks to main school building where condition has severely deteriorated or bricks are missing; - installation of new lime mortar flaunching at high level of main school building; - Re-roofing of existing roof turrets due to severe weathering. Stripping off existing roof tiles. Timber splice repairs to Louvres where damaged associated with the main school building; - Miscellaneous repairs to cast iron rainwater goods to securely fix them to the external facade and seal the connections where severely eroded, cracked and damaged beyond repair associated with the main school building; - Install new Garland high-performance felt roofing membrane in colour dark grey to newly completed roof deck on outbuilding, where the existing roofing is damaged beyond repair; - Removal of the existing damaged chain-link fence to install a new freestanding system. Rebuilding the masonry boundary wall to the northwest boundary wall to match existing, where the wall is leaning into an adjoining property and structurally unsafe; - Installing new strip foundations and new steel posts to support new perimeter fencing on the entire length of the north boundary, to replace the existing chainlink fence previously causing damage to this section of wall; - Repairs to stonework on the return waves and apex of the gables to the roof of the children's centre, where severely damaged and in danger of falling, or to prevent further deterioration; and - Roof tiles to be refixed and like for like replacements installed on the children's centre where the tiles have either slipped or are missing. Proposed works are association with Listed Building Consent application reference 18/00227/LBC.	London Borough of Hackney	02/08/2018	Approve	Josh Hackner	Linked to Listed Building Consent 18/00227/LBC.
16/00679/AOD	Approval of details (conditions)	31/03/2017	1 Smeed Road and 79-85 Monier Road, London, E3 2PS	Application for the approval of details pursuant to condition 18 (Green/Brown Roofs), 19 (Landscaping) and approval of condition 14 (Cycle Storage) of planning permission 14/00374/FUL, in relation to the residential units only.	Weston Homes Plc	02/08/2018	Approve	Josh Hackner	
17/00430/FUL	Full planning application	06/09/2017	The Lock Building, 72, High Street, Stratford, E15 2QB	Construction of 9 flats (1 x studio, 3 x 1 bed, 4 x 2 bed, 1 x 3 bed) in rear / side extension to existing building. Extensions and alterations to the commercial area of the building, including the insertion of a mezzanine floor and division of the commercial space into four units with a flexible A1/A2/A3 and/or B1 use class and the installation of glazed shopfronts	Renegade Investment Properties Ltd	03/08/2018	Approve	Russell Butchers	Minor application for creation of 9 flats through change of use of part of the ground floor commercial floorspace of this development, with alterations and extension. Stratford High Street commercial frontage retained and design and quality of residential units and external appearance considered acceptable by officers and decision therefore taken under delegated powers.
18/00369/NMA	Non-Material Amendment (Section 96A applications)	25/07/2018	Land to the east of Hancock Road and west of the River Lea Navigation, Bromley by Bow E3, (Bow River Village Phase 2)	Application for non-material amendment for the removal of condition C28b (Code for Sustainable Homes Assessment) of planning permission PA/11/02423/LBTH dated 27th September 2012 (as varied by planning reference 16/00427/VAR dated 14 November 2016) as it relates to Phase 2 of the development.	Southern Housing Group	07/08/2018	Granted NMA	Russell Butchers	The application sought to remove part b) of condition C28, which required the applicant to submit a final Code for Sustainable Homes (CSH) assessment prior to first occupation of the development. The proposed amendment would not alter the original assessment or outcome of the planning permission, as granted. Approved with amended condition wording to secure submission and approval of details of the design stage Code for Sustainable Homes Pre-Assessment, showing that the development phase will achieve a minimum of Code Level 4.
18/00353/NMA	Non-Material Amendment (Section 96A applications)	11/07/2018	International Square, Westfield, Zone 1 Stratford City, London	Application for non-material amendments to the approved planning permission 16/00486/VAR, seeking alterations to amend the design of the cycle parking, to provide conventional Sheffield stands.	Westfield Europe Ltd.	07/08/2018	Granted NMA	Russell Butchers	Application to vary the approved cycle stands to be provided to International Square (west of Stratford International Station). Proposed design was Sheffield stands which officers approved as in keeping with cycle stands used throughout QEOP area.
18/00331/106	Section 106 Details	29/06/2018	Land to the east of Hancock Road and west of the River Lea Navigation, Bromley by Bow E3, (Bow River Village Phase 2)	Submission of details pursuant to Schedule 1 (Site Specific Covenants), Clause 5.8 (Affordable Housing Provider) of the s106 agreement attached to planning application PA/11/02423/LBTH in relation to Phase 2 of the development.	Higgins Construction PLC	07/08/2018	S106 Response Letter	Russell Butchers	Agrees Southern Housing Group as the Affordable Housing provider, following consultation with LB Tower Hamlets.
18/00015/AOD	Approval of details (conditions)	18/01/2018	Hackney Wick Overground Station, Wallis Road, Hackney, London, E9 5ER	Submission of details pursuant to fully discharge condition 19 (Surface Water Drainage) of planning permission 14/00275/FUL dated 23rd September 2014.	VolkerFitzpatrick Ltd	08/08/2018	Approve	Anne Ogundiya	
18/00322/AOD	Approval of details (conditions)	14/06/2018	68 Wallis Road, Hackney, London, E9 5LH	Submission of details to fully discharge condition 5 (Flood Risk Management) of planning permission 17/00391/FUL dated 17th November 2017.	Woodridings	08/08/2018	Approve	Grant McClements	
18/00321/AOD	Approval of details (conditions)	14/06/2018	68 Wallis Road, Hackney, London, E9 5LH	Submission of details to fully discharge condition 11 (Short Stay-Cycle Parking Scheme) of planning permission 17/00391/FUL dated 17th November 2017.	Woodridings Ltd	08/08/2018	Approve	Grant McClements	
18/00316/FUL	Full planning application	08/06/2018	Unit NR2, Block 1D-D, Chobham Manor, Abercrombie Road, PDZ 6.1, Stratford	Application for proposed change of use from the permitted retail use (A1-A5 Use) to office use (B1(a) Use).	Chobham Manor LLP	09/08/2018	Approve	Grant McClements	The application related to a unit of 194sqm, which had planning permission for retail (A1-A5 Use Classes). The unit had been unoccupied since June 2017. Marketing of the unit was carried out between February 2017 and the submission of the application (8th June 2018). The applicant stated that there had been no interest from retail operators with feedback that the size of the unit was considered too large for an independent operator. Majority of the interest had been from operators looking for office space. The LCS set a maximum of 124sqm of office space within PDZ 6. The change of use results in the threshold being exceeded by 157sqm. However, whilst the proposal exceeded the development threshold for office space within PDZ6, it was considered the application was not strategic and should be considered separately to the LCS Outline Planning Permission. The proposal was considered to be acceptable on balance owing to the demand for office space and lack of retail market interest at this location. However, a condition has been imposed limiting the B1(a) use class in this unit to either 15 years or the date in which the unit becomes vacant (whichever is sooner), to allow the retail market position to be revisited. In summary, a delegated decision was considered appropriate in this instance.
18/00085/AOD	Approval of details (conditions)	26/03/2018	Chobham Farm Site Zone 4, Leyton Road, Stratford, London, E15 1DR	Submission of details pursuant to Condition AZ.79 (Interim Uses and Boundary Treatment Strategy) associated with planning permission 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA.	Telford Homes Plc	09/08/2018	Approve	Grant McClements	
18/00228/AOD	Approval of details (conditions)	02/05/2018	Endeavour Square, The International Quarter London (IQL) South, Land adjacent to Westfield Avenue, Zone 2 Stratford City, London	Approval of details pursuant to Conditions B10 (Material Samples), Q4 (Landscape part d) (lighting) and part e) (any features or artworks) of the Stratford City Outline Planning Permission (SC OPP) (ref: 10/90641/EXTODA) in so far as it relates to the artwork within Endeavour Square.	Stratford City Business District Limited	10/08/2018	Approve	Josh Hackner	

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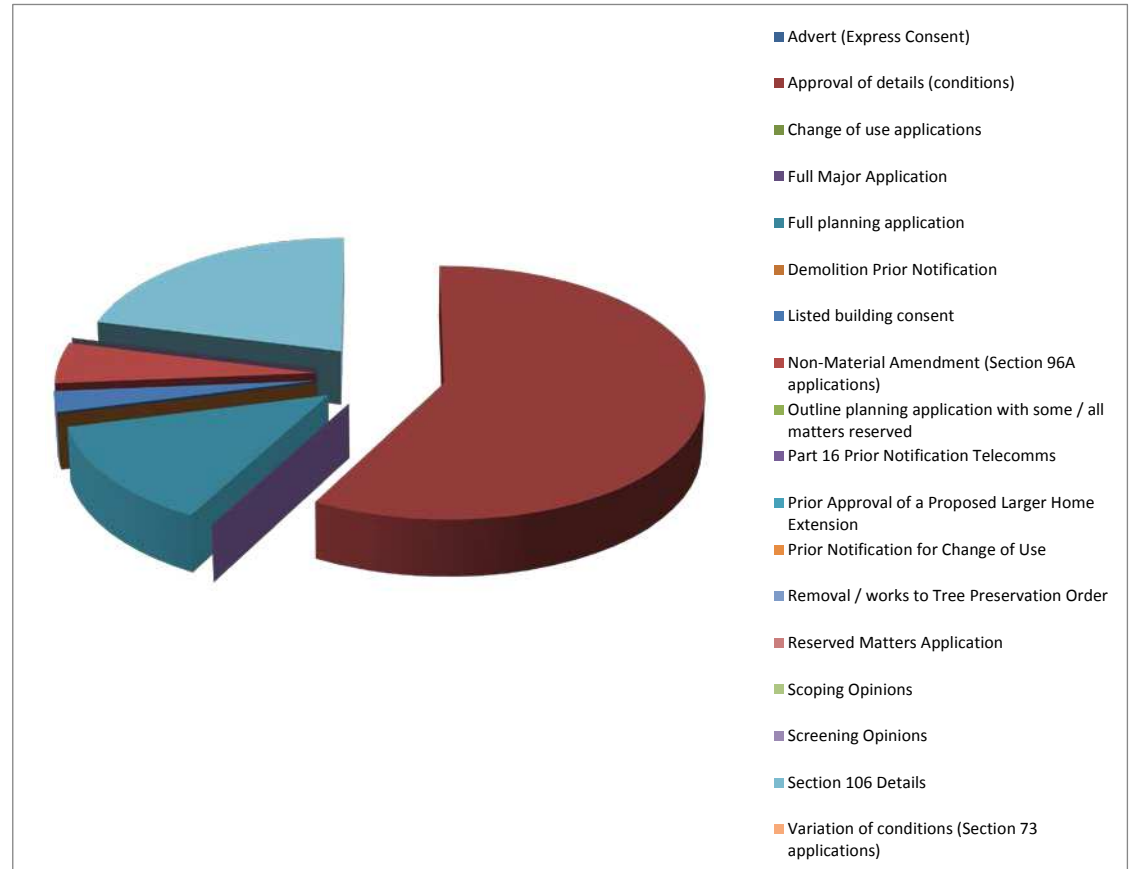
Appendix 1

Application Number	Application Type	Registration Date	Location	Full Development Description	Applicant	Decision Date	Decision Description	Officer Name	Officer Comments
18/00025/FUL	Full planning application	23/01/2018	2, Keirin Road, Stratford, LONDON, E20 1GU	Use of part of the annex roof as private amenity space, and erection of an external staircase	David Pelle	10/08/2018	Approve	Sophie Hockin	A small roof terrace at 1st floor level, where there are already a number of high level amenity spaces locally. Neighbours objections were considered, and officers concluded that, subject to appropriate conditions being applied, the development would not have an unacceptable impact upon residential amenity. Impact on the streetscene was considered acceptable, and this part of the Chobham Manor estate would benefit from the proposed softer landscaping/greening.
18/00053/106	Section 106 Details	08/02/2018	Chobham Farm Site Zone 4, Leyton Road, Stratford, London, E15 1DR	Submission of details pursuant to Schedule 10 (Zonal Travel Plans), Paragraphs 1.1.1 (Zone 4 Travel Plan) and 1.1.2 (Appointment of Travel Plan Monitoring Officer) of the Section 106 Agreement associated with planning permission 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA.	Telford Homes Plc	13/08/2018	S106 Response Letter	Hilary Wrenn	Requirement of Chobham Farm Outline Planning Permission 12/00146/FUL as amended. Travel Plan approved for Zone 4 and monitoring officer appointed.
18/00089/AOD	Approval of details (conditions)	26/02/2018	East Wick and Sweetwater, PDZ 4 and 5	Application for approval of details reserved by Condition LCS0.33 (Details of Public Realm Lighting) of outline planning permission 11/90621/OUTODA as varied by 14/00036/VAR in so far as it relates to the Specified Infrastructure Works (SIW) in PDZ4 and 5 which relate to the Bridge H14, Bridge H16 and the North-South Highway Link	East Wick and Sweetwater Projects and LLDC	14/08/2018	Approve	Daniel Davies	
18/00213/AOD	Approval of details (conditions)	01/05/2018	Chobham Farm Zone 4, Leyton Road, Stratford, London, E15 1DN	Submission of details pursuant to Conditions AZ.35 (Code for Sustainable Homes - certification), AZ.38 (BREEAM - education etc uses), AZ.40 (BREEAM - D1, D2, B1 uses) and AZ.42 (BREEAM - A1-A3 uses) associated with planning permission 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA as it relates to Zone 4 of the Chobham Farm development.	Telford Homes Plc	14/08/2018	Approve	Grant McClements	
18/00033/AOD	Approval of details (conditions)	29/01/2018	Neptune Wharf - Phase 1, Wyke Road, London, Tower Hamlets, E3 2PL	Submission of details pursuant to condition DZ.4 (Details/Samples of Materials) parts A, D, E and F of planning permission 12/00210/OUT in relation to Block A within Phase 1 of the development.	Peabody / Hill	16/08/2018	Approve	Grant McClements	
18/00041/106	Section 106 Details	01/02/2018	Chobham Farm Site Zone 4, Leyton Road, Stratford, London, E15 1DR	Submission of details pursuant to Schedule 5 (Family Housing), Paragraph 1.5.1 (Provision of Family Housing Percentage) of the Section 106 Agreement associated with planning permission 12/00146/FUM as varied by 14/00439/NMA & 14/00440/NMA.	Telford Homes Plc	20/08/2018	S106 Response Letter	Hilary Wrenn	Requirement of Chobham Farm Outline Planning Permission 12/00146/FUL as amended for Zonal Family housing to be agreed. 44.8% of the Zone 4 units have 3 or more bedrooms (exceeds the 44.3% set out in the s106 agreement).
18/00384/106	Section 106 Details	30/07/2018	Duncan House, High Street, Stratford, LONDON, E15 2JB	Submission of details pursuant to Schedule 7 (Highways Works and Improvements) Clause 2.1 (Highways Agreement) of the Section 106 Agreement associated with planning permission 15/00598/FUL dated 25 August 2016.	Watkin Jones Group	21/08/2018	S106 Response Letter	Anne Ogundiya	The application provided the LPA with evidence in the form of a signed and dated s78 agreement (highway agreement) confirming that the applicant had agreed the necessary highway works on / adjacent to the site with the Local Highway Authority. Officers were satisfied that it was appropriate to discharge the s106 clause as a delegated item as it confirmed the requirements of the s106 clause is met.
18/00364/106	Section 106 Details	23/07/2018	33-35 Monier Road, London, E3 2PR	Submission of details pursuant to Schedule 7 (Design Monitoring), Clauses 1.1, 1.2 and 3.1.1 (Design Team/Architect Retainment) of the Section 106 Agreement associated with planning permission 15/00212/FUL dated 24 March 2016; regarding the Design Team	CMA Planning	21/08/2018	S106 Response Letter	Anne Ogundiya	The application provided the LPA with evidence that the architect defined in the section 106 development are retained by the applicant to deliver the planning permission and are instructed to do so up to the completion of the project. Officers were satisfied that it is appropriate to discharge the s106 clause as a delegated item as it confirmed the requirements of the s106 clause is met.
18/00229/AOD	Approval of details (conditions)	02/05/2018	Land at Cooks Road, Pudding Mill Lane, Stratford, London, E15 2PW	Approval of Details submitted pursuant to Condition 17 (Mechanical Ventilation for Blocks B1-B3) of planning permission reference 15/00392/FUL dated 12th August 2016 as varied by 16/00534/VAR as it relates to the development at Cooks Road.	Bellway Homes (Thames Gateway) C/O Agent	22/08/2018	Approve	Daniel Davies	
18/00020/AOD	Approval of details (conditions)	24/01/2018	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details to fully discharge Condition 8 (Accessible Hotel Rooms and Leisure Facilities) attached to planning permission 10/90285/FUMODA as amended by 13/00423/VAR, dated 21 March 2014, insofar as it relates to the leisure facilities.	Creative Property (UK) LLP	22/08/2018	Approve	Daniel Davies	
18/00030/AOD	Approval of details (conditions)	22/01/2018	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details to discharge Condition 7 (Proposed Disabled Toilets) attached to planning permission 10/90285/FUMODA as amended by 13/00423/VAR, dated 21 March 2014, in so far as it relates to the 7th floor restaurant	Creative Property (UK) LLP	22/08/2018	Approve	Daniel Davies	
18/00031/AOD	Approval of details (conditions)	22/01/2018	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details to fully discharge Condition 34 (Confirmation of Hotel Rooms) attached to planning permission 10/90285/FUMODA as amended by 13/00579/VAR, dated 28 October 2014	Creative Property (UK) LLP	22/08/2018	Approve	Daniel Davies	
18/00151/AOD	Approval of details (conditions)	21/03/2018	Plot N24, Manhattan Loft Gardens, Zone 3, Stratford City, London, E20 1YY	Submission of details pursuant to Condition 13 (Water Efficiency) of planning permission 10/90285/FUMODA as amended by 13/00423/VAR, dated 21 March 2014, insofar as it relates to the hotel only.	Manhattan Loft Gardens Ltd and LCR	22/08/2018	Approve	Daniel Davies	
17/00205/AOD	Approval of details (conditions)	09/05/2017	2-12 High Street, Stratford, London, E15 2PW	Submission of details pursuant to condition 25 (Assessment of the condition of the river wall) of planning permission 10/90519/FUMODA, as varied by 14/00112/VAR.	Galliard Homes Ltd, c/o Agent	23/08/2018	Approve	Russell Butchers	
16/00111/AOD	Approval of details (conditions)	31/03/2016	2-12 High Street, Stratford, London, E15 2PW	Submission of details pursuant to Condition 5 (Ribbon Mesh) of planning permission 10/90519/FUMODA as varied by 14/00112/VAR.	Galliard Homes Ltd	23/08/2018	Approve	Russell Butchers	
17/00137/AOD	Approval of details (conditions)	12/12/2017	Land at Cooks Road, Pudding Mill Lane, Stratford, London, E15 2PW	Application for Approval of Details submitted pursuant to Condition 7 (Landscaping Plan) of planning permission reference 15/00392/FUL dated 12 August 2016.	Bellway Homes (Thames Gateway)	24/08/2018	Approve	Richard McFerran	
18/00304/DOV	Deed of Variation	31/05/2018	52-54 White Post Lane, London, E9 5EN	Proposed deed of variation for modifications to Schedule 1, Clause 2.1 and Appendix 1 of the section 106 agreement attached to planning permission 15/00416/FUL.	CMA Planning	28/08/2018	S106 Response Letter	Hilary Wrenn	Changes in Aitch's development programme on the Bagel Island site (52-54 White Post Lane/24-26 White Post Lane/25-37 Rothbury Road) means that 52-54 WPL units will be complete prior to 25-37 RR. Deed of Variation allowing occupation of up to 44 (80%) market units within 52-54 WPL block prior to delivery of affordable units in 25-37 RR. Members briefed in April 2018 and raised no objection to the principle in this instance; there is retained obligation to deliver all AH units prior to occupation of all market housing.
18/00006/106	Section 106 Details	04/01/2018	2-12 High Street, Stratford, London, E15 2PW	Submission of details pursuant to part a (v) of Clause 6.3.1 (Notification of completion of final binding transfer) of the Section 106 Agreement for planning permission 10/90519/FUMODA, as varied by 14/00112/VAR.	Galliard Homes	29/08/2018	S106 Response Letter	Russell Butchers	Fulfilling S106 requirements to inform of final binding transfers within the development.
17/00493/106	Section 106 Details	03/10/2017	2-12 High Street, Stratford, London, E15 2PW	Submission of details pursuant to part a (iv) (Notification of 172nd binding contract for a Transfer) of Clause 6.3.1 (Sales Completion of the Residential Development) of the Section 106 Agreement for planning permission 10/90519/FUMODA, as varied by 14/00112/VAR.	Galliard Homes	29/08/2018	S106 Response Letter	Russell Butchers	Fulfilling S106 requirements to inform of transfer of 172nd building sale contract within the development.
18/00186/106	Section 106 Details	10/04/2018	2-12 High Street, Stratford, London, E15 2PW	Submission of details pursuant to part 5 (TV Reception) of Clause 4.3 (Reception Consultants Report) of Schedule 1 (The Developer's Obligation) of the Section 106 Agreement for planning permission 10/90519/FUMODA, as varied by 14/00112/VAR.	Galliard Homes	29/08/2018	S106 Response Letter	Russell Butchers	Submission of TV reception impact report required by the s106 agreement.
18/00087/AOD	Approval of details (conditions)	26/02/2018	Plot N08, East Village, Land west of Celebration Avenue and north east of Victory Park, Stratford City, London, E15	Submission of details to partially discharge Condition 3 (Detailed Drawings) attached to full planning permission reference 14/00034/REM dated 29 May 2014 insofar as it relates to Part 7 (Seating and planting beneath the courtyard pergola and at upper terrace levels).	QDD Athletes Village UK Limited	30/08/2018	Approve	Grant McClements	
18/00148/AOD	Approval of details (conditions)	20/03/2018	Building S5, International Quarter London (IQL) South, land adjacent to Westfield Avenue, Zone 2 Stratford City, London	Application for the approval of details pursuant to Condition M7 (Remediation Validation Report) of Stratford City Outline Planning Permission 10/90641/EXTODA insofar as it relates to Plot S5 only.	Stratford City Business District Limited	30/08/2018	Approve	Josh Hackner	
18/00168/AOD	Approval of details (conditions)	04/04/2018	Building S6, International Quarter London, Zone 2, Stratford City	Application for the partial approval of details pursuant to condition P12 (Ventilation Details) of planning permission 10/90641/EXTODA insofar that it relates to Building S6 only.	Stratford City Business District Limited	30/08/2018	Approve	Josh Hackner	
18/00361/FUL	Full planning application	13/07/2018	19 Mallard Close, Hackney, London, E9 5JL	Erection of a pitched roof to existing building.	Ozan Sahin	30/08/2018	Approve	Russell Butchers	Minor application for development to match other roofs added since original construction of single storey flat roof housing in Trowbridge estate. Design and impact acceptable; roofspace proposed for storage use; non-contentious application.
18/00236/AOD	Approval of details (conditions)	18/05/2018	Plot M7, Zone 1, Stratford City	Submission of details to discharge condition K6 (BREEAM/Code for Sustainable Homes Ratings) associated with planning permission 10/90641/EXTODA dated 30 March 2012 (and modifications dated 22 January 2014 and 18 September 2015) in so far as it relates to Plot M7.	Westfield Europe Ltd c/o Agent	30/08/2018	Approve	Victoria Bates	



## LLDC Delegated Decisions Report August 2018 - Application Type Totals

Application Type	Total
Advert (Express Consent)	0
Approval of details (conditions)	22
Change of use applications	0
Full Major Application	0
Full planning application	5
Demolition Prior Notification	0
Listed building consent	1
Non-Material Amendment (Section 96A applications)	2
Outline planning application with some / all matters reserved	0
Part 16 Prior Notification Telecomms	0
Prior Approval of a Proposed Larger Home Extension	0
Prior Notification for Change of Use	0
Removal / works to Tree Preservation Order	0
Reserved Matters Application	0
Scoping Opinions	0
Screening Opinions	0
Section 106 Details	8
Variation of conditions (Section 73 applications)	0
Deed of Variation	1
<b>Total Applications Determined this Month</b>	<b>39</b>



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